

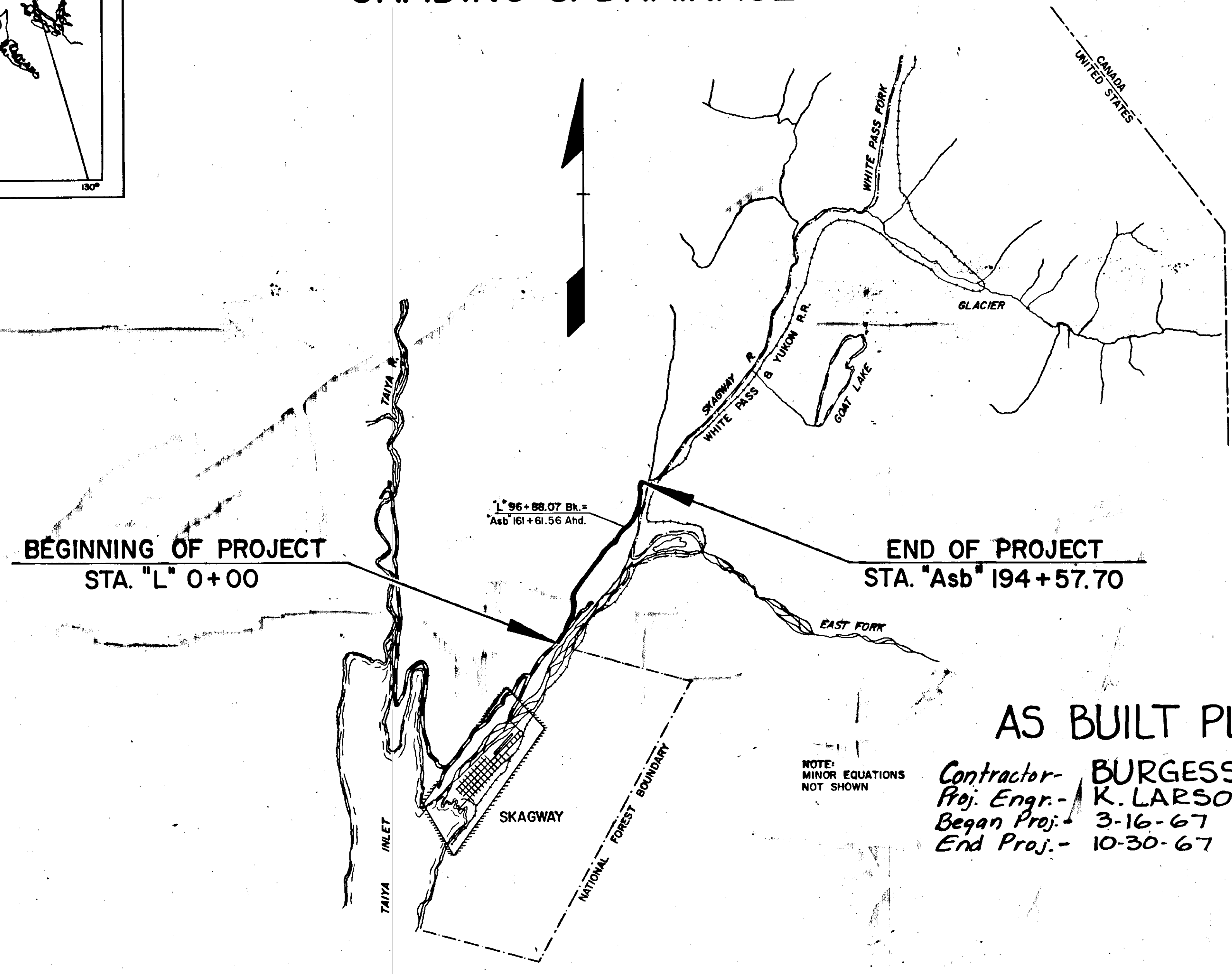
STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

PLAN AND PROFILE
PROPOSED HIGHWAY PROJECT
S-0999 (6)
SKAGWAY - CARCROSS HIGHWAY
GRADING & DRAINAGE

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	S-0999(6)	1966	1	22

INDEX OF SHEETS

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1	TITLE SHEET
2-3	TYPICAL SECTION
4	ESTIMATE OF QUANTITIES
5	TABLES SUMMARIES AND DETAILS
6	CULVERT SUMMARY
7	MATERIAL SITE
8-12	PLAN AND PROFILE
13-14	CULVERT DETAILS
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S-1	STANDARD CULVERTS
S-3	STANDARD APPROACHES
S-6	STANDARD PROJECT IDENTIFICATION SIGN
S-7	STANDARD CONCRETE RIGHT-OF-WAY MONUMENTS
S-8	STANDARD BEAM GUARDRAIL
S-13 MOD.	STANDARD CULVERT MARKER POST
S-19	STANDARD SUPERELEVATION



DESIGN DESIGNATION
ADT (1965)=20
ADT (1985)=40
DHV=8
D=35-65
T=5%
V=35 M.P.H.

PROJECT SUMMARY

WIDTH OF SUBGRADE	28'
LENGTH OF GRADING	12,974.30' = 2.457 Mi.
LENGTH OF PROJECT	12,974.30' = 2.457 Mi.

AS BUILT PLANS

Contractor- BURGESS CONST. CO.
Proj. Engr.- K. LARSON
Began Proj.- 3-16-67
End Proj.- 10-30-67

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

APPROVED
[Signature] Date 9/27/66
COMMISSIONER OF HIGHWAYS

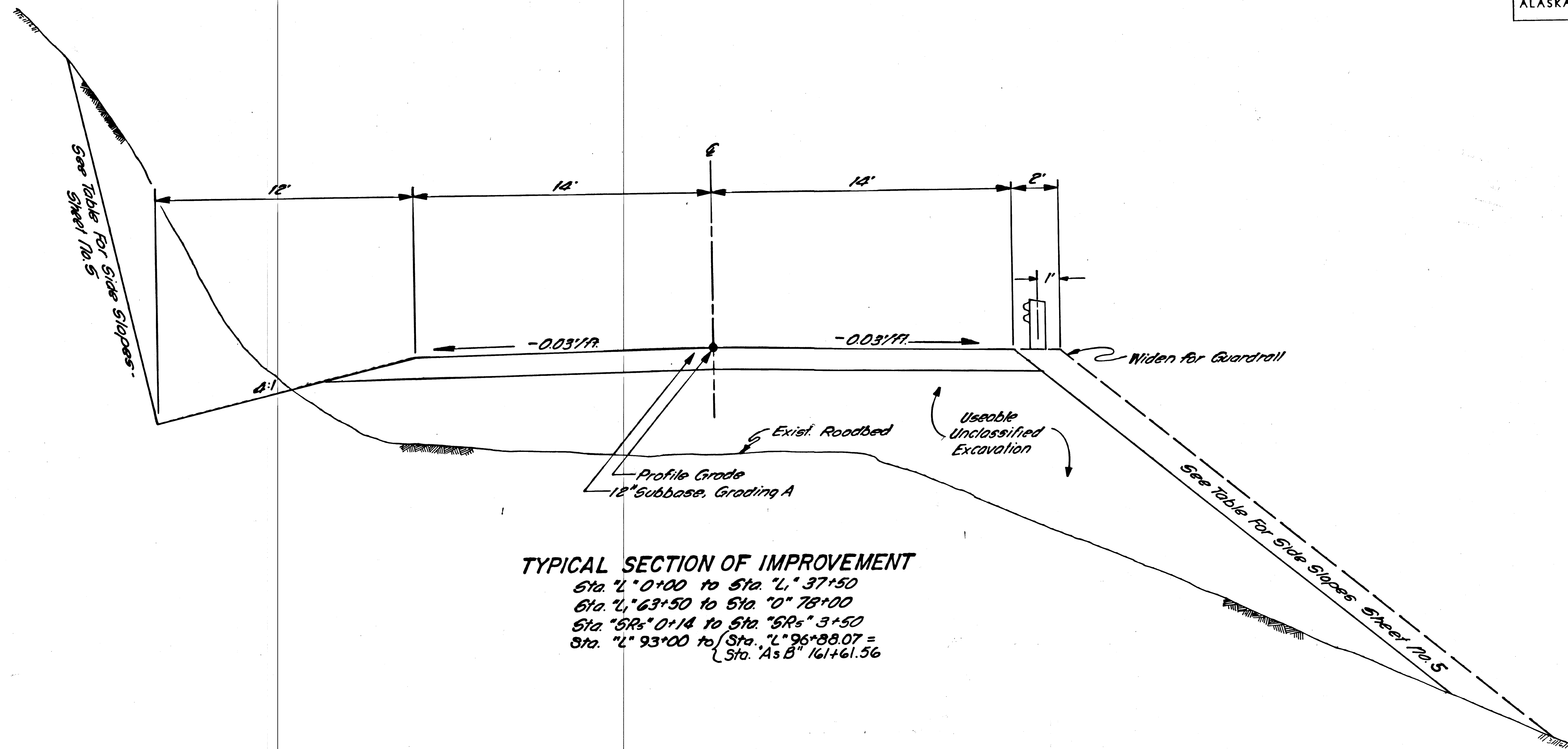
APPROVED
REGIONAL ENGINEER
BUREAU OF PUBLIC ROADS
REGION TEN

As Built entered 12-15-67 by Ken Ward

CONVENTIONAL SIGNS

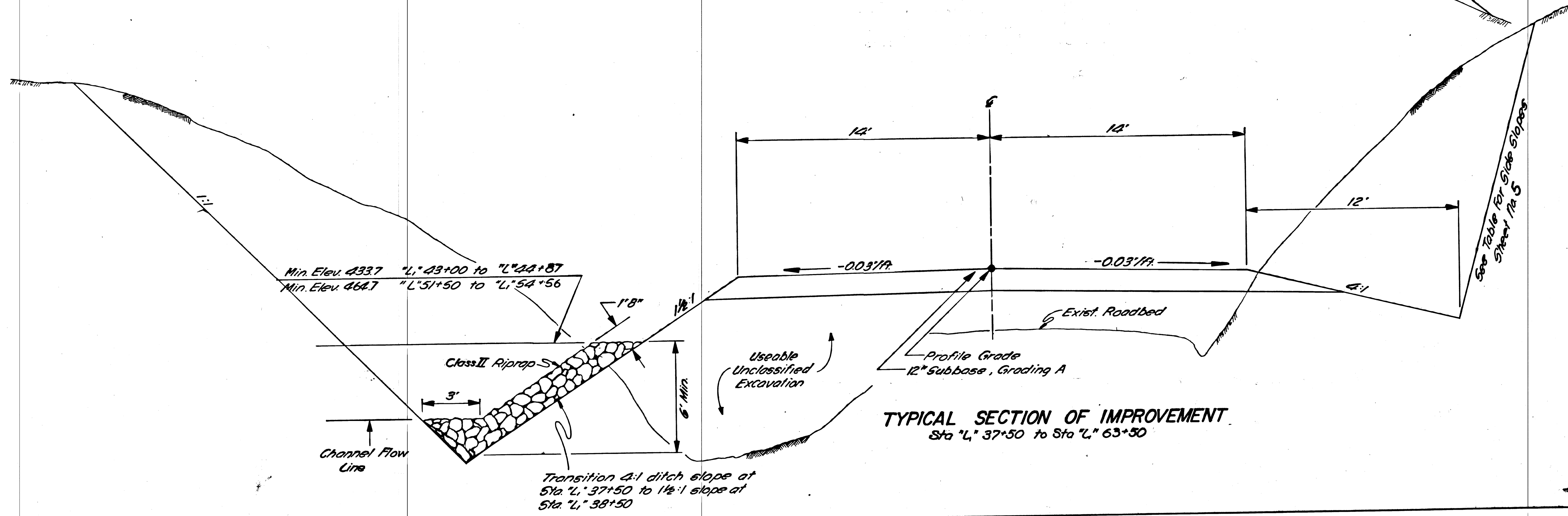
PROPOSED CONSTRUCTION CENTERLINE	
PRELIMINARY SURVEY LINE	
SLOPE LIMITS - CUT	
SLOPE LIMITS - FILL	
PROPERTY LINE	
RIGHT-OF-WAY LINE	
EASEMENT LINE	
CORROATED OR CITY LIMITS	
POWER LINE	
TELEPHONE OR TELEGRAPH LINE	
POLE ANCHOR	
LIGHT POLE	
WATER LINE	
SEWER LINE	
VALVE BOX	
CATCH BASIN	
DROP INLET	
MANHOLE	
CULVERT PROPOSED	
CULVERT EXISTING	
FIRE HYDRANT	
TRAVELED WAY	
SWAMP	
FENCE	
CURB CUT	
LAND MONUMENT	
RIGHT-OF-WAY MONUMENT	
RIPRAP	
OBLITERATE ROADWAY	
STANDARD SIGNS	

AS BUILT



TYPICAL SECTION OF IMPROVEMENT

- Sta. "L" 0+00 to Sta. "L" 37+50
- Sta. "L" 63+50 to Sta. "O" 78+00
- Sta. "SRs" 0+14 to Sta. "SRs" 3+50
- Sta. "L" 93+00 to Sta. "L" 96+88.07 = Sta. "As B" 161+61.56



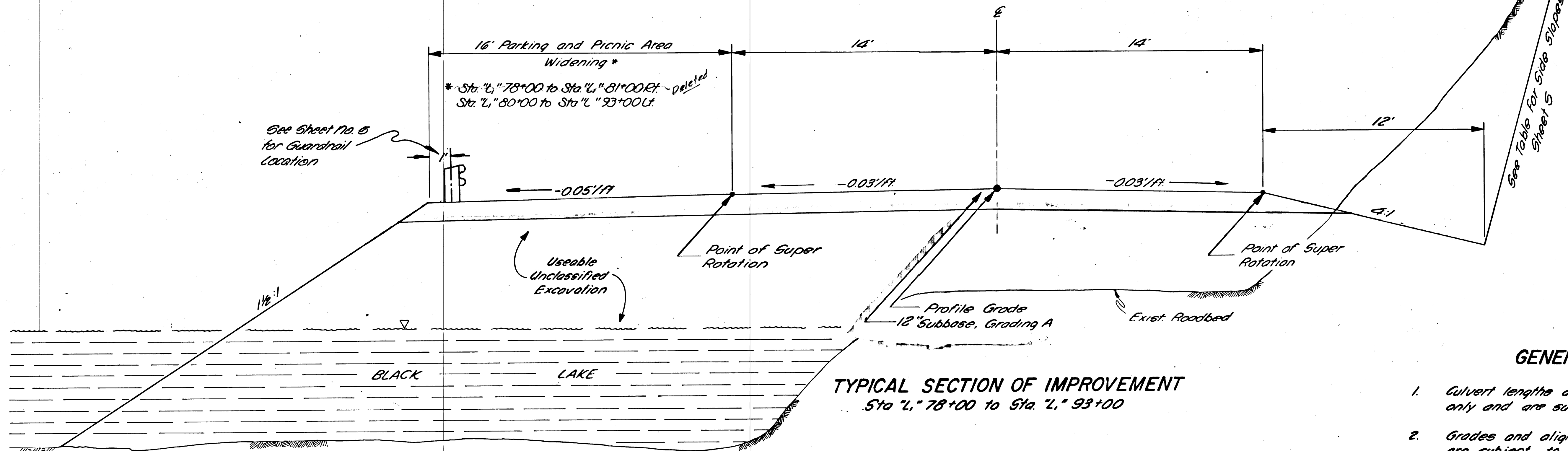
TYPICAL SECTION OF IMPROVEMENT

Sta. "L" 37+50 to Sta. "L" 63+50

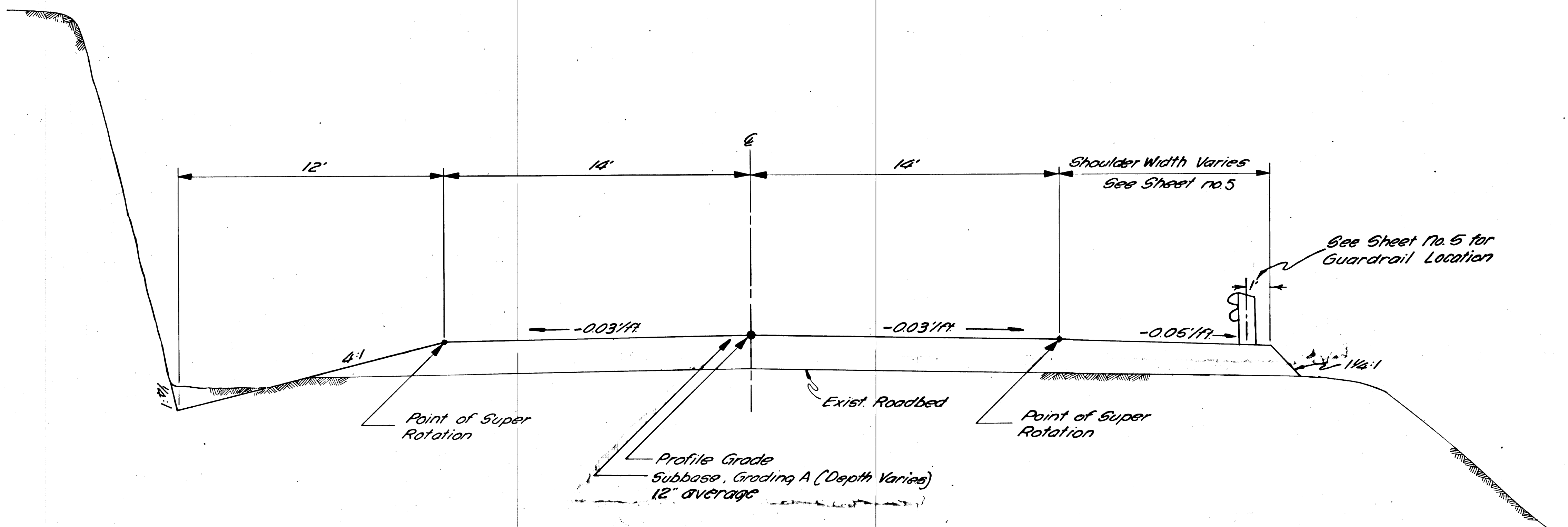
Transition 4:1 ditch slope at Sta. "L" 37+50 to 1 1/2:1 slope at Sta. "L" 38+50

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	5-0999(6)	1966	3	22

AS BUILT



TYPICAL SECTION OF IMPROVEMENT
Sta. "L" 78+00 to Sta. "L" 93+00



TYPICAL SECTION OF IMPROVEMENT
Sta. "L" 96+88.07 - }
Sta. "AsB" 161+61.66 } to Sta. "AsB" 194+57.70

GENERAL NOTES

1. Culvert lengths and locations are approximate only and are subject to minor revisions.
2. Grades and alignment shown on these plans are subject to minor revisions.
3. Miscellaneous and minor Right-of-Way encroachments, within the construction limits at the time of construction, such as fences, signs, abandoned foundations, etc., shall be removed by the contractor as directed by the engineer. No payment for this work shall be made as it is not a pay item in this contract. Such work will be considered incidental to other items of work performed under this contract.
4. All waste and/or surplus material encountered on this project will be disposed of by the contractor at locations of his own choice out of sight of the roadway and as approved by the engineer. There is no payment for overhaul of unclassified excavation designated as waste and/or surplus material by the engineer.
5. At the option of the contractor, corrugated galvanized metal pipe, corrugated aluminum pipe or reinforced concrete pipe may be furnished for pay item 608(26), pipe conduit.
6. The clearing limits shall be a neat orderly line approximately fifteen (15) feet beyond the slope limits, or to the Right-of-Way line, whichever is the lesser of the two. The cleared areas of broken talus rock and of the existing road will not be measured for payment as clearing and grubbing.

ESTIMATE OF QUANTITIES

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	S-0999(6)	1966	4	22

EM NO.	ITEM	UNIT	SHEET NUMBER												AS BUILT	CONTRACT QUANTITIES			
			8	9	10	11	12												
3)	Clearing & Grubbing	Acre	1.41	4.70	4.57	0.71	0											12,275	41,39
4)	Removal & Disposal of Culvert Pipe	Lin. Ft.	62	52	73	58												251	245
3)	Unclassified Excavation	Cu. Yd.	42295		79643		4395											122,322	126,333
1)	Overhaul	Yd. Mi.	398		3447		57											14,301	3,097
1)	Structure Excavation	Cu. Yd.	620	576	656	336												996.4	2,188
1)	Subbase, grading A	Cu. Yd.	4269		6976		3960											15,428	15,205
3)	Class W Concrete	Cu. Yd.		4.30	4.30													8.62	8.60
(26E)	18" Pipe Conduit	Lin. Ft.	36															36	36
(26G)	24" Pipe Conduit	Lin. Ft.	306		372	22												564	700
(26K)	48" Pipe Conduit	Lin. Ft.		48		216												190	264
(26L)	54" Pipe Conduit	Lin. Ft.		184	96													276	280
(26M)	60" Pipe Conduit	Lin. Ft.	98															99	98
(26P)	78" Pipe Conduit	Lin. Ft.		86	84													171	170
(1)	Beam Type Guardrail, type I Post	Lin. Ft.	200	637.5		600	2,175											4,437.5	4612.5
(1)	Riprap, Class II	Cu. Yd.	74		2096		0											2,022.9	2,170
(1)	Right of Way Monuments	Each	13	14	9	9												42	45
(2)	Culvert Marker Posts	Each	8		10	2												18	20
(1)	Standard Signs	Each	1	2														3	3

TABLE OF SIDESLOPES

STATION	EMBANKMENT	EXCAVATION
"L" 0+00		
"L" 15+00	1 1/2:1	1/4:1
"L" 27+40	1 1/2:1	1/4:1
"L" 29+00	1 1/2:1	1:1
"L" 30+00	1 1/2:1	1/4:1
"L" 32+60	1 1/2:1	1 1/2:1
"L" 64+50	1 1/2:1	1 1/2:1
"L" 69+95	1 1/2:1	1/4:1
"L" 85+00	1 1/2:1	1 1/2:1
"AsB" 161+61.56	1 1/2:1	1/4:1
"AsB" 194+57.70	1 1/2:1	1 1/2:1
"BRs" 0+14		
"BRs" 3+50	1 1/2:1	1 1/2:1

SHOULDER WIDTHS
STA "AsB" 161+61.56 to "AsB" 194+57.70

STATION	WIDTH ① (RIGHT SIDE)
"AsB" 161+61.56	0
"AsB" 170+63.5	Transition
"AsB" 172+63.5	2'
"AsB" 192+42.0	Transition
"AsB" 193+00	16'
"AsB" 194+00	Transition
"AsB" 194+57.7	2'

GUARDRAIL TABLE

STATION	LEFT RIGHT	LINEAR FEET ②
"L" 4+42.65		
0.769	RIGHT	112.5 100.0
"SRs" 0+64.41		
10+91	LEFT	600.0 100.0
"L" 5+96.60	RIGHT	
21+35		
"L" 21+87.4		
33+07	RIGHT	112.5 225.0
"L" 24+13.28		
"L" 30+71.83		
	RIGHT	225.0
"L" 32+95.68		
38+07.9		
"L" 37+49.56		
	LEFT	125.0 107.50
"L" 39+32.92		
"L" 78+00		
	RIGHT	300.0
"L" 81+00		
87+45		
"L" 90+00		
92+50	LEFT	111.9 900.0
"L" 93+00		
173+03		
"AsB" 172+63.5		1937.5
192+31.4	RIGHT	1987.5
"AsB" 192+39.1		
192+34.3		
"AsB" 192+42.0		
194+50	RIGHT	187.5
"AsB" 194+57.7		4,437.5
TOTAL GUARDRAIL		4612.5

AS BUILT

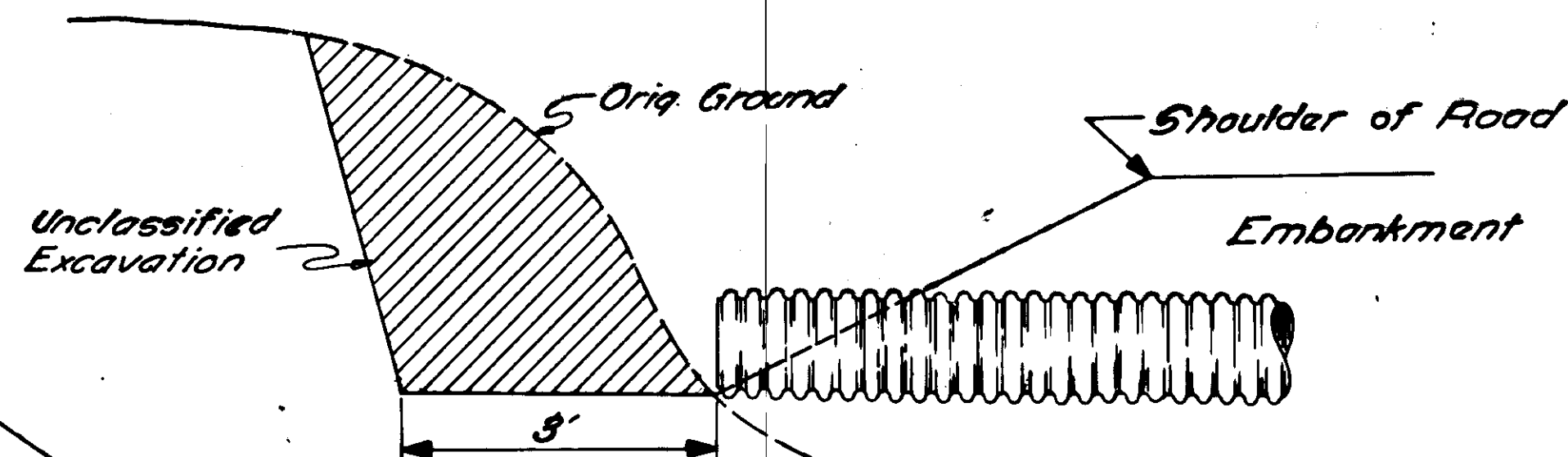
STANDARD CONCRETE
RIGHT-OF-WAY MONUMENTS

Station	Dist. from Int. Left	Dist. from Int. Right	No.	Remarks
"L" 0+00	100	40	2	B.O.P
"L" 1+98.61	100	40	2	P.C.
"L" 4+25		40	1	R/W 1/2 PT.
"L" 5+50		150	1	R/W 3/4 PT.
"L" 5+98.61	100		1	P.C.
"L" 8+00		150	1	Offset Rt. R/W
"L" 8+00	100	100	1	Offset Rt. R/W
"L" 12+35.30	100	100	2	P.C.
"L" 15+17.80	100	100	2	P.T.
"L" 19+83.28 Bk. = "L" 19+83.28 And.	100	100	2	P.C.
"L" 24+13.28	100	100	2	P.T.
"L" 30+71.83		100	1	P.C.
"L" 33+96.66	100	100	2	P.T.
"L" 36+93.90	100	100	2	P.C.
"L" 39+32.92	100		1	P.T.
"L" 41+96.44		100	1	P.C.
"L" 43+05.09 Bk. = "L" 43+12.37 And.		100	2	P.T.
"L" 46+50	100		1	P.C.
"L" 47+50	100		1	P.T.
"L" 52+76.75 Bk. = "L" 52+76.75 And.	100	100	2	P.C.
"L" 55+31.82	100	100	2	P.T.
"L" 61+68.35		100	1	P.C.
"L" 68+21.23	100	100	2	P.C.
"L" 70+74.56	100	100	2	P.T.
"L" 77+51.66	100		1	P.C.
"L" 79+93.05	100	100	2	P.T.
"L" 86+04.58		100	1	P.C.
"L" 94+60.15	100	100	2	P.C.
"L" 94+60.15		150	1	Offset Rt. R/W
"L" 94+93.30	100		1	Meet Exist. R/W
"L" 96+88.07 Bk. = "AsB" 161+61.56 And.		100	1	P.T.
Total			45	42

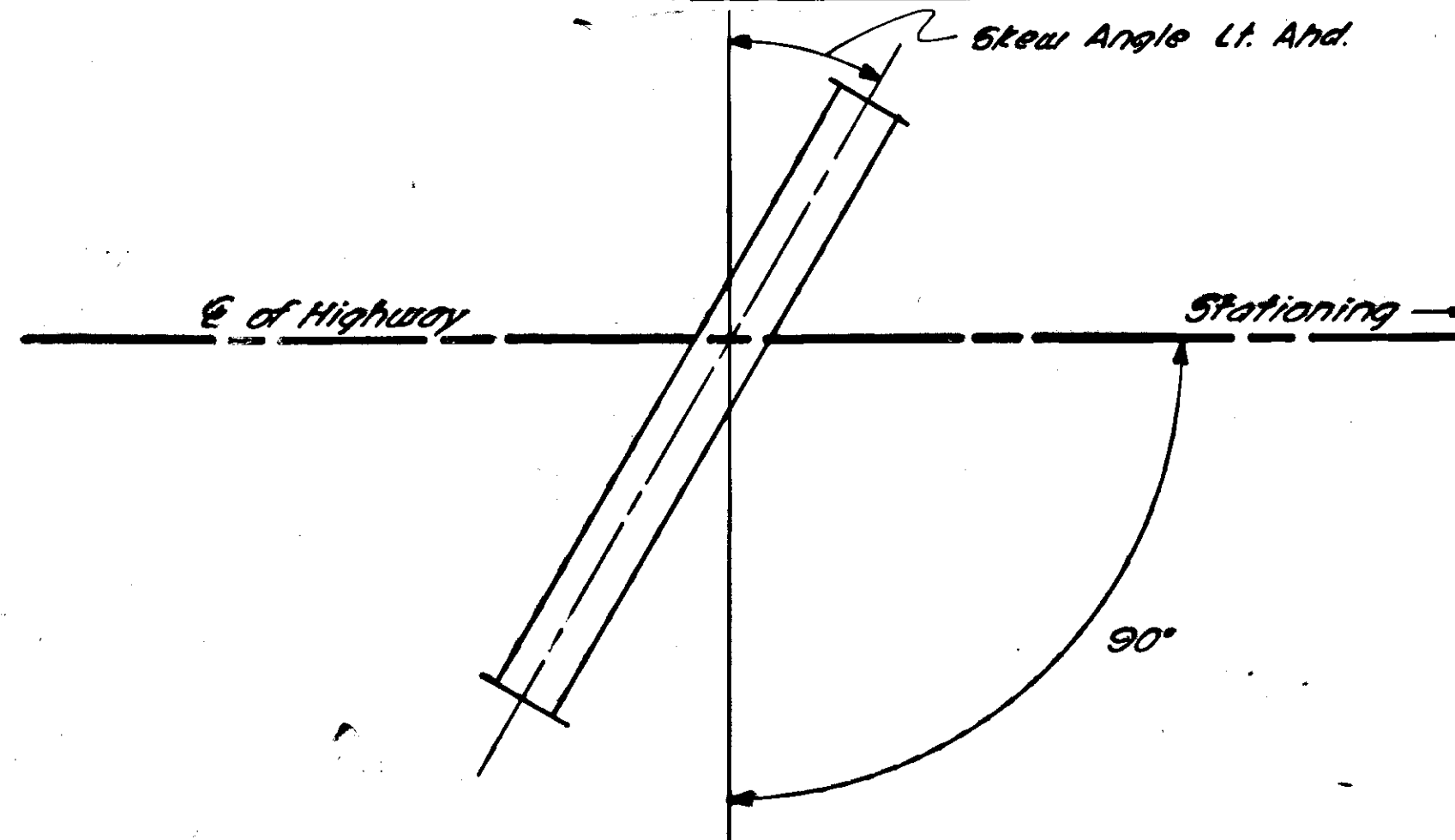
STANDARD SIGN
SUMMARY

STATION	OFFSET	TYPE
"L" 5+28	25' Rt.	RI-1
"L" 34+00	18' Rt.	WI-2L
"L" 43+00	18' Lt.	WI-2L

- ① Width includes widening for guardrail.
- ② No Standard Terminal Sections shall be provided at each installation but will not be measured for payment.
- ③ See Detail on Sheet no. 12.
- ④ See Extra Work Order #3 For Revised Guardrail Design



CULVERT INLET DETAILS



CULVERT SKEW DETAIL

CULVERT SUMMARY

AS BUILT

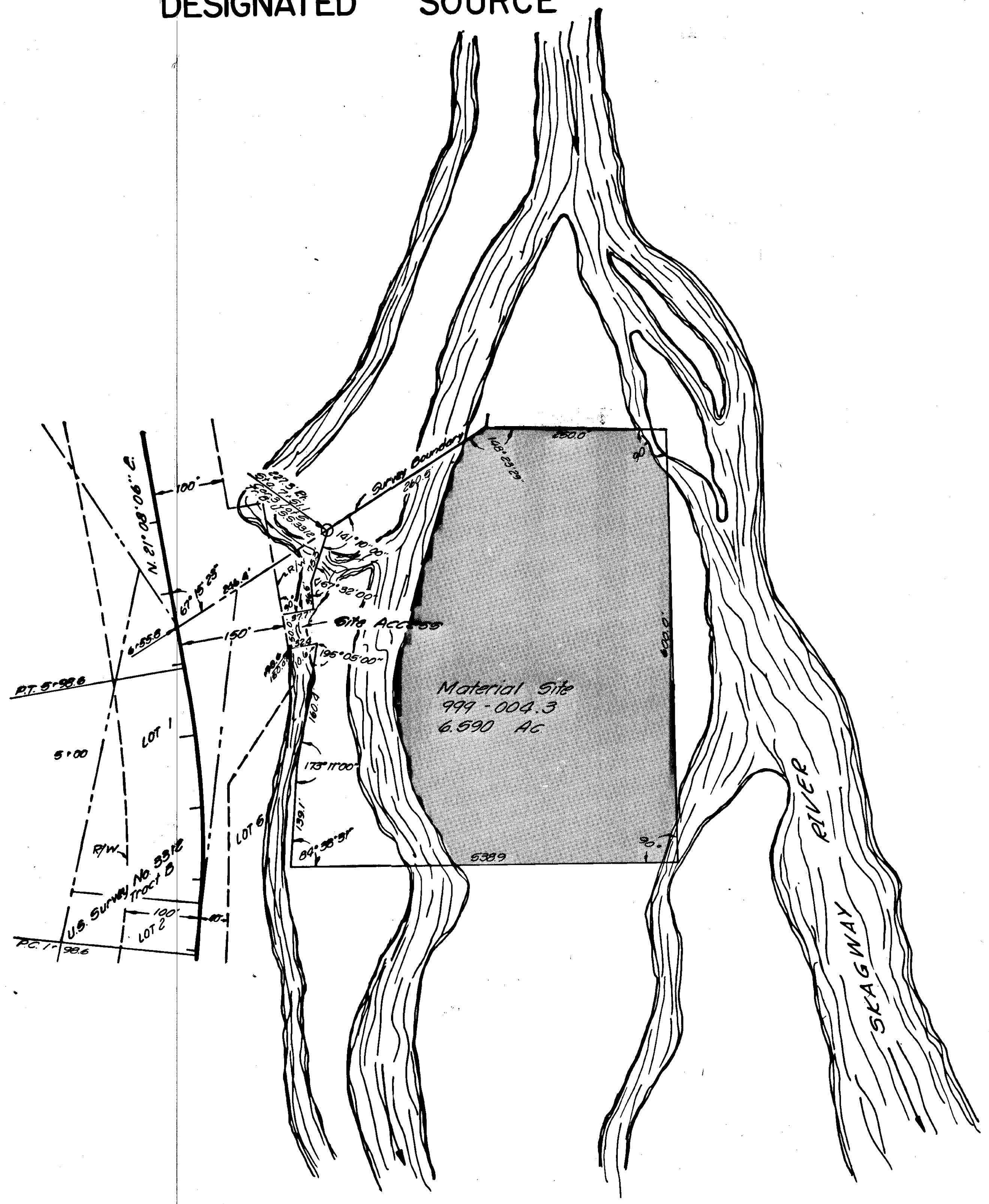
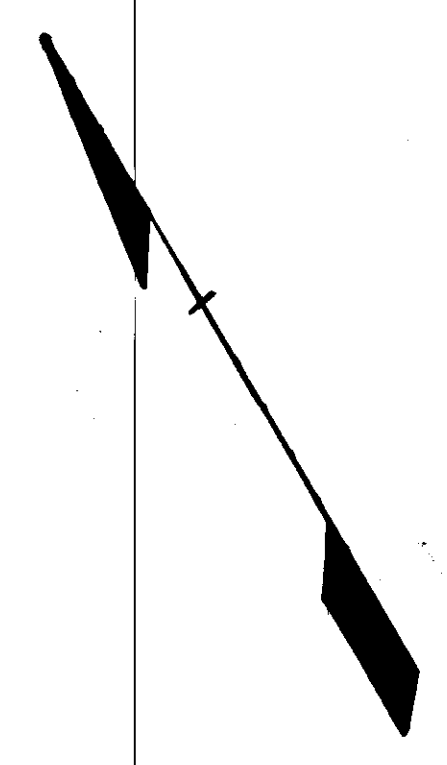
Station	202(4)	206(7)	601(3)	603(26)	603(26)	603(26)	603(26)	603(26)	603(26)	613(2)	Culvert Marker Posts	Design Cover Height	Remarks	Station	Remarks
	L.F.	C.Y.	C.Y.	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	Each					
0+00		11.44 33			52						2	2			
0+88		6.29 19		36								1	Install 26' Rt. of E		
35+1+75		17.2 47			58						2	10			
4+80 4+85		16.2 43			66						2	13			
10+50	30														
15+46	32	186.76 295						98				22	See Detail on Sheet 13		
18+00		12.43 183			130						2	18	Skew 45° Lt. Ahd.		
27+54 32+41	24	219.2 297	4.30				184					20	See Detail on Sheet 13		
42+75		14.24 217										86	86	6	Skew 36° Rt. Ahd.
46+00					48							3	Outlet at Top of Riprap		
47+00	28														
51+25		35.49 180										84	84	5	Skew 37° Lt. Ahd.
53+00	20														
60+25 60+50		11.5 52			74						2	11	Skew 30° Rt. Ahd.		
62+00 61+55					114						2	16	Skew 50° Rt. Ahd.		
63+25	30														
63+70 63+75		38.71 65			78						2	11	30° Skew 33° Rt. Ahd.		
67+75	23	166.36 212	4.30				96					5	See Detail on Sheet 14		
69+65 69+50		20.93 31			54						2	7			
74+50		33			52						2	6			
80+34 80+58	22	40.52 183			122							10	24° Skew 40° Lt. Ahd.		
92+85	36	22.38 137			94							6	Skew 45° Rt. Ahd.		
94+94		5.71 16			22						2	5	Extend Existing 24"x48" C.M.P.		
Totals		245	2188	8.60	36	700	264	280	98	170		20			

STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	5-0999161	1966	7	22

AS BUILT

MATERIAL SITE LOCATION

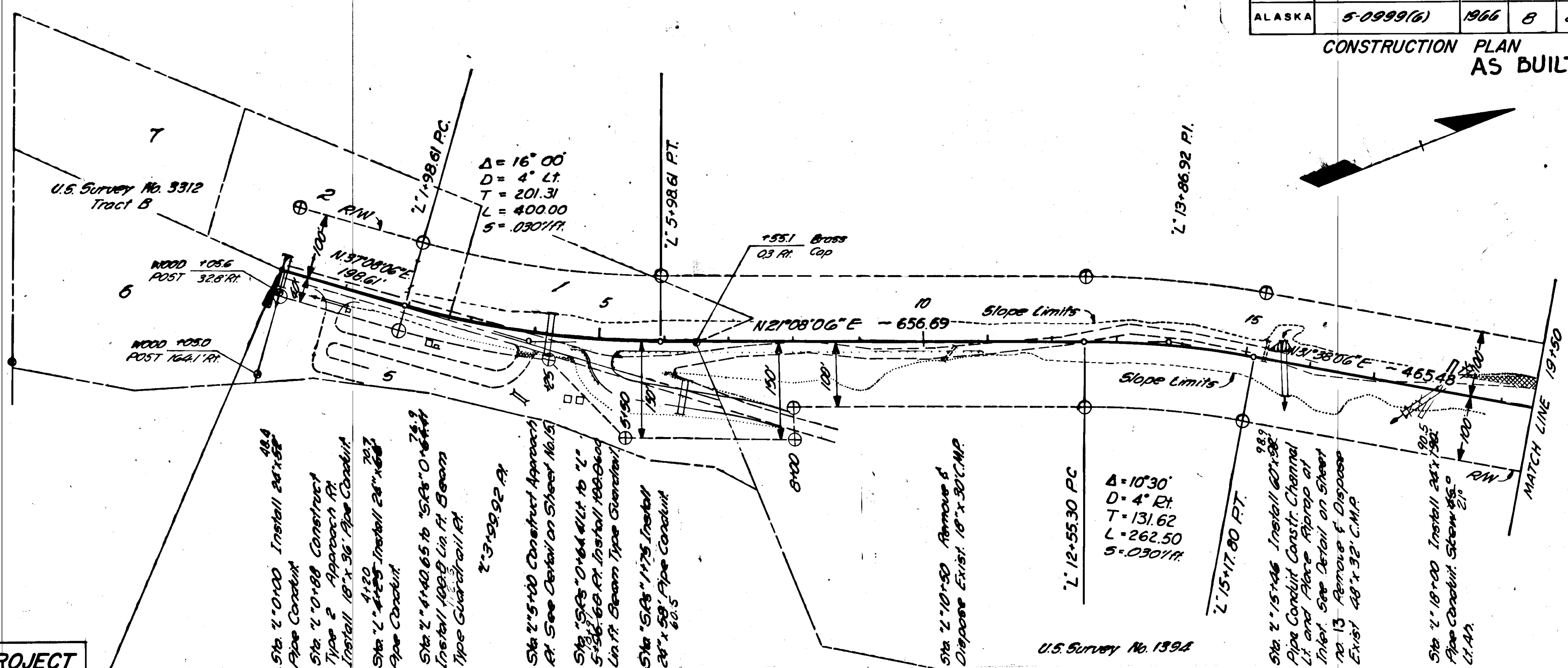
DESIGNATED SOURCE



Available Working Area

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	S-0999(6)	1966	8	22

CONSTRUCTION PLAN
AS BUILT

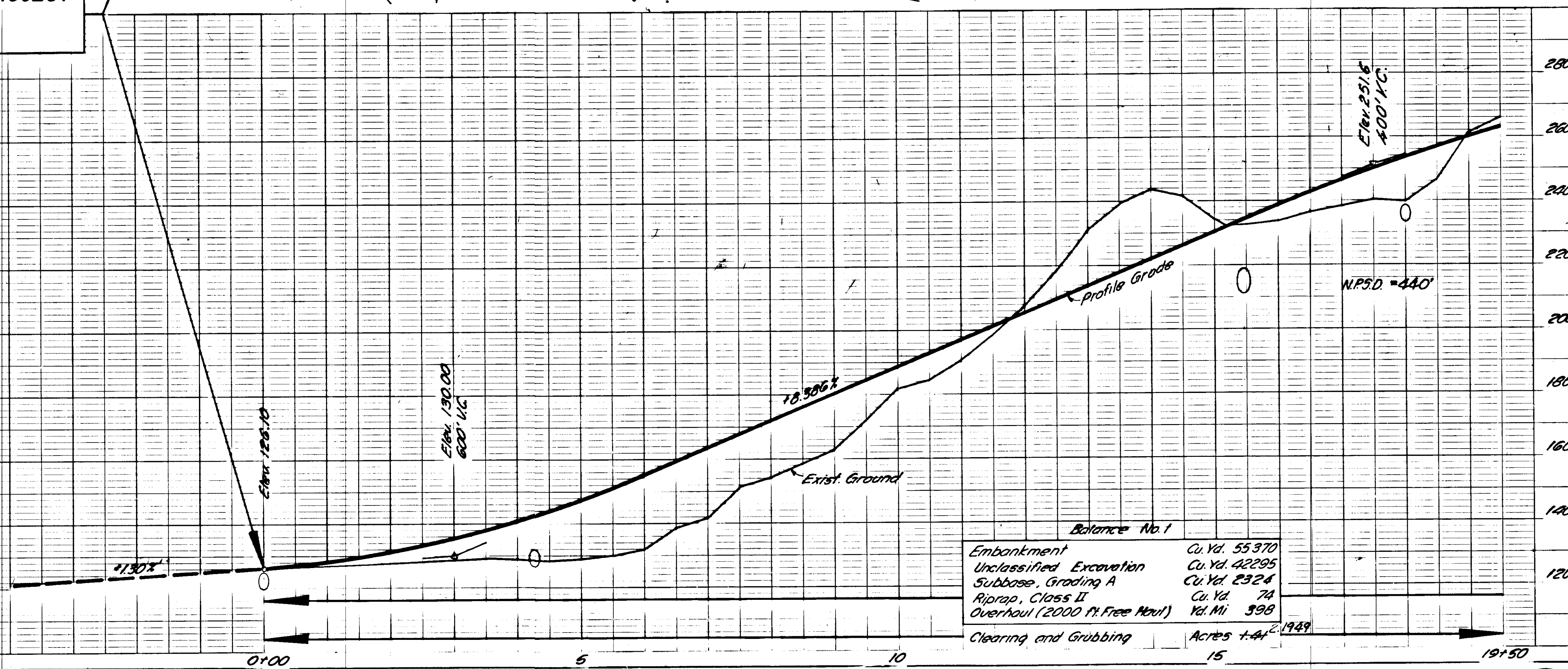


HORIZONTAL CONTROL:
The bearing between "AsB" 159+72.2 and "AsB" 166+03.22 P.I. is N54°26'E.

BEGINNING OF PROJECT
S-0999(6)
STA. "L" 0+00

VERTICAL CONTROL:
The elevation of B.M. No. 1, a nail in a 24" pine tree 25' right of STA. "L" 96+17, is 521.92' ± 522.46

EARTHWORK EQUATION
Embankment = [Unclassified Excavation - (Surplus Material Riprap)] [Swell Factor] + Subbase, Grading A.

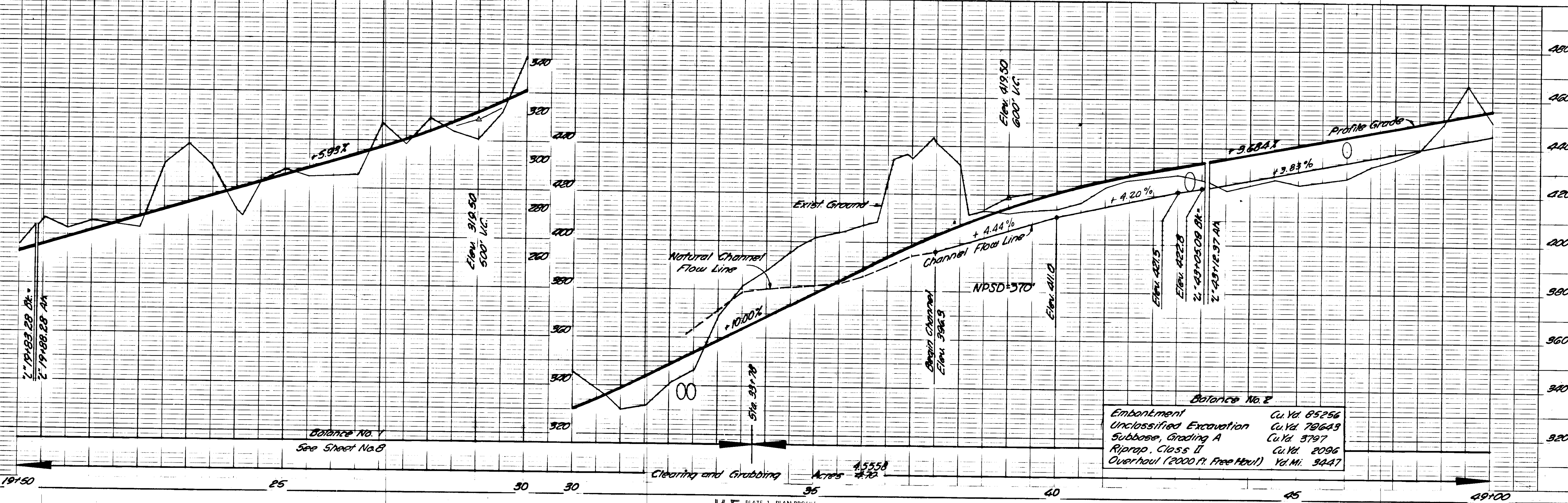
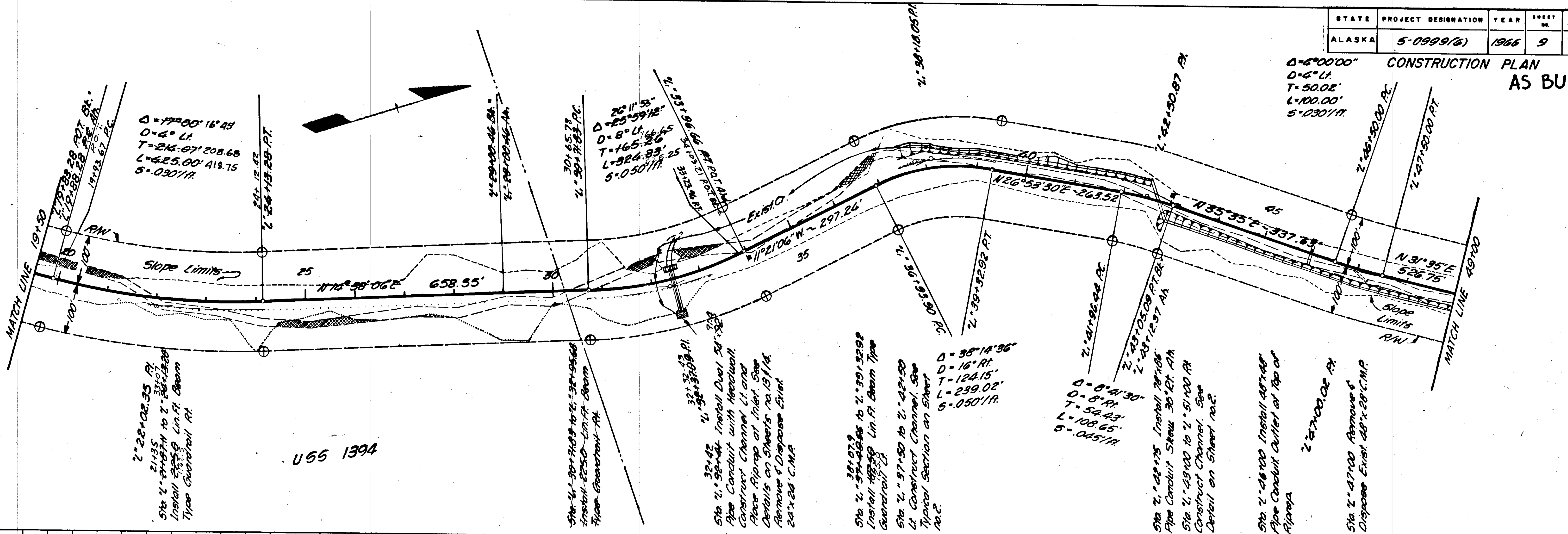


Balance No. 1

Embankment	Cu. Yd. 55370
Unclassified Excavation	Cu. Yd. 42295
Subbase, Grading A	Cu. Yd. 2324
Riprap, Class II	Cu. Yd. 74
Overhaul (2000 ft. Free Haul)	Yd. Mi. 998
Clearing and Grubbing	Acres ± 4.1949

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	5-0999(6)	1966	9	22

CONSTRUCTION PLAN
AS BUILT

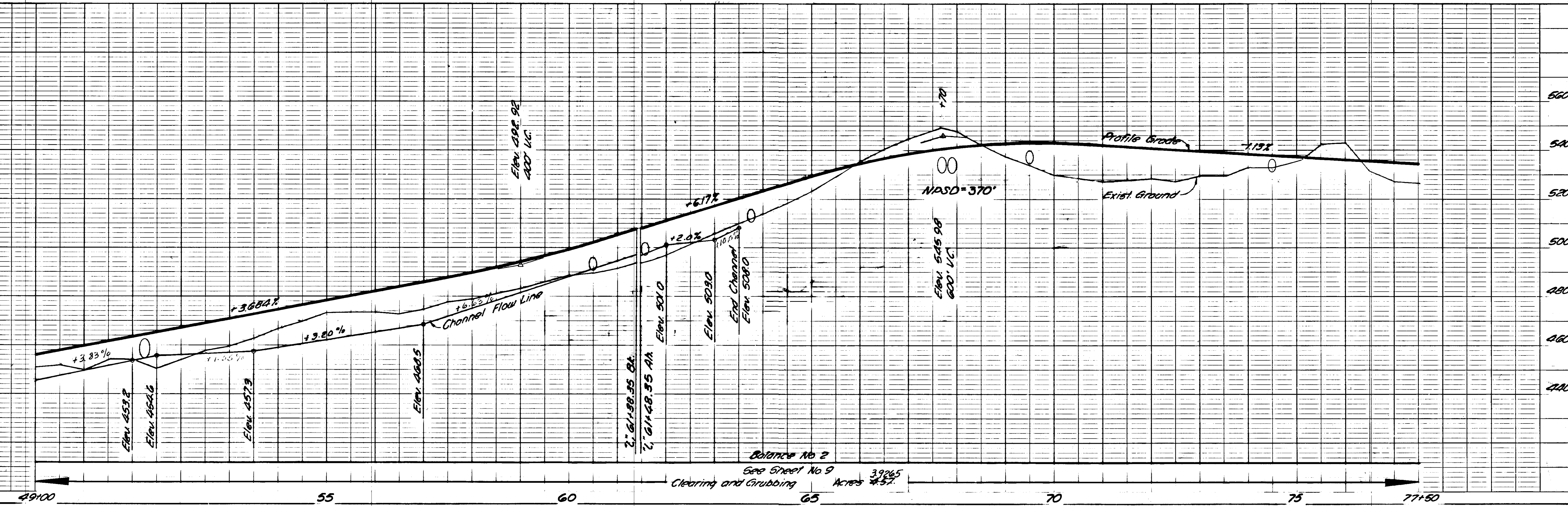
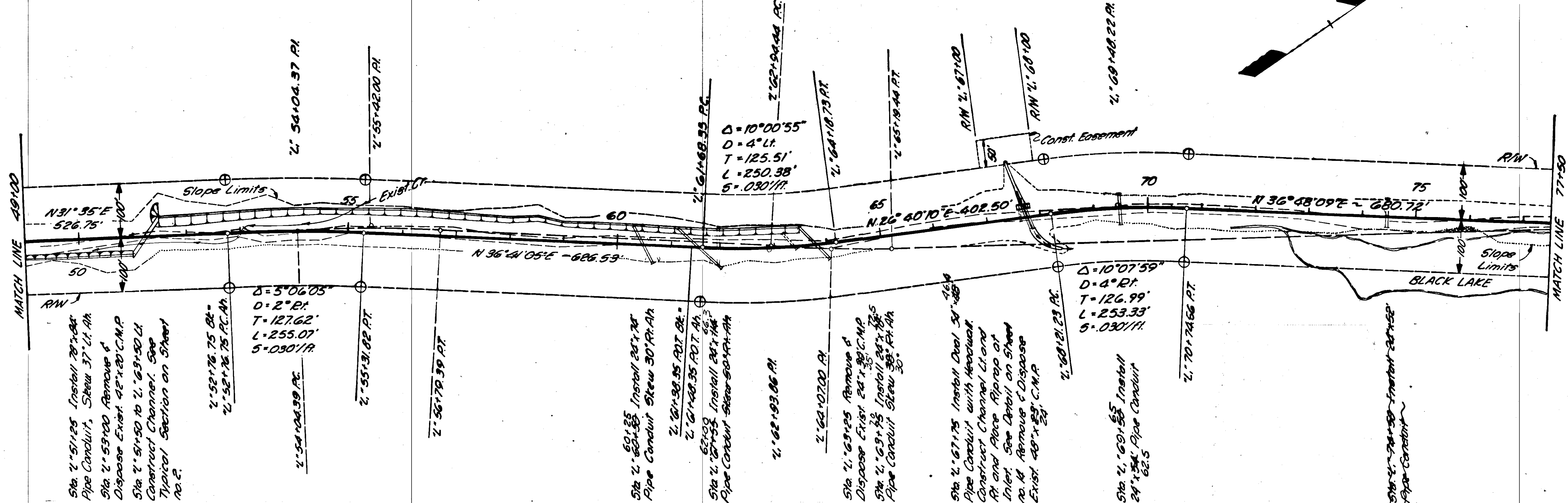


Balance No. 2

Embankment	Cu. Yd. 85256
Unclassified Excavation	Cu. Yd. 78649
Subbase, Grading A	Cu. Yd. 3797
Riprap, Class II	Cu. Yd. 2096
Overhaul (2000 Ft. Free Haul)	Yd. Mi. 9447

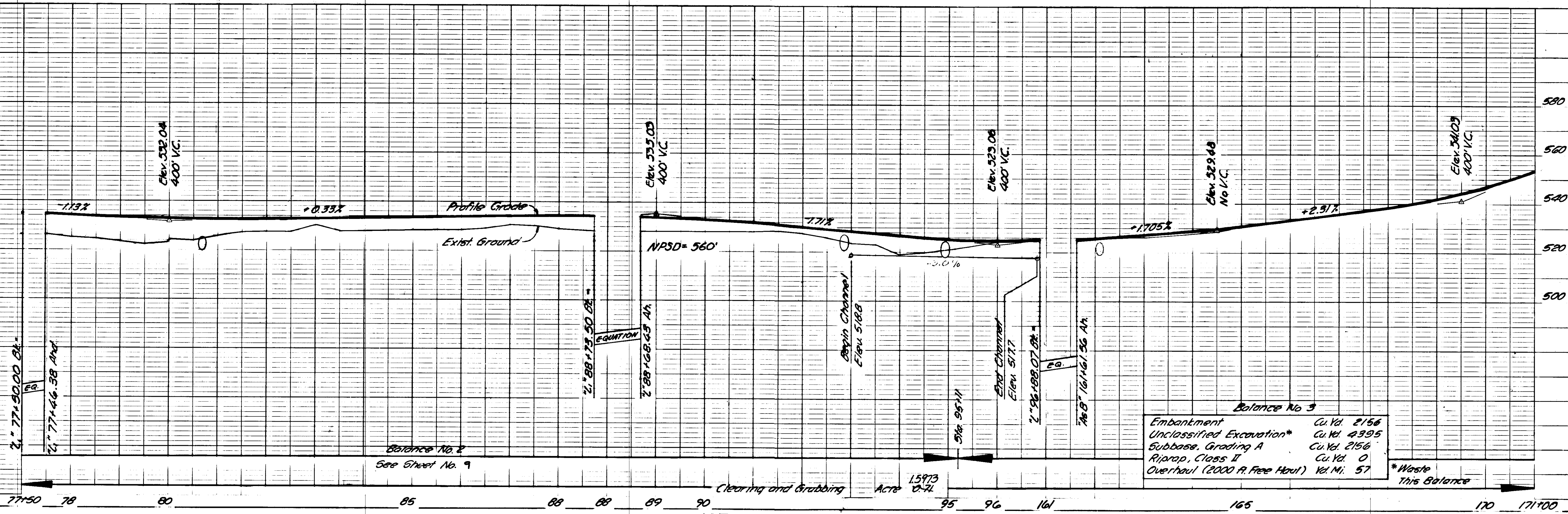
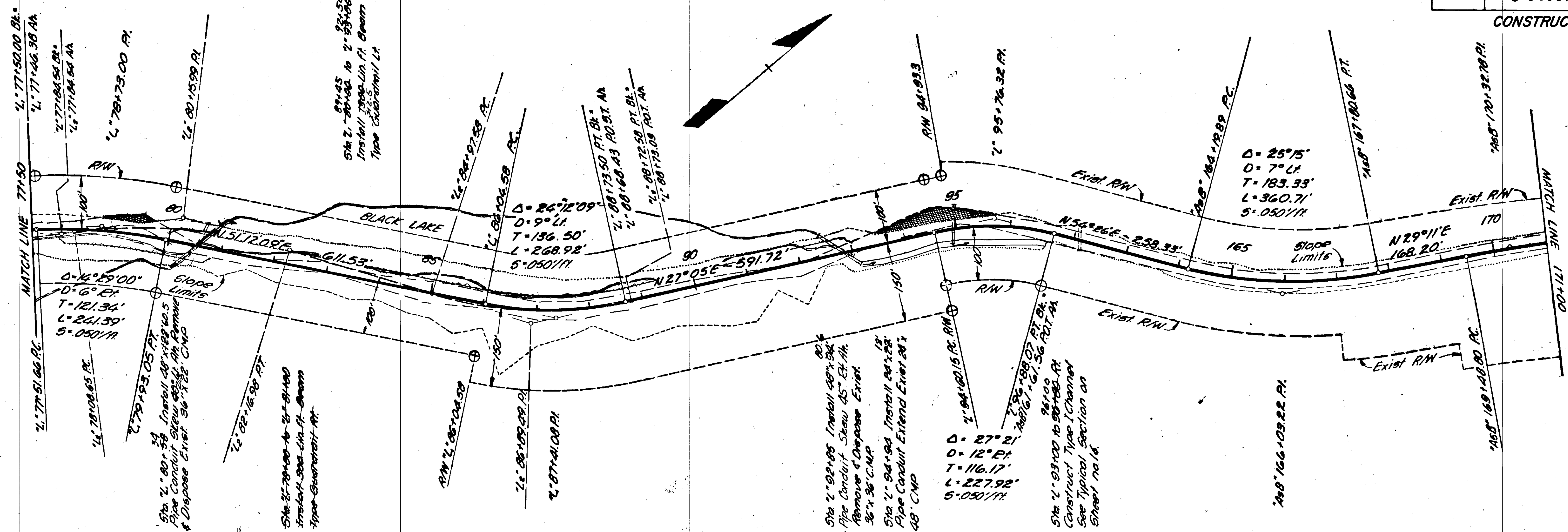
STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	5-0999(6)	1966	10	22

CONSTRUCTION PLAN
AS BUILT



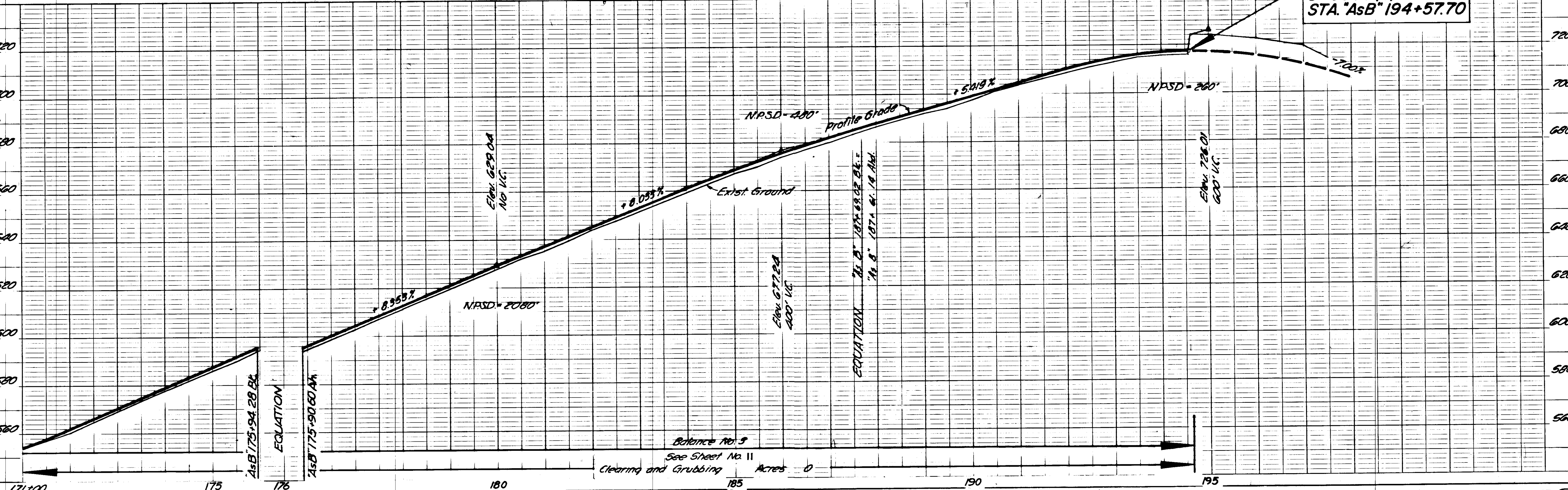
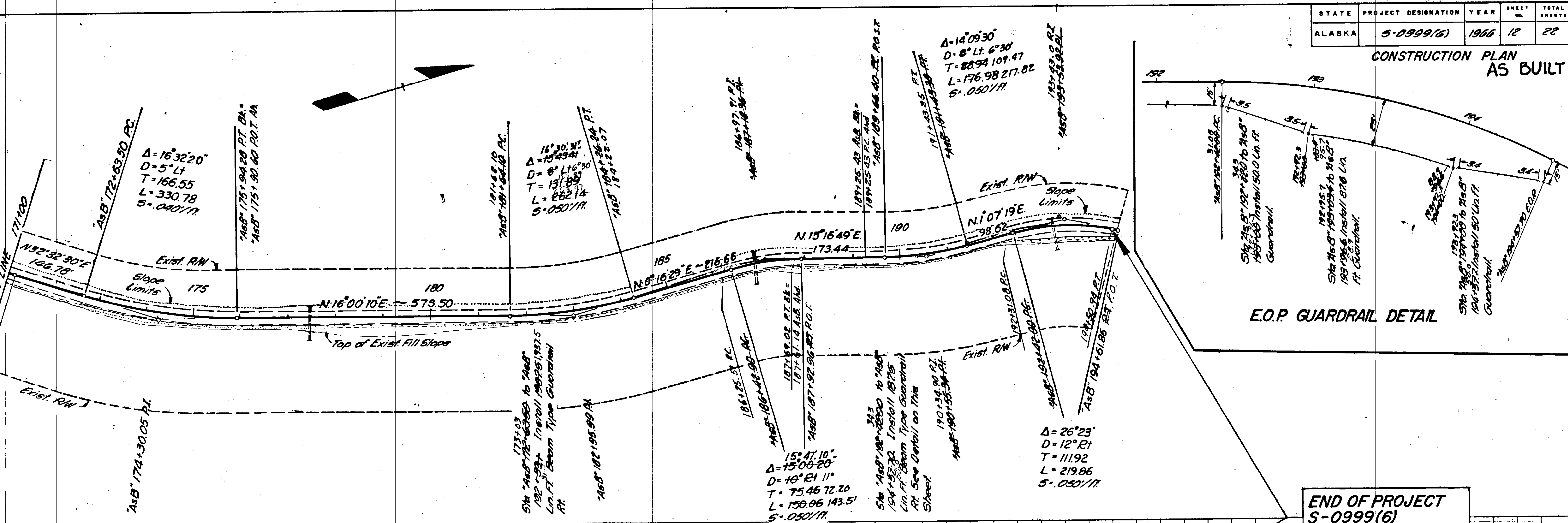
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ALASKA	5-0999(6)	1966	11	22

CONSTRUCTION PLAN
AS BUILT



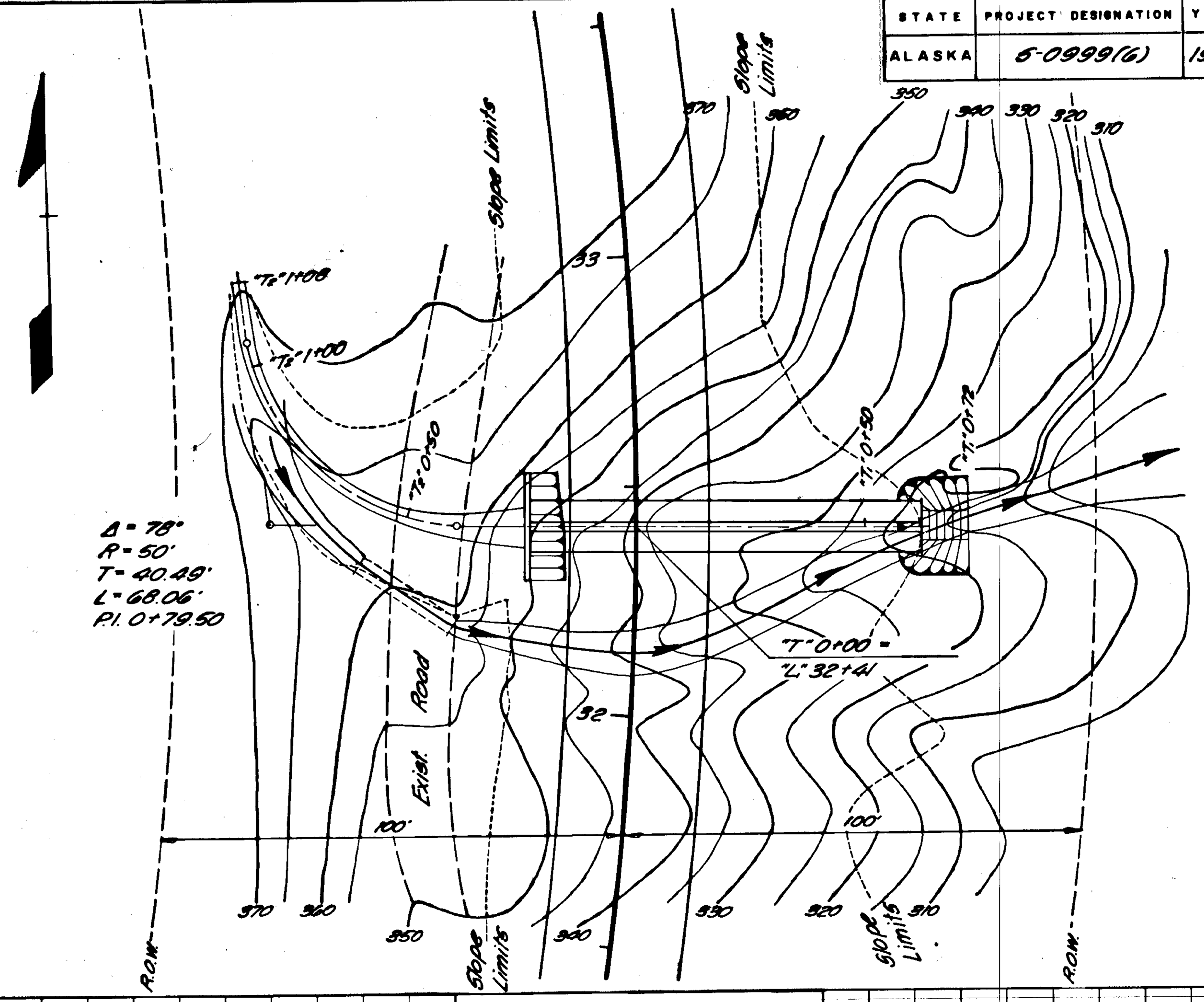
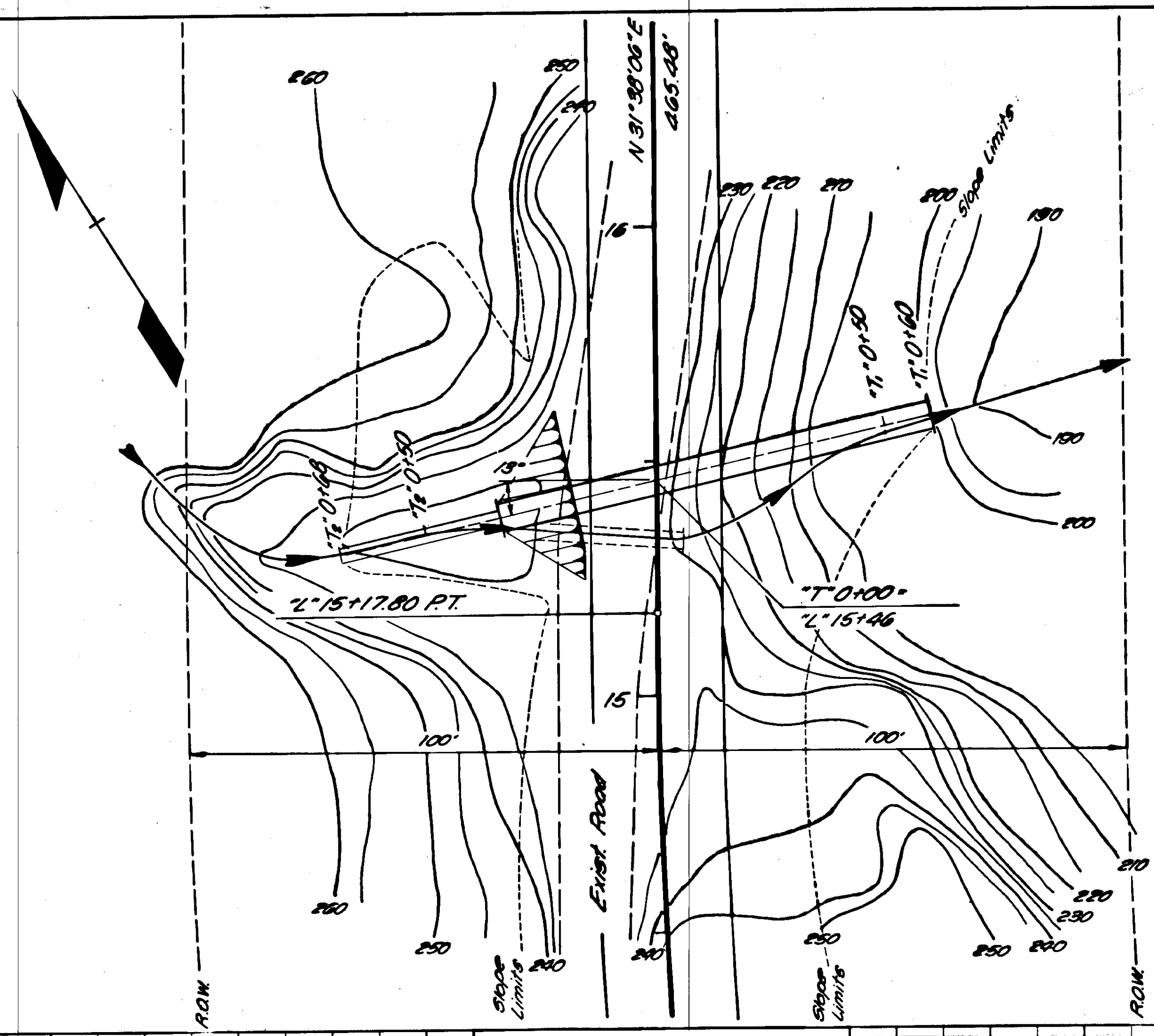
STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	S-0999(6)	1966	12	22

CONSTRUCTION PLAN
AS BUILT



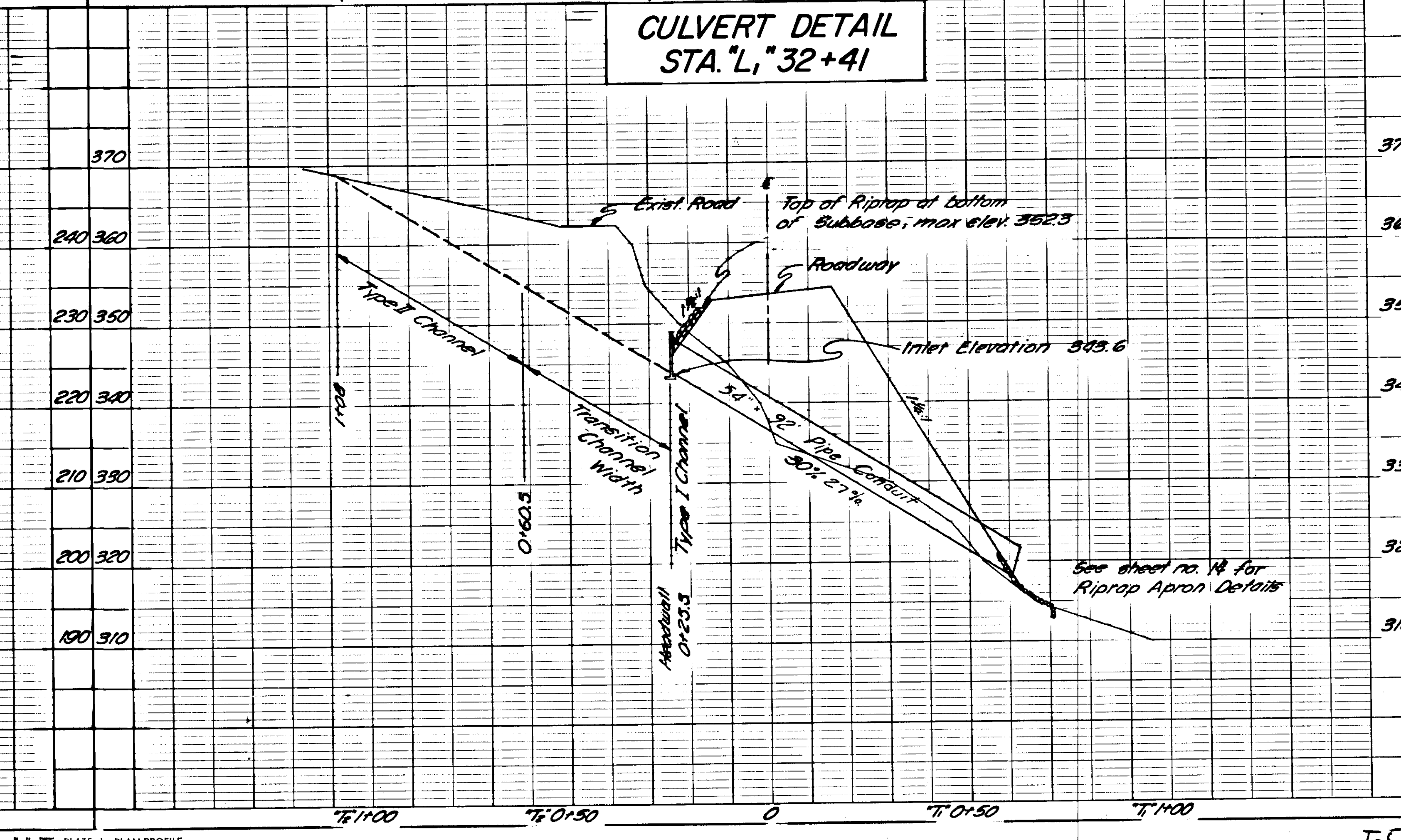
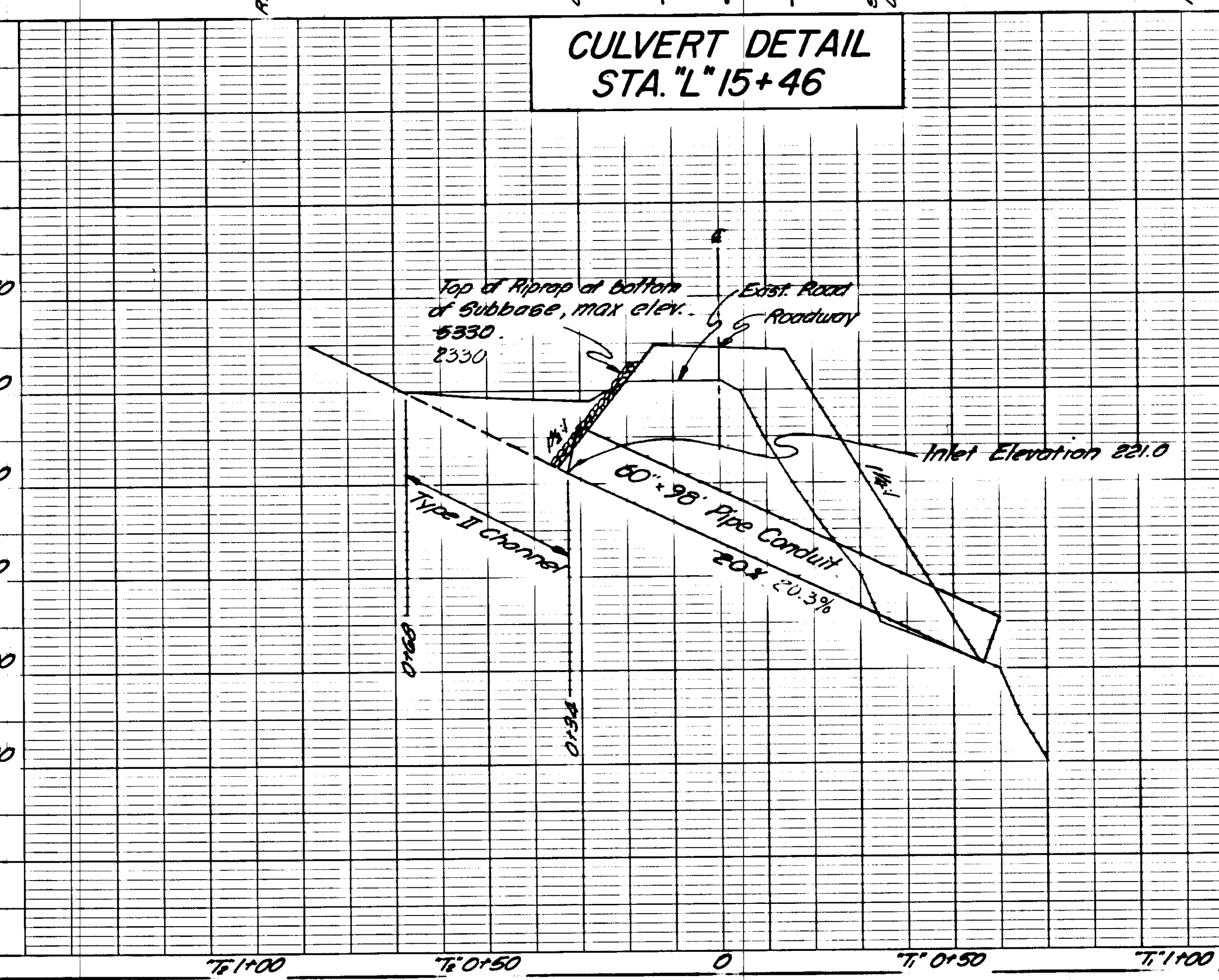
STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	6-0999(6)	1966	13	22

AS BUILT



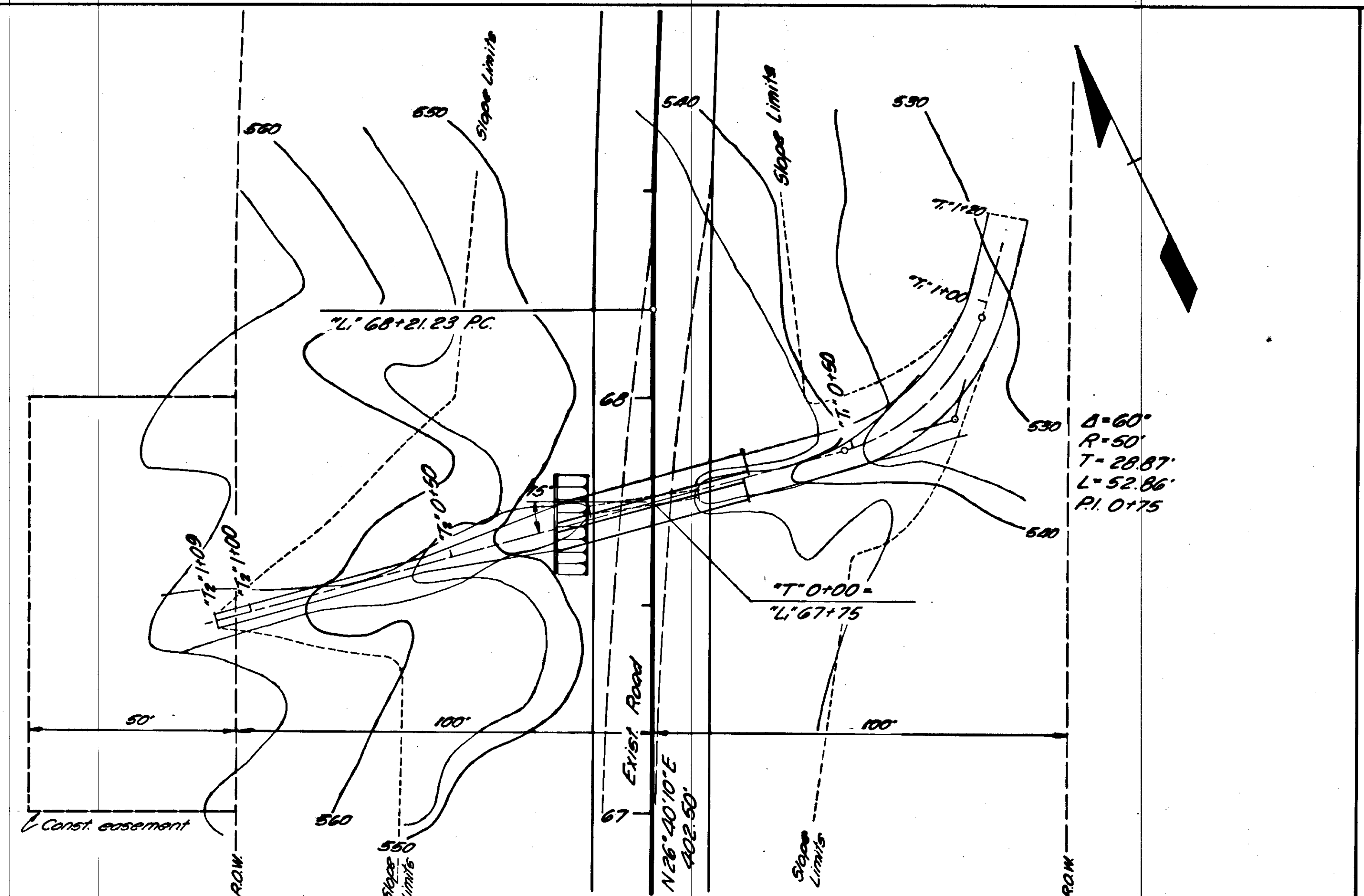
CULVERT DETAIL
STA. "L" 15+46

CULVERT DETAIL
STA. "L" 32+41

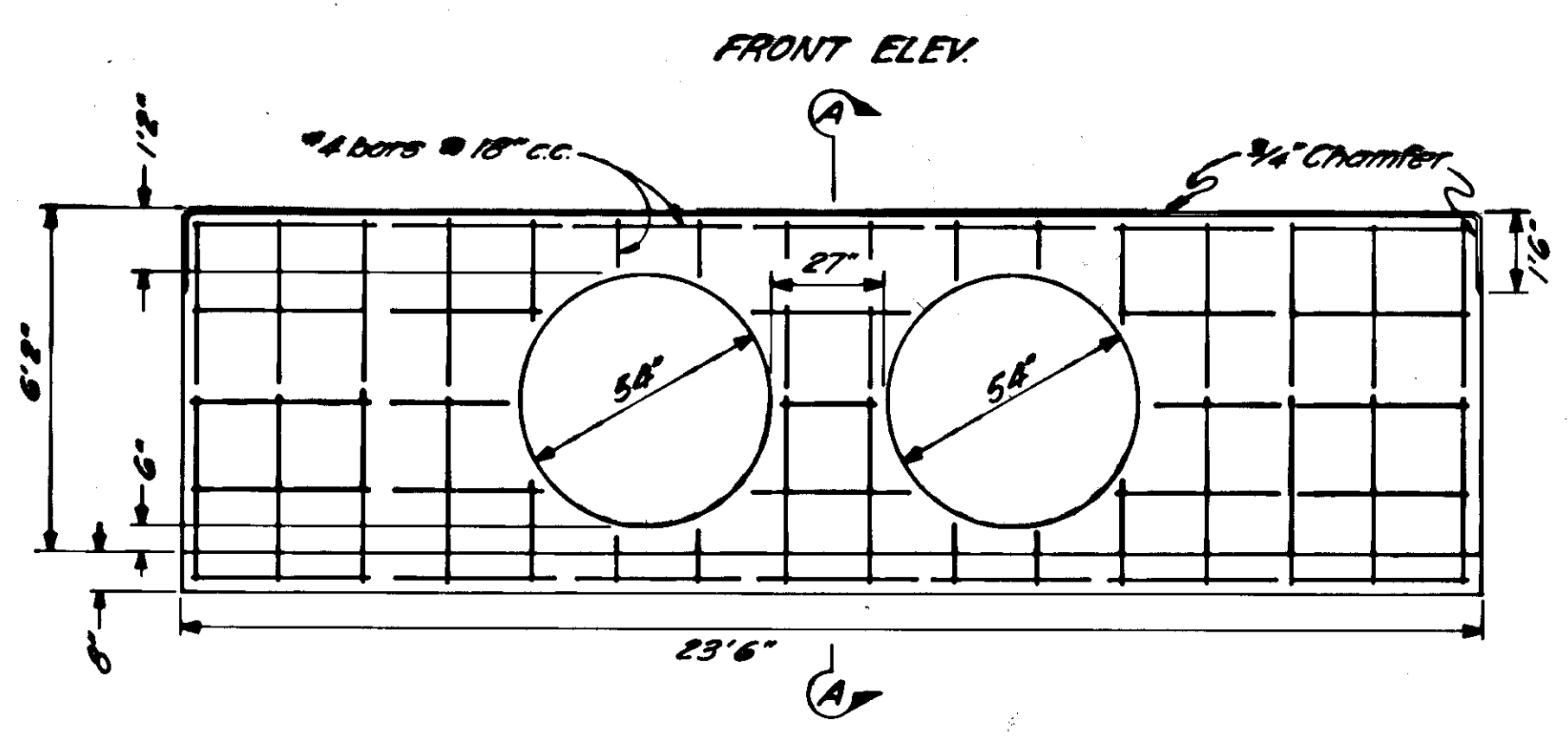
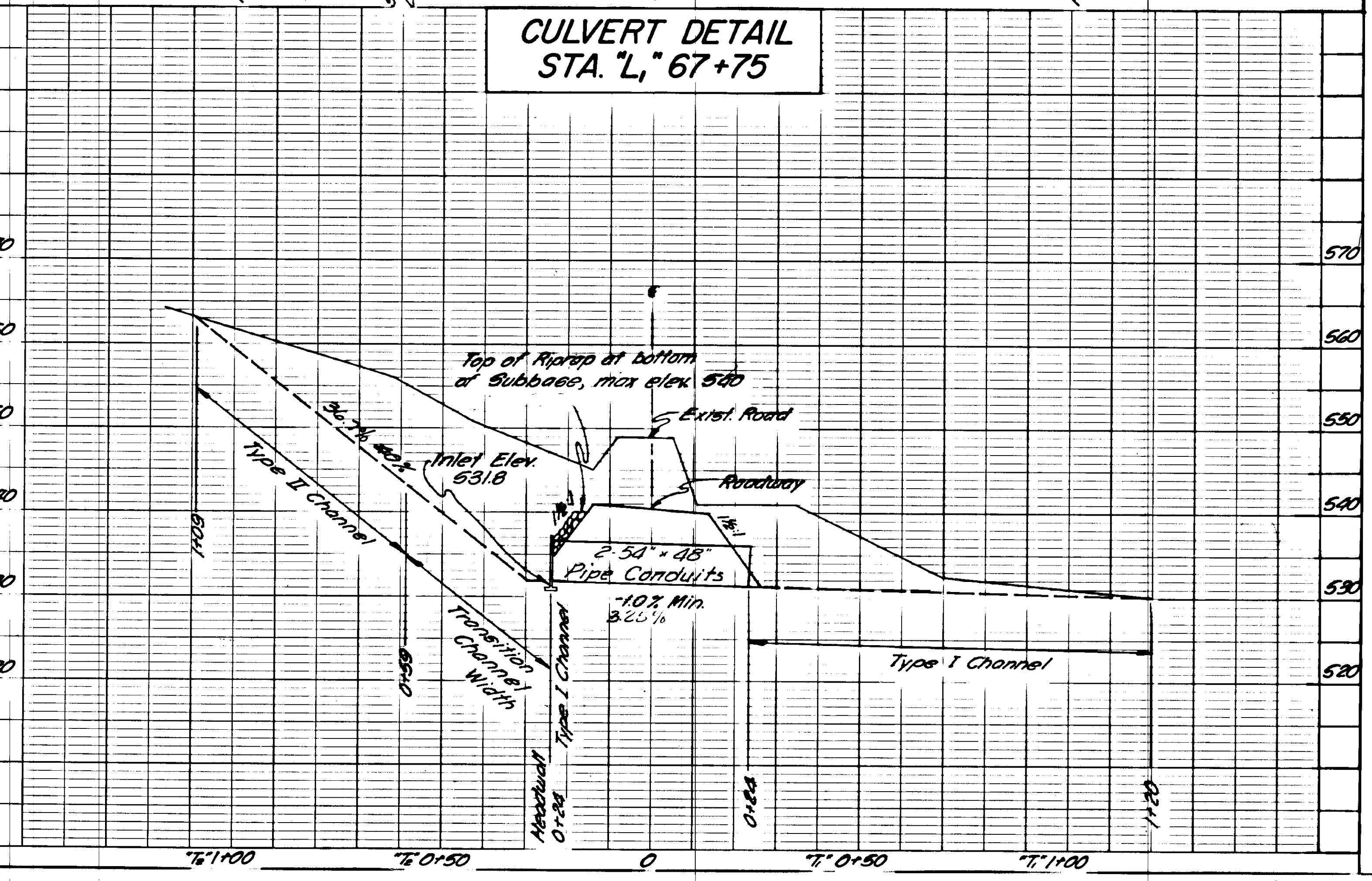


STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	5-0999(6)	1966	15	22

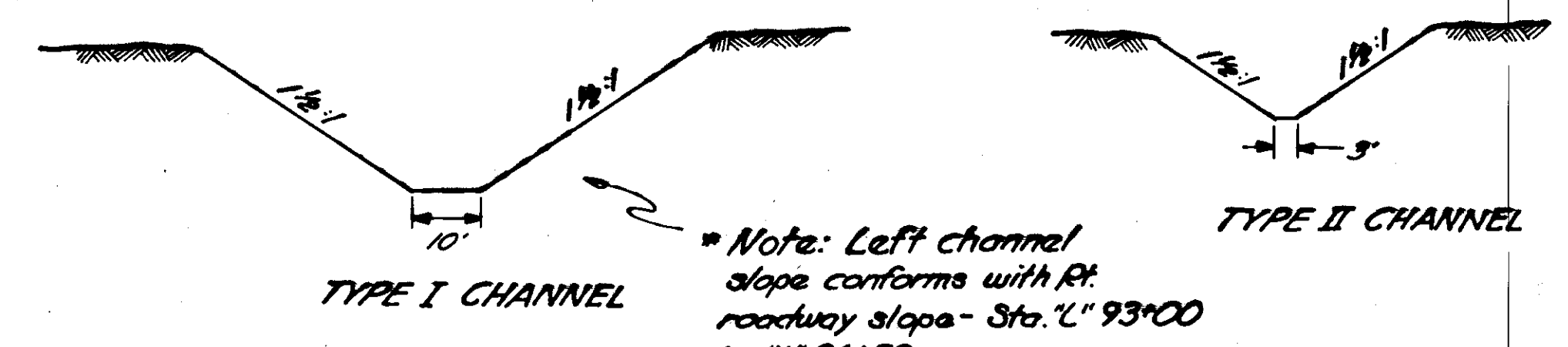
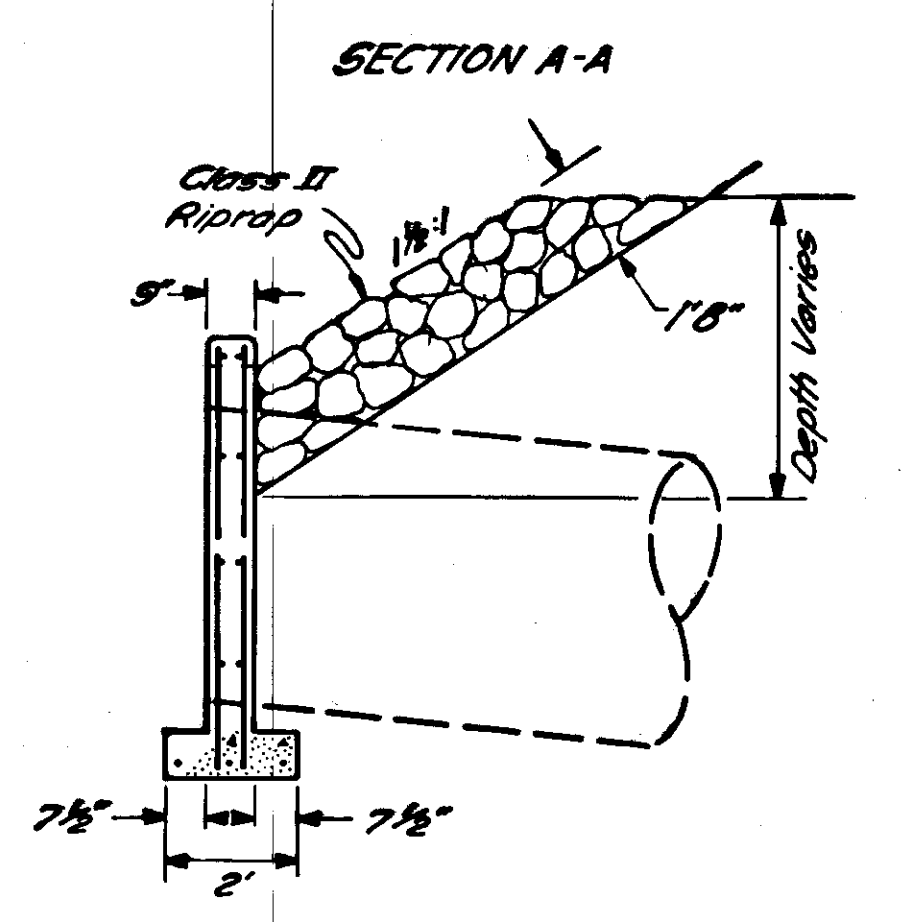
AS BUILT



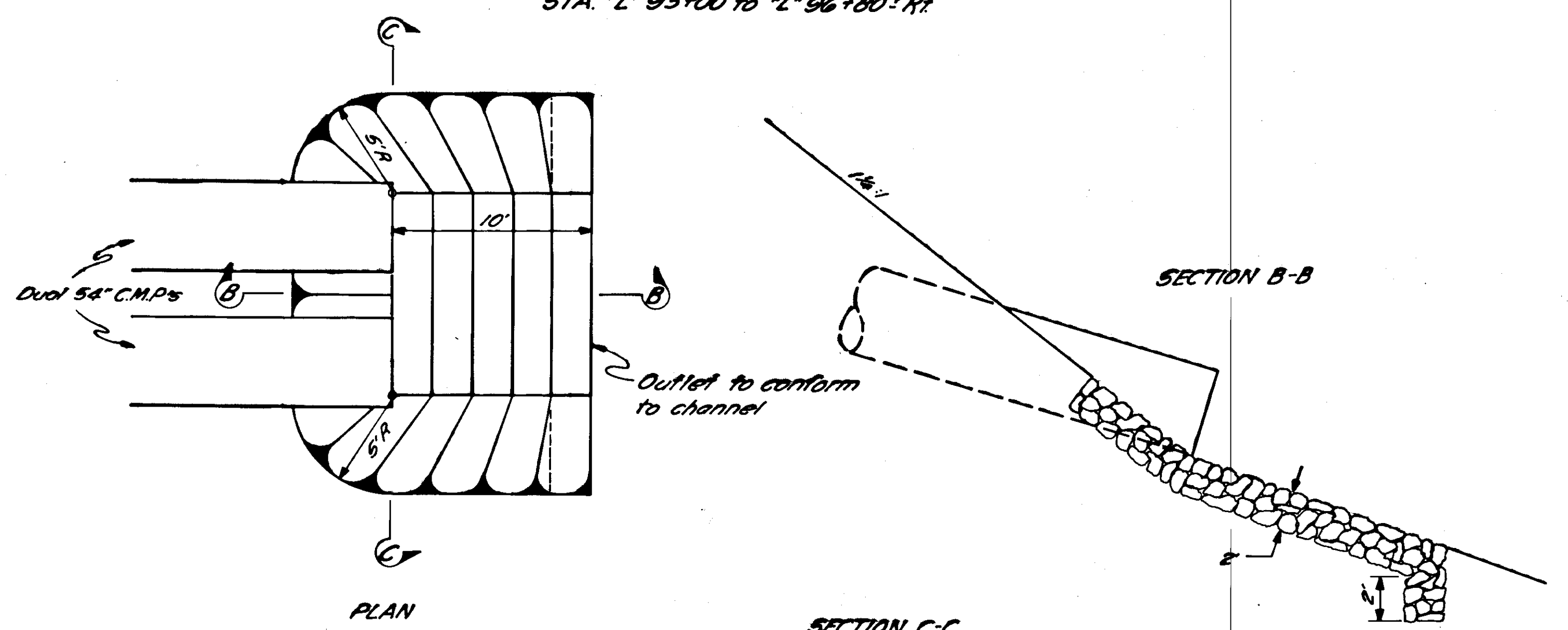
CULVERT DETAIL
STA. "L," 67+75



CONCRETE HEADWALL DETAIL
STA. "L," 92+41 & STA. "L," 67+75



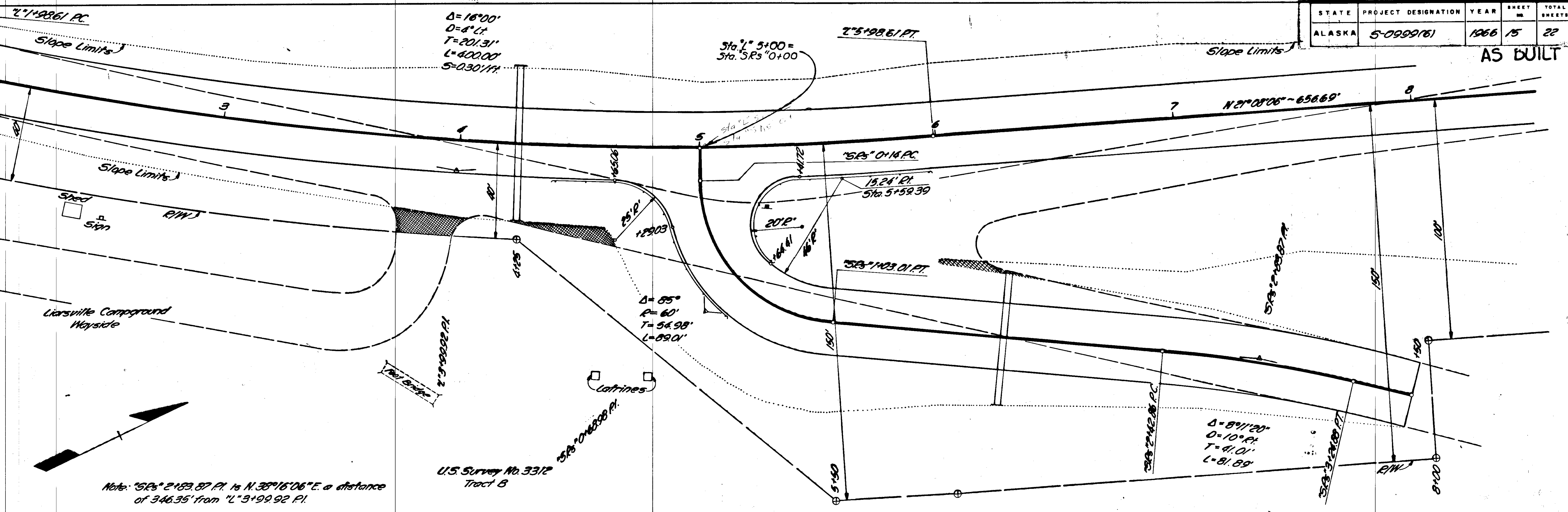
TYPICAL CHANNEL SECTION
STA. "L," 92+41 and "L," 67+75
STA. "L," 93+00 to "L," 96+80: R/I



RIP RAP APRON DETAILS
STA. "L," 92+41

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	5-0999(16)	1966	15	22

AS BUILT



Note: "SR" 2+183.87 PT is N. $38^\circ 16' 06''$ E. a distance of 346.35' from "L" 3+99.92 PI.

**SERVICE ROAD DETAIL
STA. "L" 5+00**

