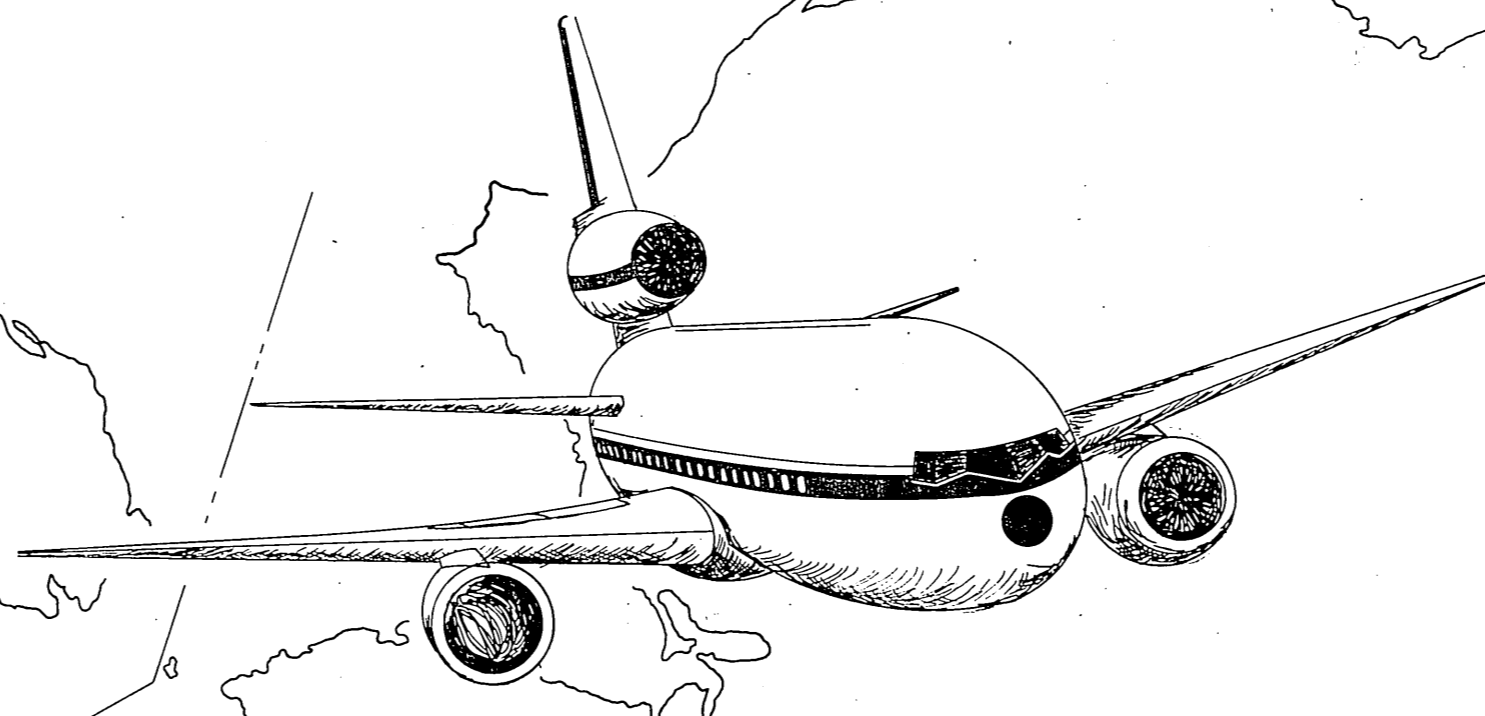


DEPARTMENT OF TRANSPORTATION  
and PUBLIC FACILITIES  
S.E. REGION AVIATION—DESIGN & CONSTRUCTION



CONSTRUCTION PLANS FOR

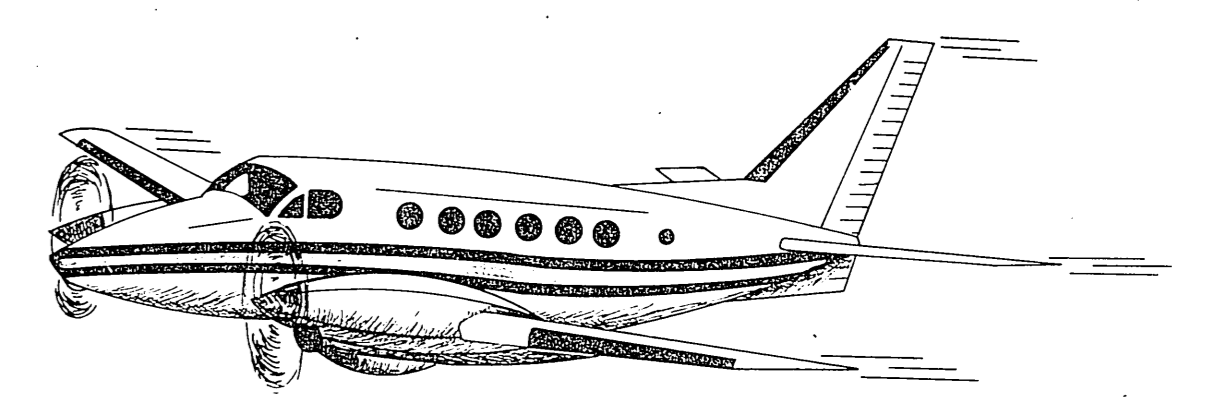
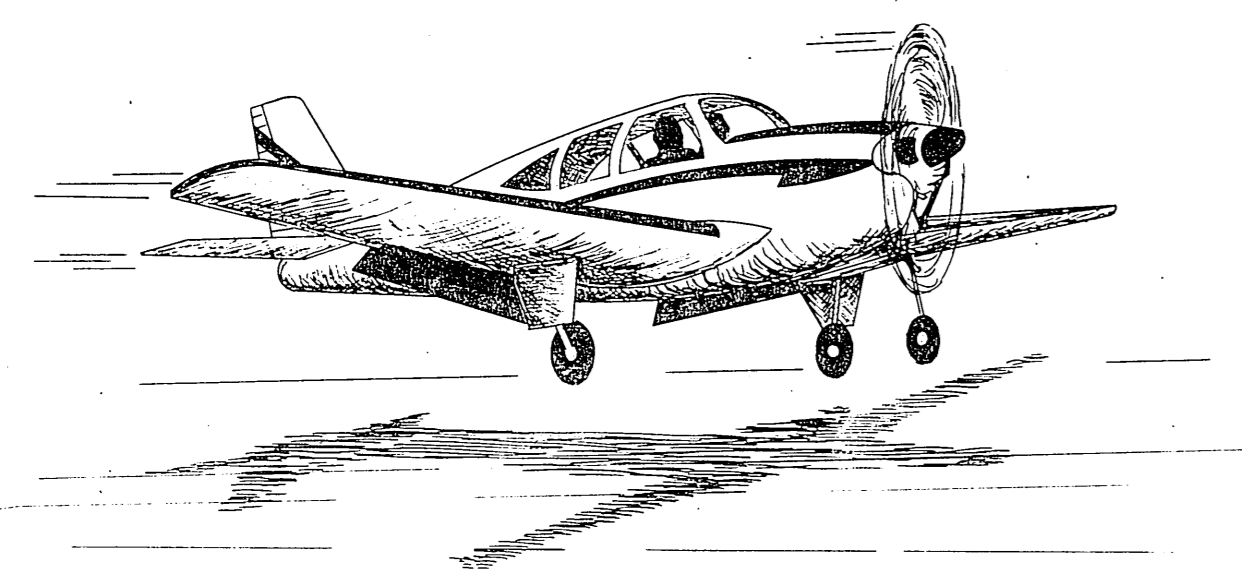
# WRANGELL AIRPORT

BITUMINOUS SURFACE COURSE OVERLAY  
CHIP SEAL FRICTION COURSE

A.I.P. No. 3-02-0323-01  
A.D.A.P. NO. 6-02-0323-06  
STATE NO. D-46762

SPONSORED BY  
THE STATE OF ALASKA  
1982

"As BUILT"  
CONTRACTOR: ASSOCIATED SAND & GRAVEL, INC.  
PROJECT ENGINEER: DALE ROBBINS  
BEGINNING DATE: MARCH 17, 1983  
COMPLETION DATE: AUGUST 17, 1983



APPROVED  
*Charles S. Matlock*  
CHARLES S. MATLOCK, DIRECTOR, S.E. REGION DESIGN & CONSTRUCTION  
DATE 7-16-82

APPROVED  
*Wallace K. Williams*  
WALLACE K. WILLIAMS, CHIEF, S.E. REGION HIGHWAY & AVIATION DESIGN & CONSTRUCTION  
DATE 8/27/82

## ESTIMATED QUANTITIES

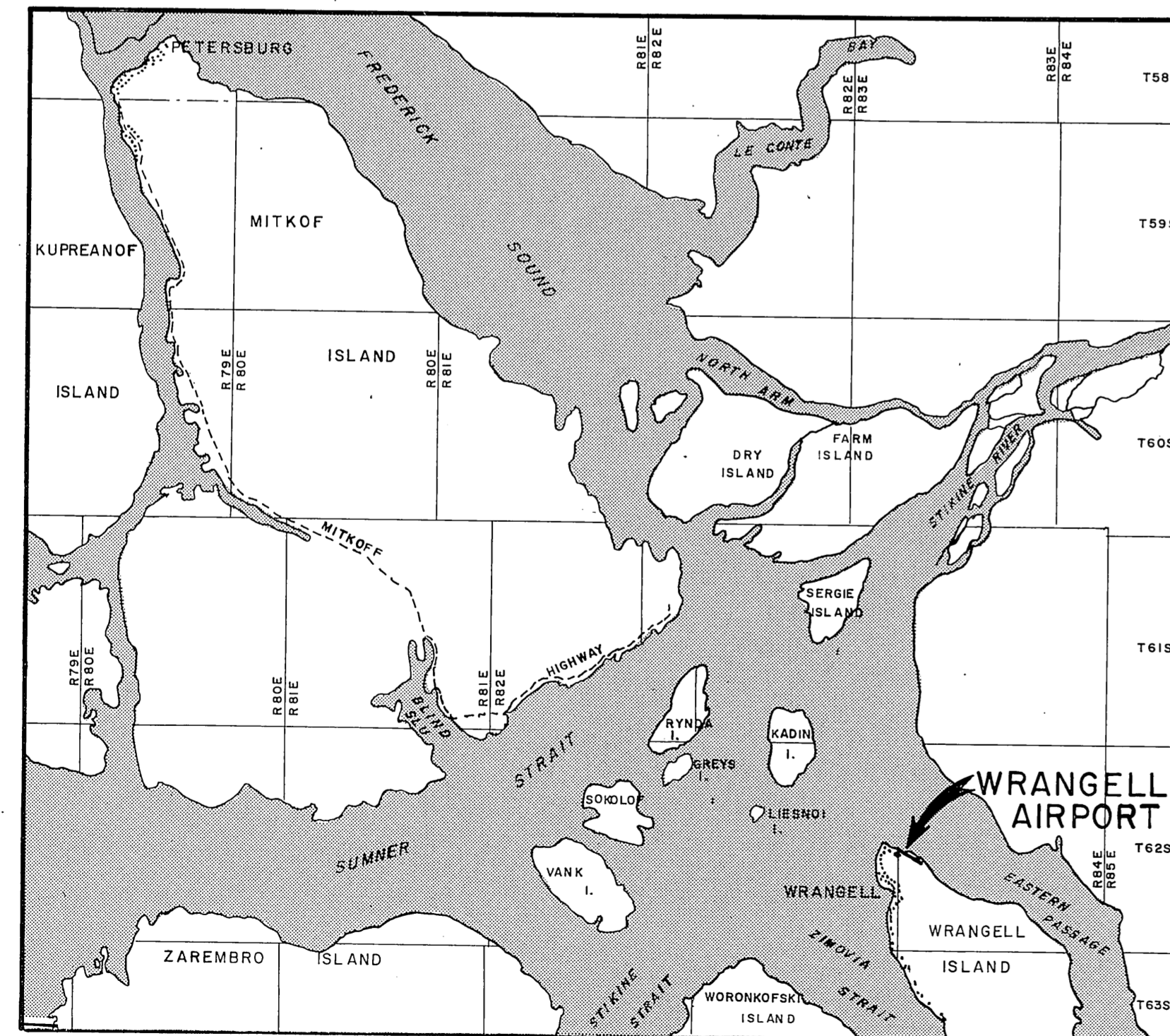
NO.	ITEM	QUANTITY	UNIT
100	MOBILIZATION AND DEMOBILIZATION	ONE JOB	LUMP SUM
125	ENGINEERS VEHICLE	2	EACH
130	ENGINEERS FIELD OFFICE WITH FIELD LAB	ONE JOB	LUMP SUM
510 a	CRUSHED AGGREGATE BASE	<del>982.18</del> 815	TONS
551	NON-WOVEN POLYPROPYLENE FABRIC	<del>125,823</del> 105,873	SQ. YDS.
610	BITUMINOUS TACK COAT AC-5	<del>103.60</del> 133.3	TONS
620 a	BITUMINOUS SEAL COAT MATERIAL	<del>111.7</del>	TONS
620 b	SEAL COAT AGGREGATE	<del>772.19</del> 1,064	TONS
660 a	BITUMINOUS SURFACE COURSE	<del>12,912.06</del> 12,158	TONS
660 c	ASPHALT CEMENT AC-5	<del>667.80</del> 790.2	TONS
700	RUNWAY AND TAXIWAY MARKING	ONE JOB	LUMP SUM
820 a	TIE-DOWN ANCHORS	33	EACH
620 c	BITUMINOUS SEAL COAT RC-800	77.81	TONS

## INDEX

SHEET TITLE	SHEET NO.
TITLE SHEET	1.
LOCATION & VICINITY MAPS, INDEX AND ESTIMATED QUANTITIES	2.
PROJECT LAYOUT PLAN	3.
TYPICAL SECTIONS	4.
AIRPORT CLOSURE & CONSTRUCTION MARKING	5.
AIRPORT MARKING PLAN	6.

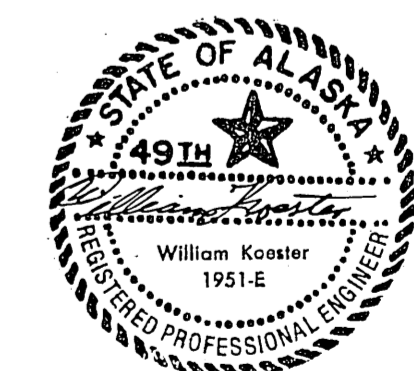


**LOCATION MAP**



**VICINITY MAP**

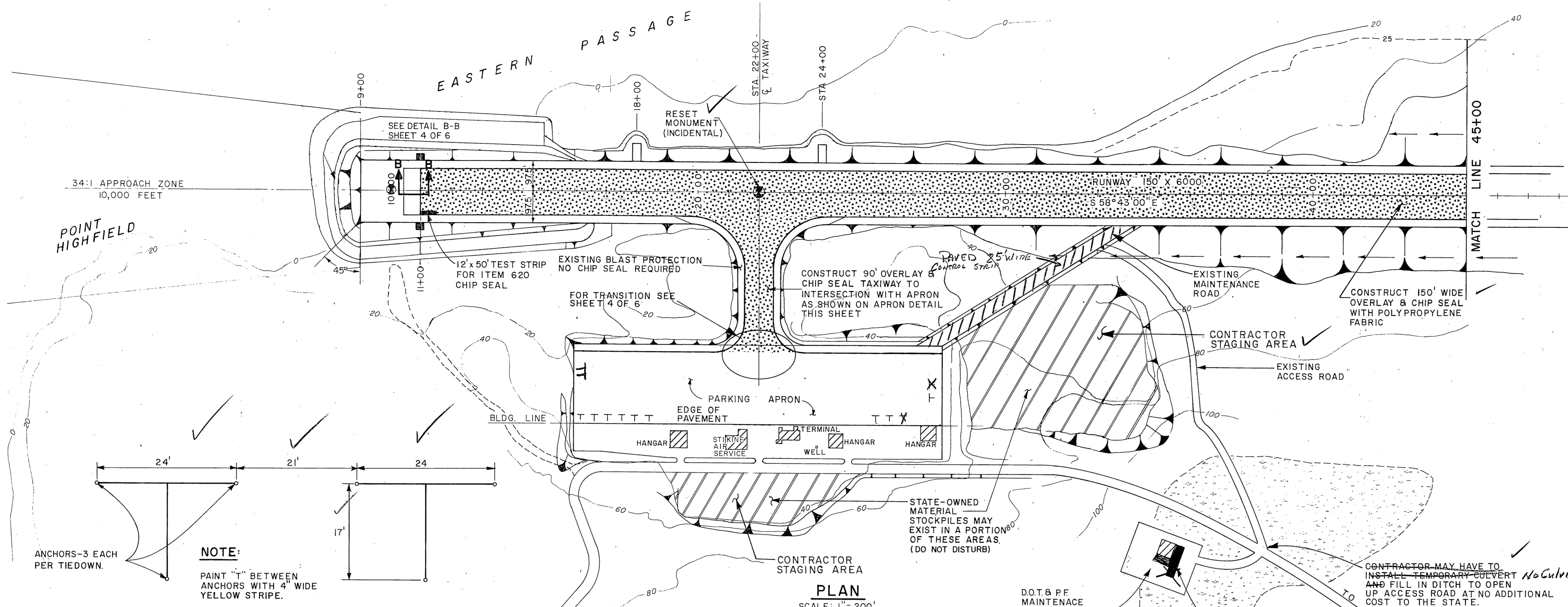
1" = 4 MILES



STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
DIVISION OF AVIATION-DESIGN AND CONSTRUCTION  
**WRANGELL AIRPORT**  
A.D.A.R. No. 6-02-0323-06  
LOCATION & VICINITY MAPS  
INDEX & ESTIMATED QUANTITIES

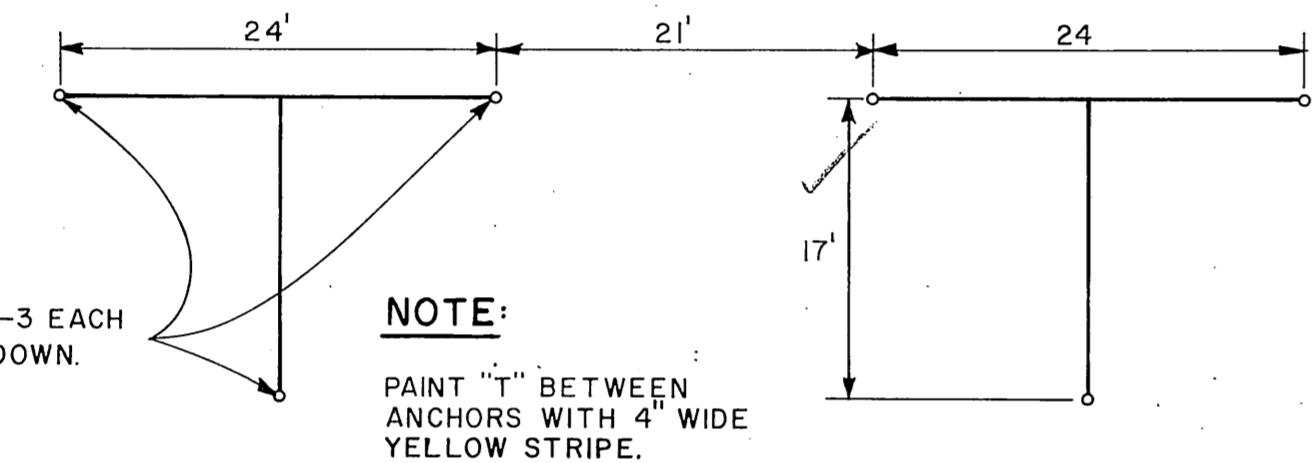
APPROVED	<i>William A. Allison</i>			CHIEF DESIGN ENGINEER
APPROVED	<i>William Koester</i>			SOUTHEASTERN REGION ENGINEER
BY	DATE	CHANGE	SCALE	DESIGNED
			AS-SHOWN	<i>J.S.</i>
REVISIONS			CHECKED	DATE
			<i>J.S.</i>	<i>10-1-81</i>
				SHEET 2 OF 6

DATE	
BY	
SURVEY	
PLOTTED	
ALIGNMENT CHECKED	
REVISIONS	
NO.	
PLAN	
NOTE BOOK	

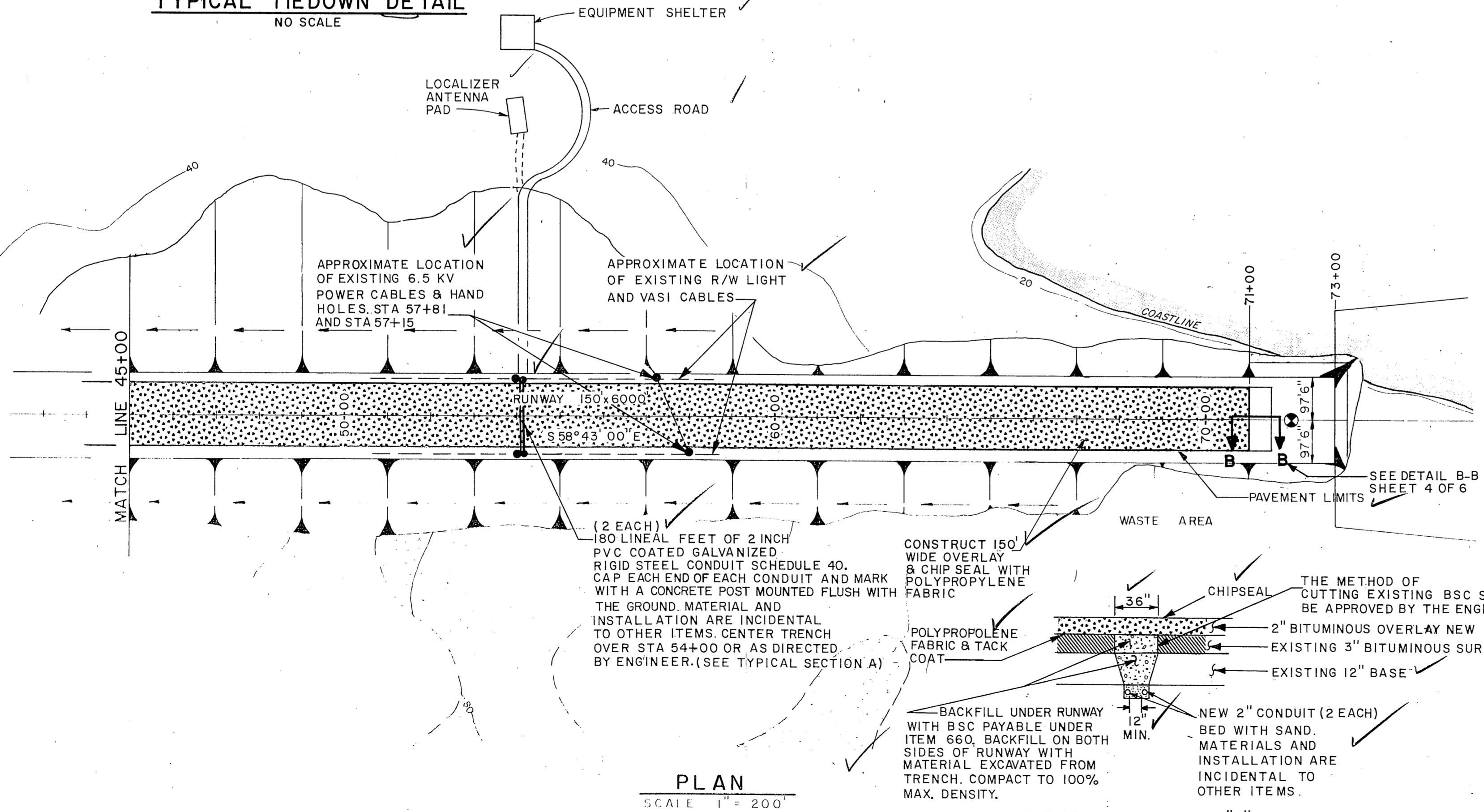


**GENERAL NOTE**  
 CONTRACTOR ACCESS IS LIMITED TO EXISTING TRAVELED WAYS, STAGING AREAS, SHOULDERS OF THE RUNWAY THE RUNWAY ITSELF AND THE MATERIAL SITE AS SHOWN IN THE SPECIAL PROVISIONS.

LEGEND	
MUSKEG	[Symbol]
CONTRACTORS STAGING AREA	[Symbol]
CONTOURS	[Symbol]
VASI PADS (24' x 55') (EXISTING)	[Symbol]
REIL PADS (24' x 20') (EXISTING)	[Symbol]
EXISTING BUILDINGS	[Symbol]
BITUMINOUS OVERLAY & CHIP SEAL	[Symbol]
SURVEY MONUMENTS ARE SET ON R/W STA 10+00 AND ELEV. 34.25' AND R/W STA 22+00 AND ELEV. 40.95' AND R/W STA. 72+00 AND ELEV. 51.94'	

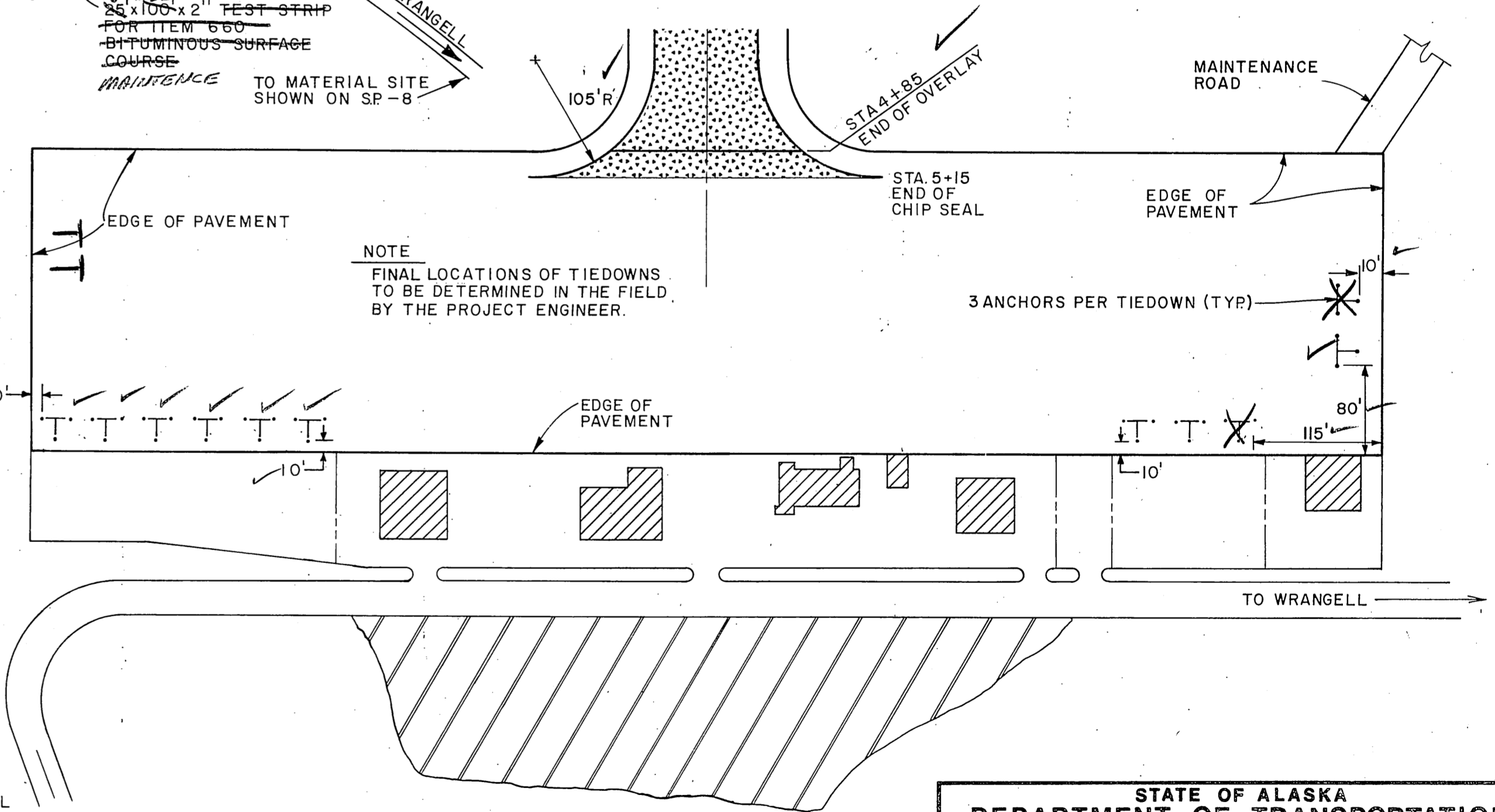


**TYPICAL TIEDOWN DETAIL**  
NO SCALE



**PLAN**  
SCALE 1"=200'

**TYPICAL SECTION "A"**  
RIGID CONDUIT TRENCH  
NO SCALE



**APRON DETAIL**  
1"=100'

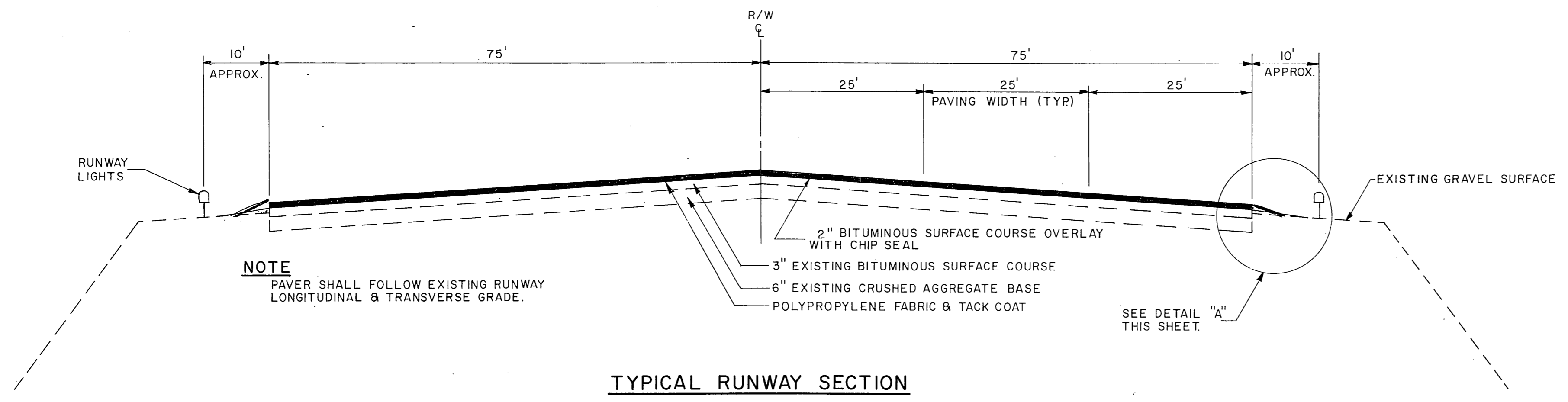


BY	DATE	CHANGE

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 DIVISION OF AVIATION-DESIGN AND CONSTRUCTION

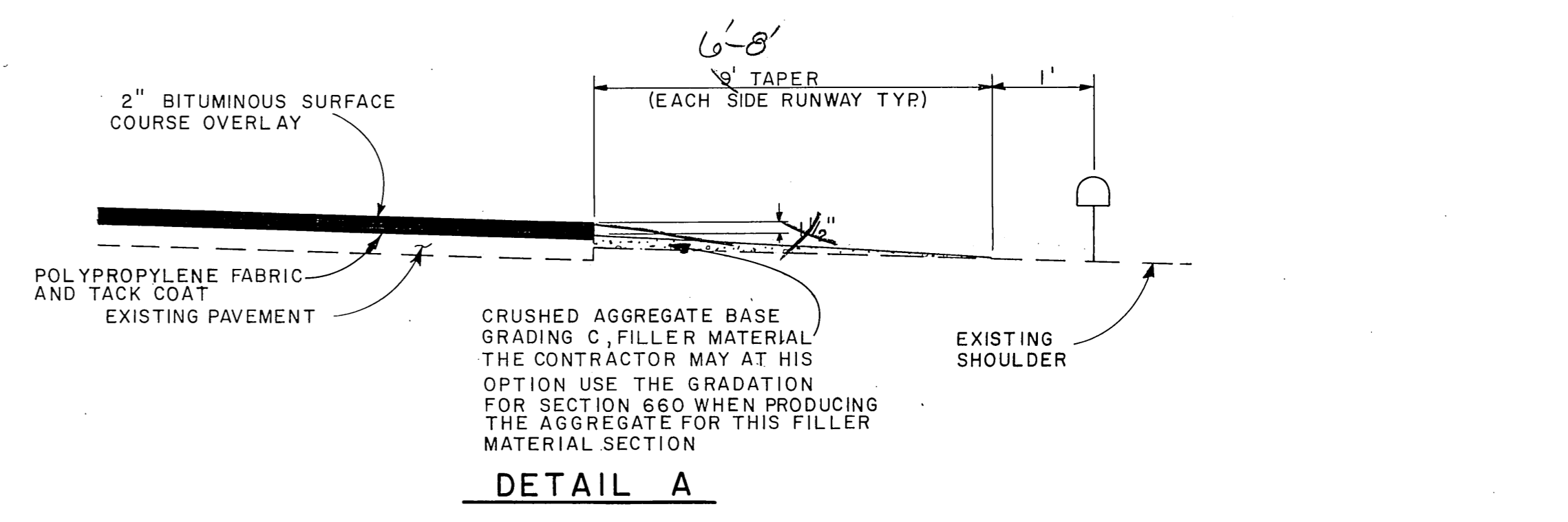
WRANGELL AIRPORT  
 A.D.A.P. No. 6-02-0323-06  
 PROJECT LAYOUT PLAN

APPROVED	<i>William A. Allison</i>	CHIEF DESIGN ENGINEER
APPROVED	<i>William Koester</i>	SOUTHEASTERN REGION ENGINEER
SCALE	AS SHOWN	DESIGNED BY
CHECKED	<i>[Signature]</i>	DATE
		10-14-81

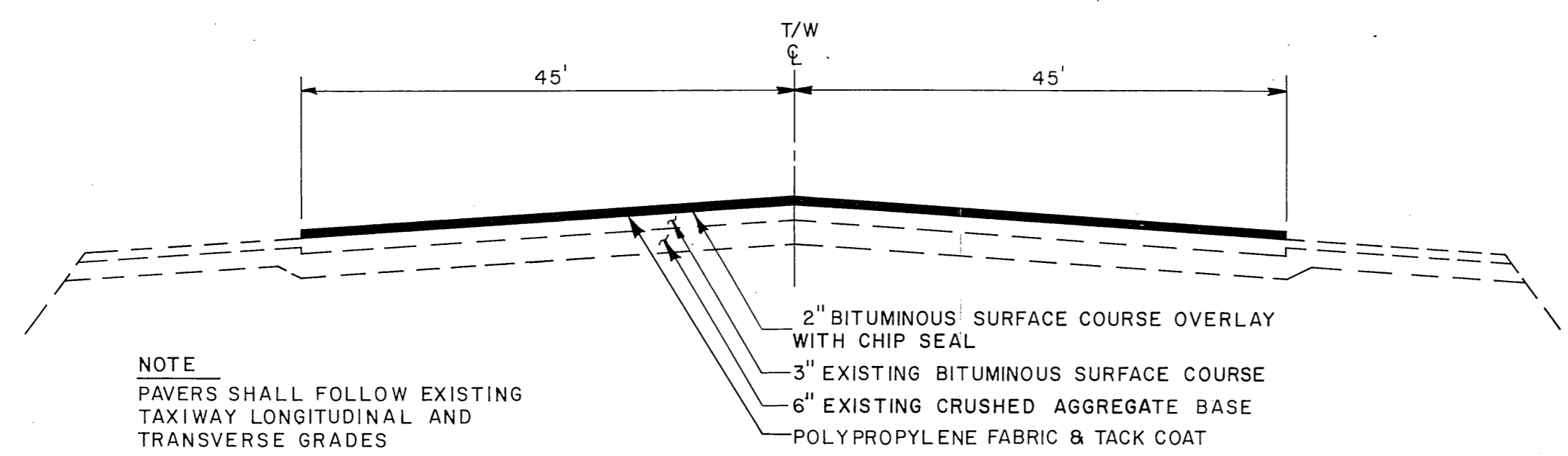


**NOTE**  
PAVER SHALL FOLLOW EXISTING RUNWAY LONGITUDINAL & TRANSVERSE GRADE.

**TYPICAL RUNWAY SECTION**

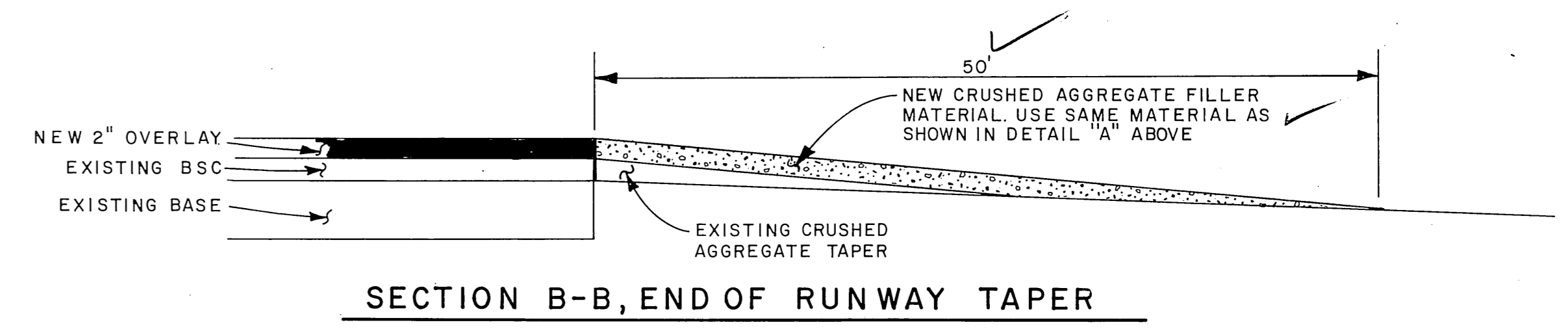


**DETAIL A**

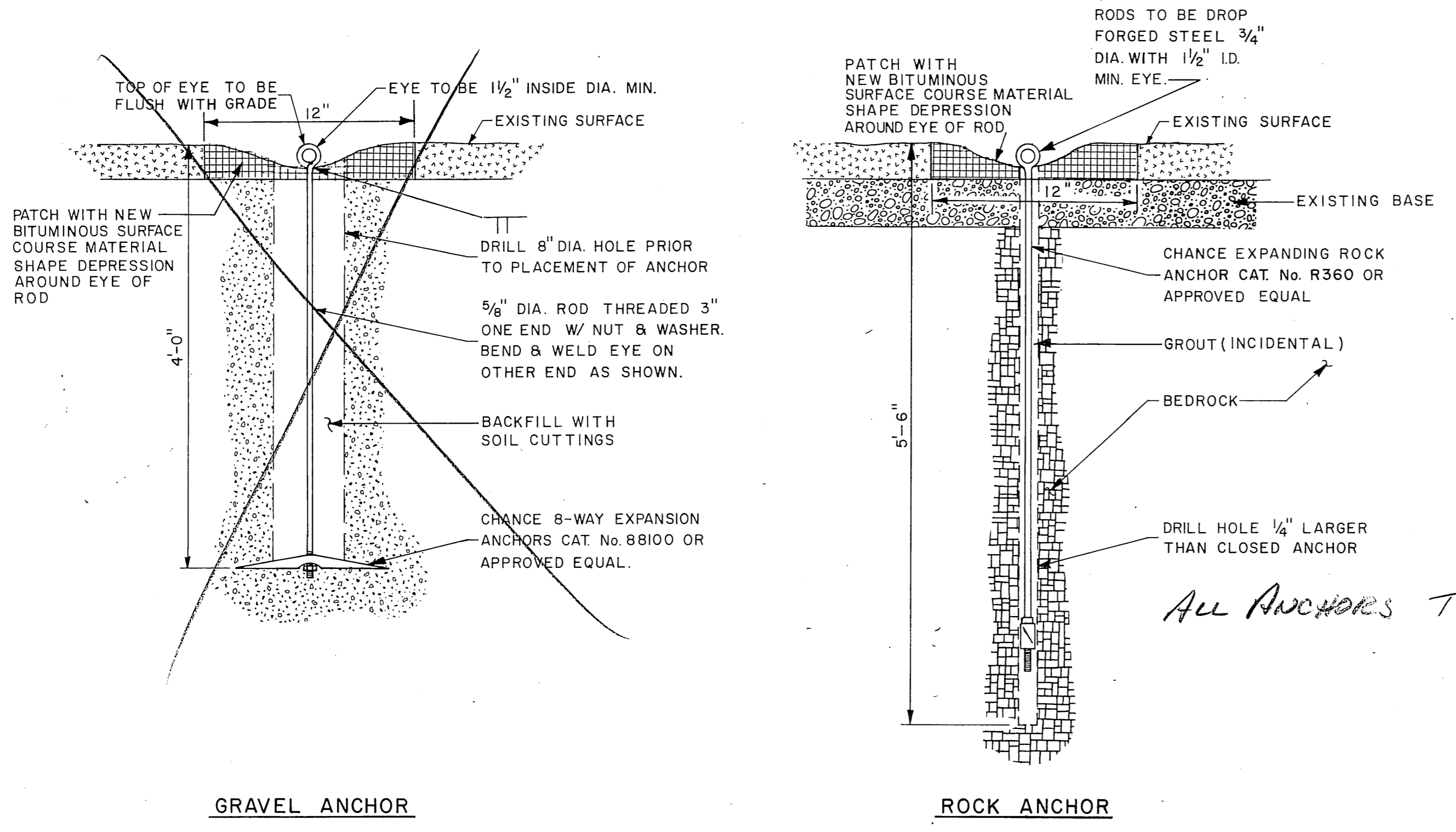


**NOTE**  
PAVERS SHALL FOLLOW EXISTING TAXIWAY LONGITUDINAL AND TRANSVERSE GRADES

**TYPICAL TAXIWAY SECTION**



**SECTION B-B, END OF RUNWAY TAPER**

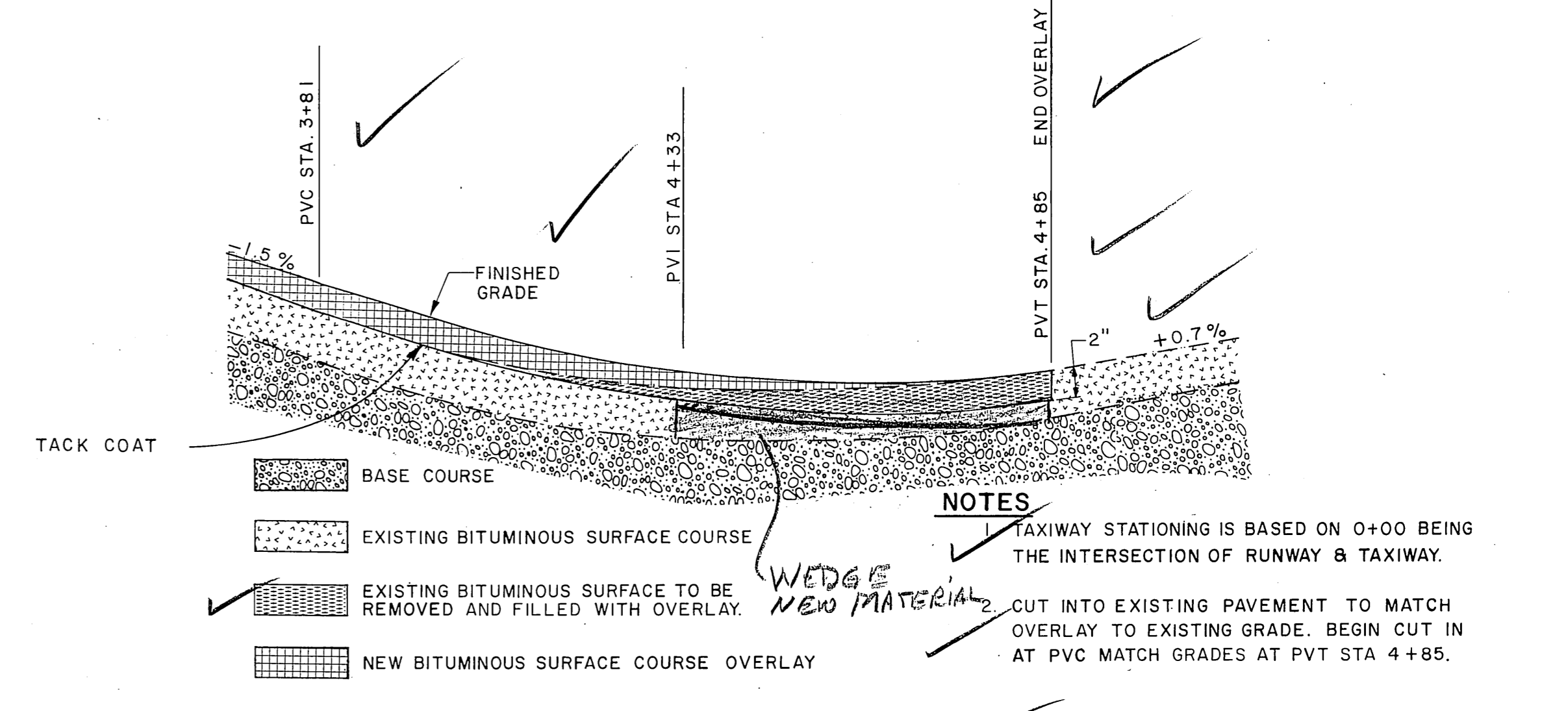


**GRAVEL ANCHOR**

**ROCK ANCHOR**

**TIE-DOWN ANCHOR DETAILS**

*ALL ANCHORS THIS STYLE*



**TAXIWAY TO APRON PAVEMENT MATCH DETAIL PROFILE VIEW**

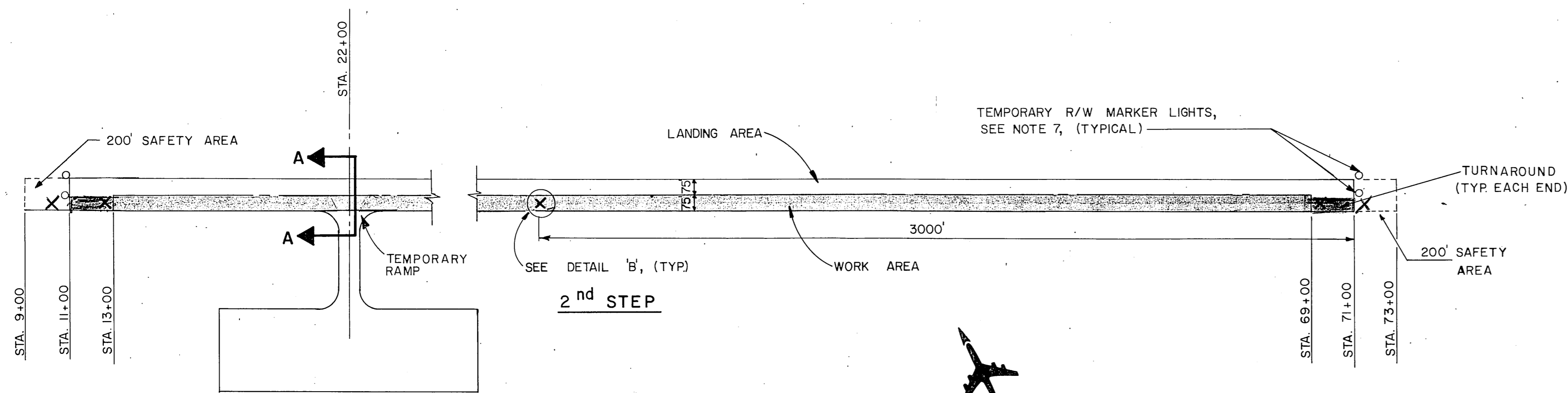
**NOTES**  
1. TAXIWAY STATIONING IS BASED ON 0+00 BEING THE INTERSECTION OF RUNWAY & TAXIWAY.  
2. CUT INTO EXISTING PAVEMENT TO MATCH OVERLAY TO EXISTING GRADE. BEGIN CUT IN AT PVC MATCH GRADES AT PVT STA 4+85.  
3. REMOVAL OF EXISTING BITUMINOUS SURFACE COURSE IS INCIDENTAL.  
4. CONTRACTOR MAY AT HIS OPTION REMOVE EXISTING BITUMINOUS SURFACE COURSE IN THE VERTICAL CURVE DOWN TO THE BASE COURSE. THE BASE SHALL BE PRIMED AND BUILT TO FINAL GRADE WITH BSC. ONLY THE FINAL 2\"/>

5. THE PVT OF THE VERTICAL CURVE IS THE CONTROLLING POINT OF THE VERTICAL CURVE LAYOUT DO TO DRAINAGE CONSIDERATIONS FROM THE APRON. AS-BUILT DATA INDICATES THE NEW PVI SHOULD OCCUR AT APPROX. STA 4+33. CONTRACTOR SHALL FIELD VERIFY LOCAL CONDITIONS AND ADJUST PVI & PVC AS REQUIRED.

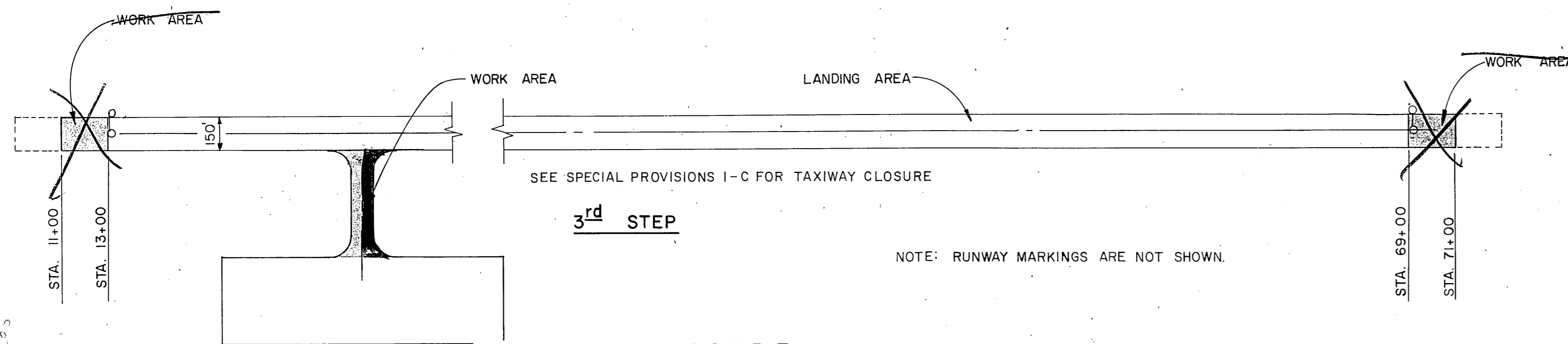
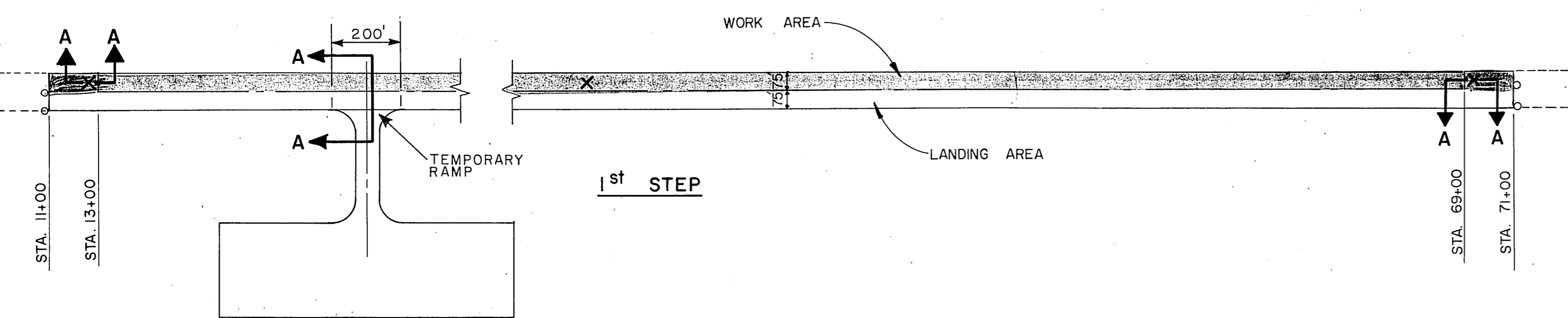


STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
DIVISION OF AVIATION-DESIGN AND CONSTRUCTION  
WRANGELL AIRPORT  
A.D.A.P. No. 6-02-0323-06  
TYPICAL SECTIONS & DETAILS

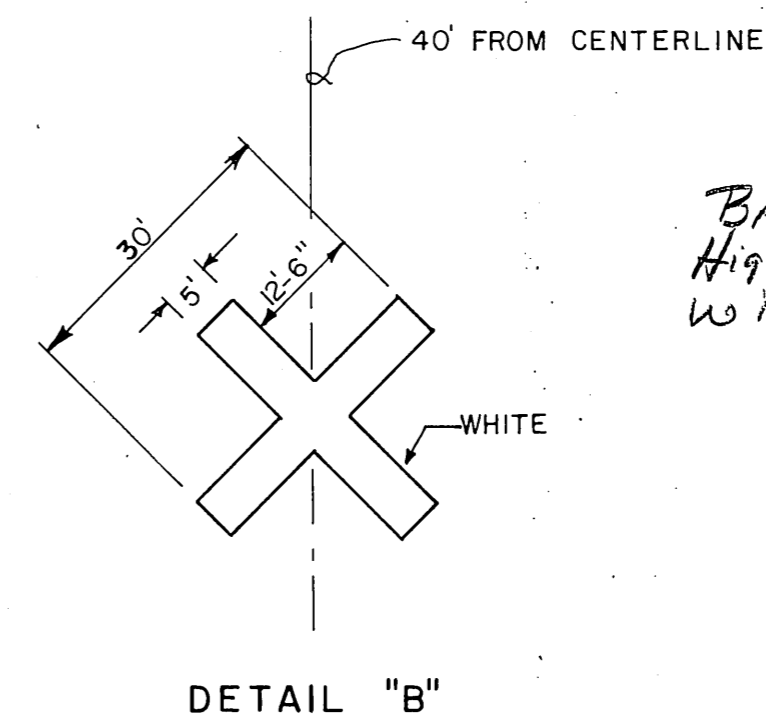
APPROVED	<i>William A. Allison</i>	WILLIAM A. ALLISON, P.E.	CHIEF DESIGN ENGINEER
APPROVED	<i>William Koester</i>	WILLIAM KOESTER, P.E.	SOUTHEASTERN REGION ENGINEER
SCALE	DESIGNED	DRAWN	SHEET 4 OF 6
NO SCALE	CHECKED	DATE 10-14-81	
BY	DATE	CHANGE	REVISIONS



Removed Threshold Lights  
AND TURNED JETS AROUND  
ON 200' SAFETY AREA



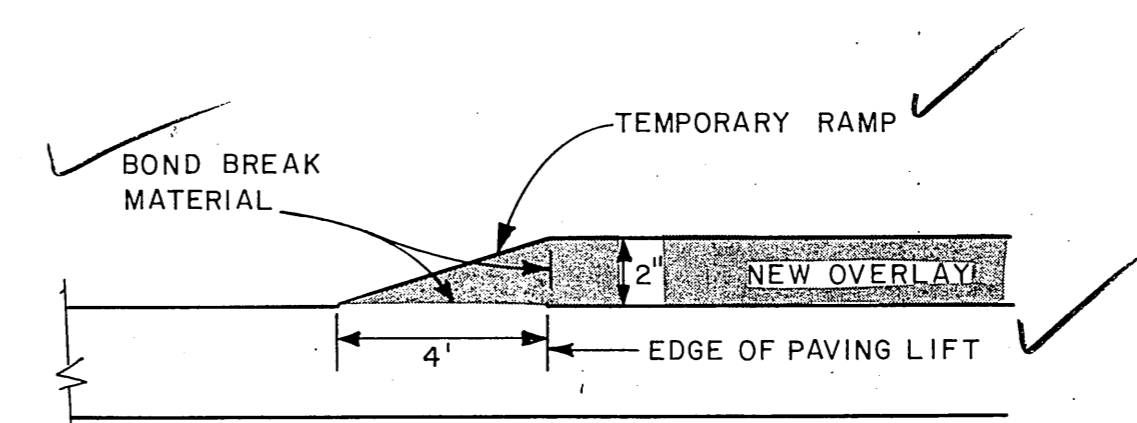
RUNWAY CLOSURE  
DID ONE HALF AT TIME



BARRICADES too  
High - INTERFERED  
with WINGS ON JET

NOTES

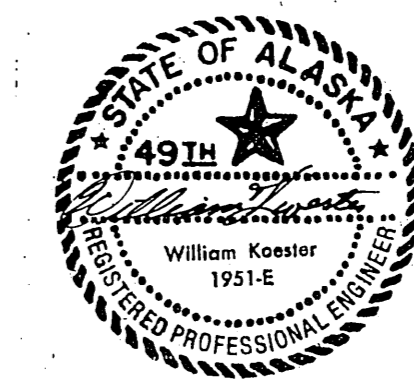
1. RUNWAY NUMBERS SHALL BE REMOVED IMMEDIATELY UPON CLOSURE. ALL OTHER MARKINGS SHALL BE RETAINED AS LONG AS POSSIBLE.
2. THE TYPE II BARRICADES SHALL BE CENTERED ON THE RUNWAY, EXCEPT WHEN THIS LOCATION INTERFERES WITH THE WORK.
3. THE "X" MARKING (DETAIL B) SHALL BE MADE OF A SUITABLE MATERIAL TO RESIST WIND, RAIN AND JET BLAST DAMAGE. PAINT SHALL NOT BE ALLOWED ON NEW A. C. PAVEMENT OR SLURRY SEAL.
4. THE CONTRACTOR SHALL BE ALLOWED TO CHIP SEAL AFTER ALL ASPHALT PAVEMENT IS PLACED. THE CLOSURE SEQUENCE SHALL THEN BE REPEATED FOR CHIP SEAL OPERATIONS.
5. THE CONTRACTOR SHALL NOT OPEN ANY PORTION TO TRAFFIC UNTIL THE CHIP SEAL HAS CURED FOR AT LEAST 24 HOURS OR AS DIRECTED BY THE ENGINEER. *4 days*
6. THE CONTRACTOR SHALL LIMIT ALL VEHICLE OPERATIONS ON THE RUNWAY TO ONLY THAT SIDE OF CENTERLINE BEING WORKED ON. VEHICLE OPERATIONS SHALL BE RESTRICTED TO THE GRAVEL SHOULDERS, OUTSIDE OF THE RUNWAY LIGHTS.
7. TEMPORARY RUNWAY END LIGHTS SHALL BE PLACED AS SPECIFIED IN THE SPECIAL PROVISIONS.



SECTION A-A TEMPORARY RAMP  
FOR HEAVY AIRCRAFT ACCESS ON TO OVERLAY

NOTE

1. THE TEMPORARY RAMP SHALL BE PLACED TO ALLOW AIRCRAFT ACCESS ON TO THE NEWLY PLACED OVERLAY PRIOR TO OPENING THE SECTION FOR TRAFFIC. MATERIAL SHALL BE MEASURED FOR PAYMENT UNDER SECTION 660.
2. THE TEMPORARY RAMP SHALL BE REMOVED PRIOR TO PLACEMENT OF THE ADJOINING OVERLAY. THE REMOVAL IS INCIDENTAL.



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WRANGELL AIRPORT  
A. D. A. P. No. 6-02-0323-06  
AIRPORT CLOSURE AND  
CONSTRUCTION MARKING

APPROVED *William A. Allison*  
WILLIAM A. ALLISON, P.E. CHIEF DESIGN ENGINEER  
APPROVED *William Koester*  
WILLIAM KOESTER, P.E. SOUTHEASTERN REGION ENGINEER

BY	DATE	CHANGE
REVISIONS		

SCALE AS SHOWN  
DESIGNED *AMD*  
DRAWN *AMD*  
CHECKED *al*  
DATE 10-14-81  
SHEET 5 OF 6

