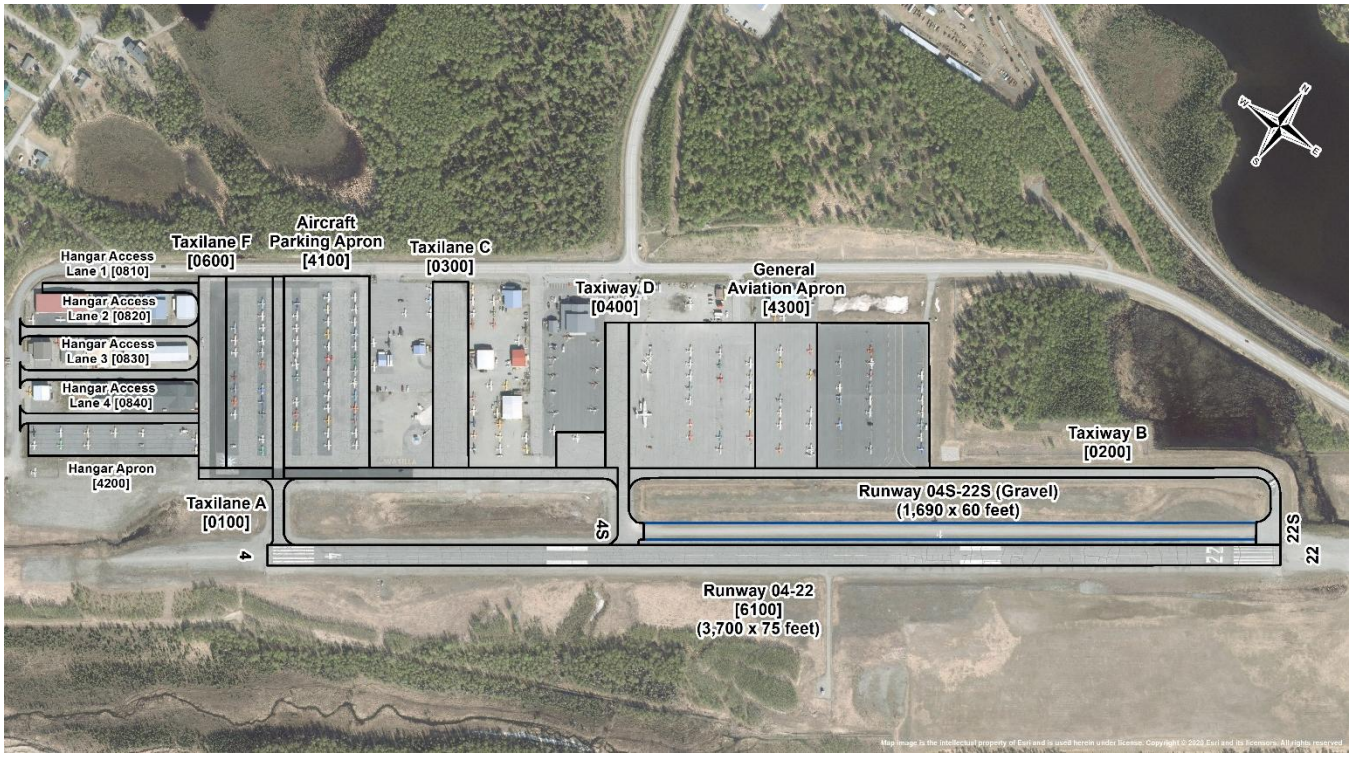
Alaska DOT&PF

Statewide Design and Engineering Services
Pavement Management and Preservation Office
5800 East Tudor Road, Anchorage AK 99507-1286

Pavement Inspection Report
Wasilla Airport



Airport Name	IATA	ICAO	Latitude	Longitude	Elevation (ft)
Wasilla Airport	IYS	PAWS	61° 34' 19.15" N	149° 32' 22.4" W	353.5

Please refer all questions or for further information about this report, please contact the AKDOT&PF Pavement Management and Preservation Office as follows:

Point of Contact	Phone	Email	Date Inspected	Date Published
Mr. Andrew Pavey, Pavement Management Engineer	(907) 269 6213	andrew.pavey@alaska.gov	July 2024	February 2025

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AIRPORT PAVEMENT INSPECTION NOTES BY BRANCH

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0100	Taxilane A	Taxiway	2	41,010	70

Taxilane A was initially constructed in 2000 and has not received major work since. A surface treatment was applied in 2022 to the majority of section 0100-02 but was not applied to section 0100-01. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low severity block cracking, low to high severity longitudinal and transverse cracking, low to medium severity raveling, and low to medium severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Pavement Condition Index (PCI)

PCI Values / General Pavement Recommendations

- (85 - 100) - Preventative Maintenance
- (70 - 84) - Preventative / Corrective Maintenance
- (55 - 69) - Rehabilitation
- (40 - 54) - Reconstruct
- (25 - 39) - Reconstruct
- (10 - 24) - Reconstruct
- (0 - 9) - Reconstruct

Aircraft Parking Apron [4100]
[4100-01]
[4100-02]
Taxilane F [0600-01]
Taxilane C [0300-01]
General Aviation Apron [4300]
Taxiway D [0400-01]
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Hangar Access Lane 1 [0810-01]
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[4300-02]
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[0200-02]
[0200-03]
[0100-01]
[0100-02]
Taxilane A [0100]
Runway 04S-22S (Gravel) (1,690 x 60 feet)
Runway 04-22 [6100-01] (3,700 x 75 feet)

Wasilla Airport

Airport Code: IYS
Site Number: 50870.3*A

Pavement Condition Index (PCI)

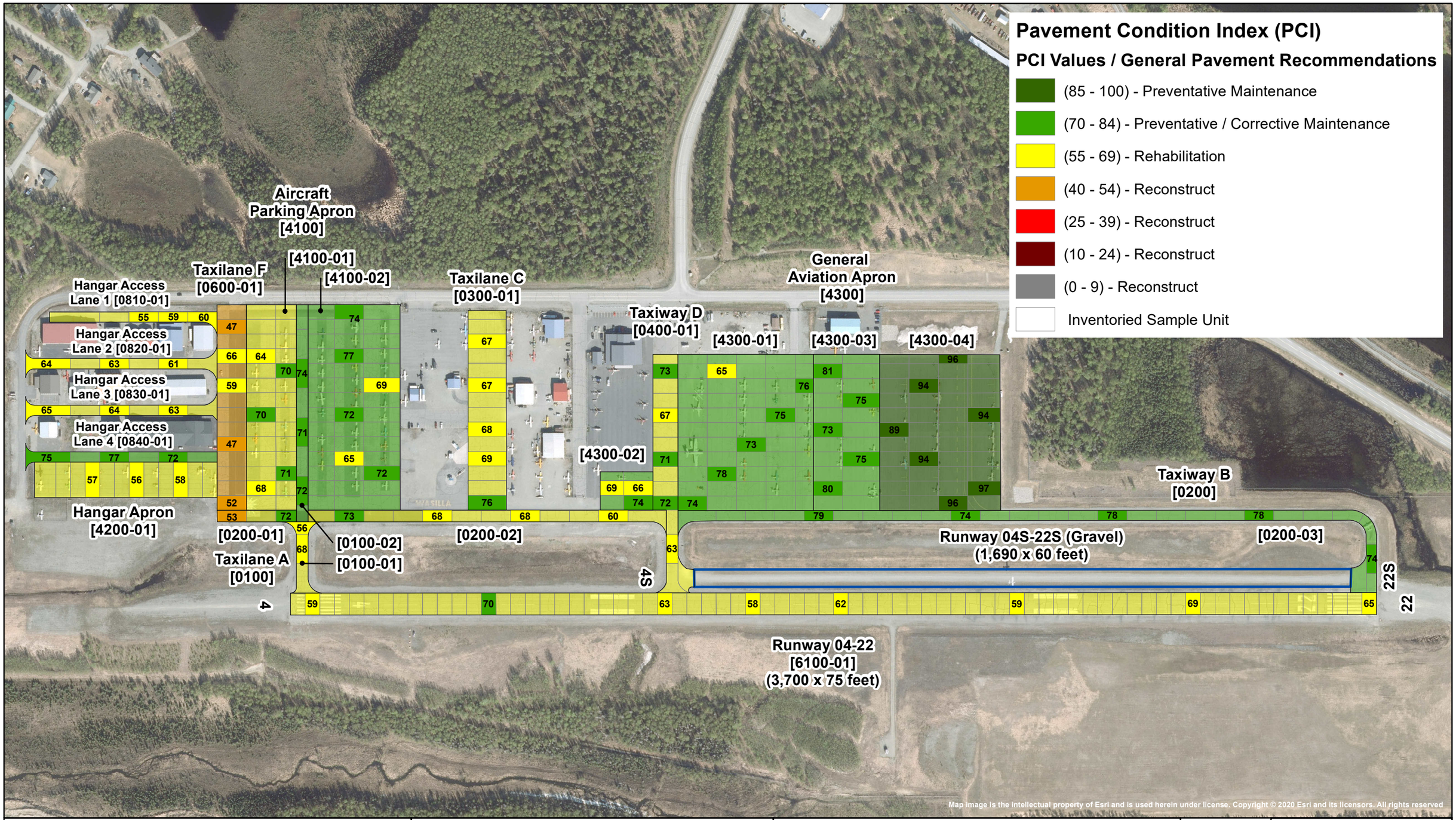
Target PCI Range for Runways: 70 to 100
Target PCI Range for Taxiways and Aprons: 60 to 100

2024 Pavement Inspection Results

0 200 400 800
Feet

Map Created by Duval Engineering for AK DOT&PF

Map 1 of 6



Wasilla Airport

Airport Code: IYS
Site Number: 50870.3*A

Sample Unit
Pavement Condition Index (PCI)
Target PCI Range for Runways: 70 to 100
Target PCI Range for Taxiways and Aprons: 60 to 100

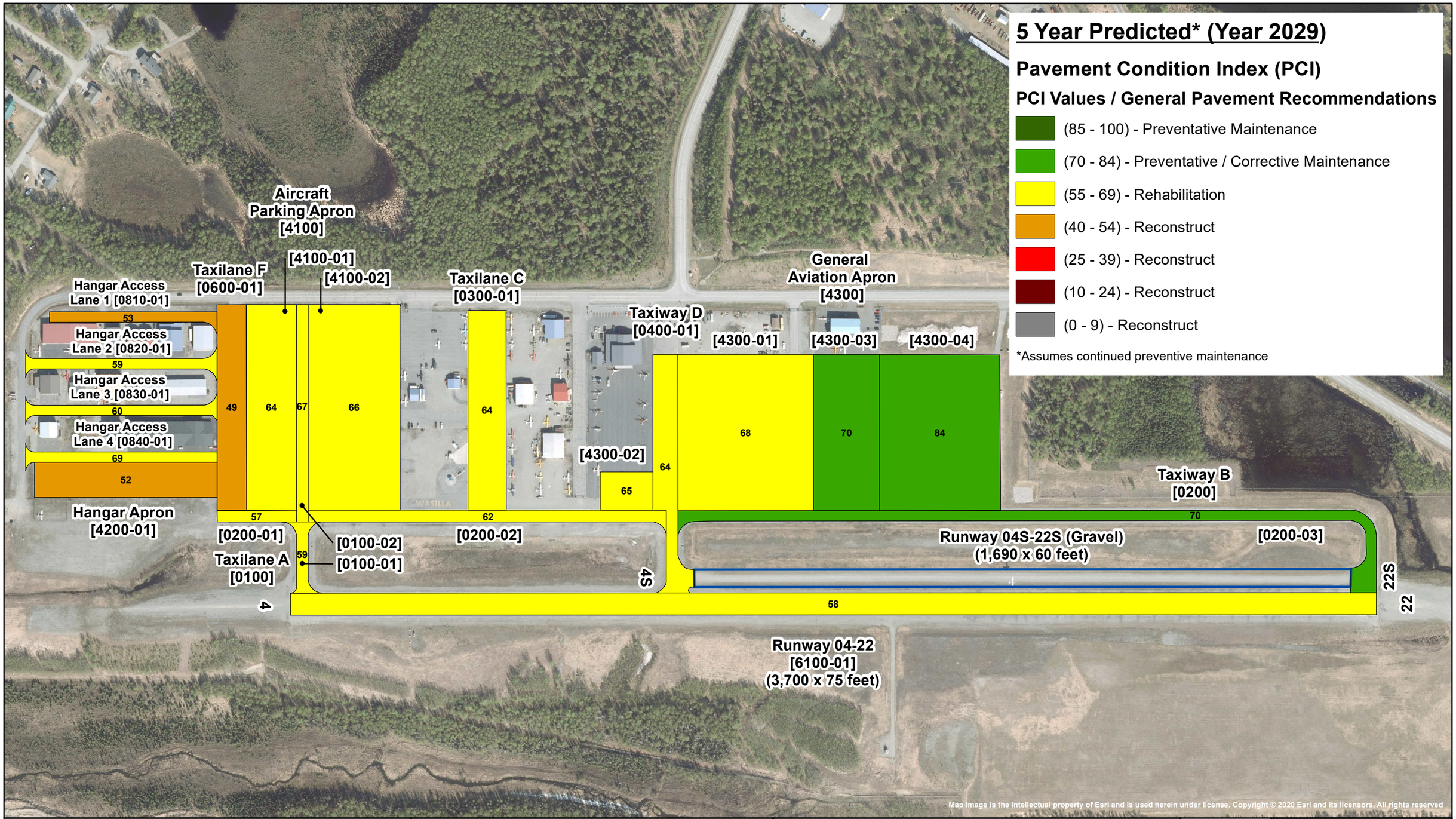
N W E S

2024 Pavement Inspection Results

0 200 400 800
Feet

Map Created by Duval Engineering for AK DOT&PF

Map 2 of 6



Wasilla Airport

Airport Code: IYS
Site Number: 50870.3*A

5 Year Predicted Pavement Condition Index (PCI)

Target PCI Range for Runways: 70 to 100
Target PCI Range for Taxiways and Aprons: 60 to 100

2024 Pavement Inspection Results

0 200 400 800
Feet

Map Created by Duval Engineering for AK DOT&PF

Map 3 of 6

10 Year Predicted* (Year 2034)

Pavement Condition Index (PCI)

PCI Values / General Pavement Recommendations

- (85 - 100) - Preventative Maintenance
- (70 - 84) - Preventative / Corrective Maintenance
- (55 - 69) - Rehabilitation
- (40 - 54) - Reconstruct
- (25 - 39) - Reconstruct
- (10 - 24) - Reconstruct
- (0 - 9) - Reconstruct

*Assumes continued preventive maintenance

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[4100-02]
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[0200-01]
[0200-02]
[0200-03]
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[0100-01]
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Runway 04S-22S (Gravel) (1,690 x 60 feet)
Runway 04-22 [6100-01] (3,700 x 75 feet)

Wasilla Airport

Airport Code: IYS
Site Number: 50870.3*A

10 Year Predicted Pavement Condition Index (PCI)

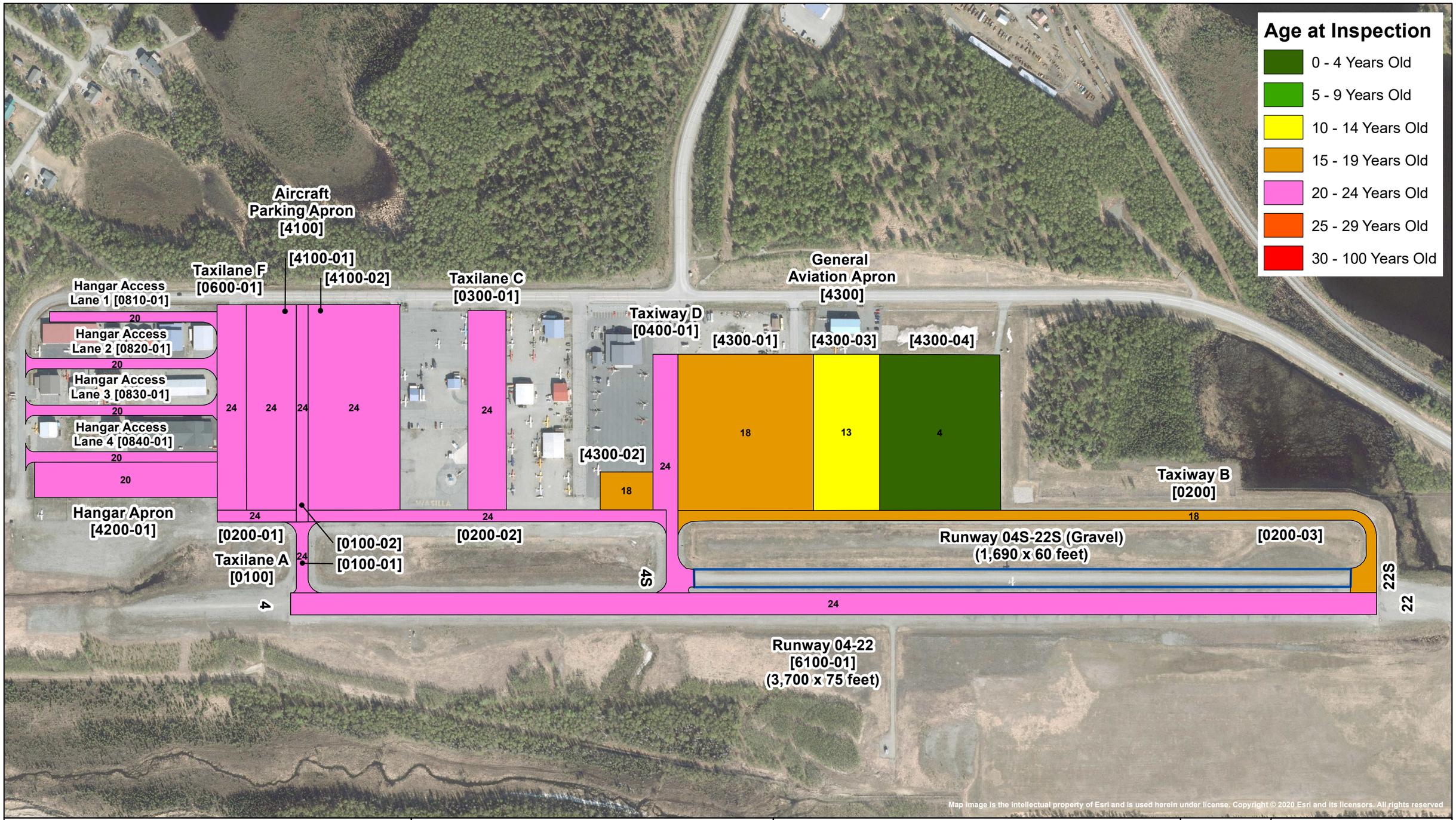
Target PCI Range for Runways: 70 to 100
Target PCI Range for Taxiways and Aprons: 60 to 100

2024 Pavement Inspection Results

0 200 400 800
Feet

Map Created by Duval Engineering for AK DOT&PF

Map 4 of 6



Wasilla Airport

Airport Code: IYS
Site Number: 50870.3*A

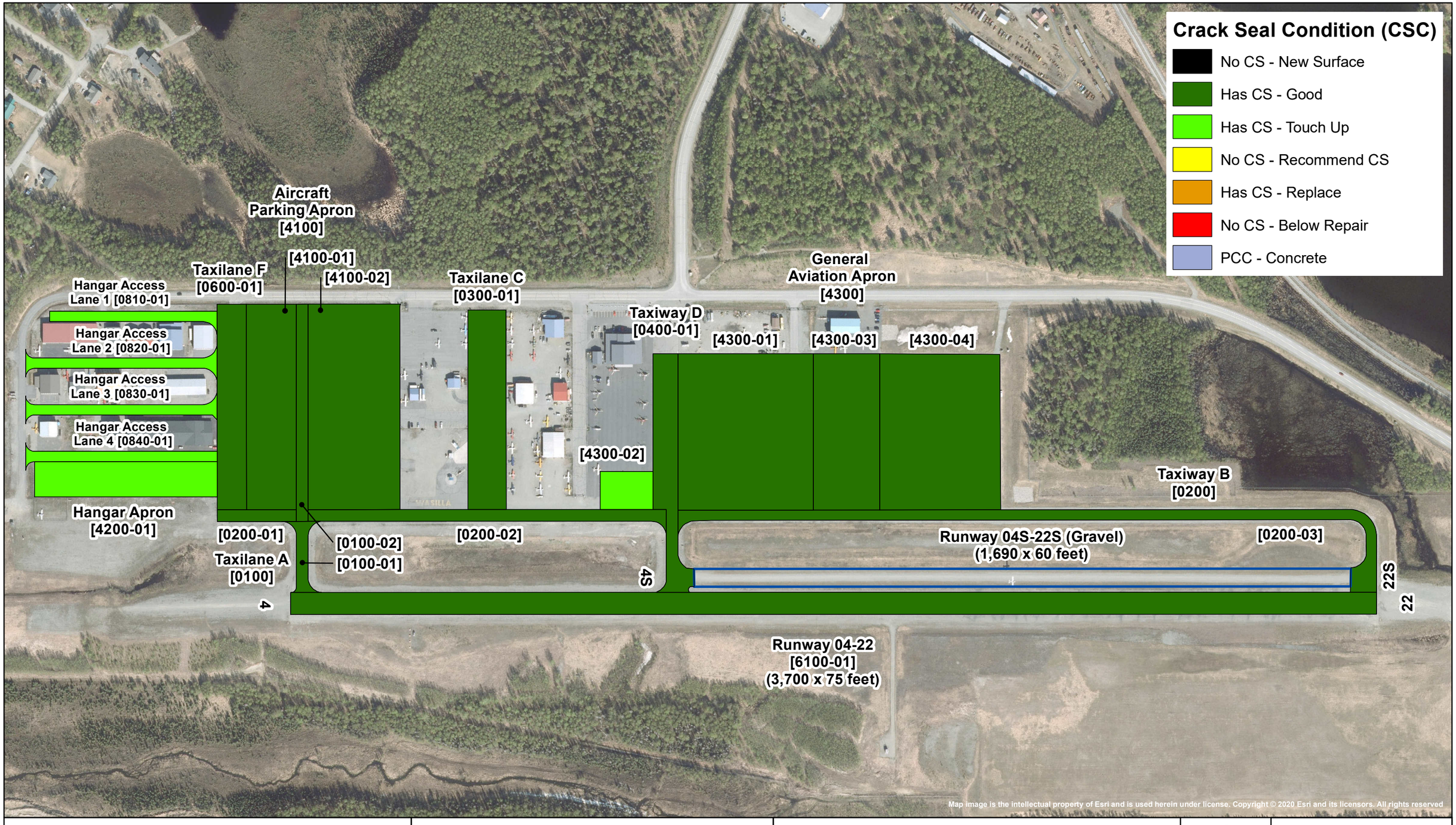
Pavement Age at Inspection

2024 Pavement Inspection Results

0 200 400 800
Feet

Map Created by Duval Engineering for AK DOT&PF

Map 5 of 6



Wasilla Airport

Airport Code: IYS
Site Number: 50870.3*A

Pavement Crack Seal Condition (CSC)

2024 Pavement Inspection Results

0 200 400 800
Feet

Map Created by Duval Engineering for AK DOT&PF

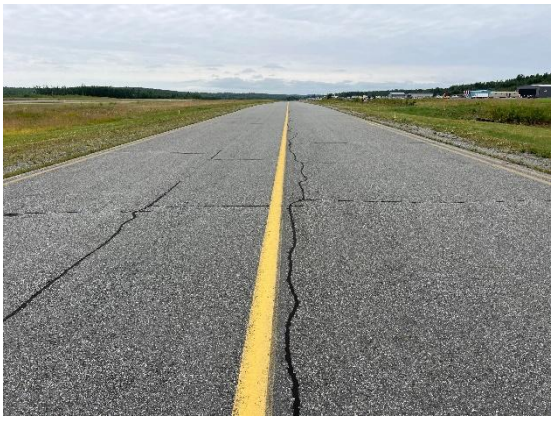
Map 6 of 6

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0200	Taxiway B	Taxiway	3	156,845	73

Section 0200-01 (61 PCI), 0200-02 (67 PCI)

Taxiway B sections 0200-01 and 0200-02 were initially constructed in 2000 and have not received any major work since. A surface treatment was applied in 2022 to a majority of section 0200-01 and a portion of section 0200-02. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low to medium severity longitudinal and transverse cracking, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Section 0200-03 (77 PCI)



Taxiway B section 0200-03 was initially constructed in 2006 and has not received any major work since. Occasional crack seal operations have been performed on the branch. The most common distresses observed include low to medium severity longitudinal and transverse cracking, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0300	Taxilane C	Taxiway	1	88,400	69

Taxilane C was initially constructed in 2000 and has not received major work since. A surface treatment was applied in 2023 to the entire section. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low severity block cracking, low to medium severity longitudinal and transverse cracking, and low severity raveling. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

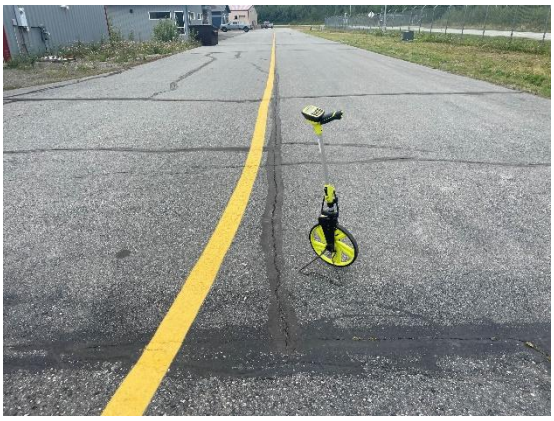
Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0400	Taxiway D	Taxiway	1	61,452	69

Taxiway D was initially constructed in 2000 and has not received major work since. A surface treatment was applied in 2023 to the majority of the section. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low severity block cracking, low severity depression, low to medium severity longitudinal and transverse cracking, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0600	Taxilane F	Taxiway	1	70,000	54

Taxilane F was initially constructed in 2000 and has not received major work since. A surface treatment was applied in 2022 to approximately half of the section. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low to medium severity block cracking, low to medium severity longitudinal and transverse cracking, oil spillage, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0810	Hanger Access Lane 1	Taxiway	1	20,518	58



Hangar Access Lane 1 was initially constructed in 2004 and has not received major work since. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low to high severity longitudinal and transverse cracking, low severity patching, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration. A few of the transverse cracks are beginning to sag, creating a ride quality issue.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0820	Hanger Access Lane 2	Taxiway	1	24,297	63

Hangar Access Lane 2 was initially constructed in 2004 and has not received major work since. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low severity depression, low to high severity longitudinal and transverse cracking, low severity patching, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration. A few of the transverse cracks are beginning to sag, creating a ride quality issue.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0830	Hanger Access Lane 3	Taxiway	1	23,896	64

Hangar Access Lane 3 was initially constructed in 2004 and has not received major work since. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low to high severity longitudinal and transverse cracking, low severity patching, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration. A few of the transverse cracks are beginning to sag, creating a ride quality issue.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
0840	Hanger Access Lane 4	Taxiway	1	23,203	75

Hangar Access Lane 4 was initially constructed in 2004 and has not received major work since. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low to medium severity longitudinal and transverse cracking, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
4100	Aircraft Parking Apron	Apron	2	338,100	70

The Aircraft Parking Apron was initially constructed in 2000 and has not received major work since. A surface treatment was applied in 2022 to approximately half of both sections of the apron. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low severity depression, low to medium severity longitudinal and transverse cracking, oil spillage, low to medium severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
4200	Hanger Apron	Apron	1	74,400	57

The Hangar Apron was initially constructed in 2004 and has not received major work since. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low to medium severity longitudinal and transverse cracking, oil spillage, low severity patching, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
4300	GA Apron	Apron	4	607,536	81

Section 4300-01 (73 PCI), 4300-02 (70 PCI)

The GA Apron section 4300-01 and 4300-02 were initially constructed in 2006 and have not received major work since. A surface treatment was applied in 2023 to approximately half of section 4300-01 and was not applied to section 4300-02. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low severity block cracking, low to medium severity longitudinal and transverse cracking, oil spillage, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
4300	GA Apron	Apron	4	607,536	81

Section 4300-03 (77 PCI)

The GA Apron section 4300-03 was initially constructed in 2006 and has not received major work since. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low to medium severity longitudinal and transverse cracking, oil spillage, low severity raveling, and low severity weathering. Field observations note the continued deterioration of cracks, causing the sealant to detach and allowing vegetation growth. This leads to higher severity distresses, increased water infiltration, and an accelerated rate of deterioration.

Section 4300-04 (94 PCI)

The GA Apron section 4300-04 was initially constructed in 2020 and has not received major work since. Occasional crack sealing operations have been performed on the branch. The most common distresses observed include low severity longitudinal and transverse cracking and oil spillage. Field observations note the initial development of cracking across the section and the first sightings of oil spillage.

Branch ID	Branch Name	Branch Use	No. of Sections	Area (sf)	Weighted Average PCI
6100	Runway 04/22	Runway	1	277,500	63




Runway 04/22 was initially constructed in 2000 and has not received any major work since. In 2021, localized maintenance and repair was performed to address large cracks developing across the runway. The work included milling cracks to a depth of two inches and a width of 12 inches. This involved grinding down the top layer of AC around the cracks to create a clean, smooth surface for repair. By removing the deteriorated material, the milling ensures better adhesion of the new materials and prevents further deterioration. After milling, sand or grout was placed into the milled cracks, followed by the application of a tack coat to enhance the bond between the old and new AC layers. Periodic crack sealing operations have been performed on the branch. The most common distresses observed are low to medium severity longitudinal and transverse cracking, low to medium severity patching, low severity raveling, and low severity weathering. Field observations note the placement of patches across the runway, correcting high severity longitudinal and transverse cracking recorded during the 2020 inspection. These patches are stabilizing the PCI, preventing further water infiltration, and avoiding a high rate of deterioration.

BRANCH CONDITION REPORT

Branch ID	No. of Sections	Sum Section Length (Ft)	Avg Section Width (Ft)	True Area (Sq Ft)	Use	Average PCI	Standard Deviation PCI	Weighted Average PCI
0100	2	982	40	41,010	TAXIWAY	67.70	4.80	69.83
0200	3	4,085	38	156,845	TAXIWAY	68.23	6.47	72.57
0300	1	680	130	88,400	TAXIWAY	69.30	0.00	69.30
0400	1	812	85	61,452	TAXIWAY	69.00	0.00	69.00
0600	1	700	100	70,000	TAXIWAY	54.20	0.00	54.20
0810	1	570	35	20,518	TAXIWAY	58.20	0.00	58.20
0820	1	650	35	24,297	TAXIWAY	63.00	0.00	63.00
0830	1	650	35	23,896	TAXIWAY	64.10	0.00	64.10
0840	1	650	35	23,203	TAXIWAY	74.90	0.00	74.90
4100	2	1,400	242	338,100	APRON	69.85	1.55	70.31
4200	1	620	120	74,400	APRON	57.20	0.00	57.20
4300	4	1,726	320	607,536	APRON	78.40	9.43	81.26
6100	1	3,700	75	277,500	RUNWAY	63.10	0.00	63.10

Note: the dimensions in the Branch Condition Report are derived from area calculations and may not reflect actual dimensions of individual sections. Refer to the maps for actual section dimensions.

BRANCH USE CONDITION REPORT

Use Category	No. of Sections	Total Area (Sq Ft)	Arithmetic Average PCI	Standard Deviation PCI	Weighted Average PCI
APRON	7	1,020,036	72.93	10.33	75.87
RUNWAY	1	277,500	63.10	0.00	63.10
TAXIWAY	12	509,621	66.07	6.48	67.50
ALL	20	1,807,157	68.32	8.62	71.55

SECTION CONDITION REPORT

Branch ID	Section ID	Last Const. Date	Surface	Use	Rank	True Area (Sq Ft)	Last Inspection Date	Age At Inspection	PCI
0100	0100-01	9/1/2000	AC	TAXIWAY	T	11,410	7/20/2024	24	63
0100	0100-02	9/1/2000	AC	TAXIWAY	T	29,600	7/20/2024	24	73
0200	0200-01	9/1/2000	AC	TAXIWAY	T	10,800	7/20/2024	24	61
0200	0200-02	9/1/2000	AC	TAXIWAY	T	49,341	7/20/2024	24	67
0200	0200-03	7/1/2006	AC	TAXIWAY	T	96,704	7/20/2024	18	77
0300	0300-01	9/1/2000	AC	TAXIWAY	T	88,400	7/20/2024	24	69
0400	0400-01	9/1/2000	AC	TAXIWAY	T	61,452	7/20/2024	24	69
0600	0600-01	9/1/2000	AC	TAXIWAY	T	70,000	7/20/2024	24	54
0810	0810-01	9/1/2004	AC	TAXIWAY	T	20,518	7/20/2024	20	58
0820	0820-01	9/1/2004	AC	TAXIWAY	T	24,297	7/20/2024	20	63
0830	0830-01	9/1/2004	AC	TAXIWAY	T	23,896	7/20/2024	20	64
0840	0840-01	9/1/2004	AC	TAXIWAY	T	23,203	7/20/2024	20	75
4100	4100-01	9/1/2000	AC	APRON	T	119,000	7/20/2024	24	68
4100	4100-02	9/1/2000	AC	APRON	T	219,100	7/20/2024	24	71
4200	4200-01	9/1/2004	AC	APRON	T	74,400	7/20/2024	20	57
4300	4300-01	7/1/2006	AC	APRON	T	245,784	7/20/2024	18	73
4300	4300-02	7/1/2006	AC	APRON	T	23,400	7/20/2024	18	70
4300	4300-03	8/1/2011	AC	APRON	T	120,232	7/20/2024	13	77
4300	4300-04	6/4/2020	AAC	APRON	T	218,120	7/20/2024	4	94
6100	6100-01	9/1/2000	AC	RUNWAY	T	277,500	7/20/2024	24	63

SECTION CONDITION REPORT (SUMMARY BY AGE CATEGORY)

Age Category	Average Age at Inspection	Total Area (Sq Ft)	Number of Sections	Arithmetic Average PCI	Standard Deviation PCI	Weighted Average PCI
03-05	4	218,120	1	94.20	0.00	94.20
11-15	13	120,232	1	76.50	0.00	76.50
16-20	19	532,202	8	67.13	7.07	70.07
21-25	24	936,603	10	65.87	5.29	66.49
ALL	21	1,807,157	20	68.32	8.62	71.55

Work History Report

Pavement Database: Alaska

Network: Wasilla Airport **Branch:** 0100 Taxilane A **Section:** 0100-01 **Surface:** AC
L.C.D. 9/1/2000 **Use:** TAXIWAY **Rank:** T **Length:** 242.00 (Ft) **Width:** 40.00 (Ft) **True Area:** 11410 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2021	CS-AC	Crack Sealing - AC	0.00	0.00	☐	Rout and fill unsealed pavement crack
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0100 Taxilane A **Section:** 0100-02 **Surface:** AC
L.C.D. 9/1/2000 **Use:** TAXIWAY **Rank:** T **Length:** 740.00 (Ft) **Width:** 40.00 (Ft) **True Area:** 29600 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2022	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Approximately 27,000 sgft
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0200 Taxiway B **Section:** 0200-01 **Surface:** AC
L.C.D. 9/1/2000 **Use:** TAXIWAY **Rank:** T **Length:** 270.00 (Ft) **Width:** 40.00 (Ft) **True Area:** 10800 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2022	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Approximately 7,000 sqft
8/1/2021	CS-AC	Crack Sealing - AC	0.00	0.00	☐	Rout and fill unsealed pavement crack
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0200 Taxiway B **Section:** 0200-02 **Surface:** AC
L.C.D. 9/1/2000 **Use:** TAXIWAY **Rank:** T **Length:** 1,220.00 (Ft) **Width:** 40.00 (Ft) **True Area:** 49341 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2022	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Approximately 18,000 sqft
8/1/2021	CS-AC	Crack Sealing - AC	0.00	0.00	☐	Rout and fill unsealed pavement crack
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0200 Taxiway B **Section:** 0200-03 **Surface:** AC
L.C.D. 7/1/2006 **Use:** TAXIWAY **Rank:** T **Length:** 2,595.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 96704 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2021	CS-AC	Crack Sealing - AC	0.00	0.00	☐	Rout and fill unsealed pavement crack
7/1/2006	NC-IN	New Construction - Initial	0.00	0.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0300 Taxilane C **Section:** 0300-01 **Surface:** AC
L.C.D. 9/1/2000 **Use:** TAXIWAY **Rank:** T **Length:** 680.00 (Ft) **Width:** 130.00 (Ft) **True Area:** 88400 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2023	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Applied to entire area
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0400 Taxiway D **Section:** 0400-01 **Surface:** AC

L.C.D. 9/1/2000 **Use:** TAXIWAY **Rank:** T **Length:** 812.00 (Ft) **Width:** 85.00 (Ft) **True Area:** 61452 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2023	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Approximately 46,000 sqft
8/1/2021	CS-AC	Crack Sealing - AC	0.00	0.00	☐	Rout and fill unsealed pavement crack
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0600 Taxilane F **Section:** 0600-01 **Surface:** AC

L.C.D. 9/1/2000 **Use:** TAXIWAY **Rank:** T **Length:** 700.00 (Ft) **Width:** 100.00 (Ft) **True Area:** 70000 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2022	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Approximately 35,000 sqft
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0810 Hanger Access Lan **Section:** 0810-01 **Surface:** AC

L.C.D. 9/1/2004 **Use:** TAXIWAY **Rank:** T **Length:** 570.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 20518 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
9/1/2004	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0820 Hanger Access Lan **Section:** 0820-01 **Surface:** AC

L.C.D. 9/1/2004 **Use:** TAXIWAY **Rank:** T **Length:** 650.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 24297 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
9/1/2004	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0830 Hanger Access Lan **Section:** 0830-01 **Surface:** AC

L.C.D. 9/1/2004 **Use:** TAXIWAY **Rank:** T **Length:** 650.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 23896 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
9/1/2004	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 0840 Hanger Access Lan **Section:** 0840-01 **Surface:** AC

L.C.D. 9/1/2004 **Use:** TAXIWAY **Rank:** T **Length:** 650.00 (Ft) **Width:** 35.00 (Ft) **True Area:** 23203 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
9/1/2004	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 4100 Aircraft Parking A **Section:** 4100-01 **Surface:** AC

L.C.D. 9/1/2000 **Use:** APRON **Rank:** T **Length:** 700.00 (Ft) **Width:** 170.00 (Ft) **True Area:** 119000 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2022	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Approximately 57,000 sqft
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 4100 Aircraft Parking A **Section:** 4100-02 **Surface:**AC
L.C.D. 9/1/2000 **Use:** APRON **Rank:** T **Length:** 700.00 (Ft) **Width:** 313.00 (Ft) **True Area:** 219100 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2022	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Approximately 110,000 sqft
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 4200 Hanger Apron **Section:** 4200-01 **Surface:**AC
L.C.D. 9/1/2004 **Use:** APRON **Rank:** T **Length:** 620.00 (Ft) **Width:** 120.00 (Ft) **True Area:** 74400 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
9/1/2004	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 4300 GA Apron **Section:** 4300-01 **Surface:**AC
L.C.D. 7/1/2006 **Use:** APRON **Rank:** T **Length:** 532.00 (Ft) **Width:** 462.00 (Ft) **True Area:** 245784 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2023	ST-SS	Surface Treatment - Slurry Seal	0.00	0.00	☐	Approximately 140,000 sqft
7/1/2006	NC-IN	New Construction - Initial	0.00	0.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 4300 GA Apron **Section:** 4300-02 **Surface:**AC
L.C.D. 7/1/2006 **Use:** APRON **Rank:** T **Length:** 130.00 (Ft) **Width:** 180.00 (Ft) **True Area:** 23400 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
7/1/2006	NC-IN	New Construction - Initial	0.00	0.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 4300 GA Apron **Section:** 4300-03 **Surface:**AC
L.C.D. 8/1/2011 **Use:** APRON **Rank:** T **Length:** 532.00 (Ft) **Width:** 226.00 (Ft) **True Area:** 120232 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2011	NC-IN	New Construction - Initial	0.00	0.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 4300 GA Apron **Section:** 4300-04 **Surface:**AAC
L.C.D. 6/4/2020 **Use:** APRON **Rank:** T **Length:** 532.00 (Ft) **Width:** 410.00 (Ft) **True Area:** 218120 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
6/4/2020	NC-IN	New Construction - Initial	0.00	0.00	☑	(Funded via AIP)

Network: Wasilla Airport **Branch:** 6100 04/22 **Section:** 6100-01 **Surface:**AC
L.C.D. 9/1/2000 **Use:** RUNWAY **Rank:** T **Length:** 3,700.00 (Ft) **Width:** 75.00 (Ft) **True Area:** 277500 (SqFt)

Work Date	Work Code	Work Description	Cost	Thickness (in)	Major M&R	Comments
8/1/2021	CS-AC	Crack Sealing - AC	0.00	0.00	☐	Rout and fill unsealed pavement crack
8/1/2021	PA-AS	Patching - AC Shallow	0.00	2.00	☐	Mill partial depth (2" deep x 12" wide)
9/1/2000	NC-IN	New Construction - Initial	0.00	2.00	☑	(Funded via AIP)

Work History Report

Pavement Database: Alaska

Summary:

Work Description	Section Count	Area Total (SqFt)	Thickness Avg (in)	Thickness STD (in)
Crack Sealing - AC	6	507,207.00	0.00	0.00
New Construction - Initial	20	1,807,157.01	1.50	0.87
Patching - AC Shallow	1	277,500.00	2.00	0.00
Surface Treatment - Slurry Seal	9	893,477.00	0.00	0.00

PHYSICAL PROPERTY DATA

		Pavement		Base		Subbase		Subgrade	
Branch ID	Section ID	Thick (in)	Type	Thick (in)	Type	Thick (in)	Type	Type	CBR
Taxilane A 0100	0100-01	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
	0100-02	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
Taxiway B 0200	0200-01	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
	0200-02	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
	0200-03	2	P-401	6 [1]	P-208	24 [1]	P-154	ML (F4)	2.7
Taxilane C 0300	0300-01	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
Taxiway D 0400	0400-01	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
Taxilane F 0600	0600-01	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
Hangar Access Lane 1 0810	0810-01	2	P-401	6 [1]	P-208	24 [1]	P-154	ML (F4)	2.7
Hangar Access Lane 2 0820	0820-01	2	P-401	6 [1]	P-208	24 [1]	P-154	ML (F4)	2.7
Hangar Access Lane 3 0830	0830-01	2	P-401	6 [1]	P-208	24 [1]	P-154	ML (F4)	2.7
Hangar Access Lane 4 0840	0840-01	2	P-401	6 [1]	P-208	24 [1]	P-154	ML (F4)	2.7
Aircraft Parking Apron 4100	4100-01	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
	4100-02	2	P-401	6	P-208	24	P-154	ML (F4)	2.7
Hangar Apron 4200	4200-01	2	P-401	6 [1]	P-208	24 [1]	P-154	ML (F4)	2.7
General Aviation Apron 4300	4300-01	2	P-401	6 [1]	P-208	24 [1]	P-154	ML (F4)	2.7
	4300-02	2	P-401	6 [1]	P-208	24 [1]	P-154	ML (F4)	2.7
Runway 4/22 6100	6100-01	2	P-401	6	P-208	30	P-154	ML (F4)	2.7

Notes:
[1] Estimated, no as-built construction records

AIRCRAFT FLEET MIX

No.	Aircraft	Gross Wt (lb)	% Gross Wt on Main Gear	Tire Pressure (psi)	Annual Departures	20 Yr Coverages
1	Cessna 208B	8,750	95.0	75	4	20
2	S-10	10,450	95.0	52	3	18
3	PA-31-325 Navajo C/R	6,536	95.0	66	23,659	110,079
4	Beechcraft King Air	12,590	95.0	98	1	8
5	S-15	16,975	95.0	57	1	7
6	S-10	9,850	95.0	49	1	6
7	Cessna 206	3,612	95.0	52	1	4
8	D-15	17,120	95.0	63	2	20
9	Learjet 35/36/35A/36A	18,000	95.0	171	2	15
10	Bombardier CL-604/605	48,200	95.0	145	2	22
11	Cessna 172 Skyhawk	2,558	95.0	50	2	7
12	Gulfstream G-V	90,900	95.0	188	2	25
13	D-50	50,000	95.0	80	2	28
14	Cessna 182 Skylane	3,110	95.0	50	23,659	93,058
15	Cessna Citation X	36,000	95.0	189	90	807

PAVEMENT CLASSIFICATION RATINGS

Runway	Critical Aircraft	Max Allowable Wt (lb)	Subgrade Mr (psi)	Evaluation Thickness (in)	Pass to Traffic Cycle Ratio	PCR
4-22	Cessna Citation X	36,000	4,000	38.0	1.0	129/F/D/X/U

PCR CALCULATION NOTES

- 1% traffic growth assumed
- Subgrade strength reduction for frost applied
- S-10 refers to “generic single gear aircraft as modeled in FAARFIELD
- Aircraft fleet mix and annual departures were provided by Alaska DOT&PF
- The number of Beechcraft King Air annual departures were doubled to 10 to allow the FAARFIELD software to complete the PCR calculation
- Technical evaluation of RW 4-22 PCR using FAARFIELD 2.0 resulted in a higher PCR that shown above; however, since the RW 4-22 pavement section does not conform to FAA design standards, application of FAARFIELD 2.0 to evaluate PCR is called into question. Alaska DOT&PF applied the “Using Aircraft” methodology described in AC 150/5335-5C to establish the PCR reported above.

REFERENCES

Year	Project No.	Document Title
2024	Email from Airport Manager	Wasilla Airport Runway 3/21, As-Built Data
2024		Wasilla Airport, AIP Grant list
1990	1990 3-02-0417-03, 55908	Construct Runway 3/21, Plans
1990	1990 3-02-0417-03, 55908	Construct Taxiway, Plans
1990	1990 3-02-0417-03, 55908	Construct Apron, Plans
1990	55908	Wasilla Airport, Geotechnical Report