

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEASTERN REGION  
DESIGN AND CONSTRUCTION DIVISION

**BIG SALT LAKE ROAD  
KLAWOCK TO THE AIRPORT**  
Grading, Paving and Drainage  
RS - 0929 (1) 69275  
AND  
**CRAIG TO KLAWOCK  
OVERLAY**  
Grading, Paving and Drainage  
RS - 0924 (12) 70190

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12-13	INTERSECTION DETAILS
14-17	TRAFFIC CONTROL PLAN
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The following Standard Drawings apply to this project :  
A-1, C-01.03, C-02.01, C-03.01, D-01.01, D-04.10, D-05.10, D-06.01, D-20.01, D-23.00, D-24.00, E-09.00, G-02.00, G-04.03S, G-04.04W, G-14.04S, G-14.04W, I-20.10, I-40.00, I-81.00, M-13.01, M-16.01, S-00.00, S-05.00, S-30.01, T-20.00, T-21.01, U-03.00.

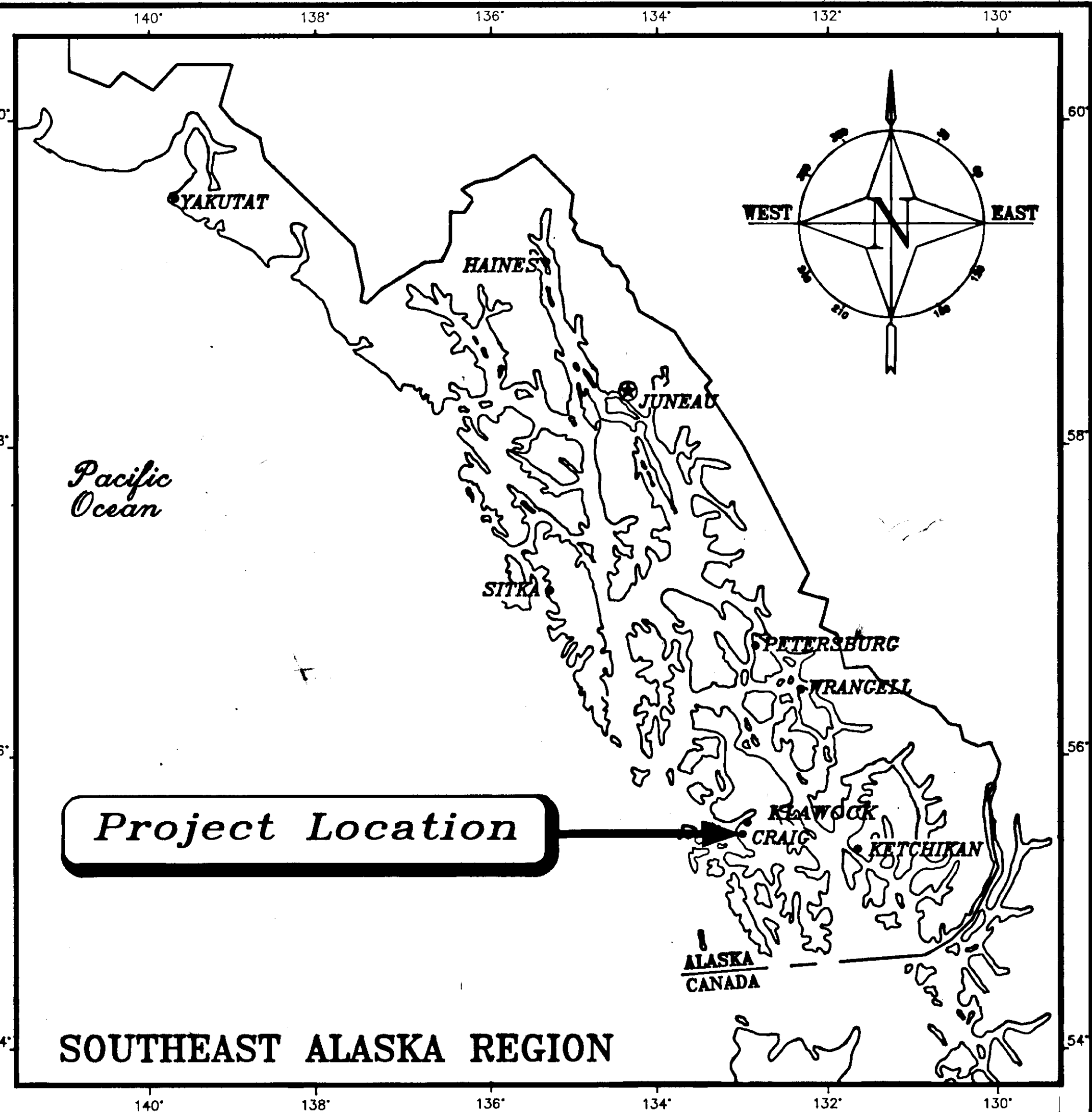
**"AS-BUILT" PLANS**

NOTICE TO PROCEED: 5-29-91  
PROJECT COMPLETION: 9-9-92  
CONTRACTOR: WILDER CONST.  
PROJECT ENGINEER: BRIAN BELT

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEASTERN REGION DESIGN SECTION

APPROVED  
*Amesnil* Date 10-4-90  
S.E. Region Design Chief  
APPROVED  
*E.V. Dannel* Date 10-4-90  
Director, S.E. Region Design & Construction

PROJECT NUMBER: <b>69275, 70190</b>	ENGINEER'S SEAL
DATE: <b>SEPT 1990</b>	
SHEET <b>1</b> OF <b>29</b>	



**RS-0929 (1) 69275  
PROJECT SUMMARY  
BIG SALT LAKE ROAD**  
WIDTH OF PAVING = 36'  
LENGTH OF PROJECT = 11,107.53 = 2.10 MILE  
LENGTH OF GRADING = 11,107.53 = 2.10 MILE  
LENGTH OF PAVING = 11,107.53 = 2.10 MILE  
**AIRPORT SPUR ROAD**  
WIDTH OF PAVING = 28'  
LENGTH OF PROJECT = 2,360 = 0.45 MILE  
LENGTH OF GRADING = 2,360 = 0.45 MILE  
LENGTH OF PAVING = 2,360 = 0.45 MILE

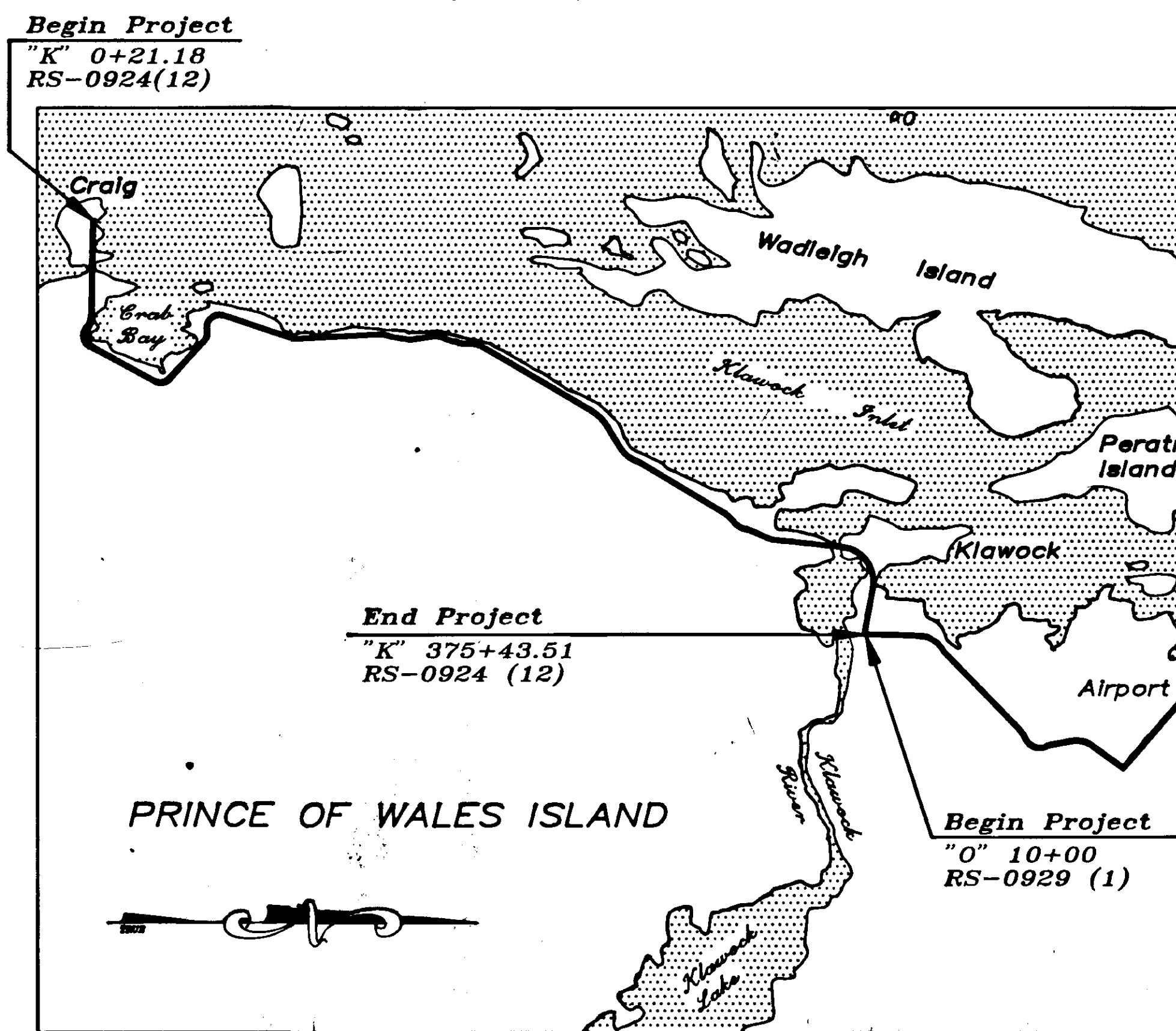
DESIGN DESIGNATION BIG SALT LAKE ROAD	
ADT 1990	= 800
ADT 2010	= 1300
D.H.V. 12%	= 160
%T	= 7%
T.I.	= 7.5
E.A.L.	= 238,860
V	= 40 M.P.H.

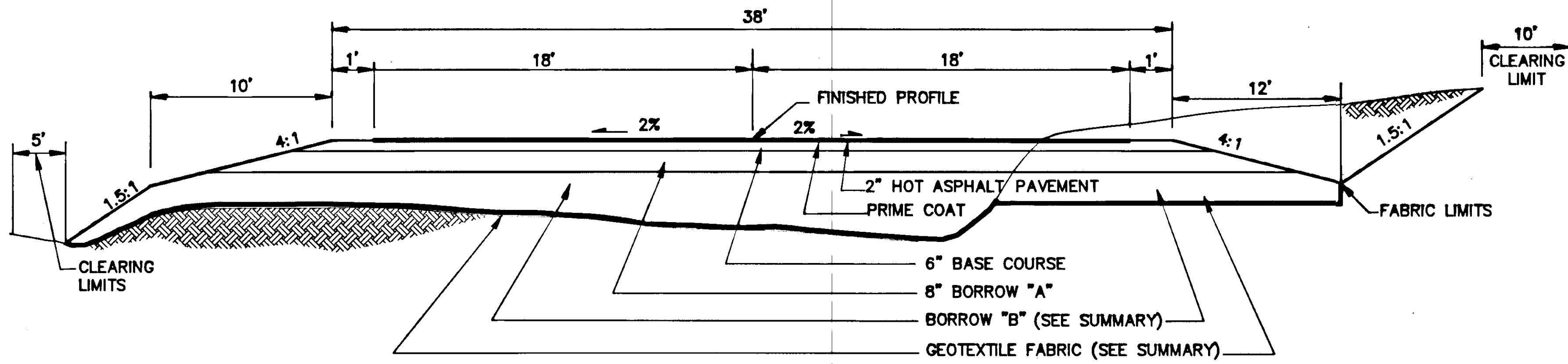
DESIGN DESIGNATION AIRPORT SPUR ROAD	
ADT 1990	= 80
ADT 2010	= 200
D.H.V. 12%	= 28
%T	= 4%
T.I.	= 5.5
E.A.L.	= 11,500
V	= 20 M.P.H.

**RS-0924 (12) 70190  
PROJECT SUMMARY  
CRAIG TO KLAWOCK OVERLAY**  
WIDTH OF PAVING = 31' AND 34'  
LENGTH OF PROJECT = 37,522.34 = 7.82 MILE  
LENGTH OF GRADING = 37,522.34 = 7.82 MILE  
LENGTH OF PAVING = 37,522.34 = 7.82 MILE  
LENGTH OF EXCEPTION = ~~4,600.00~~

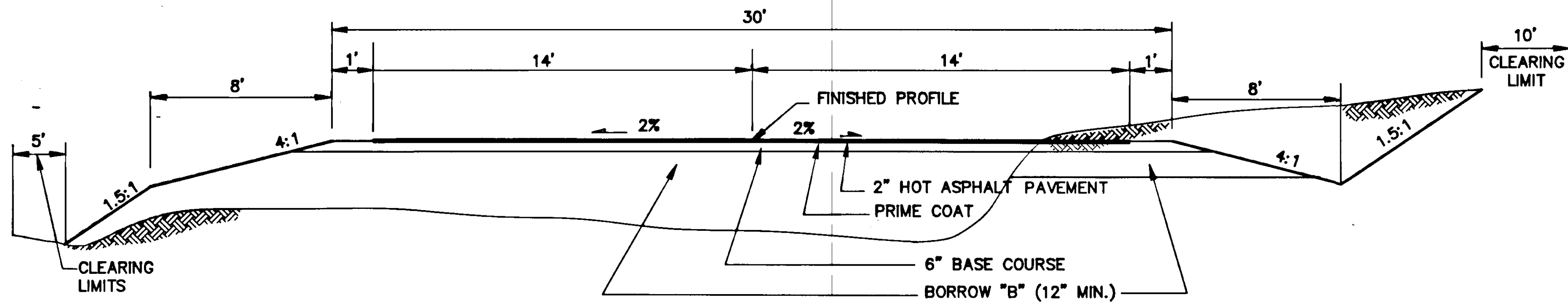
DESIGN DESIGNATION CRAIG TO KLAWOCK OVERLAY	
ADT 1990	= 1868
ADT 2010	= 2776
D.H.V. 12%	= 333
%T	= 7%
T.I.	= 7.5
E.A.L.	= 259,850
V	= 50 M.P.H.



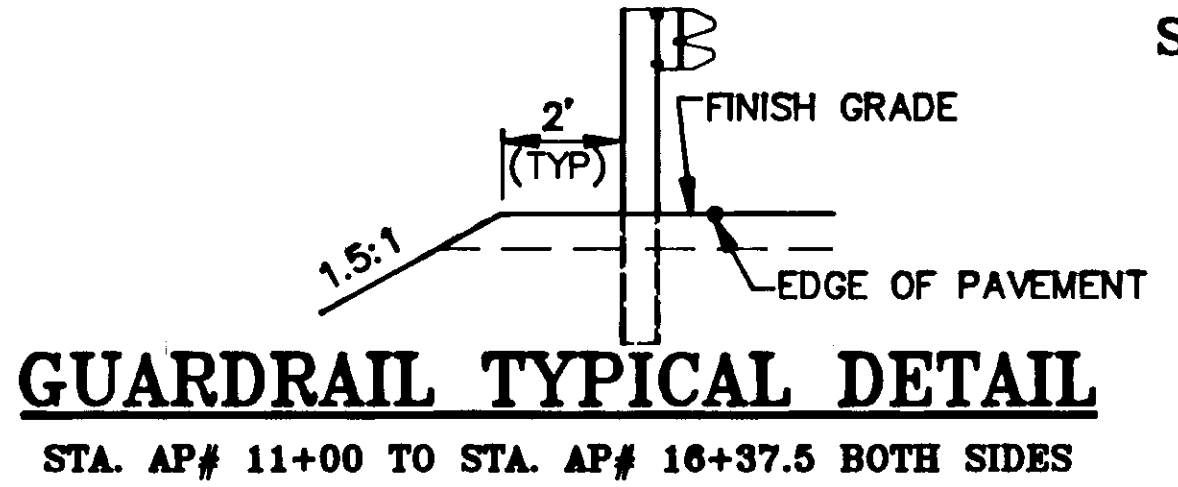
VICINITY MAP



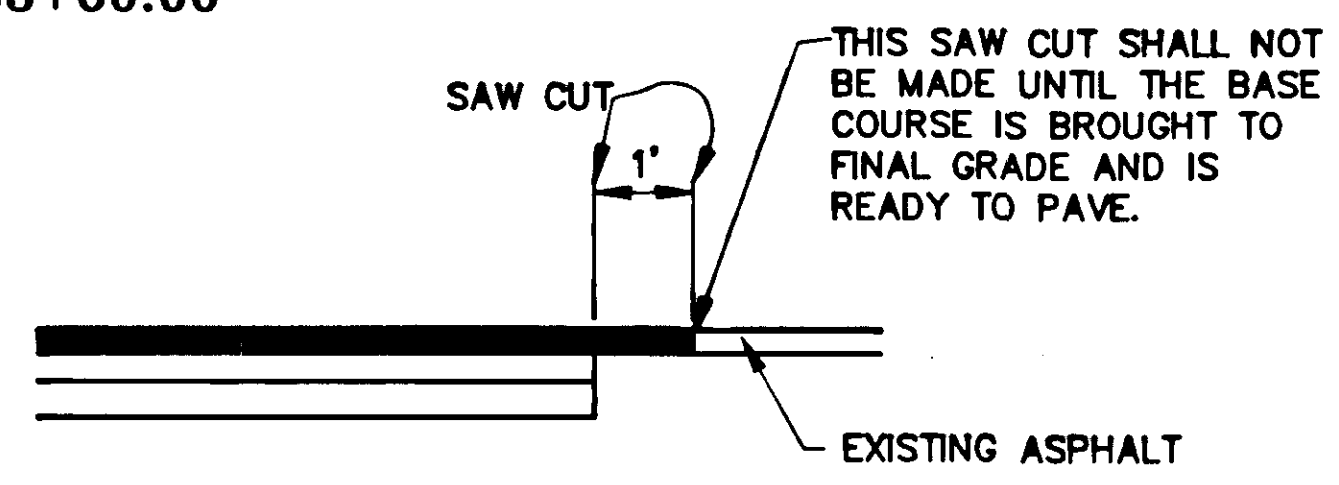
**TYPICAL SECTION  
BIG SALT LAKE ROAD**  
STA. "O" 10+00 TO STA. "L" 122+00



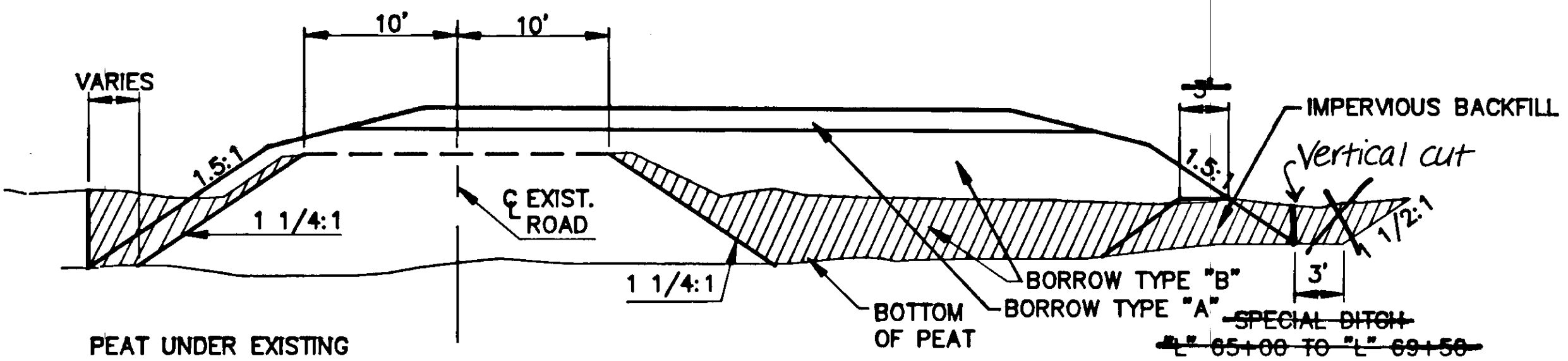
**TYPICAL SECTION  
AIRPORT ACCESS ROAD**  
STA. "AP" 10+48.50 STA. "AP" 33+60.00



**GUARDRAIL TYPICAL DETAIL**  
STA. AP# 11+00 TO STA. AP# 16+37.5 BOTH SIDES



**PAVEMENT JOINT DETAIL**



**PEAT EXCAVATION DETAIL**  
STATION TO STATION  
"L" 64+50 TO "L" 76+00

HAND CLEARING SUMMARY		
STATION TO STATION	ACRE	
"O" 19+50 TO "O" 25+00	0.8	
"L" 55+00 TO "L" 69+00	1.6	
"L" 93+00 TO "L" 102+00	1.2	

GEOTEXTILE REINFORCEMENT FABRIC		
STATION TO STATION	SQ. YD.	
"L" 93+00 TO "L" 102+50	7,498	
"L" 93+15 TO "L" 101+30	5,711	

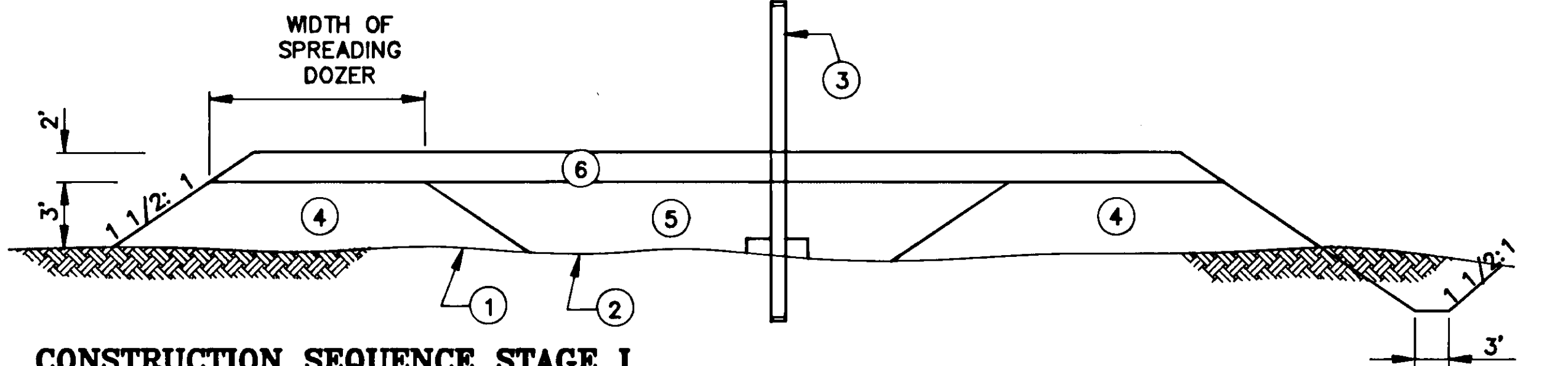
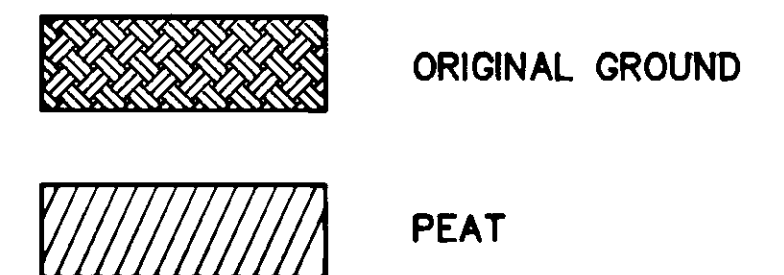
GEOTEXTILE SEPARATION FABRIC		
STATION TO STATION	SQ. YD.	
"O" 10+00 TO "O" 19+50	6,289	
"L" 76+00 TO "L" 86+50	7,023	
"O" 10+40 TO "O" 20+00	4,810	
"O" 25+00 TO "O" 26+10	645	
"L" 79+50 TO "L" 80+65	230	

BORROW "B" SUMMARY		
STA. TO STA.	MIN. DEPTH	REMARKS
"O" 10+00 TO "O" 20+00	16"	Subexcavation reqd.
"O" 20+00 TO "O" 25+50	36"	
"O" 25+50 TO "L" 44+00	12"	
"L" 44+00 TO "L" 69+50	36"	
"L" 69+50 TO "L" 93+00	12"	Subexc. thru most
"L" 93+00 TO "L" 102+00	36"	
"L" 102+00 TO "L" 122+00	12"	
"L" 102+00 TO "L" 106+00	36"	Subexcavation reqd.

**GENERAL NOTES**

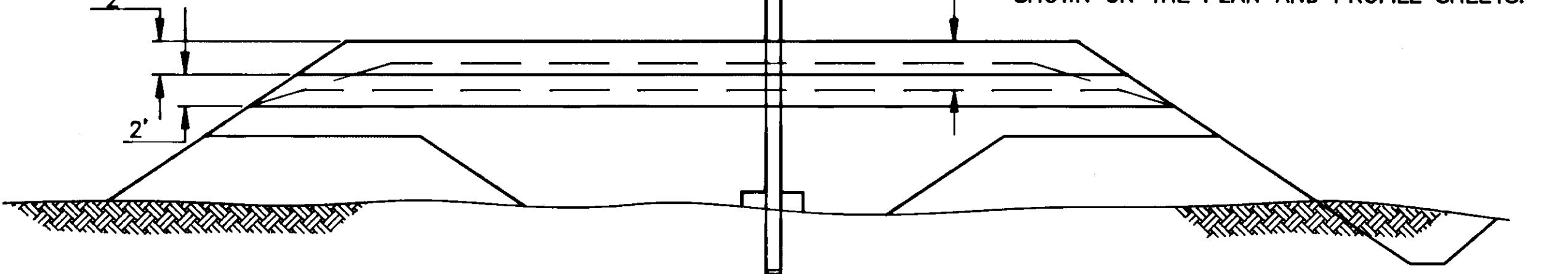
- GRADES AND ALIGNMENT SHOWN ON THESE PLANS ARE SUBJECT TO MINOR REVISION.
- CULVERT LENGTHS AND LOCATIONS ARE APPROXIMATE ONLY AND ARE SUBJECT TO MINOR FIELD REVISIONS.
- THE DEPTH OF THE 12 INCH D.I. WATER MAIN SHOWN ON THE PLANS IS APPROXIMATELY 4' BELOW THE EXISTING ROAD.
- GUARDRAIL LENGTHS AND LOCATIONS ARE APPROXIMATE ONLY AND ARE SUBJECT TO MINOR FIELD REVISION.
- PEAT DEPTHS VARY, SEE GEOTECHNICAL REPORT.
- SUPERELEVATION IS ROTATED ON CENTERLINE.
- ALL FORESLOPES AND BACKSLOPES SHALL BE SEEDED FROM THE INTERSECTION WITH THE ORIGINAL GROUND TO THE SHOULDER HINGE POINT.
- DRIVEWAYS AND INTERSECTING STREETS OR ROADS SHALL BE CONSTRUCTED WITH SAME STRUCTURAL SECTION AS SHOWN ON THE BIG SALT LAKE ROAD TYPICAL SECTION.

**LEGEND**



**CONSTRUCTION SEQUENCE STAGE I**

- HAND CLEARING
- GEOTEXTILE FABRIC (SEE SUMMARY)
- SETTLEMENT PLATFORMS (100' ON CENTER) SEE DETAIL SHEET 5
- CONSTRUCT OUTSIDE SECTION OF INITIAL LIFT (MODIFIED BORROW TYPE "B")
- CONSTRUCT MID-SECTION OF INITIAL LIFT (MODIFIED BORROW TYPE "B")
- CONSTRUCT SECOND LIFT (2' MAX.)



**CONSTRUCTION SEQUENCE STAGE II**

- WAIT 10 DAYS FROM STAGE I CONSTRUCTION
- CONSTRUCT 2' LIFT
- WAIT 10 DAYS
- REPEAT STEPS 2 AND 3 TO THE TOP OF SURCHARGE HEIGHT

**CONSTRUCTION SEQUENCE STAGE III**

- REMOVE SURCHARGE
- CONSTRUCT TYPICAL SECTION

**SURCHARGE DETAIL**

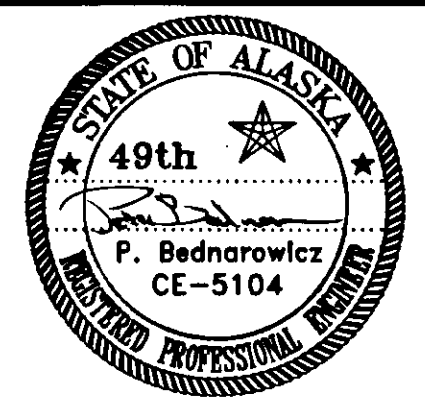
STATION TO STATION	
"O" 21+00	"O" 25+50
"L" 52+50	"L" 64+50
"L" 93+00	"L" 102+50

BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS-BUILT

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

KLAWOCK  
KLAWOCK TO AIRPORT ACCESS  
RS-0929(1)  
ALASKA  
**TYPICAL SECTIONS**

DESIGNED BY: F. MURPHY	PROJECT NO. RS-0929(1)
DRAWN BY: AUTOCADD/CSA	DATE:
CHECKED BY:	SHEET 2 OF 29



# ESTIMATE OF QUANTITIES

ITEM NO.	ITEM	UNIT	RS-0929(1) SHEET NUMBERS							RS-0929(1) SUBTOTAL	RS-0924(12) SUBTOTAL	TOTAL
			6	7	8	9	10	11				
120(1)	DBE ADJUSTMENT	CS							ALL REQ'D.	ALL REQ'D.	ALL REQ'D.	
201(1A)	CLEARING	ACRE	6.44	3.64	1.99	4.45	1.53	3.57	-21.62 16.08		-21.62 16.08	
201(4A)	HAND CLEARING	ACRE	0.8	0.8	0.8	1.2			-3.6 4.53		-3.6 4.53	
202(2)	Removal of Pavement	SY								325.56	325.56	
202(4)	REMOVAL OF CULVERT PIPE	LF	40	121	145	212	225	110	-853 953	224	-853 1177	
202(12)	REMOVAL OF CURB AND SIDEWALK	LF								3000 2082	3000 2082	
203(3)	UNCLASSIFIED EXCAVATION	CY	63,828	2,632	25,038	19,239	9700	7743	128,180 161,285.3		128,180 161,285.3	
203(6A)	BORROW, TYPE "A"	TON	6489	5,263	5,475	6095	1848		25,170 24,017.6		25,170 24,017.6	
203(6B)	BORROW, TYPE "B"	TON	38,111	37,079	56,214	67,608	3346	64,755	267,113 349,951.8		267,113 349,951.8	
203(11)	DITCH GRADING	STATION								280 339.41	280 339.41	
203(12)	DITCH GRADING, ROCK	STATION								-10 0	-10 0	
205(5)	BEDDING MATERIAL	TON							-935 4179.5		-935 4179.5	
205(6)	STREAMBED MATERIAL	CYVM		31		38	20	26	115 106.89		115 106.89	
301(1)	CRUSHED AGGREGATE BASE COURSE	TON	4462	3894	4020	4425	1230	2957	20,988 21,348.06	1200 3134.96	22,188 24,483.02	
308(1)	CRUSHED ASPHALT BASE COURSE	SY								43,500 40,188.1	43,500 40,188.1	
401(1)	ASPHALT CONCRETE, TYPE II	TON	1508	1087	1223	1352	428	852	6450 6674.98	5925 15,905.37	11,475 22,880.35	
401(1A)	ASPHALT CONCRETE, TYPE IV	TON								7500 0	7500 0	
401(2)	AC-5 ASPHALT CEMENT	TON	91	65	73	81	26	51	387 410.08	300 977.16	687 1387.24	
401(2A)	AC-20 ASPHALT CEMENT	TON								525 0	525 0	
401(6)	ASPHALT PATCHING	SY								1100 466.47	1100 466.47	
402(1)(A)	MC-30 LIQUID ASPHALT FOR TACK COAT	TON								58 33.66	58 33.66	
403(1)(A)	MC-30 LIQUID ASPHALT FOR PRIME COAT	TON	12.7	9.2	10.6	11.4	3.6	7.2	54.7 30.92		54.7 30.92	
501(6)	CLASS W CONCRETE	CY								8 5.35	8 5.35	
505(9)	STRUCTURAL STEEL SHT. PILES, FURN. & DRIVEN	SF								530 557.7	530 557.7	
508(1)	MEMBRANE WATERPROOFING	LS							ALL REQ'D.		ALL REQ'D.	
603(3-18)	END SECT. FOR 18 INCH CORRUGATED STEEL PIPE	EACH								2 0	2 0	
603(3-24)	END SECT. FOR 24 INCH CORRUGATED STEEL PIPE	EACH								2 0	2 0	
603(9-48)	48 INCH CORRUGATED ALUMINUM PIPE	LF								180 145.2	180 145.2	
603(11-18)	END SECTION FOR 18 INCH CORRUGATED ALUM. PIPE	EACH								4 7	4 7	
603(11-24)	END SECTION FOR 24 INCH CORRUGATED ALUM. PIPE	EACH								19 33	19 33	
603(11-36)	END SECTION FOR 36 INCH CORRUGATED ALUM. PIPE	EACH								12 5	12 5	
603(17-18)	18 INCH PIPE	LF		36		36	45		117 204		117 204	
603(17-24)	24 INCH PIPE	LF	292	132	192	308	214	64	1202 1564		1202 1564	
603(17-48)	48 INCH PIPE	LF	70						-70 90		-70 90	
603(17-60)	60 INCH PIPE	LF						188	188 208		188 208	
603(17-96)	96 INCH PIPE	LF		140					140		140	
603(19)	7'X5'-1" PIPE ARCH	LF					148	78	-226 248		-226 248	
603(22)	STORM SEWER INLET CLEAN-OUT	EACH								13 15	13 15	
603(23)	STORM SEWER PIPE CLEAN-OUT	LF								1400 1418	1400 1418	
603(24)	BEVEL CULVERT ENDS	EACH								107 20	107 20	
603(25)	CULVERT CLEAN-OUT	EACH								29 35	29 35	
604(3)	RECONSTRUCT EXISTING MANHOLE	EACH	2		1			1	-4 2	-4 3	-8 5	
604(4)	ADJUST EXISTING MANHOLE	EACH								3 13	3 13	
606(1)	W-BEAM GUARDRAIL	LF						1075	-1075 2837.50	337.5 425	1112.5 3262.50	
606(6)	END ANCHORAGES	EACH						4	-4 11	-2 3	-6 14	
606(8)	ADJUST GUARDRAIL HEIGHT	LF							9500 8537.5		9500 8537.5	
608(3)	ASPHALT SIDEWALK	SY							1500 2197.5		1500 2197.5	
609(1)	CURB, TYPE I	LF							3000 4162		3000 4162	
611(1)	RIPRAP, CLASS II	CY	16	26		36	18	19	-115 951.27	440 142.73	-255 1094	
614(1)	SURVEY MONUMENTS	EACH	3	1	1	6	1	1	13	-34 31	-47 44	
614(2)	MONUMENT CASES	EACH	3	1	1	6	1	1	13	-34 31	-47 44	
615(1)	STANDARD SIGN	SQ.FT.	115.0	40.50		28.0	13.5	25.75	-222.75 249.25	-90 58.08	-312.75 307.33	
615(6)	DELINEATORS, FLEXIBLE	EACH	16	16					-32 40		-32 40	
618(1)(A)	SEEDING Modified Seeding	ACRE	2.22	1.12	.99	2.13	2.54		-9.0 19.32	11 5.38	-20 24.70	
618(3)	WATER FOR SEEDING	M-GAL.								80 6.2	80 6.2	
627(1A)	12 INCH DUCTILE IRON WATER CONDUIT CLASS 50 RELOCATION	LF			400	731			-1131 2460		-1131 2460	
627(6)	FIRE HYDRANT RELOCATION	EACH	2						2		2	
627(9)	INSTALL 12 INCH GATE VALVE	EACH				1		1	-2 4		-2 4	
627(10)	ADJUSTMENT OF VALVE BOX	EACH	1		1			1	-4 8	13 22	17 30	
627(12)	TEMPORARY WATER CONDUIT	LUMP SUM							ALL REQ'D.		ALL REQ'D.	
630(1)	GEOTEXTILE, SEPARATION	SQ.YD.	6289		7023				13,312 866.3		13,312 866.3	
630(2)	GEOTEXTILE, REINFORCEMENT	SQ.YD.				7496			7496 5906.1		7496 5906.1	
631(2)	GEOTEXTILE, RIPRAP LINER	SY								-300 222.6	-300 222.6	
635(1)	INSULATION BOARD	MBM		0.13		0.64	0.19	2.0	-2.96 3.46		2.96 3.46	
626(2)	Klawock Utility Crossings	LS							All Req'd.		All Req'd.	
627(1B)	Waterline Relocation Oranges	LS							All Req'd.		All Req'd.	

# ESTIMATE OF QUANTITIES

ITEM NO.	ITEM	UNIT	RS-0929(1) SUBTOTAL	RS-0924(12) SUBTOTAL	TOTAL
641(1)	TEMPORARY EROSION & POLLUTION CONTROL	CS	ALL REQ'D.	ALL REQ'D.	ALL REQ'D.
642(1)	CONSTRUCTION SURVEYING	LS	ALL REQ'D.	ALL REQ'D.	ALL REQ'D.
642(2)	THREE PERSON SURVEY PARTY	HOURLY	10 26		10 26
643(1)	TRAFFIC MAINTENANCE	LS	ALL REQ'D.	ALL REQ'D.	ALL REQ'D.
643(3)	PERMANENT CONSTRUCTION SIGNING	EA/CAL DAY	2695 3089	540 712	3235 3801
643(4)	CONSTRUCTION SIGN	EA/DAY	3675 3872	1600 508	5275 4380
643(5)	TYPE II BARRICADE	EA/DAY	2000 2671	1000 224	3000 2895
643(6)	TYPE III BARRICADE	EA/DAY	2000 2511		2000 2511
643(7)	TRAFFIC CONE	EA/DAY	12,000 3801	4800 1709	16,800 5510
643(13)	TEMPORARY PAVEMENT MARKINGS	LS	ALL REQ'D.	ALL REQ'D.	ALL REQ'D.
643(15)	FLAGGING	HOURLY	2000 4161	800 282.3	2800 2882.3
645(1)	TRAINING PROGRAM	MAN-HR	1000 1302.5		1000 1302.5
670(1)	PAINTED TRAFFIC MARKINGS	LS	ALL REQ'D.	ALL REQ'D.	ALL REQ'D.
670(8)	RECESSED PAVEMENT MARKERS	EACH	223 216	680 621	903 837
670(9)	Painted JWP Bar	C.O.13	5	27	32
670(10)	Painted Cross Walk	C.O.13		1	1
670(11)	Painted Stripe Removal	C.O.13		850	850
643(16)	Pilot Car	C.O.14	23.5	130.5	154
642(3)	Sidewalk Extension Surveying	C.O.18		All Req'd.	All Req'd.
642(4)	Craig Intersection Surveying	C.O.19		All Req'd.	All Req'd.

## BASIS OF ESTIMATE

ITEM NO.	DESCRIPTION	EST. FACTOR
203(6A)	BORROW, TYPE "A"	1.85 TONS/CY
203(6B)	BORROW, TYPE "B"	1.85 TONS/CY
301(1)	CRUSHED AGGREGATE BASE COURSE	1.96 TONS/CY
401(1)	ASPHALT CONCRETE TYPE II	116 # YD <sup>2</sup> IN. DEPTH
401(1A)	ASPHALT CONCRETE TYPE IV	116 # YD <sup>2</sup> IN. DEPTH
401(2)	AC-5 ASPHALT CEMENT	6.0% OF 401(1)
401(2A)	AC-20 ASPHALT CEMENT	7.0% OF 401(1A)
402(1)	CSS-1 EMULSIFIED ASPHALT FOR TACK COAT	0.10 GAL/YD <sup>2</sup> 253 GAL/TON
403(1)	MC-30 LIQUID ASPHALT FOR PRIME COAT	0.25 GAL/YD <sup>2</sup> 256 GAL/TON

### Estimate of Quantities (cont.) Change Order

Item No.	Item	Unit	RS-0929(1)	RS-0924(12)	Total
109(1)	Craig Intersection Force Account	C.O.19	L.S.	All Req'd.	All Req'd.
109(2)	Drainage Force Account	C.O.20	L.S.	All Req'd.	All Req'd.
109(3)	Shoulder Work Force Account	C.O.21	L.S.	All Req'd.	All Req'd.
203(3A)	Sidewalk Excavation	C.O.18	Sta.	21	21
203(4A)	Sidewalk Borrow	C.O.18	Sta.	20.5	20.5
203(13)	Culvert Subexcavation	C.O.1	C.Y.	2,860.63	2,860.63
203(14)	Roadway Drill and Shoot	C.O.2	L.S.	All Req'd.	All Req'd.
203(14B)	Drainage Extra Work, 1991	C.O.4	L.S.	All Req'd.	All Req'd.
203(15)	"AP" Lite Drill and shoot	C.O.5	L.S.	All Req'd.	All Req'd.
203(16)	Rock Slope Facing	C.O.6	L.S.	All Req'd.	All Req'd.
501(8)	Bridge Concrete Slab	C.O.15	L.S.	All Req'd.	All Req'd.
603(9-18)	18 Inch Corrugated Aluminum Pipe	C.O.18	L.F.	129	355
603(9-18a)	Furnish 18" Corrugated Aluminum Pipe	C.O.18	L.F.	250	250
603(9-24)	24 Inch Corrugated Aluminum Pipe	C.O.20	L.F.	132	326
603(9-24a)	Furnish 24" Corrugated Aluminum Pipe	C.O.18	L.F.	280	280
603(26)	Furnish Tee Saddle	C.O.19	Each	3	3
604(5A)	Curb Inlet	C.O.18	Each	7	7
604(5B)	Field Inlet	C.O.19	Each	2	3
605(7)	Underdrain	C.O.17	L.S.	All Req'd.	All Req'd.
606(2)	Thrie-Beam Guardrail	L.F.	75	75	
606(5)	Removal and Disposal of Guardrail	C.O.11	L.F.	300	650
606(9)	Buried End Anchorage	C.O.11	Each	1	1
606(10)	Radiused Guardrail	C.O.11	L.F.	250	287.50
606(11)	Repaired Guardrail	C.O.11	L.F.	575	575
607(1)	Airport Security Gate	C.O.16	L.S.	All Req'd.	All Req'd.
604(5)	Monument Case Removal	C.O.12	Each	60	60

BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS-BUILT

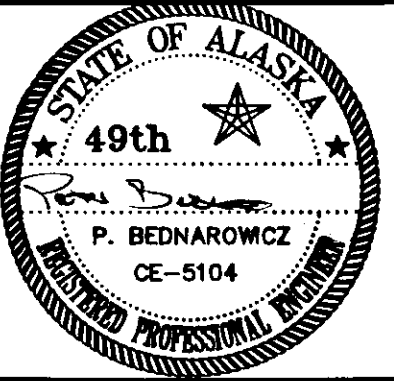
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG, ALASKA  
CRAIG TO KLAWOCK  
RS-0929(1), 69275 AND RS-0924(12), 70190  
ESTIMATE OF QUANTITIES  
BOTH PROJECTS

KLAWOCK, ALASKA

DESIGNED BY: P. JONES  
DRAWN BY: AUTOCADD/C. Anderson  
CHECKED BY: F. MURPHY

PROJECT NO. RS-0929(1) RS-0924(12)  
DATE:  
SHEET 3 OF 17



## SIGN SUMMARY

No.	Station	Offset		Code No.	Legend	Size	Area S.F.	No. Posts	Facing Traffic	Remarks
		Lt.	Rt.							
1	"AS BUILT" 32+23		12'	D2-1	CRAIG 7	48"x12"	4	1		
2	"AS BUILT" 42+73		12'	D2-3	HOLLIS 21 CLARK BAY 27 FERRY	114"x36"	28.5	2		FRAMED
3	"O" 10+50	35'		R1-1	STOP	30"x30"	6.25	1		
4	"O" 12+60	30'		D1-3	KLAWOCK CRAIG → ← CLARK BAY	108"x36"	27.0	2		FRAMED
5	"O" 15+00		30'	R2-1	SPEED LIMIT 40	30"x36"	7.50	1		
6	"O" 15+20	30'		W3-1	STOP AHEAD	36"x36"	9.0	1		
7	"A" 10+22	33'		R1-1	STOP	30"x30"	6.25	1		
8	"L" 42+00	40'		R2-1	SPEED LIMIT 40	30"x36"	7.50	1		
9	"B" 10+35	35'		R1-1	STOP	30"x30"	6.25	1		
10	"L" 52+00 57+00		30'	R2-1	SPEED LIMIT 40	30"x36"	7.50	1		
11	"B" 10+62		28'	W14-2	NO OUTLET	30"x30"	6.25	1		
12	"L" 86+50		18'	OM-L	OBJECT MARKER	12"x36"	3.0	1		
13	"L" 86+50		18'	OM-R	OBJECT MARKER	12"x36"	3.0	1		
14	"L" 86+60		18'	I-3	BENNETT CREEK	48"x24"	8.0	1		
15	"L" 87+70	25'		I-3	BENNETT CREEK	48"x24"	8.0	1		
16	"L" 87+70	18'		OM-R	OBJECT MARKER	12"x36"	3.0	1		
17	"L" 87+70		18'	OM-L	OBJECT MARKER	12"x36"	3.0	1		
18	"L" 114+30	40'		D2-2	SPEED LIMIT 40	30"x36"	7.50	1		
19	"L" 117+70		40'	I-5	← AIRPORT	24"x36"	6.0	1		
20	"AP" 10+60	18'		R1-1	STOP	30"x30"	6.25	1		
21	"AP" 13+00 13+00	20'		D2-2	← THORNE BAY KLAWOCK →	72"x24"	12.0	2		
22	"AP" 15+50		20'	R2-2	SPEED LIMIT 20	30"x36"	7.50	1		
23	"A" 11+00	15'		W1-6	→	36"x18"	4.5	1		
24	"A" 13+35	15'		W1-1R	→	36"x36"	9.0	1		
25	"A" 13+35	15'		W1-13	10 MPH	24"x24"	4.0			MOUNT BELOW 24
26	"A" 11+75	15'		W3-1	STOP AHEAD	36"x36"	9.0	1		
27	"B" 13+07	15'		W1-1L	←	36"x36"	9.0	1		
28	"B" 13+07	15'		W1-13	15 MPH	24"x24"	4.0			MOUNT BELOW 27

### Signing Summary Notes :

- SIGN LOCATIONS ARE APPROXIMATE ONLY AND ARE SUBJECT TO MINOR REVISIONS.
- POST LENGTHS SHALL BE DETERMINED AS PER STANDARD DRAWING S-05.00.
- ALL NEW SIGNS SHALL BE UNFRAMED UNLESS OTHERWISE NOTED.
- EXISTING SIGNS AND POSTS TO BE REMOVED SHALL BECOME PROPERTY OF THE CONTRACTOR.

### KEY TO SYMBOLS:

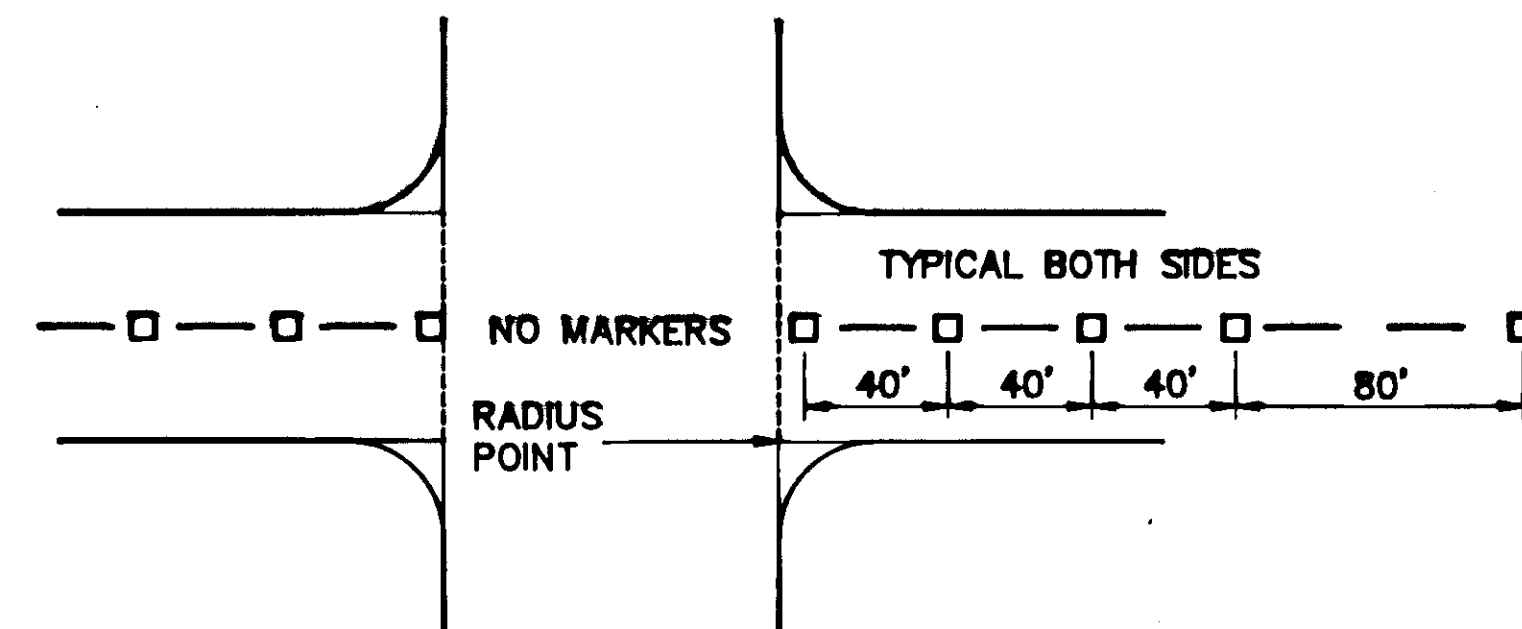
- TWO SIDED YELLOW MARKER
- TWO SIDED MARKER, WHITE FACING TRAFFIC, RED AWAY FROM TRAFFIC.

## GUARDRAIL SUMMARY

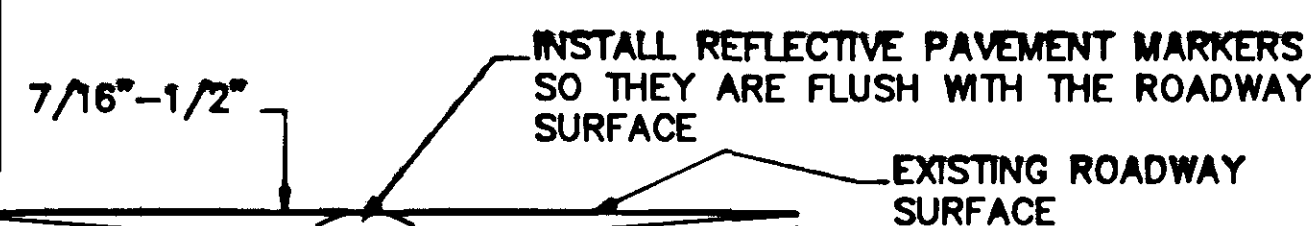
"A" Sta. to "A" Sta.	Length		Term. End	Remark
	Lt.	Rt.		
"AP" 11+00 "AP" 16+37.50		537.5	2	
"AP" 11+00 "AP" 16+37.50	537.5		2	
TOTAL		1075	4	

## DRIVEWAY / APPROACH SUMMARY

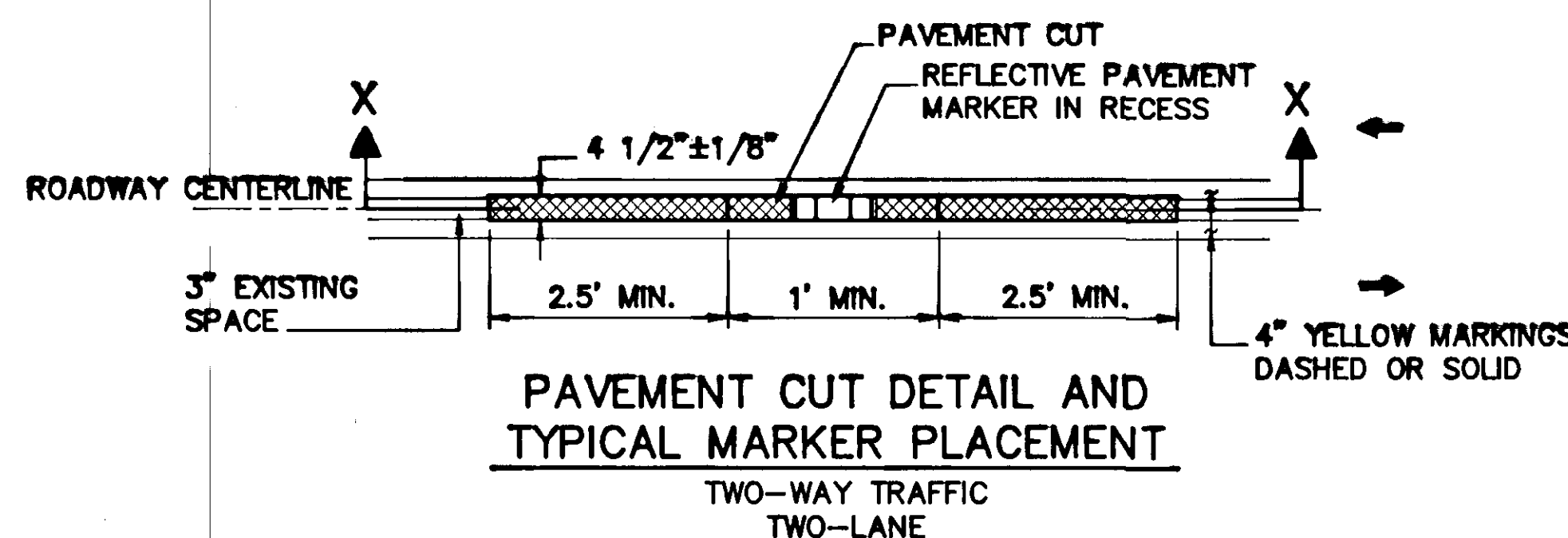
Station	Offset		Radius		Width	Remarks
	Lt.	Rt.	Lt.	Rt.		
"AP" 28+15	X		25'	25'	35'	Maintenance Shop
"AP" 29+50	X		25'	25'	35'	Maintenance Shop
"L" 44+00		X	25'	25'	20'	Remove & replace 18"x36" CMP
"L" 84+50		X	25'	25'	20'	Remove & replace 18"x36" CMP
"L" 109+20		X	25'	25'	20'	
"L" 113+62		X	25'	25'	20'	Remove & replace 18"x45" CMP



INTERSECTION APPROACH DETAIL



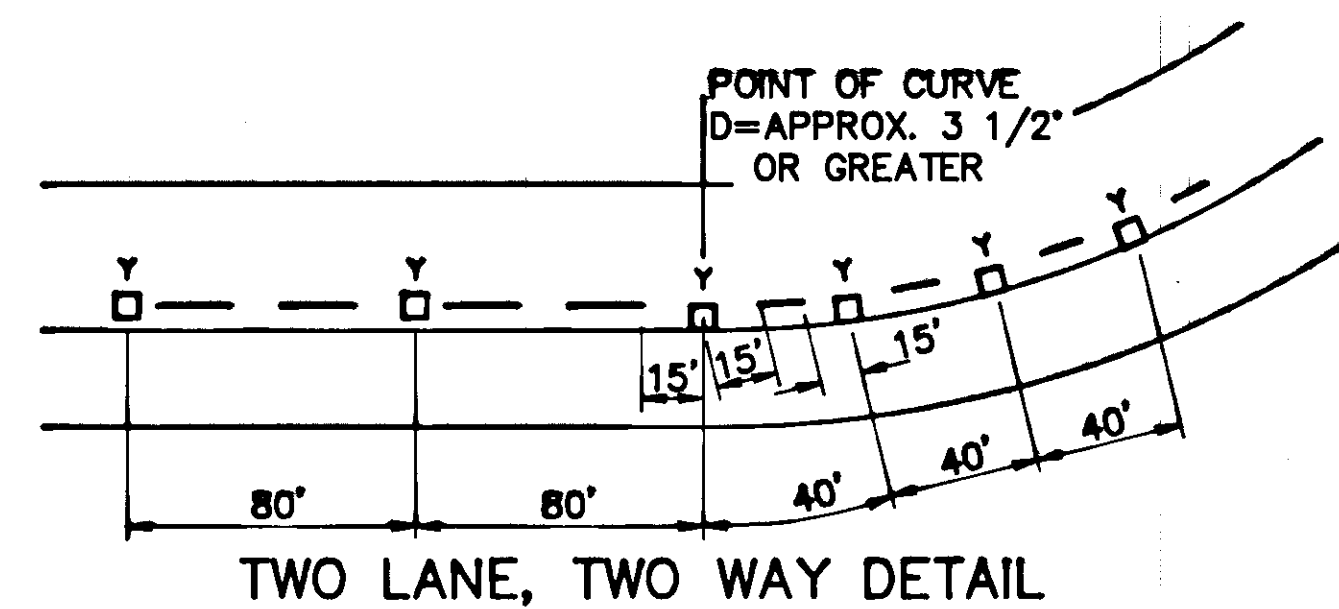
SECTION X-X



PAVEMENT CUT DETAIL AND TYPICAL MARKER PLACEMENT

## CULVERT SUMMARY

Station	18"φ	24"φ	48"φ	60"φ	7x5'-1"	96"φ	Culvert Removal & Disposal
	L.F.	L.F.	L.F.	L.F.	L.F.	L.F.	
"O" 10+25		112					18"x40'
"O" 15+00		60					
"O" 20+49			70				
"O" 35+00		64					
"A" 11+75		56					
"L" 40+73		70					24"x40'
"L" 44+00	36						18"x36'
"L" 44+62		62					24"x45'
"L" 48+05						140	
"L" 64+20							18"x60'
"L" 65+00		68					
"L" 76+20		64					24"x42'
"L" 81+40		60					18"x43'
"L" 84+50	36						18"x36'
"L" 91+26		60					18"x59'
"L" 95+50		72					
"L" 101+00		76					
"L" 107+74		100					18"x41'
"L" 111+14					74		6'x4'x38'
"L" 111+23					74		6'x4'x38'
"L" 113+84					78		6'x4'x46'
"L" 116+31		68					24"x40'
"L" 118+00		76					18"x54'
"L" 119+90		70					36"x40'
"L" 113+62	45						18"x45'
"AP" 14+20				100			
"AP" 15+20				88			
"AP" 16+00							48"x50'
"AP" 23+50							24"x60'
<b>TOTAL (L.F.)</b>	<b>117</b>	<b>1202</b>	<b>70</b>	<b>188</b>	<b>226</b>	<b>140</b>	<b>853</b>



TWO LANE, TWO WAY DETAIL

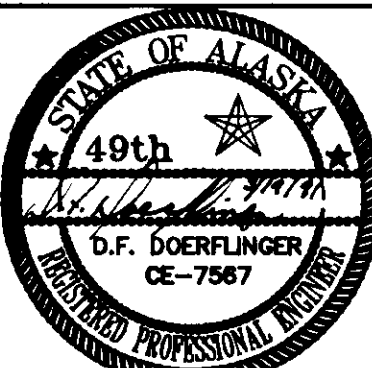
### NOTES :

- RECESSED MARKERS SHALL BE SPACED EVERY 80' ON TANGENT, AND EVERY 40' ON CURVES SHARPER THAN 3'.
- RPM QUANTITIES ARE ESTIMATED AND WILL VARY.
- RPM'S SHALL NOT BE PLACED IN INTERSECTIONS WITH PUBLIC ROADWAYS. RPM SPACING SHALL START OVER AT THE INTERSECTION BOUNDARY.
- ONLY PUBLIC ROADWAYS WILL BE MARKED AS AN INTERSECTION APPROACH. MARKERS SHALL BE PLACED BETWEEN EXISTING MARKINGS ON SECTIONS WITH DOUBLE YELLOW OR SOLID YELLOW+INTERMITTENT MARKINGS. ON SECTIONS WITH INTERMITTENT MARKINGS, OR WHERE MARKINGS ARE MISSING, MARKERS SHALL BE PLACED ON CENTERLINE OR THE LANE DIVIDING LINE.

## MONUMENT SUMMARY

Station	Location	Case
"O" 10+20	18.5 LT.	1
"O" 27+50	18.5 LT.	1
"O" 33+00	18.5 LT.	1
"L" 58+50	18.5 LT.	1
"L" 77+50	18.5 LT.	1
"L" 87+50	18.5 RT.	1
"L" 91+00	18.5 RT.	1
"L" 95+00	18.5 RT.	1
"L" 101+50	18.5 LT.	1
"L" 106+00	18.5 LT.	1
"L" 111+50	18.5 LT.	1
"L" 121+00	18.5 RT.	1
"AP" 29+00	14.5 RT.	1

### ENGINEER'S SEAL



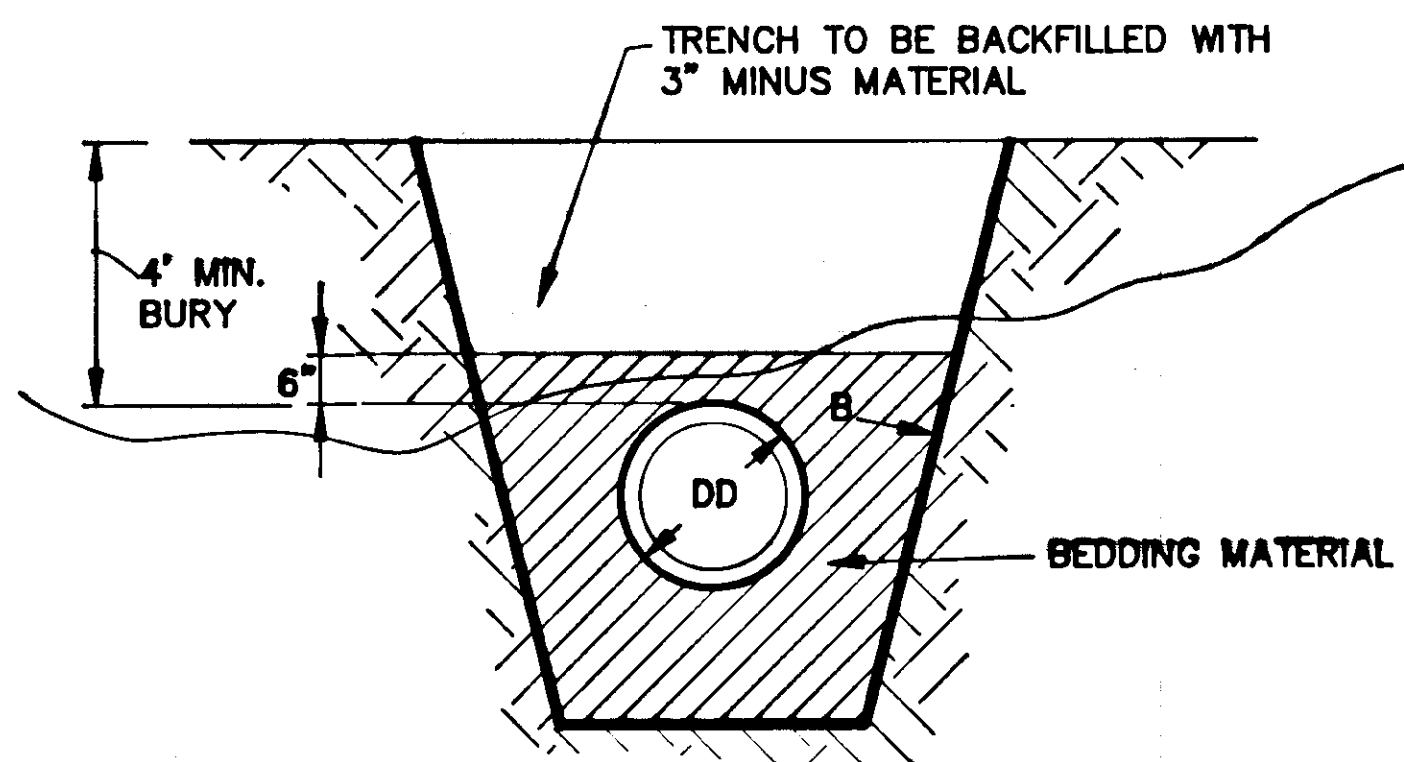
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AG-BUILT

**RECORD OF REVISIONS**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

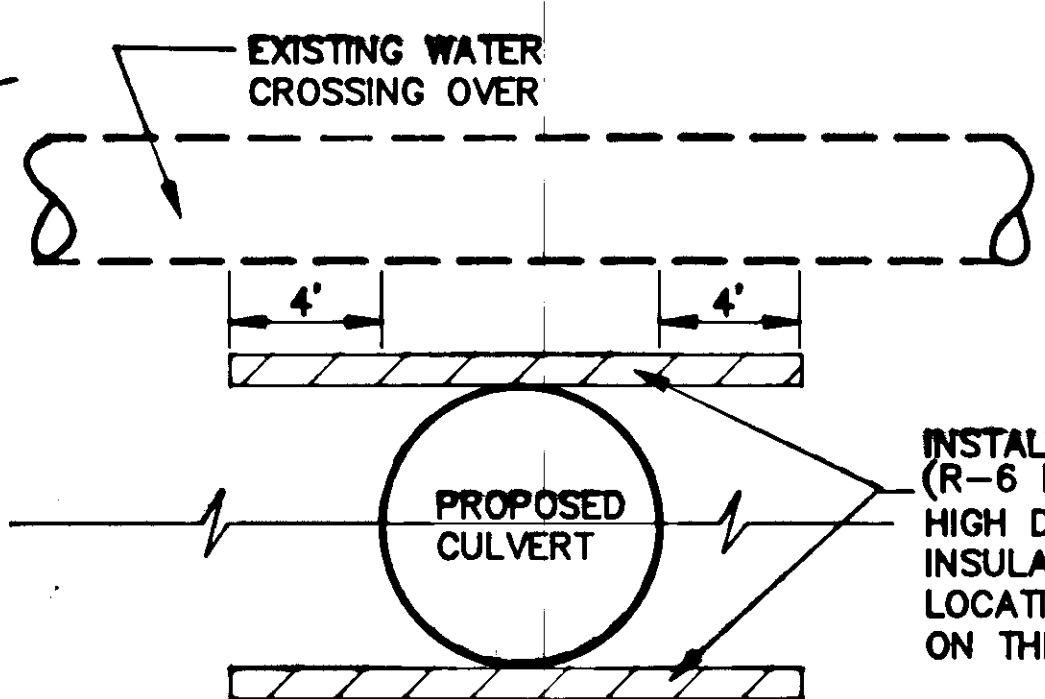
KLAWOCK TO AIRPORT  
RS-0929(1) - 69275  
**SIGN SUMMARY, REFLECTIVE PAVEMENT MARKERS,  
INTERSECTION APPROACH DETAILS,  
MISC. SUMMARIES**

ALASKA  
DESIGNED BY: P. JONES  
DRAWN BY: AutoCAD / B.W.B.  
CHECKED BY: P. BEDNAROWCZ  
PROJECT No. NONE  
DATE: SEPT. 1990  
SHEET 4 OF 29

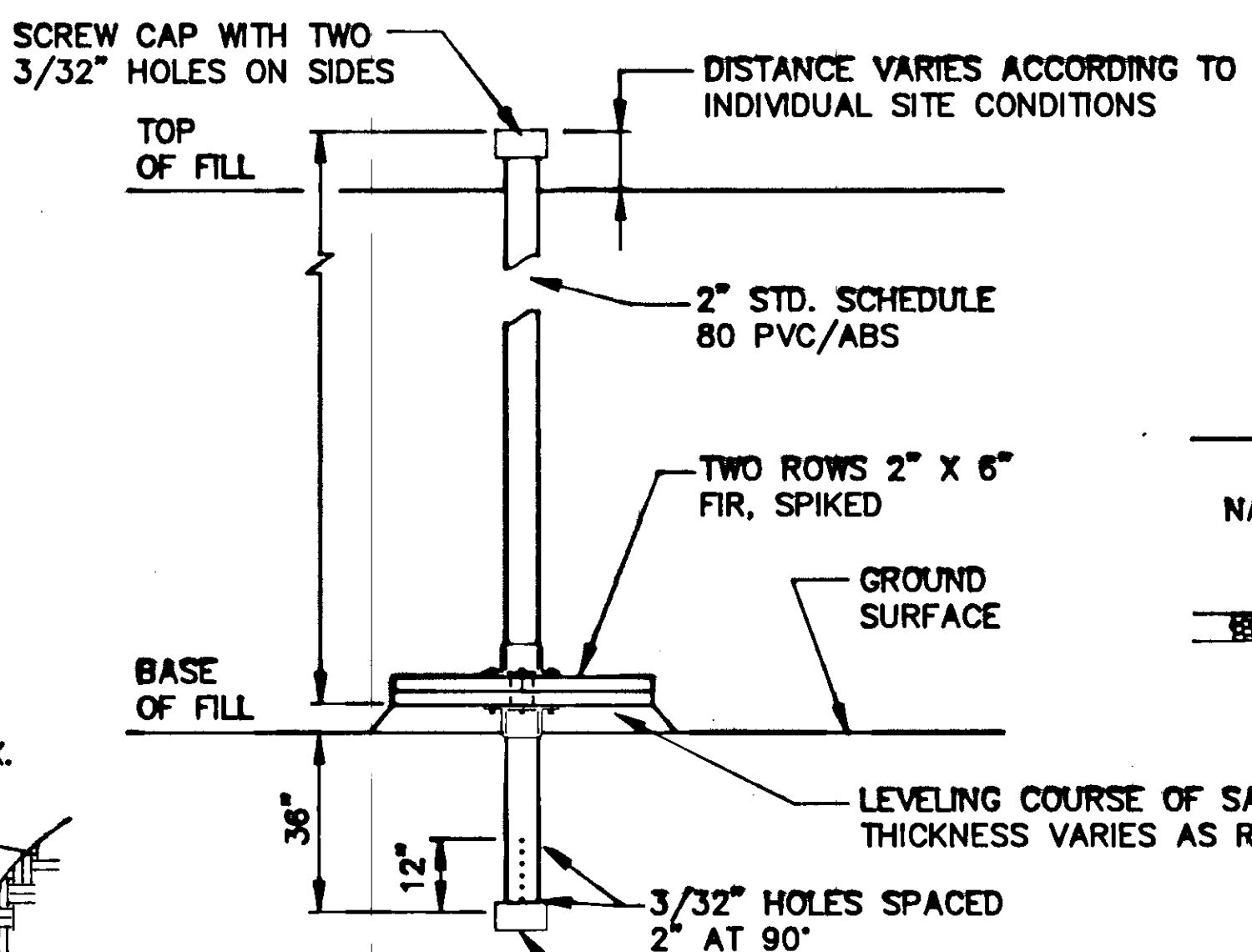


- A. TRENCH BEDDING MATERIAL TO BE CAREFULLY TAMPED AND COMPACTED AROUND AND UNDER PIPE SO THAT PIPE IS FIRMLY SUPPORTED THROUGHOUT ITS ENTIRE LENGTH PRIOR TO BACKFILLING OF TRENCH.
- B. TRENCH WALL SLOPES WILL VARY WITH SOIL STRENGTH AND CHARACTERISTICS, SLOPES TO CONFORM TO SAFETY STANDARDS.
- C. BEDDING MATERIAL SHALL NOT BE GREATER THAN 2" DIAMETER.

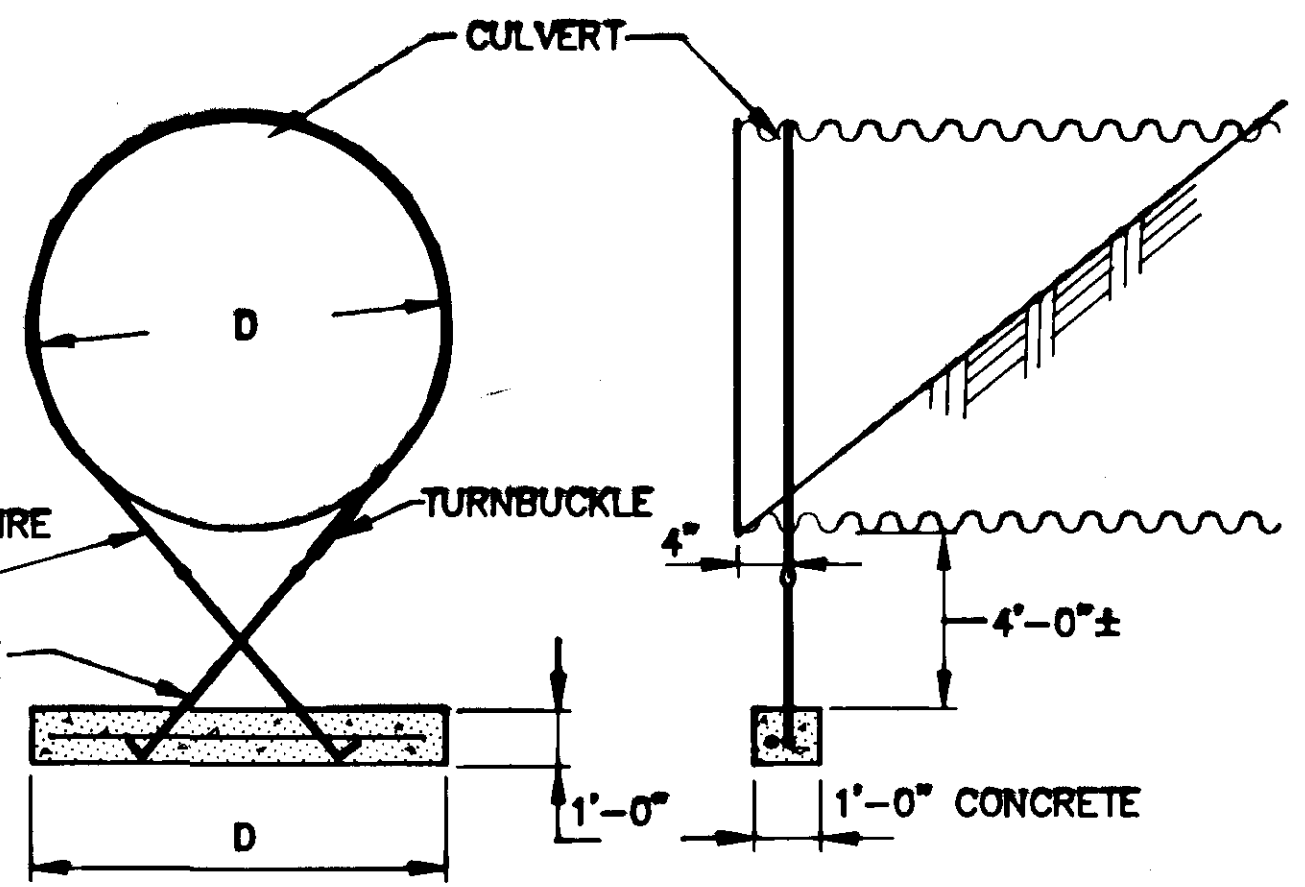
**WATER TRENCH DETAIL**



**INSULATION DETAIL**



**SETTLEMENT AND PIEZOMETER GAUGE**



**CULVERT DEADMAN DETAIL**

ADJUSTMENT OF VALVE BOX SUMMARY			
STATION	OFFSET	LT./RT.	REMARKS
"O" 10+14.6	7.55	LT.	
"L" 72+60	22	LT.	
"L" 115+70	22	LT.	
"AP" 28+12	15	LT.	

12 INCH GATE VALVE SUMMARY		
STATION	OFFSET	REMARKS
"L" 82+50	40' LT.	
"L" 94+75	32' RT.	
"L" 101+00	35' RT.	
"L" 110+55	24' LT.	

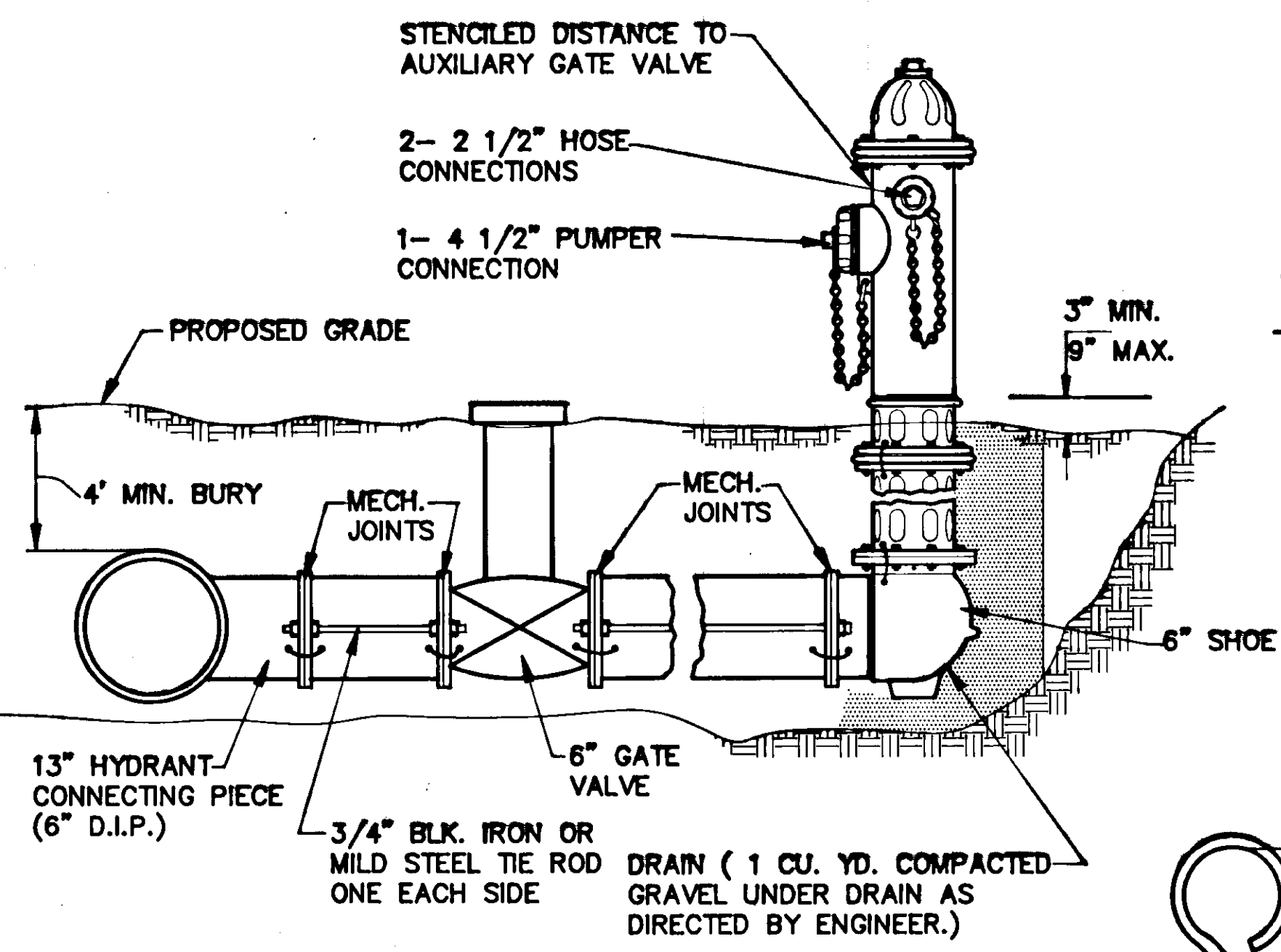
FIRE HYDRANT RELOCATION SUMMARY			
STATION	EXIST. LOCATION	PROPOSED LOCATION	REMARKS
"O" 11+06.00	16.8' RT.	35' RT.	
"L" 44+80	60' RT.	110' LT.	
"L" 43+50	30' LT.	10' "L" 16+40,	14.5' LT.

MEMBRANE WATERPROOFING		
STATION TO STATION	S.Y.	
"L" 86+56	"L" 87+44	294

INSULATION BOARD SUMMARY		
STATION	SQUARE FEET	REMARKS
"L" 40+73	64'	CULVERT CROSSING
"L" 91+26	64'	CULVERT CROSSING
"L" 107+26	64'	CULVERT CROSSING
"L" 111+14	96'	ARCH PIPE CROSSING
"L" 111+23	96'	ARCH PIPE CROSSING
"L" 113+84	96'	ARCH PIPE CROSSING
"AP" 27+00 TO 29+50	1000'	
<b>TOTAL</b>	<b>1480 L.F.</b>	

MANHOLE RECONSTRUCTION SUMMARY		
STATION	OFFSET	REMARKS
"O" 10+39.16	19.35 LT.	
"O" 11+35.38	16.54 LT.	
"O" 35+15	45.0 LT.	A LINE INTERSECTION
"AP" 28+25	6.0 LT.	

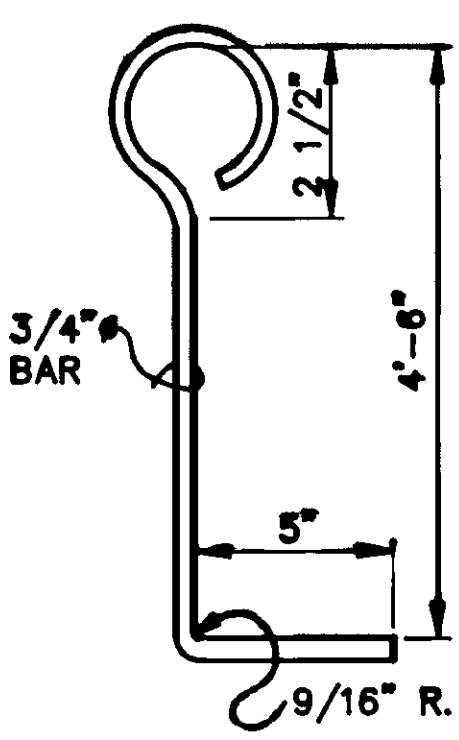
12" D.I. WATER MAIN RELOCATION SUMMARY		
BEGIN STATION	END STATION	LENGTH
"L" 82+50	"L" 86+50	400 L.F.
"L" 87+44	"L" 94+75	731 L.F.
<b>TOTAL</b>		<b>1,131 L.F.</b>



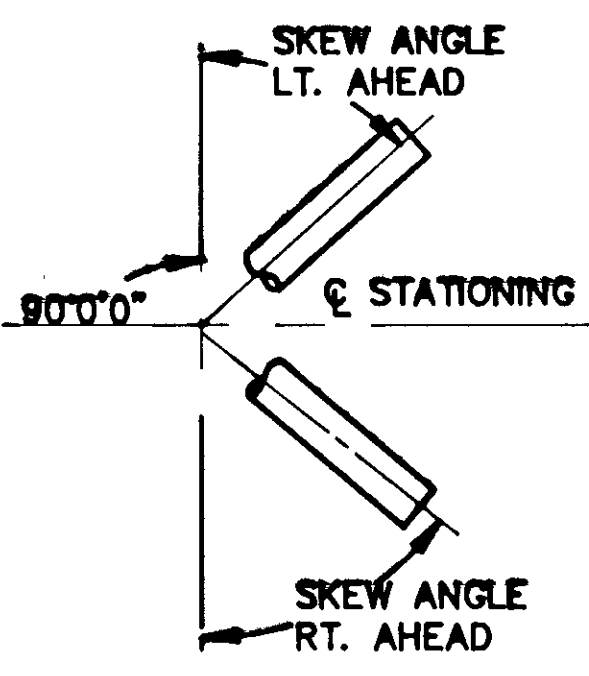
**HYDRANT INSTALLATION NOTES**

1. HYDRANT BARREL MUST BE INSTALLED PLUMB AND THE LEG MUST BE INSTALLED LEVEL.
2. DRAIN PLUG TO BE REMOVED BY CONTRACTOR.
3. ALL HYDRANTS SHALL BE PAINTED CATERPILLAR YELLOW.
4. AUXILIARY GATE VALVE BOX TO BE INSTALLED ACCORDING TO DETAIL FOR TYPICAL VALVE BOX.

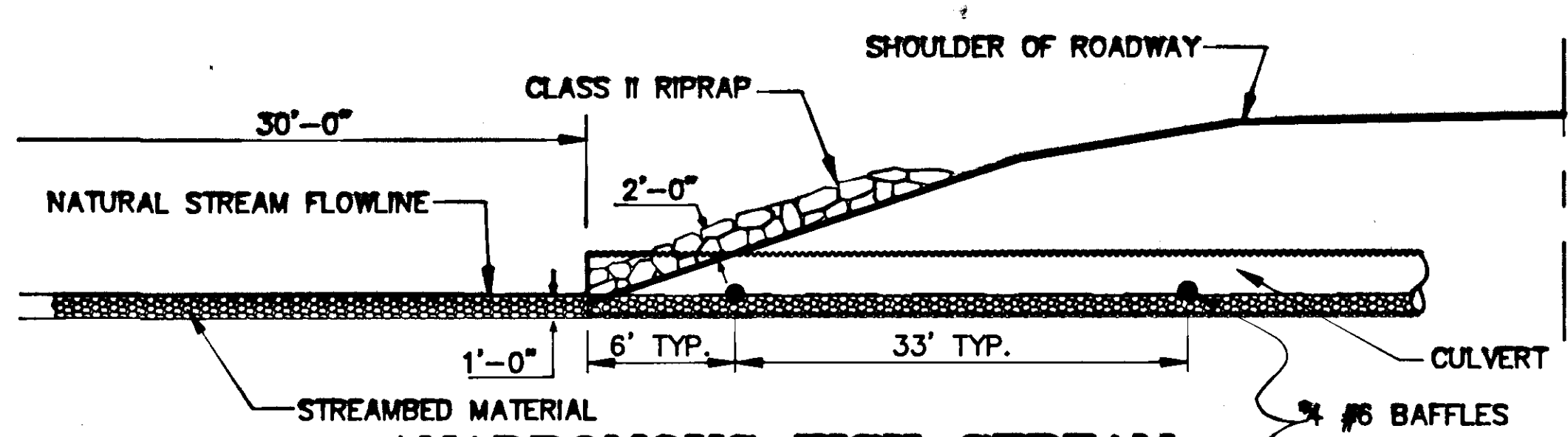
**FIRE HYDRANT DETAIL**



**EYE BOLT DETAIL**

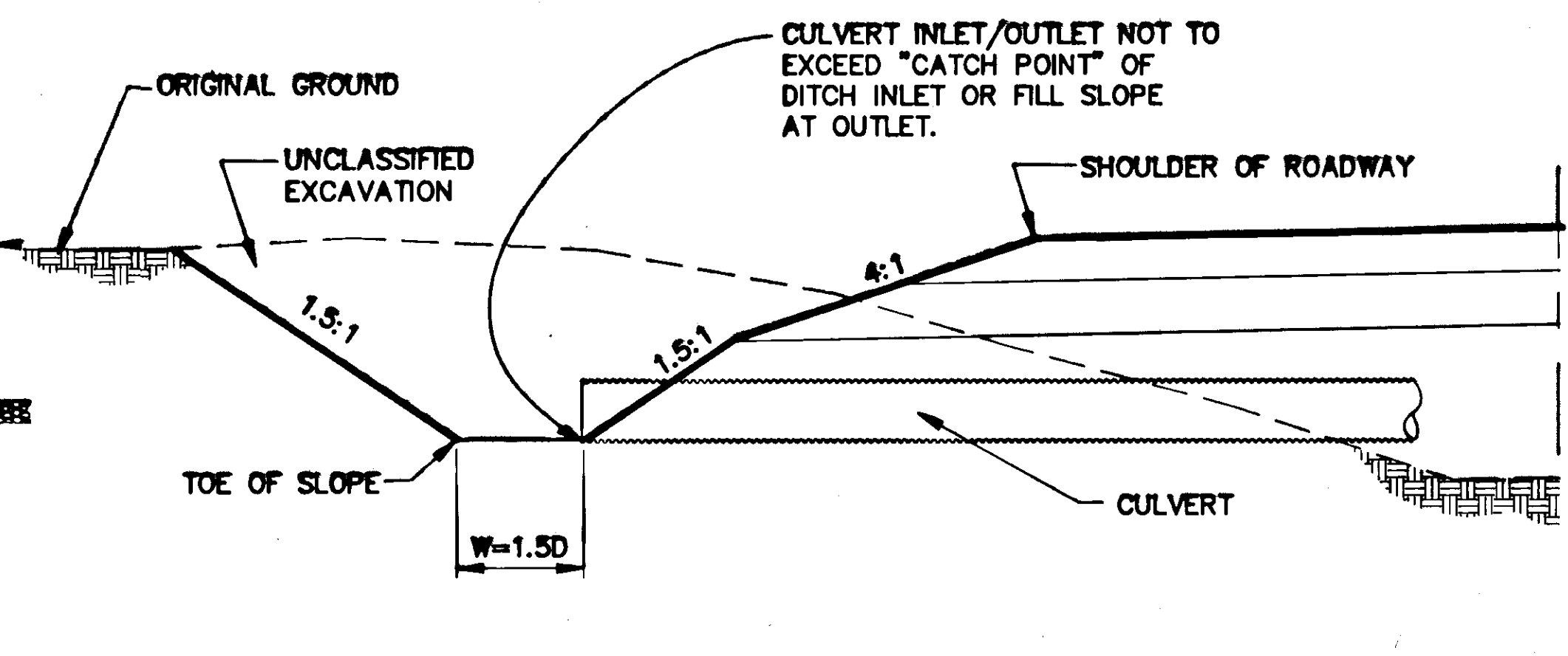


**CULVERT SKEW ANGLE**

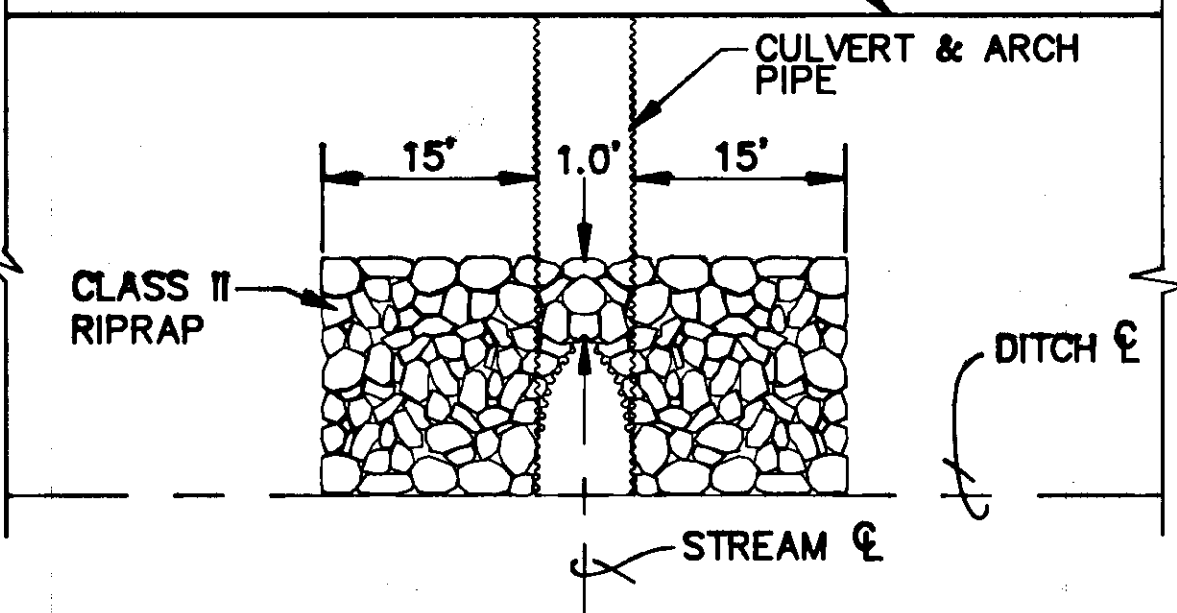


**ANADROMOUS FISH STREAM**

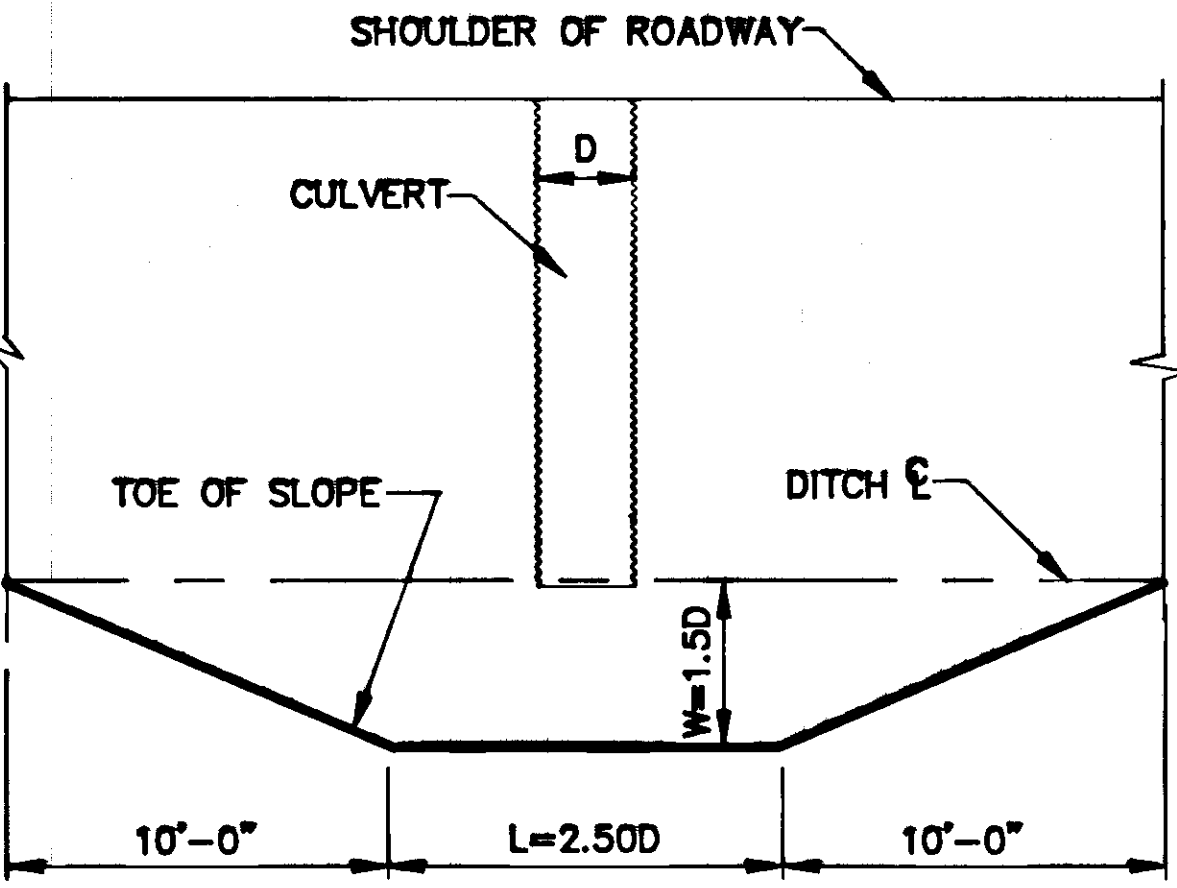
- STA. = "L" 48+80
- "L" 111+14
- "L" 111+23
- "L" 113+84
- "AP" 14+20
- "AP" 15+20



**TYPICAL SECTION**



**PLAN VIEW**



**CULVERT INLET DETAIL**

**PLAN VIEW**

BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS BUILT

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

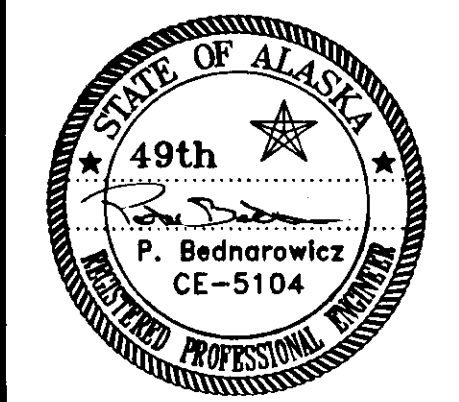
KLAWOCK

KLAWOCK TO AIRPORT  
 RS-0929(1)  
 WATERLINE AND DRAINAGE DETAILS

ALASKA

DESIGNED BY: F. MURPHY  
 DRAWN BY: AUTOCADD/CSA  
 CHECKED BY: P. BEDNAROWICZ

PROJECT NO. RS-0929(1)  
 DATE:  
 SHEET 5 OF 29



**KLAWOCK TO AIRPORT**

**ADDITIONAL SYMBOLS**

- ⊙ MONUMENT (PROPERTY CORNER)
- ⊙ MONUMENT (PROPERTY CORNER)
- #-#-# EASEMENT LINE
- ▨ OBLITERATE OLD ROADWAY

**Curve Data**

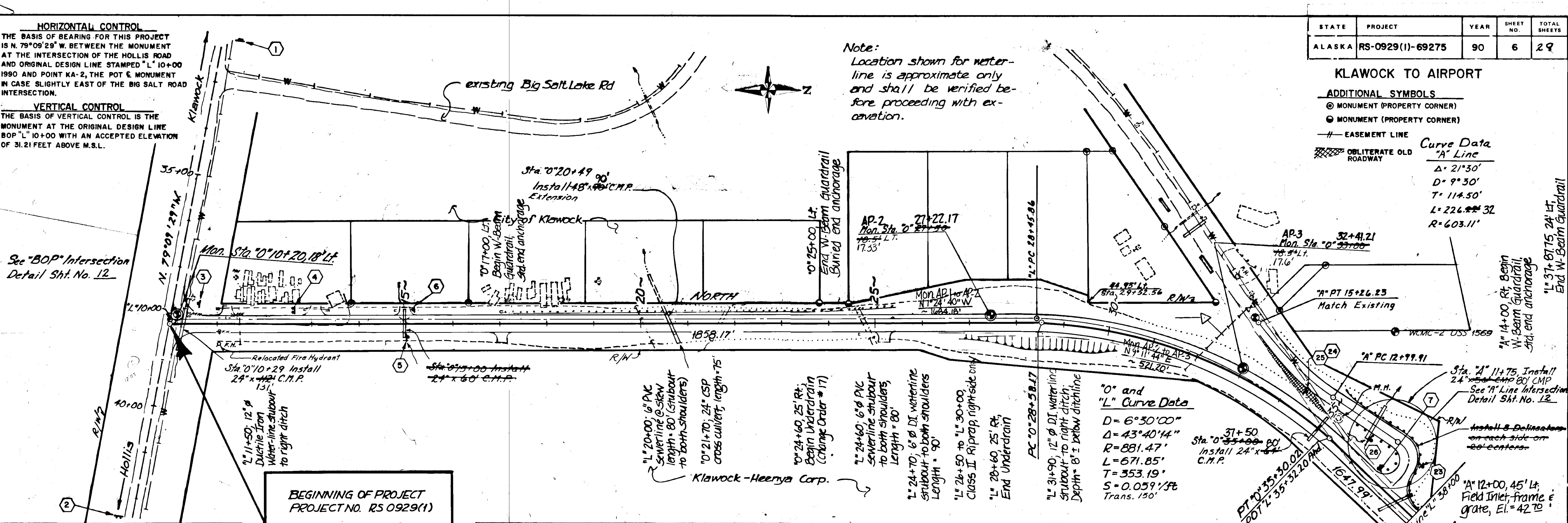
**"A" Line**

Δ	21°30'
D	9°30'
T	114.50'
L	226.22' 32
R	603.11'

Note:  
Location shown for water-line is approximate only and shall be verified before proceeding with excavation.

**HORIZONTAL CONTROL**  
THE BASIS OF BEARING FOR THIS PROJECT IS N. 79°09'29" W. BETWEEN THE MONUMENT AT THE INTERSECTION OF THE HOLLIS ROAD AND ORIGINAL DESIGN LINE STAMPED "L" 10+00 1990 AND POINT KA-2, THE POT C MONUMENT IN CASE SLIGHTLY EAST OF THE BIG SALT ROAD INTERSECTION.

**VERTICAL CONTROL**  
THE BASIS OF VERTICAL CONTROL IS THE MONUMENT AT THE ORIGINAL DESIGN LINE BOP "L" 10+00 WITH AN ACCEPTED ELEVATION OF 31.21 FEET ABOVE M.S.L.

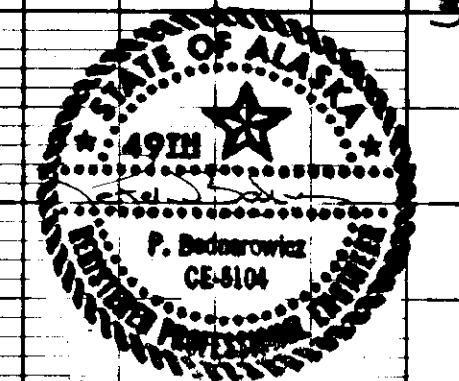
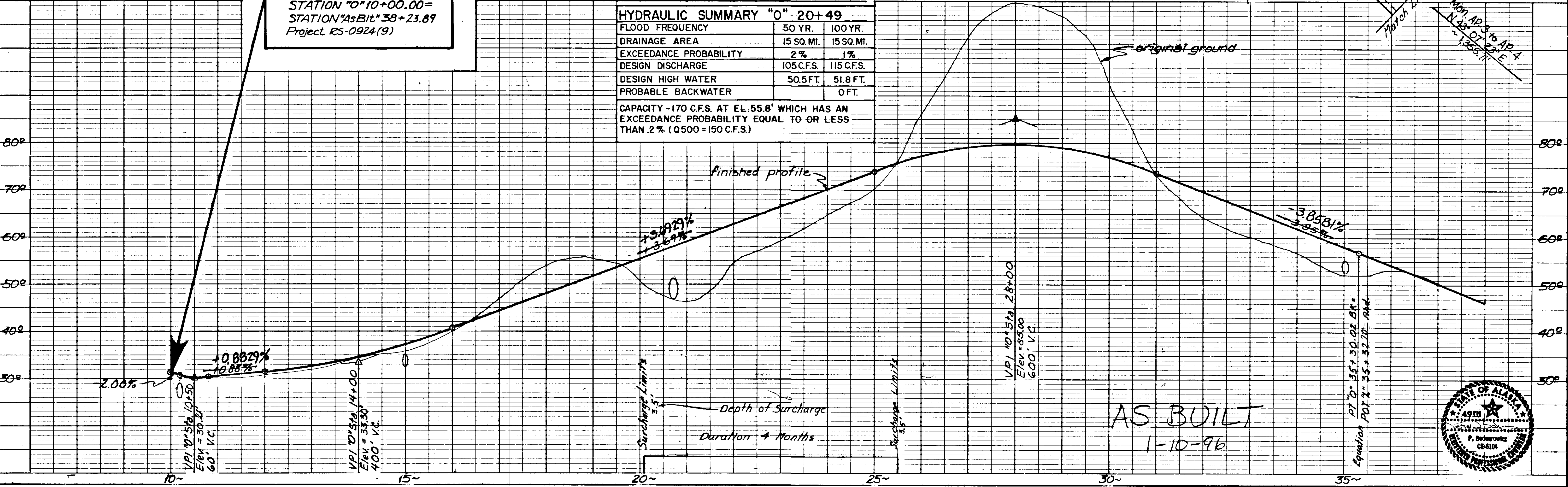


**BEGINNING OF PROJECT**  
PROJECT NO. RS 0929(1)  
STATION "0" 10+00.00=  
STATION "As Bilt" 38+23.89  
Project RS-0924(9)

**HYDRAULIC SUMMARY "0" 20+49**

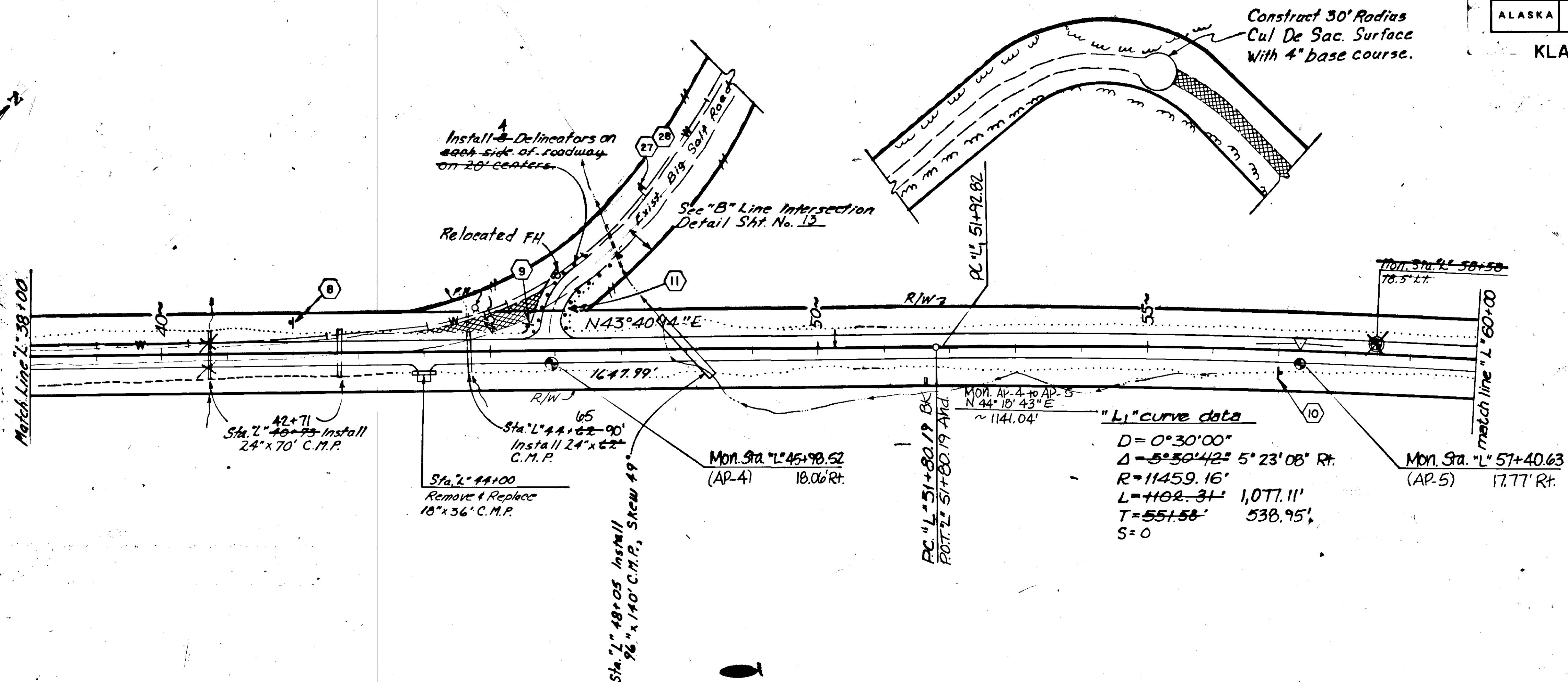
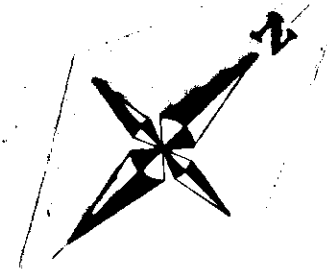
FLOOD FREQUENCY	50 YR.	100 YR.
DRAINAGE AREA	15 SQ. MI.	15 SQ. MI.
EXCEEDANCE PROBABILITY	2%	1%
DESIGN DISCHARGE	105 C.F.S.	115 C.F.S.
DESIGN HIGH WATER	50.5 FT.	51.8 FT.
PROBABLE BACKWATER		0 FT.

CAPACITY - 170 C.F.S. AT EL. 55.8' WHICH HAS AN EXCEEDANCE PROBABILITY EQUAL TO OR LESS THAN .2% (Q500 = 150 C.F.S.)

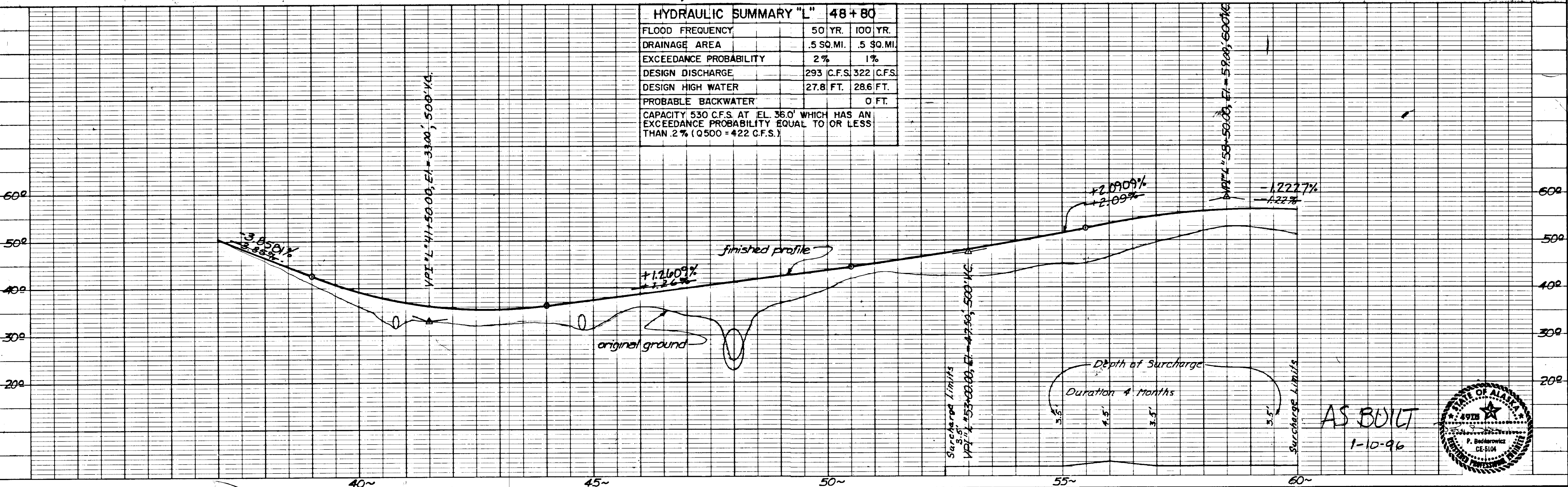


STATE	PROJECT	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	RS-0929(1)-69275	90	7	29

KLAWOCK TO AIRPORT

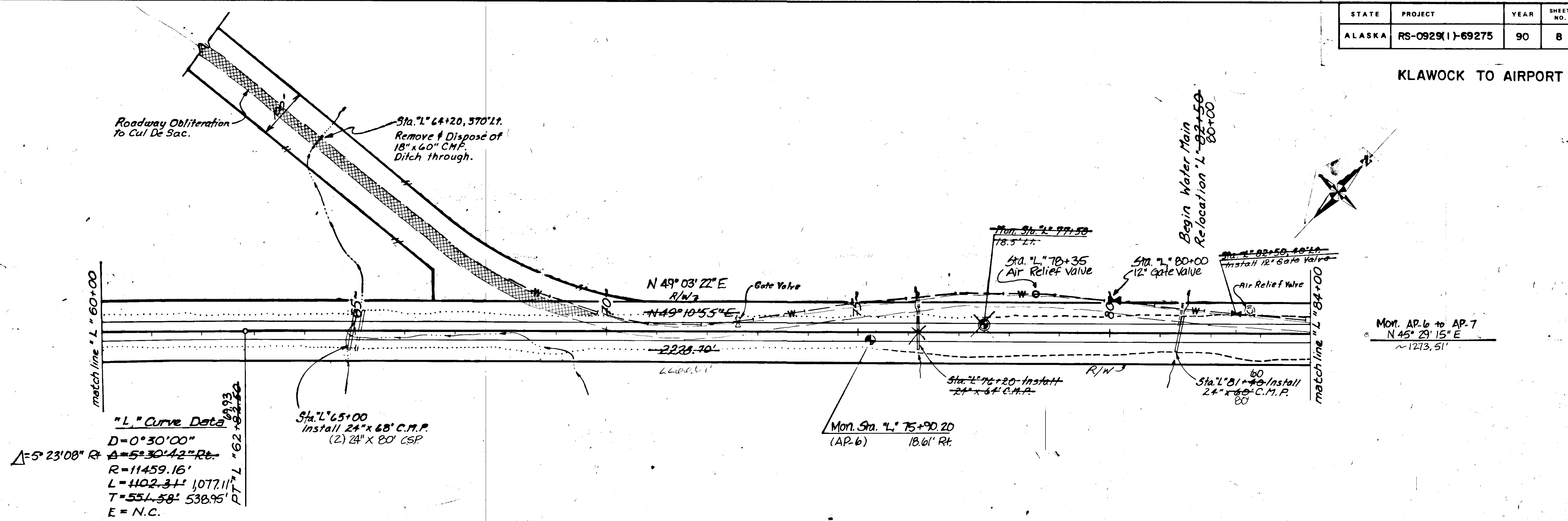


HYDRAULIC SUMMARY "L" 48+80		
FLOOD FREQUENCY	50 YR.	100 YR.
DRAINAGE AREA	.5 SQ. MI.	.5 SQ. MI.
EXCEEDANCE PROBABILITY	2%	1%
DESIGN DISCHARGE	293 C.F.S.	322 C.F.S.
DESIGN HIGH WATER	27.8 FT.	28.6 FT.
PROBABLE BACKWATER		0 FT.
CAPACITY 530 C.F.S. AT EL. 36.0' WHICH HAS AN EXCEEDANCE PROBABILITY EQUAL TO OR LESS THAN 2% (Q500 = 422 C.F.S.)		

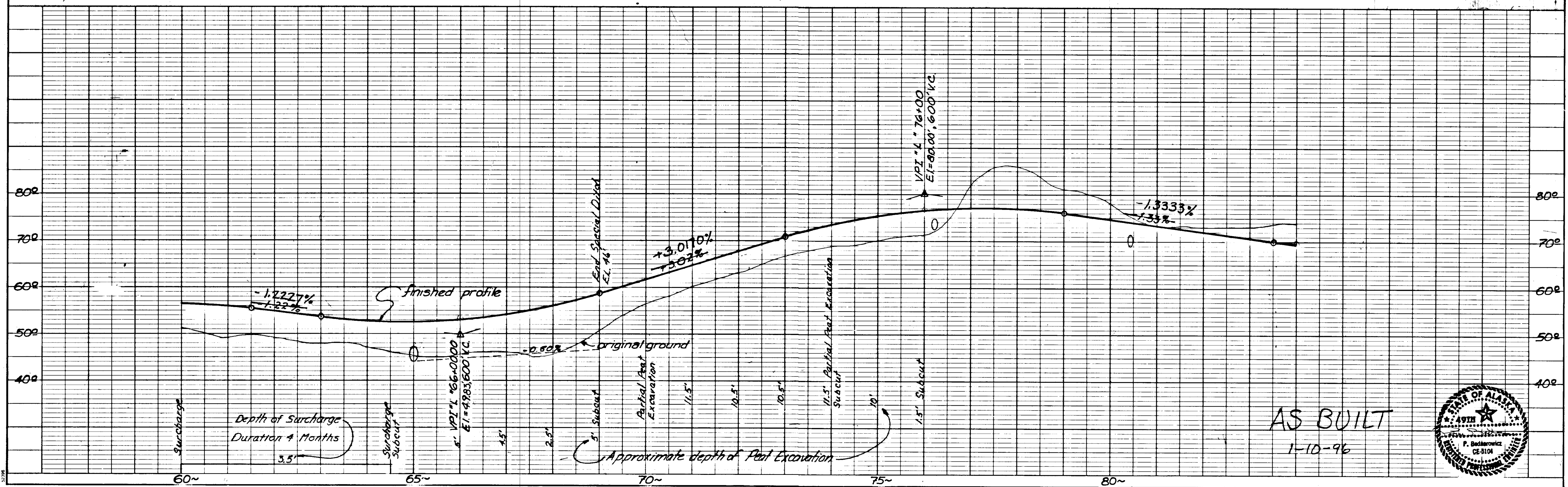


STATE	PROJECT	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	RS-0929(1)-69275	90	8	29

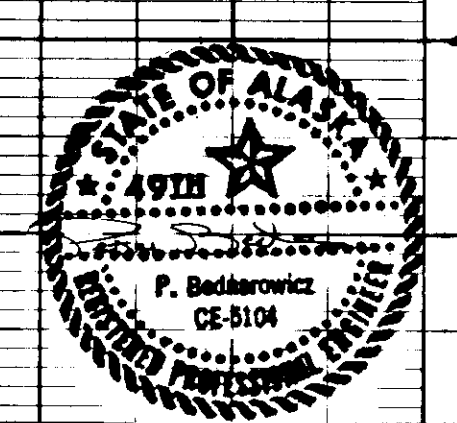
KLAWOCK TO AIRPORT



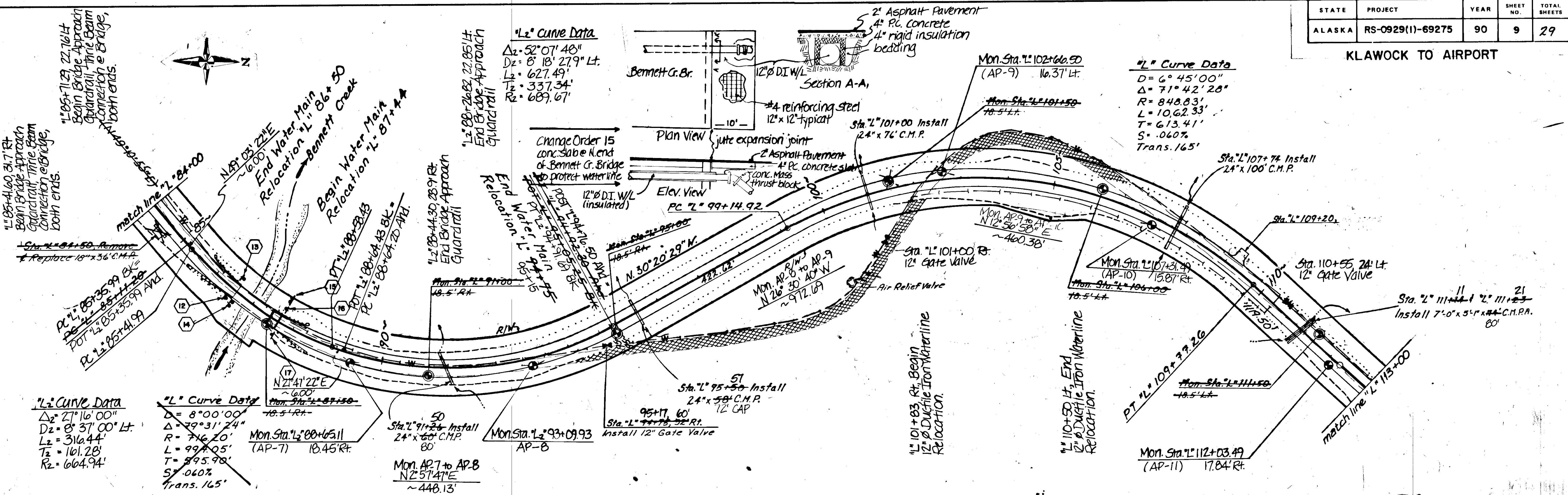
"L" Curve Data  
 $\Delta = 5^{\circ} 23' 00''$  Rt.  
 $D = 0^{\circ} 30' 00''$   
 $A = 5^{\circ} 30' 42''$  Rt.  
 $R = 11459.16'$   
 $L = 1102.31'$  1077.11'  
 $T = 554.58'$  538.95'  
 $E = N.C.$



AS BUILT  
1-10-96



KLAWOCK TO AIRPORT



**"L" Curve Data**  
 $\Delta = 27^\circ 16' 00''$   
 $D = 8^\circ 37' 00''$  Lt.  
 $L = 316.44'$   
 $T = 161.28'$   
 $R = 664.94'$

**"L" Curve Data**  
 $\Delta = 8^\circ 00' 00''$   
 $D = 79^\circ 31' 24''$   
 $R = 776.70'$   
 $L = 997.05'$   
 $T = 595.98'$   
 $S = .060\%$   
 $Trans. 165'$

**"L" Curve Data**  
 $D = 6^\circ 45' 00''$   
 $\Delta = 71^\circ 42' 28''$   
 $R = 848.83'$   
 $L = 1062.33'$   
 $T = 613.41'$   
 $S = .060\%$   
 $Trans. 165'$

HYDRAULIC SUMMARY "L" III+14

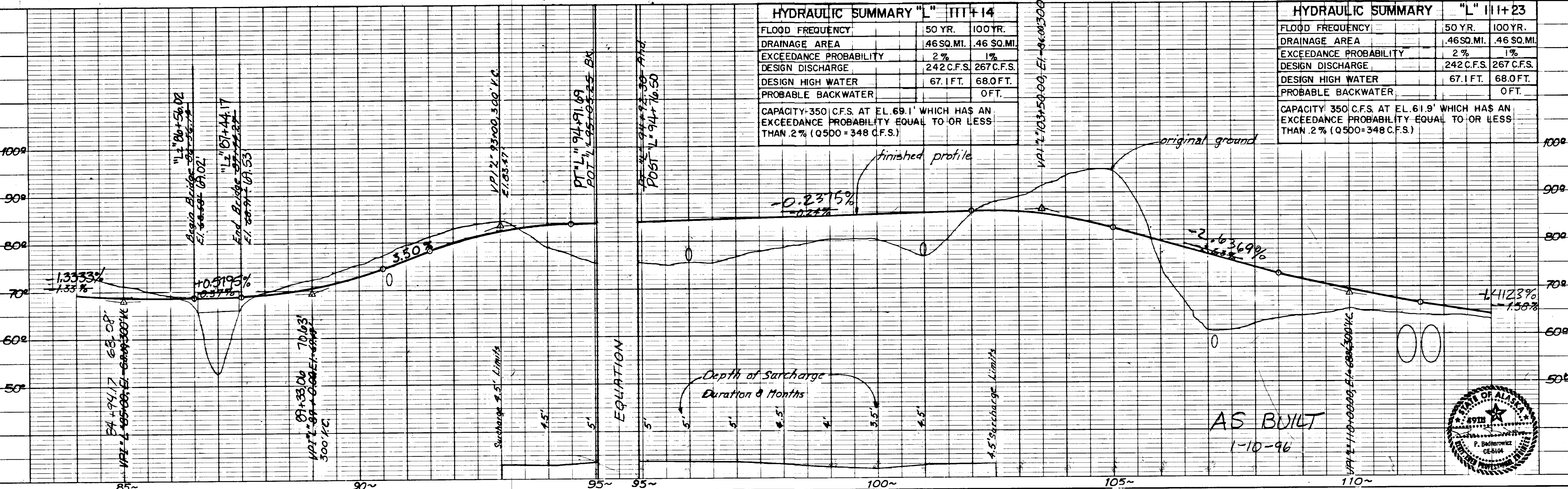
FLOOD FREQUENCY	50 YR.	100 YR.
DRAINAGE AREA	46 SQ. MI.	46 SQ. MI.
EXCEEDANCE PROBABILITY	2%	1%
DESIGN DISCHARGE	242 C.F.S.	267 C.F.S.
DESIGN HIGH WATER	67.1 FT.	68.0 FT.
PROBABLE BACKWATER		0 FT.

CAPACITY 350 C.F.S. AT EL. 69.1' WHICH HAS AN EXCEEDANCE PROBABILITY EQUAL TO OR LESS THAN 2% (Q500 = 348 C.F.S.)

HYDRAULIC SUMMARY "L" III+23

FLOOD FREQUENCY	50 YR.	100 YR.
DRAINAGE AREA	46 SQ. MI.	46 SQ. MI.
EXCEEDANCE PROBABILITY	2%	1%
DESIGN DISCHARGE	242 C.F.S.	267 C.F.S.
DESIGN HIGH WATER	67.1 FT.	68.0 FT.
PROBABLE BACKWATER		0 FT.

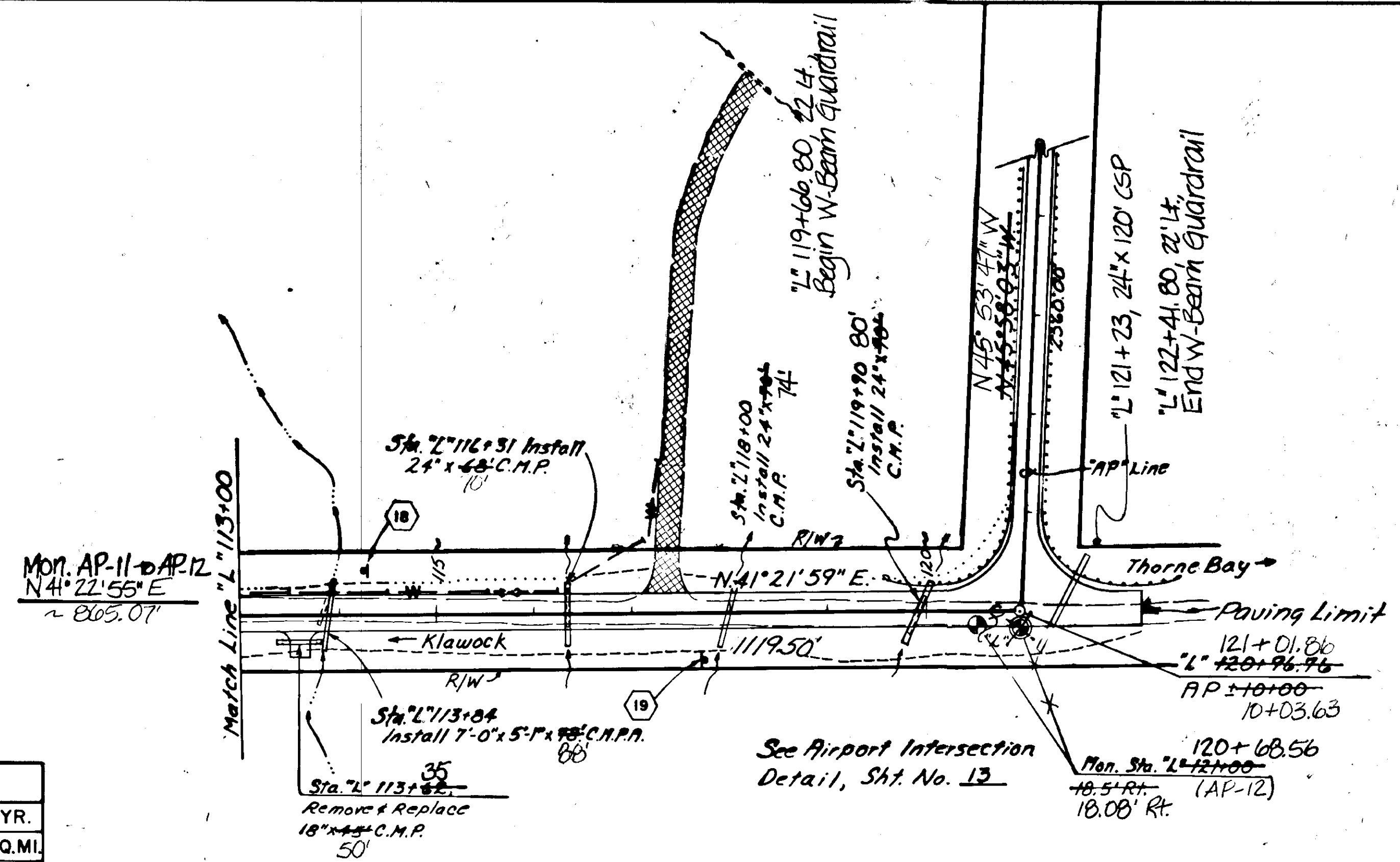
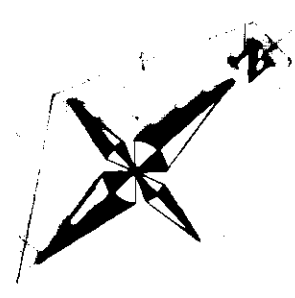
CAPACITY 350 C.F.S. AT EL. 69.1' WHICH HAS AN EXCEEDANCE PROBABILITY EQUAL TO OR LESS THAN 2% (Q500 = 348 C.F.S.)



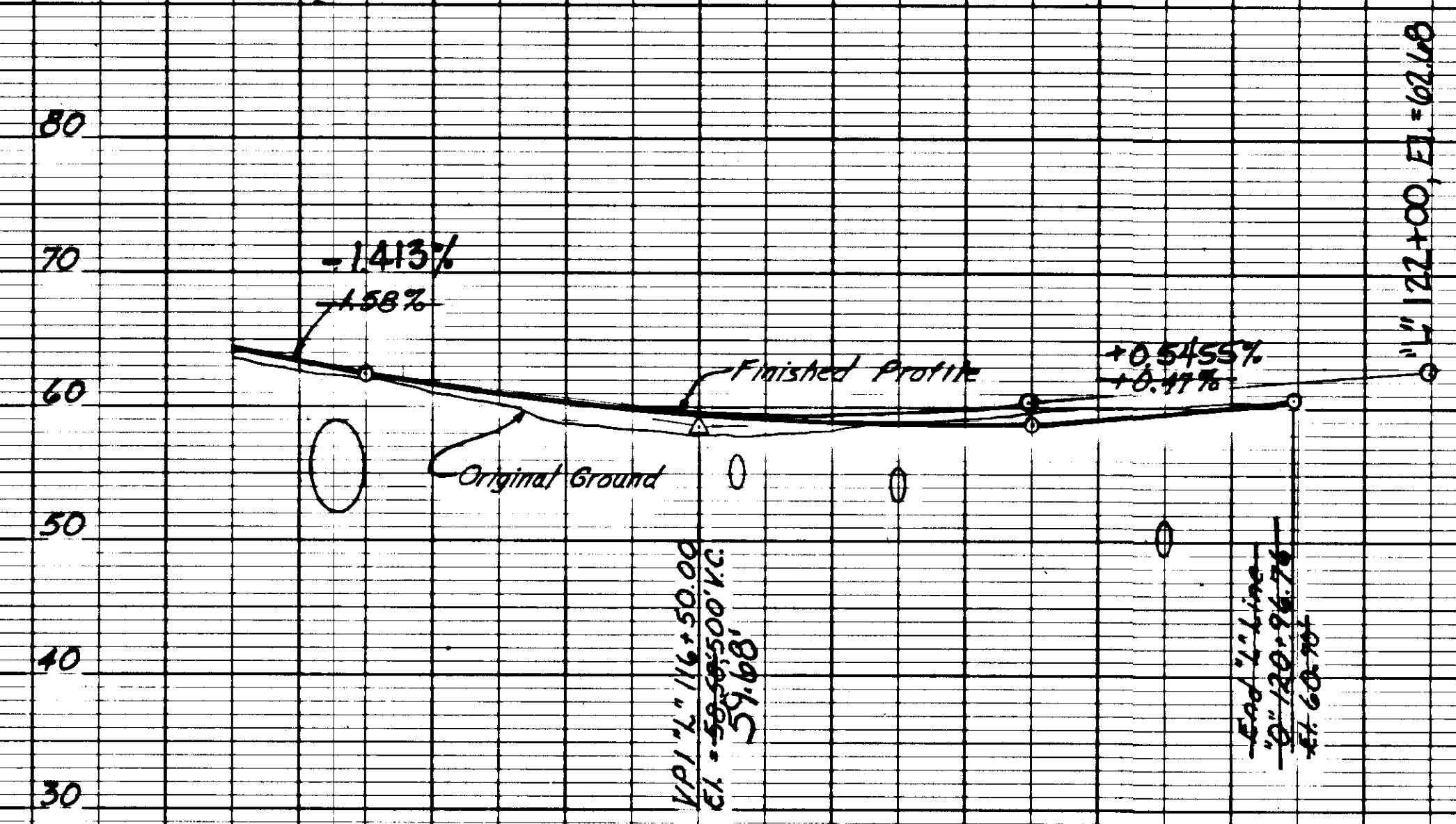
AS BUILT  
1-10-96



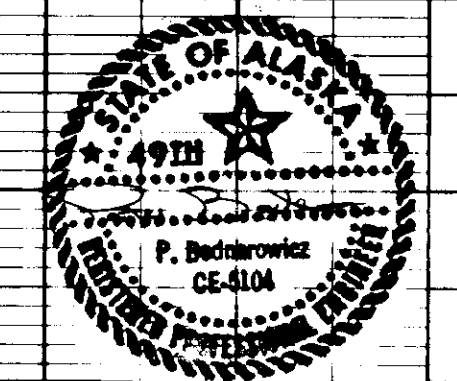
KLAWOCK TO AIRPORT



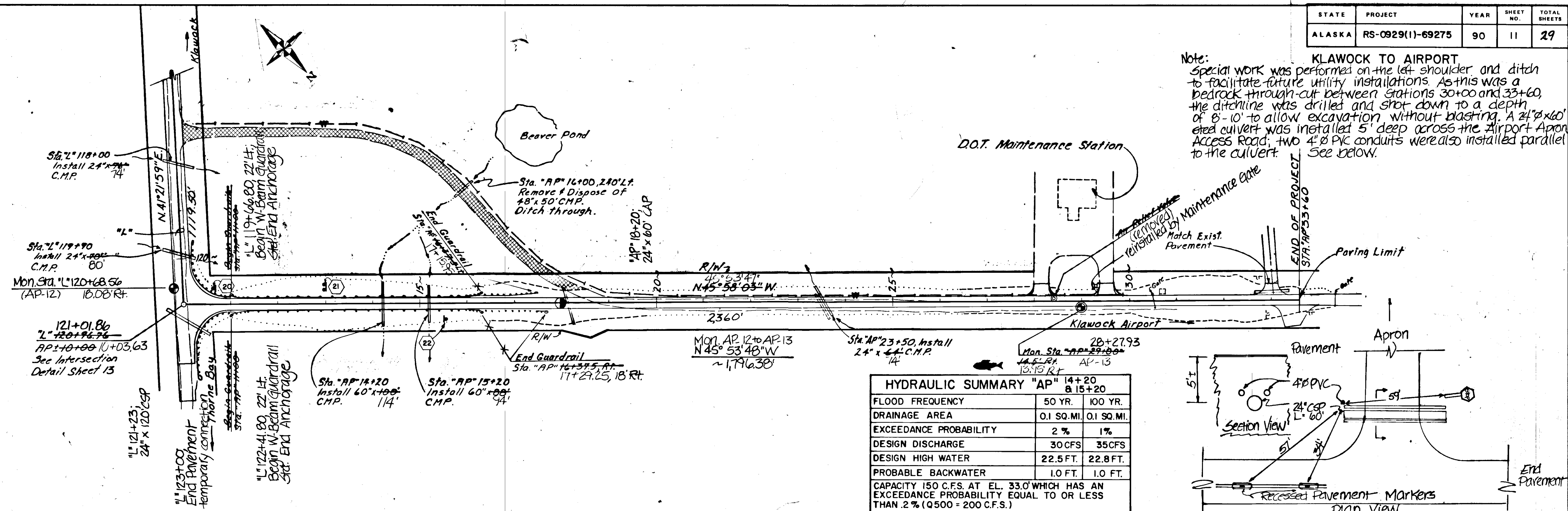
HYDRAULIC SUMMARY "L" 113+84		
FLOOD FREQUENCY	50 YR.	100 YR.
DRAINAGE AREA	.5 SQ.MI.	.5 SQ.MI.
EXCEEDANCE PROBABILITY	2%	1%
DESIGN DISCHARGE	121 C.F.S.	133 C.F.S.
DESIGN HIGH WATER	62.6 FT.	63.5 FT.
PROBABLE BACKWATER	0 FT.	
CAPACITY 175 C.F.S. AT EL. 64.6' WHICH HAS AN EXCEEDANCE PROBABILITY EQUAL TO OR LESS THAN 2% (Q 500 = 474 C.F.S.)		



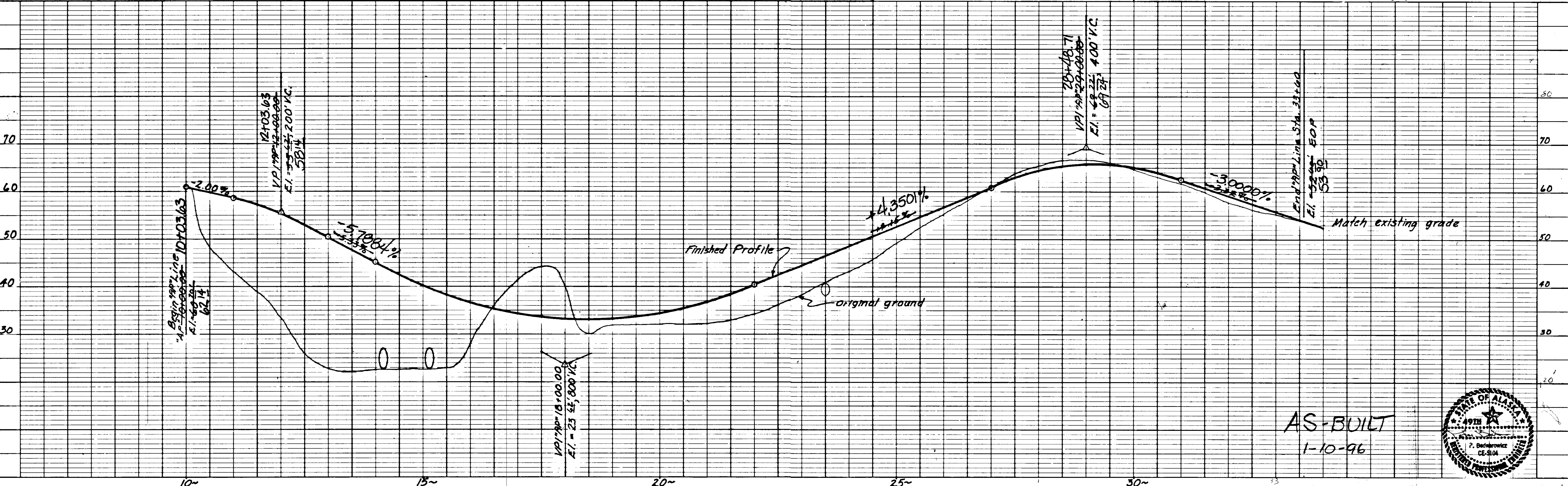
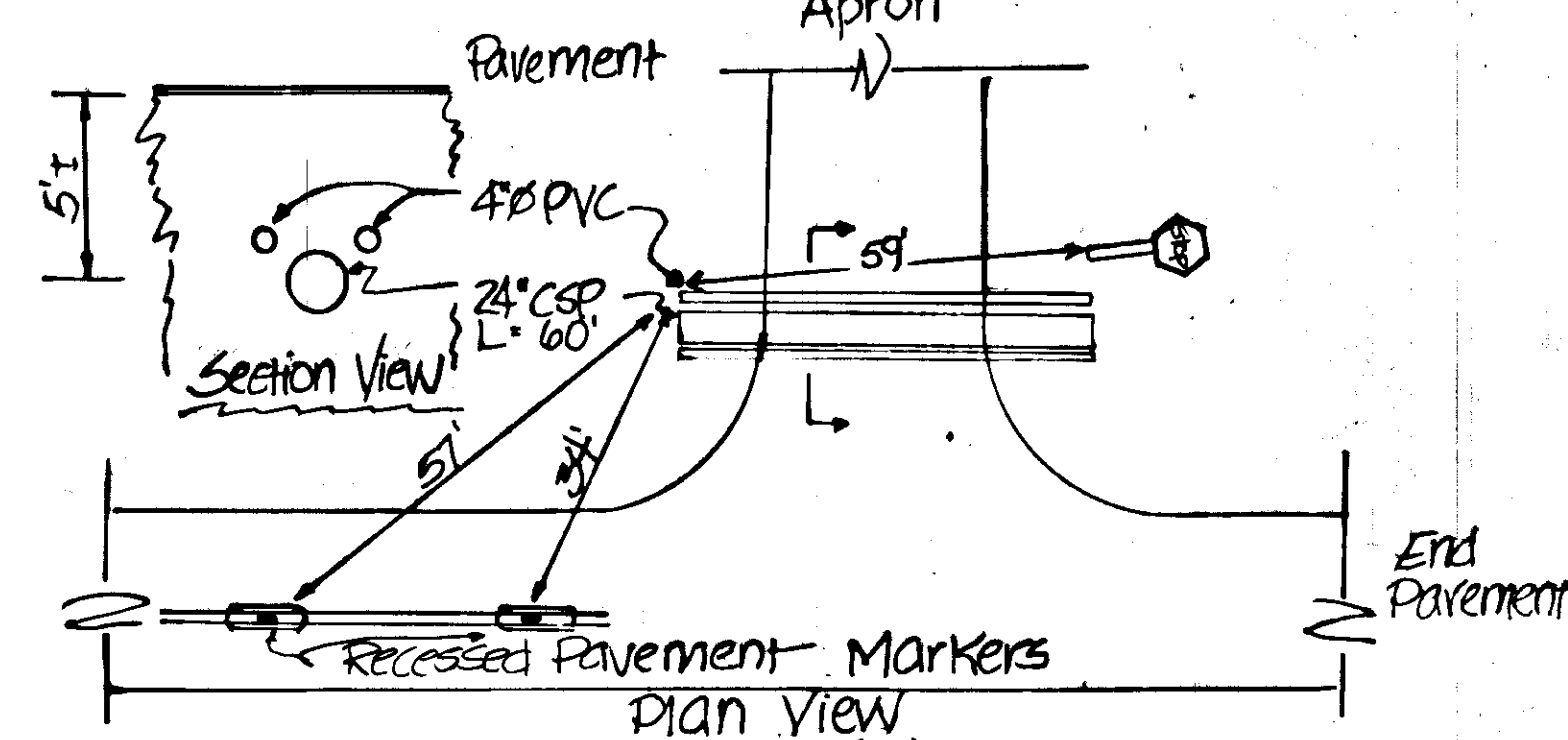
AS BUILT  
1-10-96



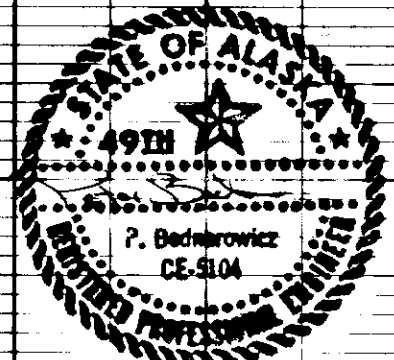
Note: Special work was performed on the left shoulder and ditch to facilitate future utility installations. As this was a bedrock through-cut between stations 30+00 and 33+60, the ditchline was drilled and shot down to a depth of 8'-10' to allow excavation without blasting. A 24"Ø x 60' steel culvert was installed 5' deep across the Airport Apron Access Road; two 4"Ø PVC conduits were also installed parallel to the culvert. See below.

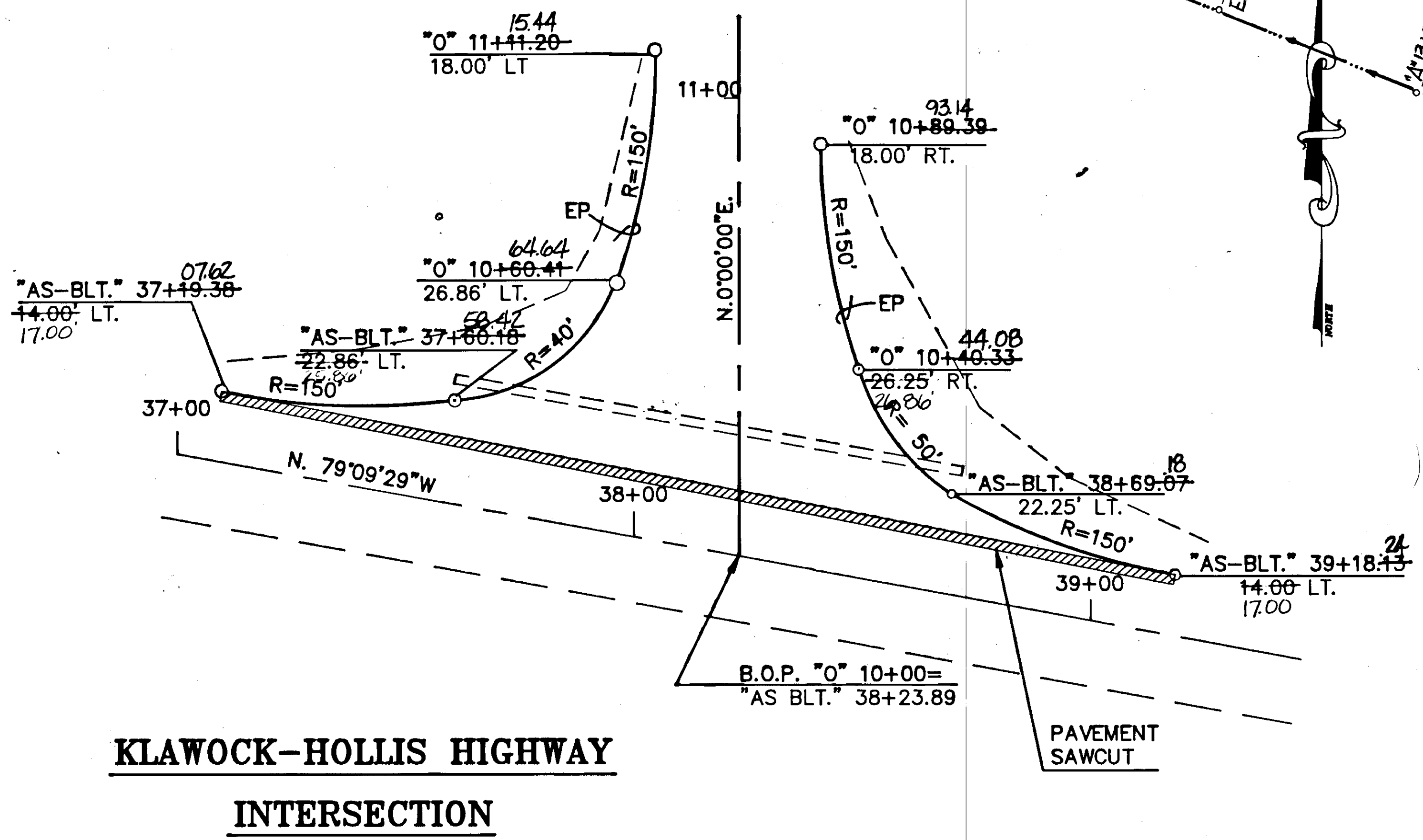


FLOOD FREQUENCY	50 YR.	100 YR.
DRAINAGE AREA	0.1 SQ. MI.	0.1 SQ. MI.
EXCEEDANCE PROBABILITY	2 %	1 %
DESIGN DISCHARGE	30 CFS	35 CFS
DESIGN HIGH WATER	22.5 FT.	22.8 FT.
PROBABLE BACKWATER	1.0 FT.	1.0 FT.
CAPACITY 150 C.F.S. AT EL. 33.0' WHICH HAS AN EXCEEDANCE PROBABILITY EQUAL TO OR LESS THAN 2 % (Q500 = 200 C.F.S.)		

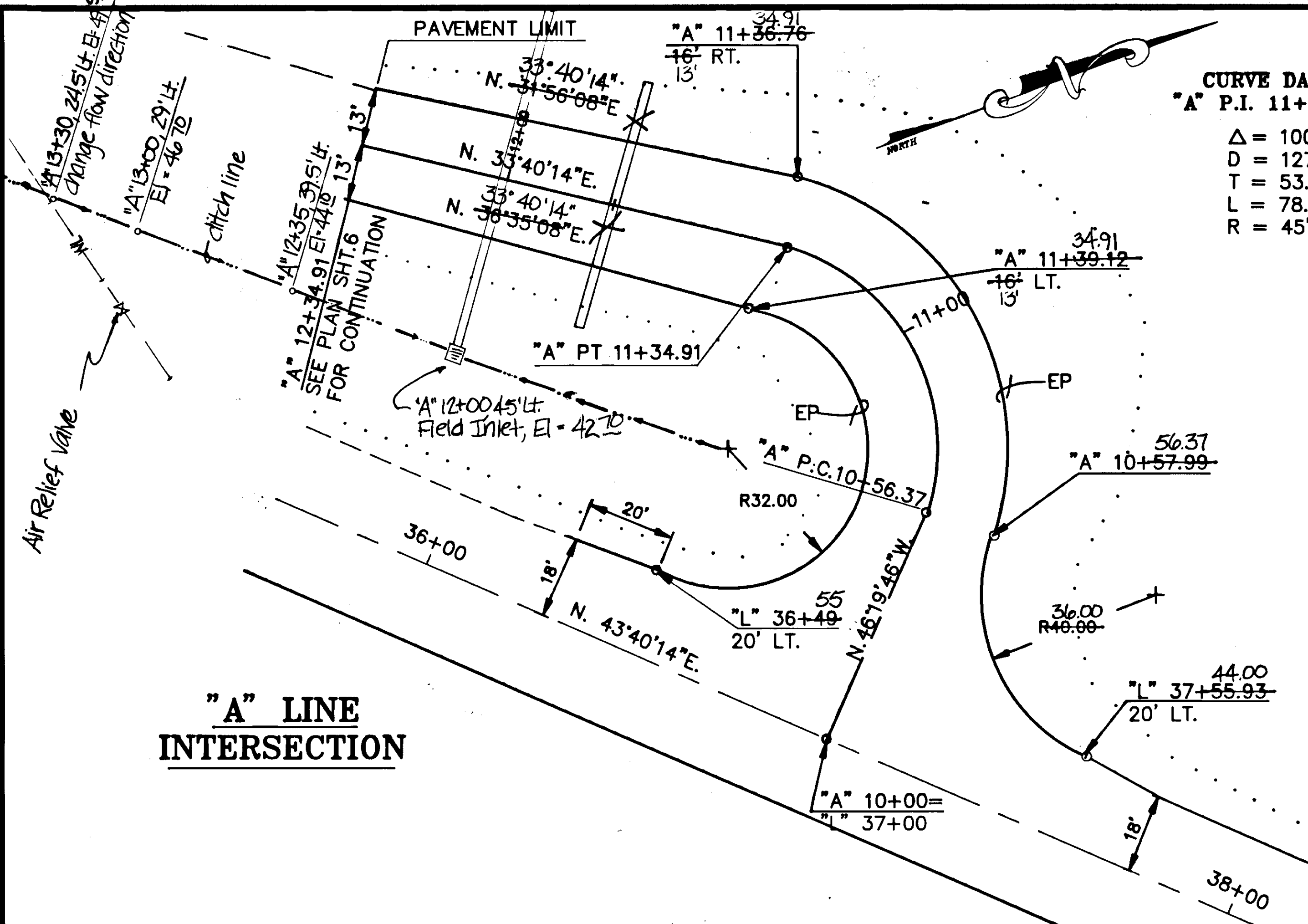


AS-BUILT  
1-10-96





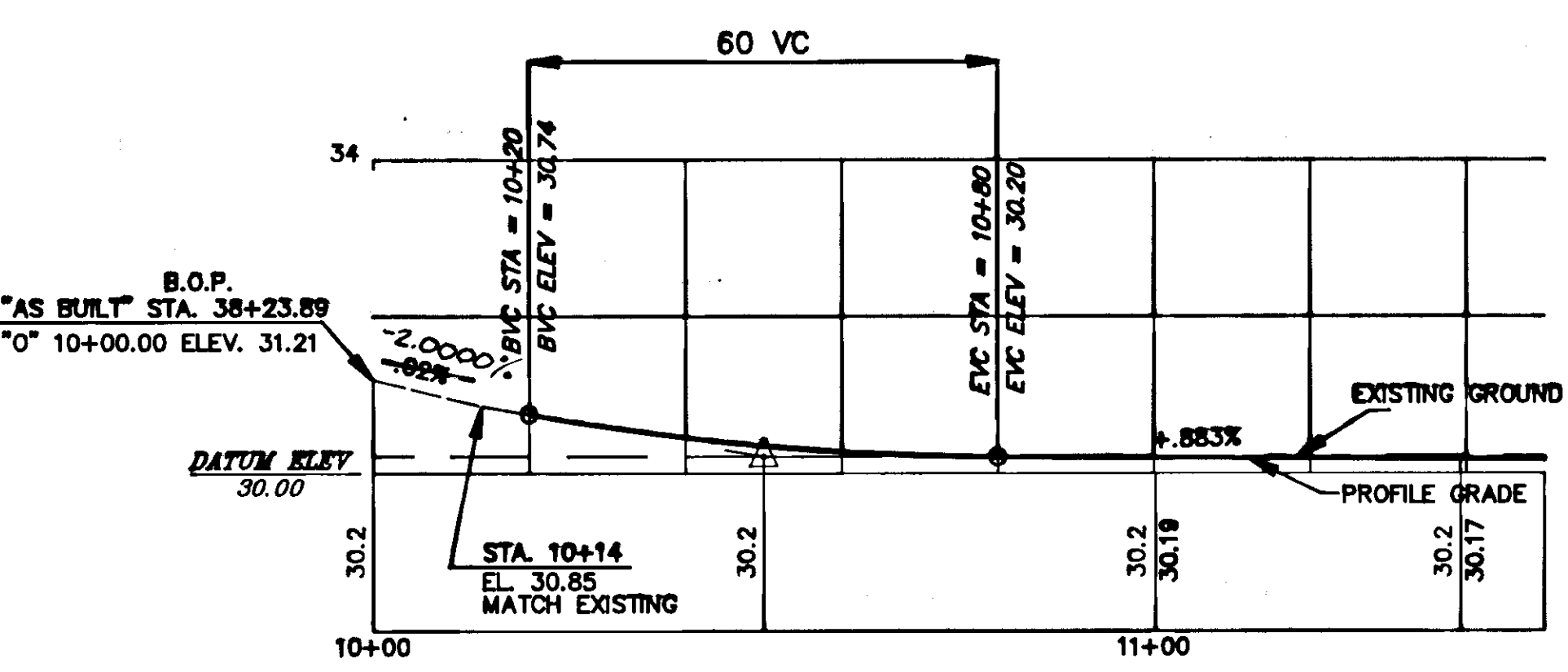
**KLAWOCK-HOLLIS HIGHWAY  
INTERSECTION**



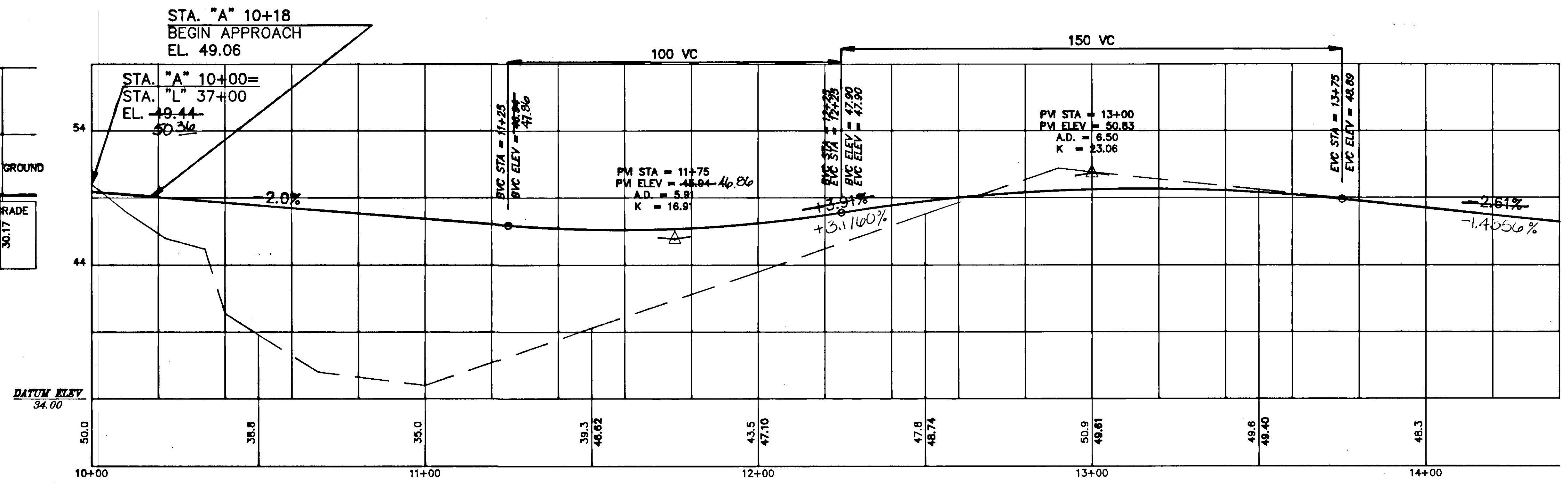
**"A" LINE  
INTERSECTION**

**CURVE DATA**  
"A" P.I. 11+10.00

Δ = 100°
D = 127°19'26"
T = 53.63'
L = 78.54'
R = 45'



**"O" LINE PROFILE LINE  
B.O.P. TO 11+50**



**"A" LINE PROFILE LINE**

BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS-BUILT

**RECORD OF REVISIONS**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

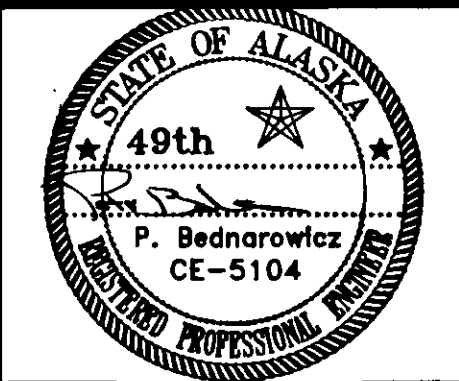
KLAWOCK

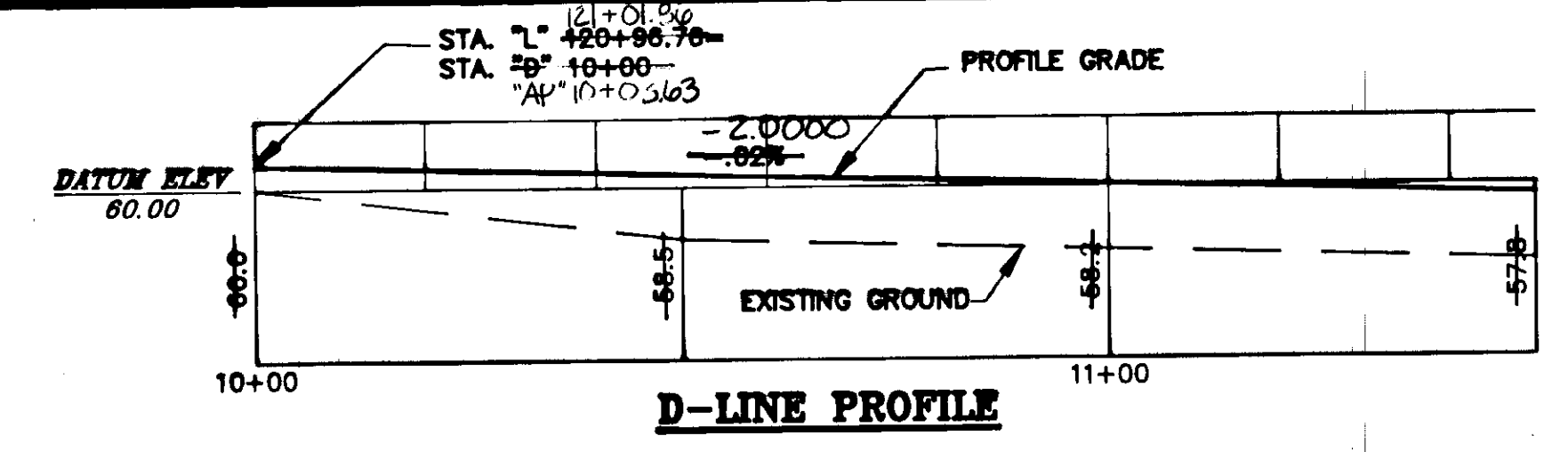
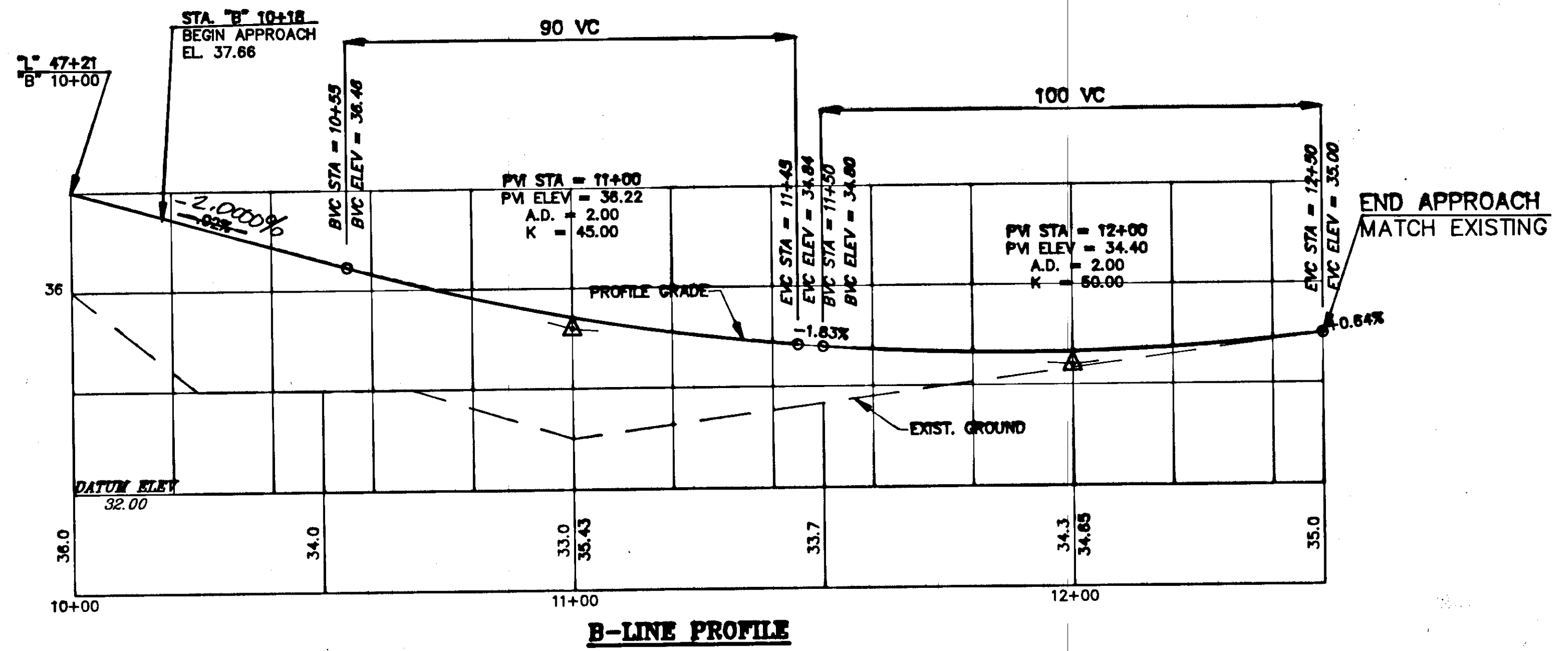
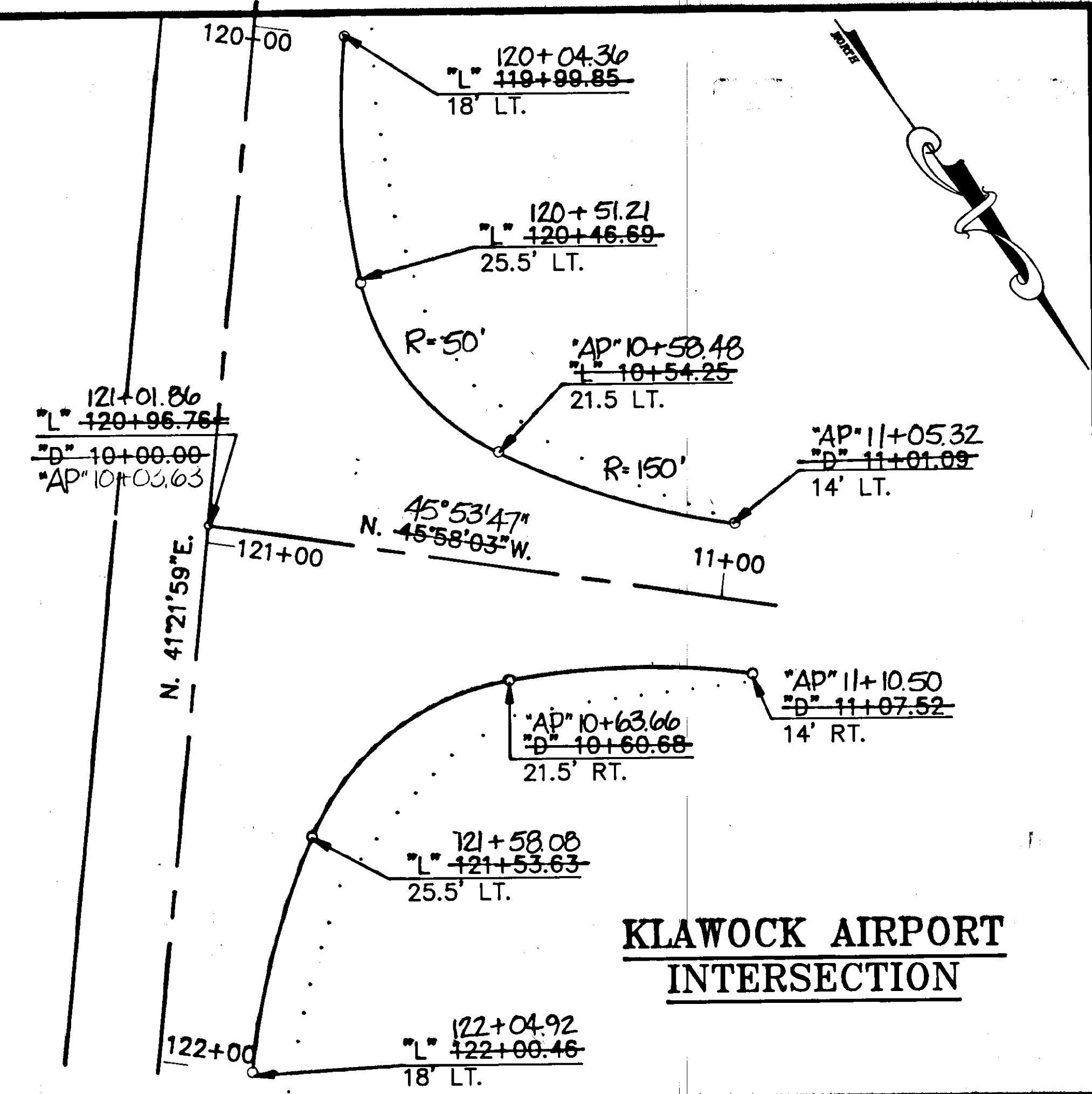
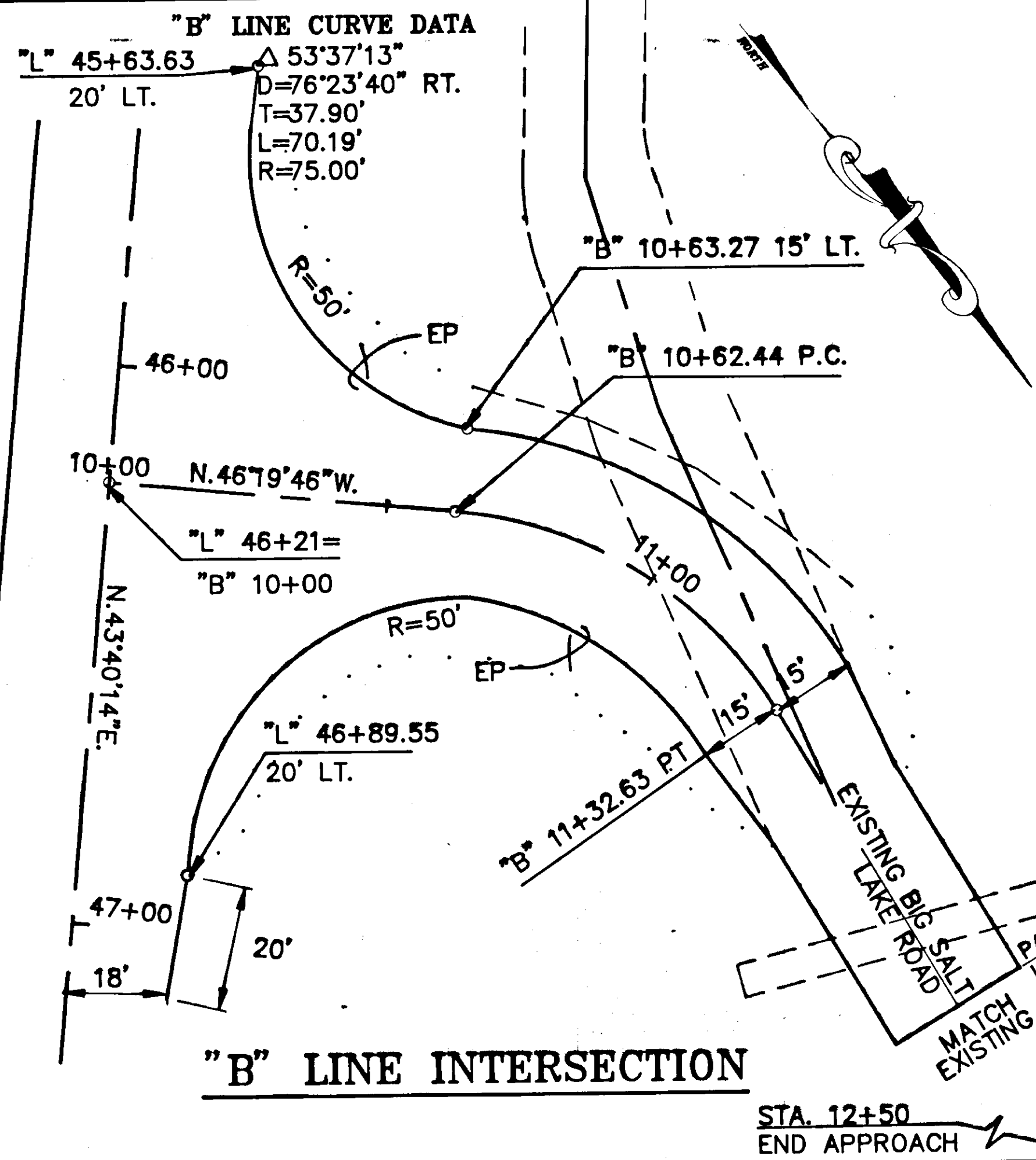
KLAWOCK TO THE AIRPORT  
RS-0929(1)  
**INTERSECTION DETAILS**

ALASKA

DESIGNED BY: P. JONES  
DRAWN BY: AUTOCADD/CSA  
CHECKED BY: P. BEDNAROWICZ

PROJECT NO. RS-0929(1)  
DATE:  
SHEET 12 OF 29





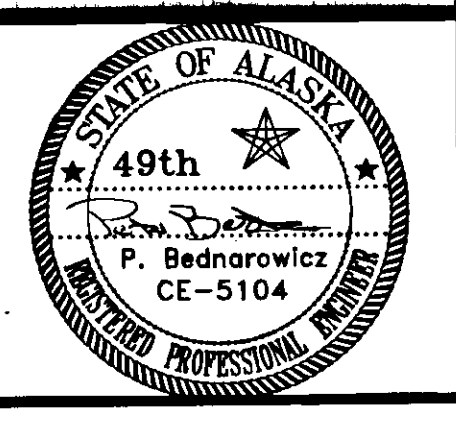
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS BUILT

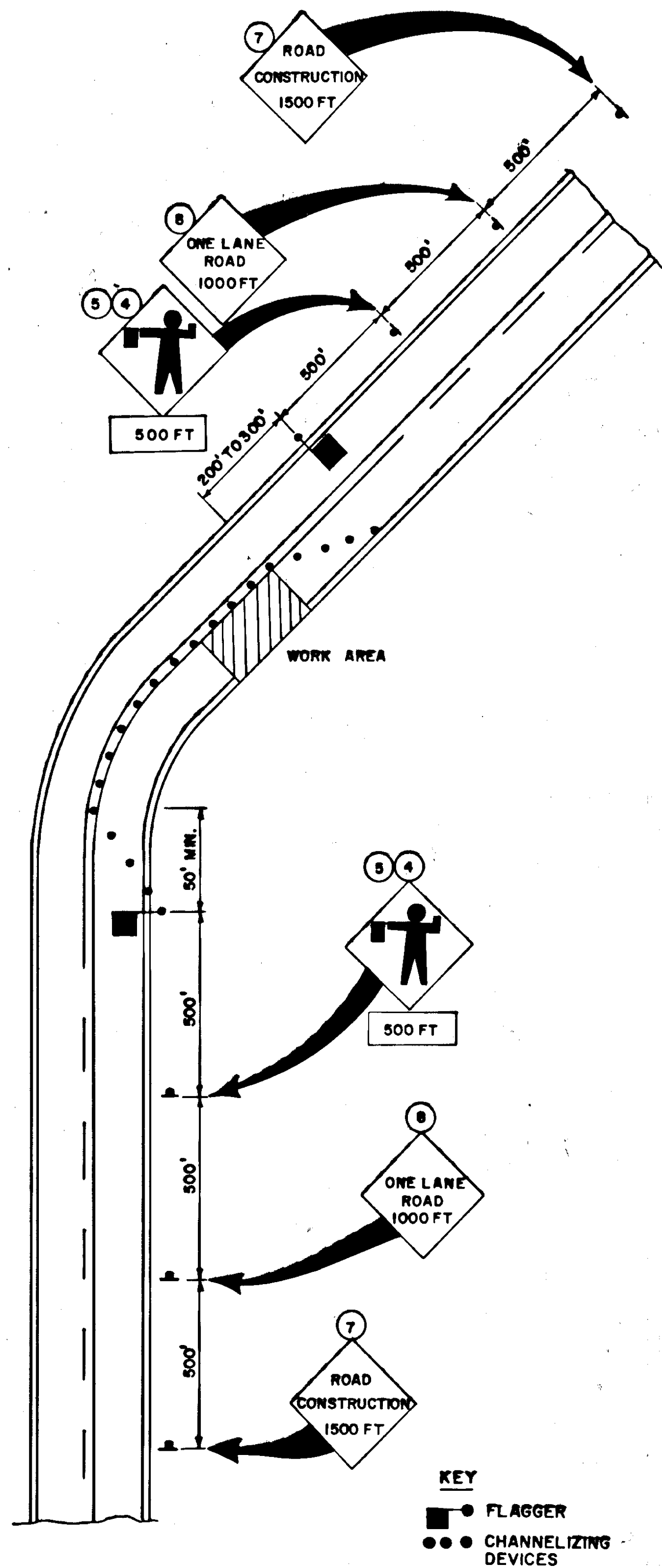
**RECORD OF REVISIONS**

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

KLAWOCK  
 KLAWOCK TO AIRPORT  
 RS-0929(1)  
 INTERSECTION DETAILS  
 ALASKA

DESIGNED BY: P. JONES	PROJECT NO. RS-0929(1)
DRAWN BY: AUTOCADD/CSA	DATE:
CHECKED BY: P. BEDNAROWICZ	SHEET 13 OF 29





TYPICAL LANE CLOSURE DETAIL

KEY  
 ● FLAGGER  
 ●● CHANNELIZING DEVICES

CONSTRUCTION SIGNING				
NO.	CODE	DESCRIPTION	SIZE	QUAN. *
1	R11-2	ROAD CLOSED	48" X 30"	8
2R	M4-10R	DETOUR (RIGHT)	48" X 18"	6
2L	M4-10L	DETOUR (LEFT)	48" X 18"	2
3R	M4-9R	DETOUR (RIGHT)	30" X 24"	6
3L	M4-9L	DETOUR (LEFT)	30" X 24"	1
4	CW20-7	FLAGGER SYMBOL	36" X 36"	4
5	CW70-7	FLAGGER PLAQUE	24" X 24"	4
6	WB-1	20 M.P.H.	18" X 18"	2
7	M4-10R	ROAD CONSTRUCTION 1500'	48" X 48"	4
8	CW20-8	ONE LANE ROAD 1000'	48" X 48"	4

\* THE NUMBER OF SIGNS SHOWN IS AN ESTIMATE. THE ACTUAL NUMBER WILL VARY WITH THE CONTRACTORS SEQUENCING OF OPERATIONS. IT IS RECOMMENDED THAT THE CONTRACTOR HAVE AT LEAST THIS QUANTITY OF SIGNS ON HAND.

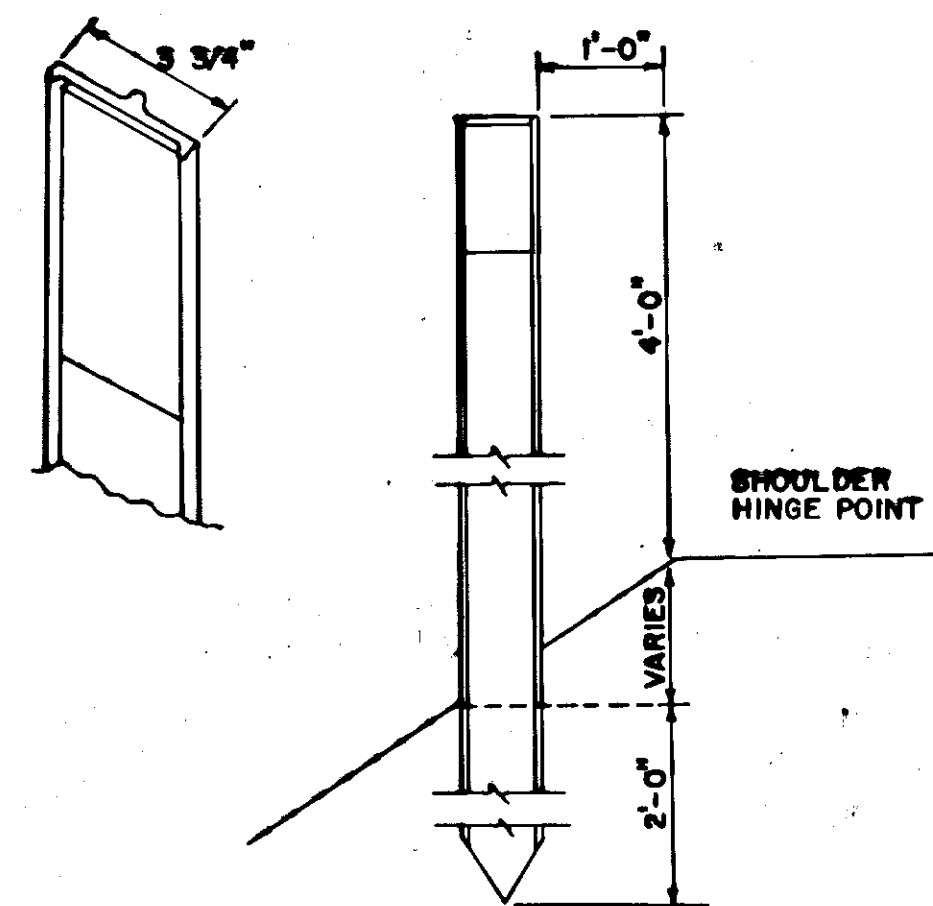
TYPICAL LANE CLOSURE NOTES

- IF ENTIRE WORK AREA IS VISIBLE FROM ONE STATION, A SINGLE FLAGGER MAY BE USED.
- WARNING LIGHTS SHOULD BE USED TO MARK CHANNELIZING DEVICES AT NIGHT AS NEEDED.
- CHANNELIZING DEVICES ARE TO BE EXTENDED TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.

GENERAL NOTES

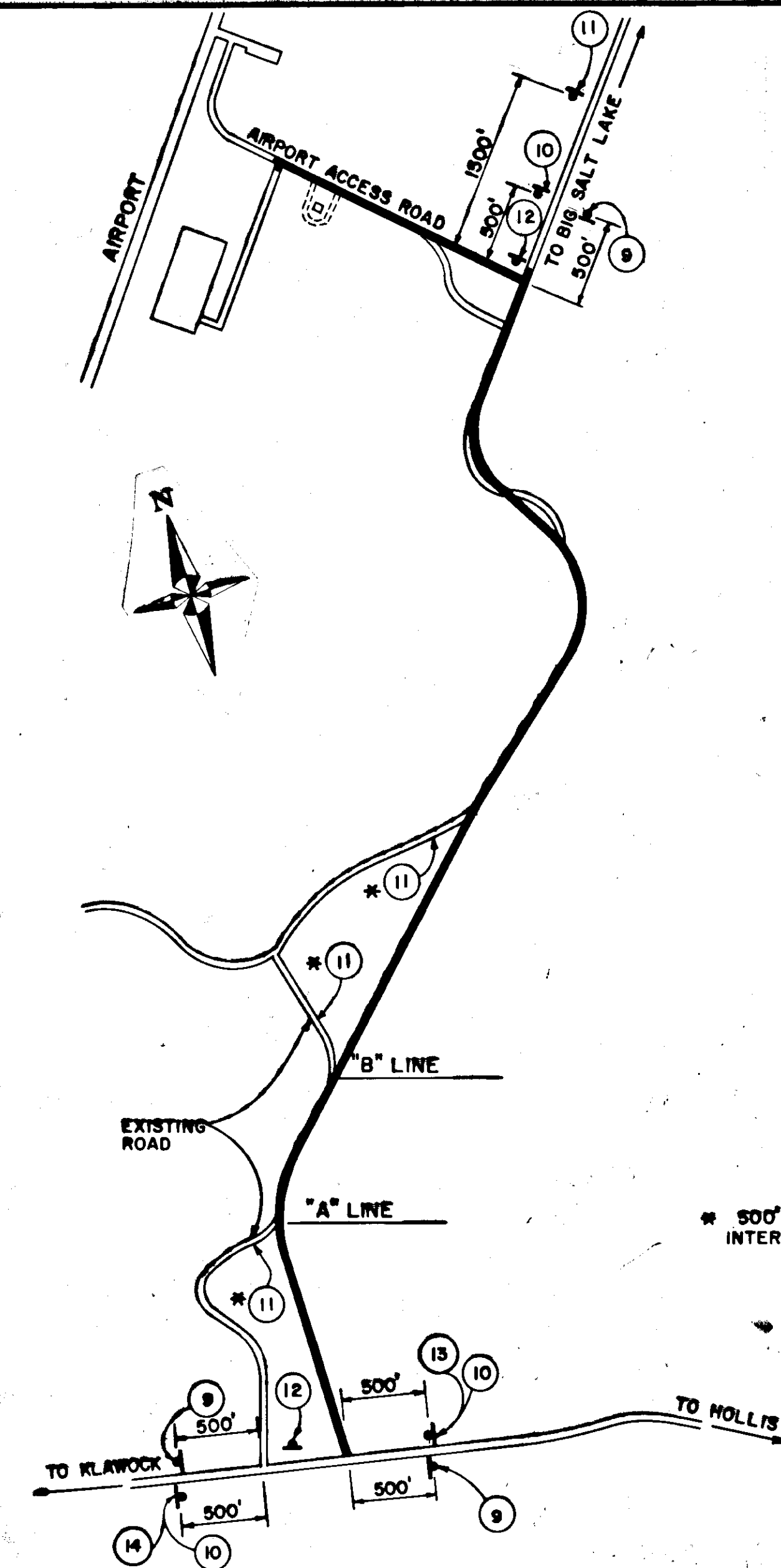
- SIGNS AND OTHER DEVICES SHOWN IN "PHASE A" DWG'S. SHALL BE IN PLACE AFTER WORK COMMENCES ON NEW ALIGNMENT AT EACH ROADWAY SEGMENT. "PHASE B" SIGNS SHALL BE IN PLACE WHEN THE NEW ALIGNMENT IS OPEN TO THE PUBLIC AND PRIOR TO OBLITERATION OF THE OLD ROADWAY ALIGNMENT.
- SPEED ADVISORY SIGNS MAY BE ATTACHED BELOW OTHER CONSTRUCTION SIGNS. THEY SHALL NOT BE INSTALLED AS STAND ALONE SIGNS. THEIR USE SHALL BE CONFINED TO ACTIVE WORK ZONES. INDISCRIMINATE USE WILL RESULT IN THE SIGNS BEING IGNORED.

PERMANENT CONSTRUCTION SIGNS				
NO.	CODE	DESCRIPTION	SIZE	QUAN.
9	G20-2	END CONSTRUCTION	60" X 24"	3
10	CW20-1F	ROAD CONSTRUCTION AHEAD	48" X 48"	5
11	CW20-1A	ROAD CONSTRUCTION 500'	48" X 48"	1
12	G20-1	ROAD CONSTRUCTION NEXT 2 MI.	60" X 36"	2
13	CW1-6R		48" X 24"	1
14	CW1-6L		48" X 24"	1



- NOTES:
- MIN. POST LENGTH SHALL BE 82'
  - 3" X 6" WHITE REFLECTIVE SHEETING BOTH SIDES OF WHITE POST.

FLEXIBLE DELINEATOR POST DETAIL



PERMANENT CONSTRUCTION SIGN DETAIL

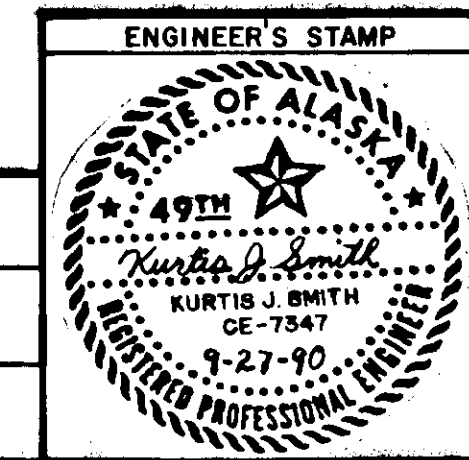
BY	DATE	DESCRIPTION OF CHANGE
KK	11/04/90	AS-BUILT

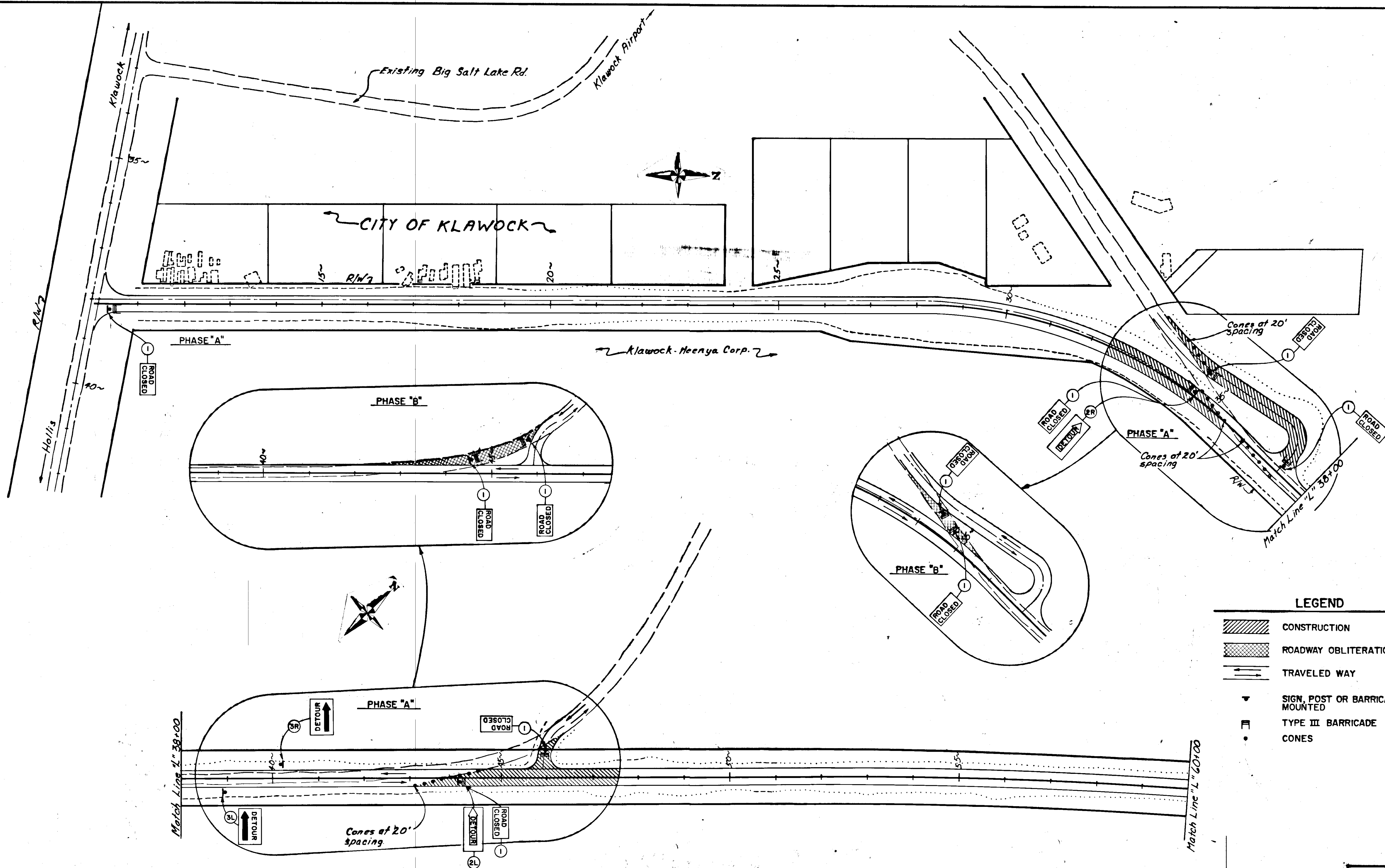
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION & PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

KLAWOCK TO AIRPORT  
 RS-0929(1)-69275  
 TRAFFIC CONTROL PLAN

APPROVED BY: \_\_\_\_\_  
 DESIGN GROUP CHIEF DATE \_\_\_\_\_  
 RECOMMENDED BY: \_\_\_\_\_  
 DESIGN ENGINEER, GROUP DATE \_\_\_\_\_  
 PREPARED BY: \_\_\_\_\_  
 PROJECT MANAGER LEAD DESIGNER

DESIGNED BY: P.J.  
 DRAWN BY: B.A.  
 CHECKED BY: R.P.  
 SCALE: NONE  
 DATE: \_\_\_\_\_  
 SHEET 14 OF 29





**LEGEND**

	CONSTRUCTION
	ROADWAY OBLITERATION
	TRAVELED WAY
	SIGN, POST OR BARRICADE MOUNTED
	TYPE III BARRICADE
	CONES

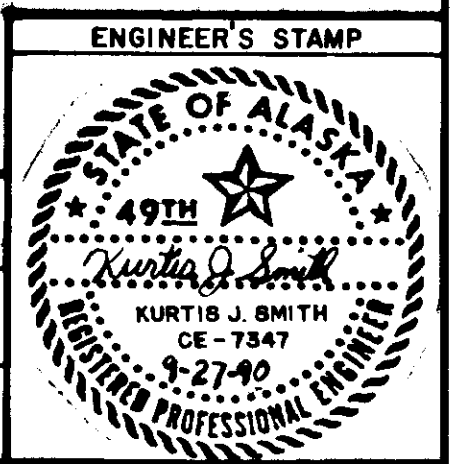
KK	1/10/96	AS-BUILT
BY	DATE	DESCRIPTION OF CHANGE
RECORD OF REVISIONS		

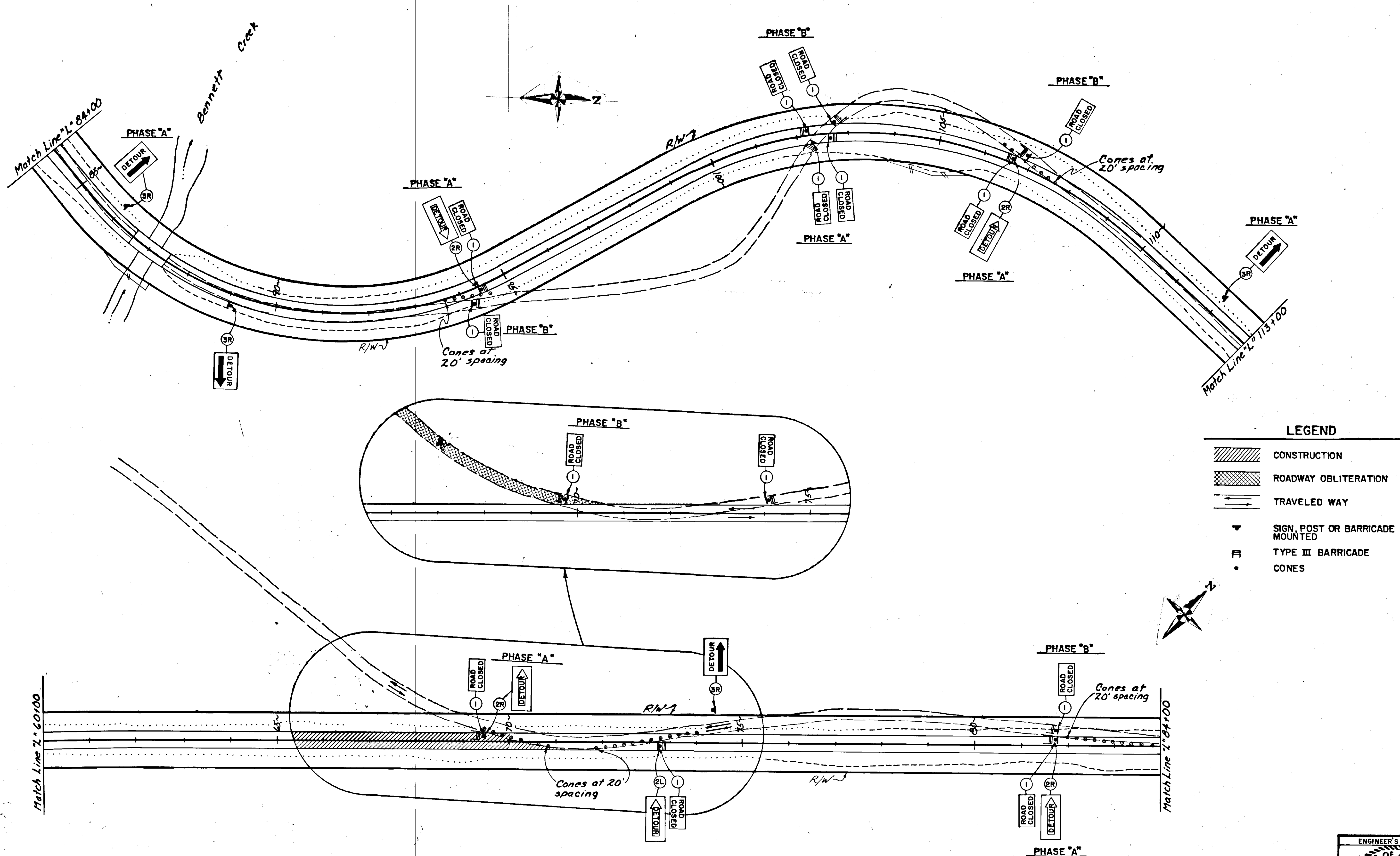
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 & PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

KLAWOCK TO AIRPORT  
 RS-0929(1)-69275  
 TRAFFIC CONTROL PLAN

APPROVED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
 DESIGN GROUP CHIEF  
 RECOMMENDED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
 DESIGN ENGINEER, GROUP "A"  
 PREPARED BY: \_\_\_\_\_  
 PROJECT MANAGER

DESIGNED BY: R.P.  
 DRAWN BY: B.A.  
 CHECKED BY: P.J.  
 SCALE: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 SHEET 15 OF 29





BY	DATE	DESCRIPTION OF CHANGE
AK	11/10/96	AS-BUILT

RECORD OF REVISIONS

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 & PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

KLAWOCK TO AIRPORT  
 RS-0929(1)-69275  
 TRAFFIC CONTROL PLAN

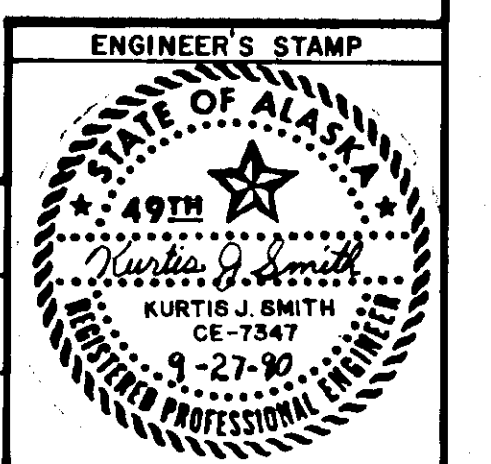
APPROVED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
 DESIGN GROUP CHIEF

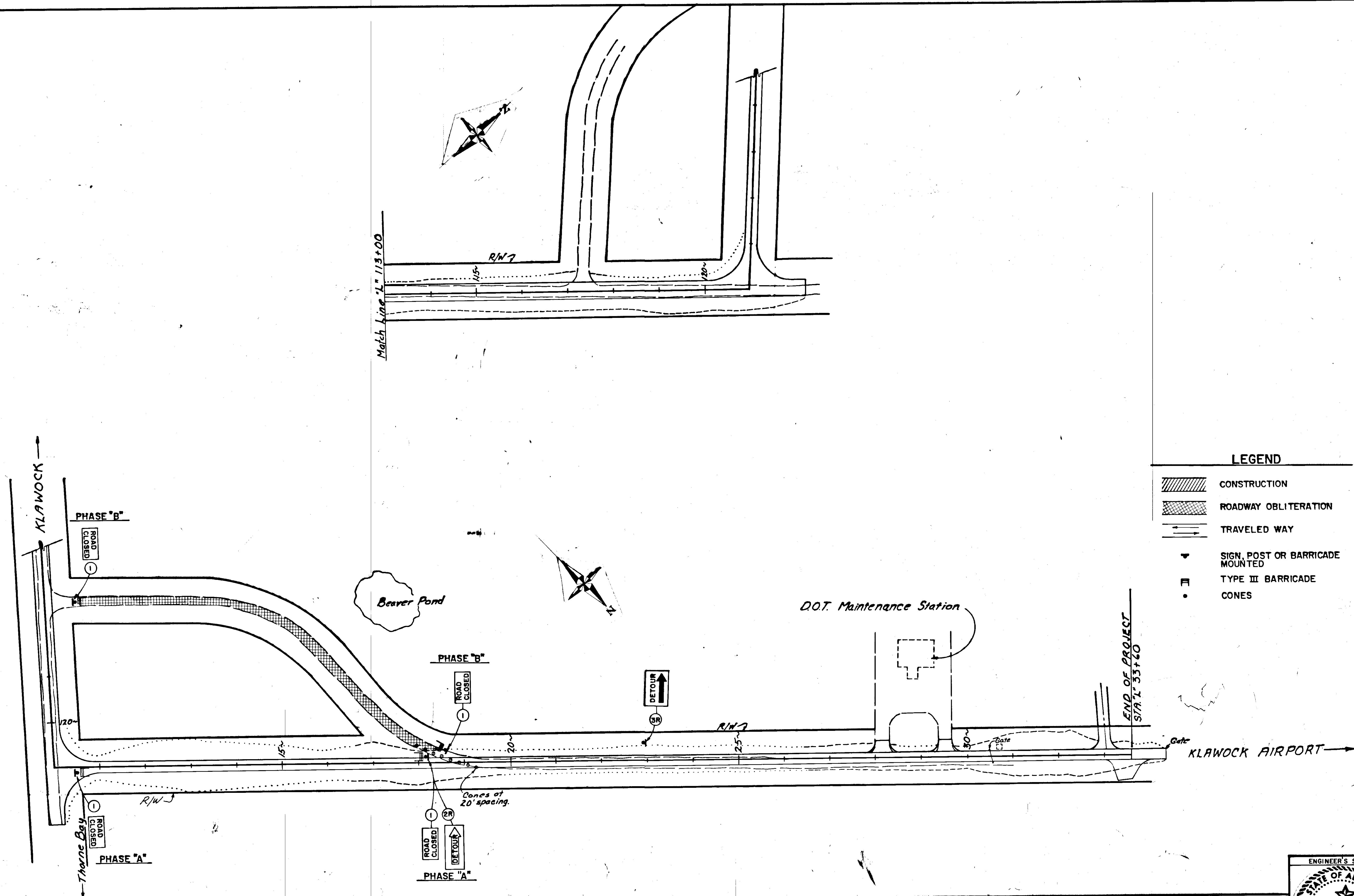
RECOMMENDED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
 DESIGN ENGINEER, GROUP

PREPARED BY: \_\_\_\_\_ DATE \_\_\_\_\_  
 PROJECT MANAGER



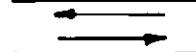



DESIGNED BY: R.P.  
 DRAWN BY: B.A.  
 CHECKED BY: P.J.

SCALE: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 SHEET 16 OF 29





**LEGEND**

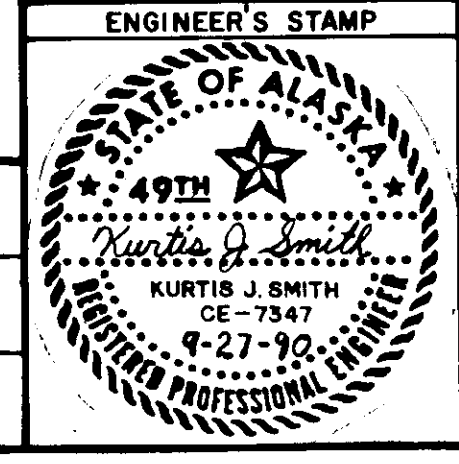
-  CONSTRUCTION
-  ROADWAY OBLITERATION
-  TRAVELED WAY
-  SIGN, POST OR BARRICADE MOUNTED
-  TYPE III BARRICADE
-  CONES

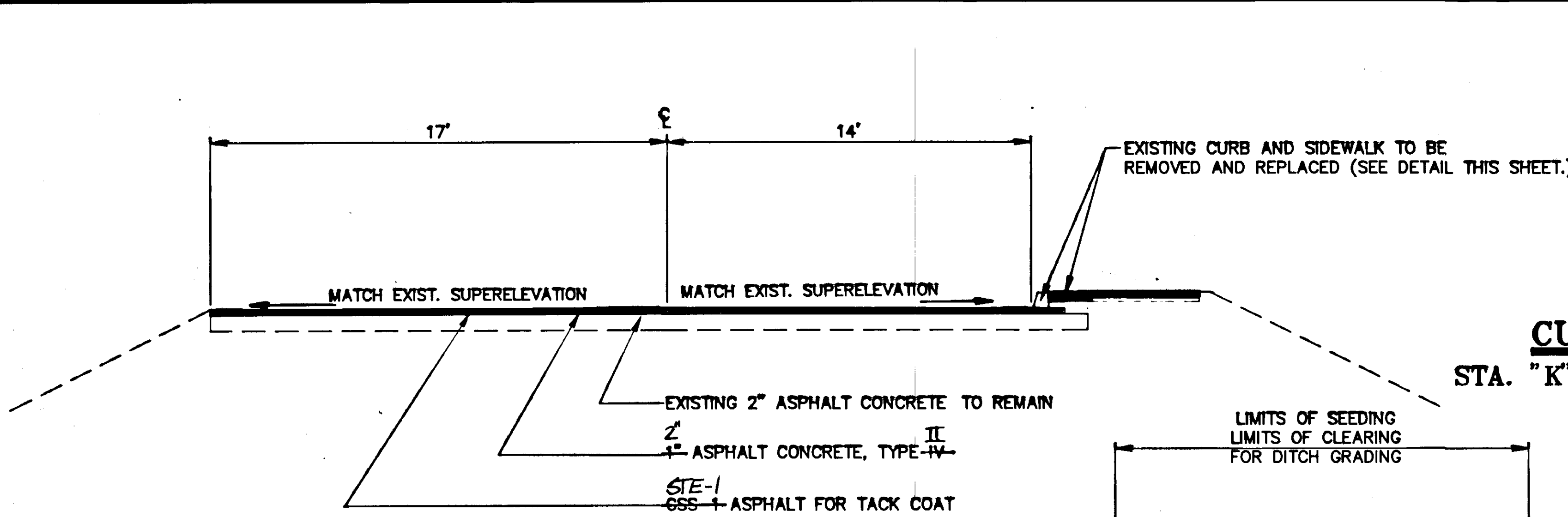
KK	1/10/90	AS BUILT
BY	DATE	DESCRIPTION OF CHANGE
RECORD OF REVISIONS		

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 & PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

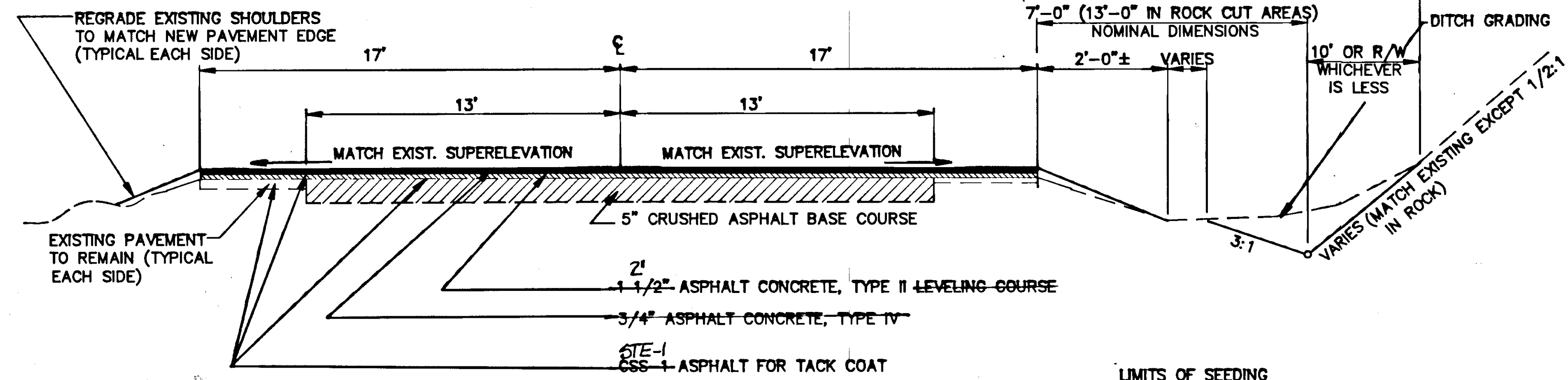
KLAWOCK TO AIRPORT  
 RS-0929 (1)-69275  
 TRAFFIC CONTROL PLAN

APPROVED BY:	DESIGNED BY:	SCALE:
DESIGN GROUP CHIEF	R.P.	
RECOMMENDED BY:	DRAWN BY:	DATE:
DESIGN ENGINEER, GROUP	B.A.	
PREPARED BY:	CHECKED BY:	SHEET 17 OF 29
PROJECT MANAGER	P.J.	
	LEAD DESIGNER	

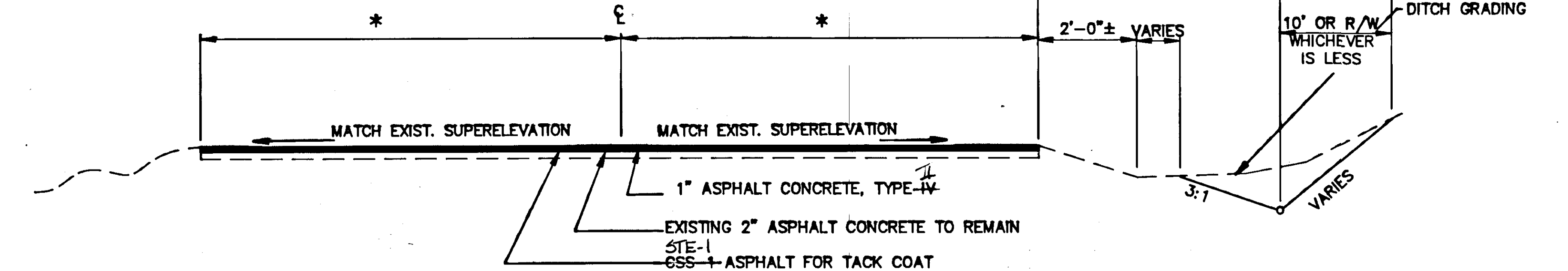




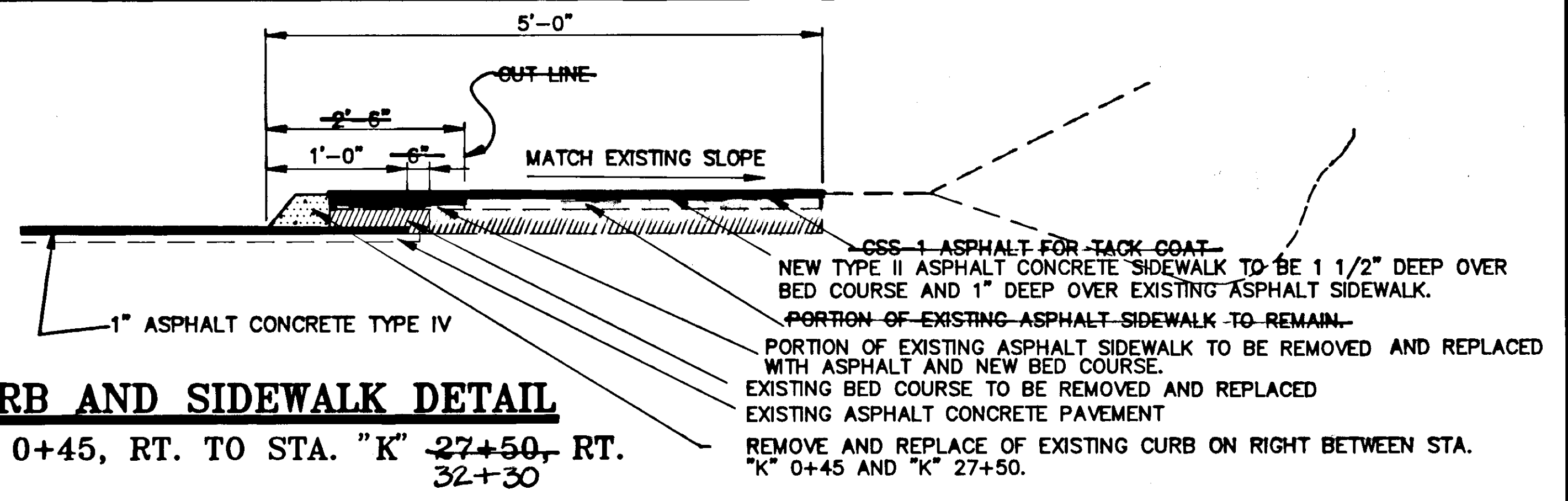
**STATION "K" 0+45.00 TO "K" 32+30**



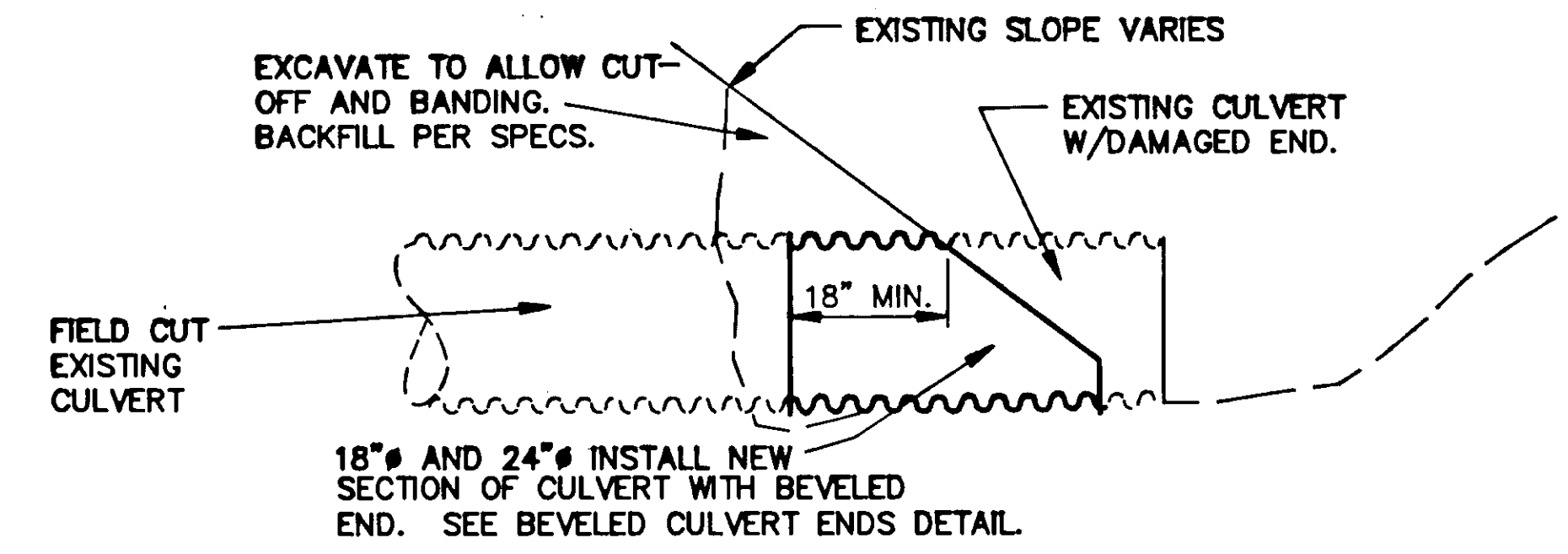
**STATION "K" 40+00 TO "K" 125+00  
"K" 210+00 TO "K" 275+00  
Various Locations**



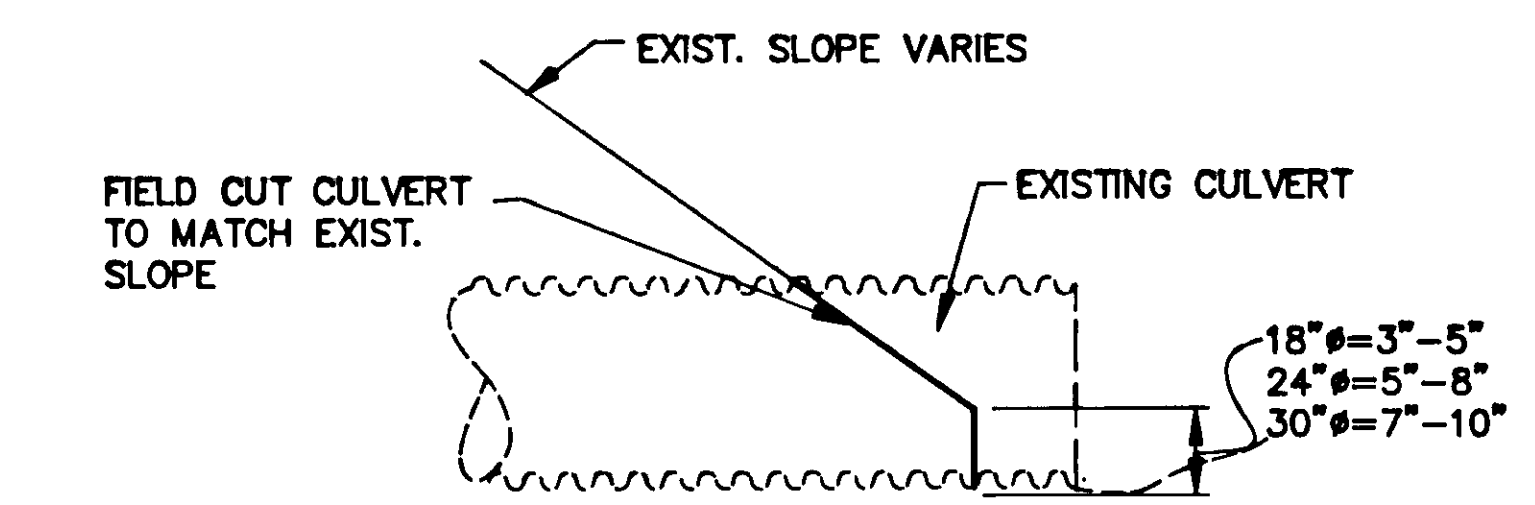
STATION "K" 32+30 TO "K" 40+00	17'
"K" 125+00 TO "K" 210+00	17'
"K" 275+00 TO "K" 333+36.23	17'
"K" 333+36.23 TO "K" 333+86.24	17' TO 20'
"K" 333+86.24 TO "K" 348+86.24	20'
"K" 348+86.25 TO "K" 375+43.51	17'



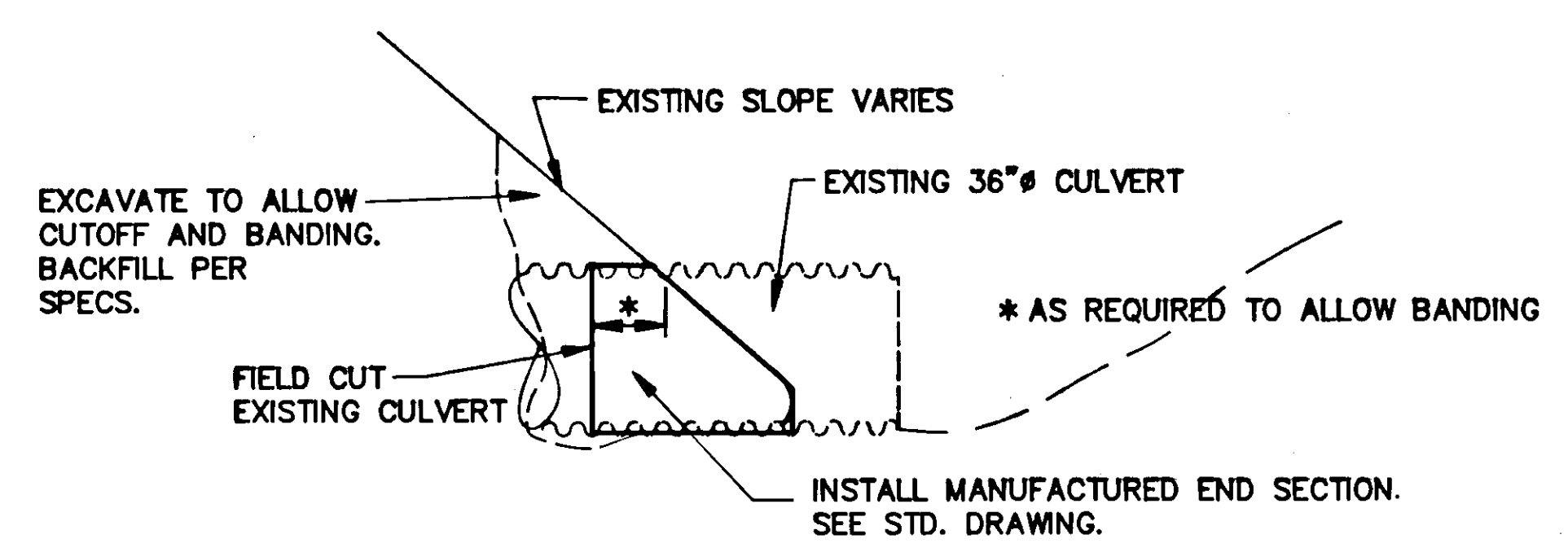
**CURB AND SIDEWALK DETAIL**  
STA. "K" 0+45, RT. TO STA. "K" 27+50, RT.  
32+30



**NEW FIELD CUT END SECTIONS FOR DAMAGED CULVERTS**



**BEVEL EXISTING CULVERT ENDS**



**NEW MANUFACTURED END SECTION FOR 36" CULVERTS**

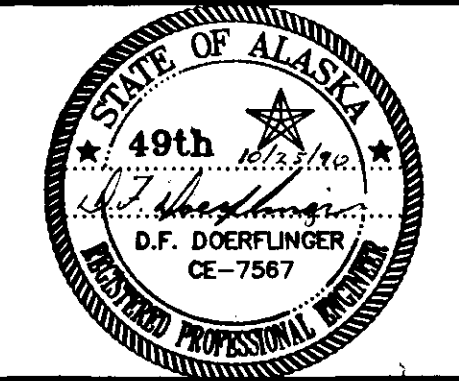
NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

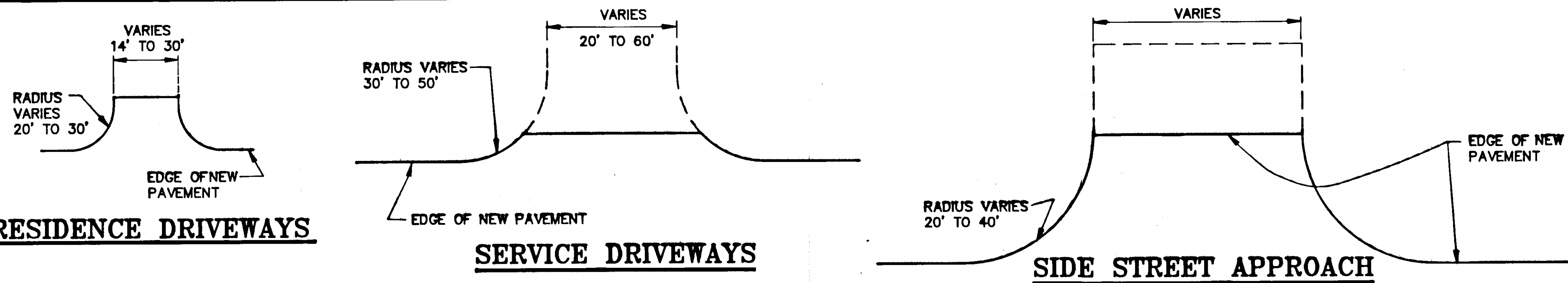
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1/10/96	AS BUILT

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KILAWOCK OVERLAY  
RS-0924(12)  
**TYPICAL SECTIONS OF IMPROVEMENTS**

DESIGNED BY: D. STERLEY	PROJECT NO. RS-0924(12)
DRAWN BY: AUTOCADD/CSA	DATE: 9/90
CHECKED BY: D. DOERFLINGER	SHEET 18 OF 29





**RESIDENCE DRIVEWAYS**

**SERVICE DRIVEWAYS**

**SIDE STREET APPROACH**

① UTILITY ADJUSTMENTS & RECONSTRUCTION		
STATION	OFFSET	REMARK
0+30	23' LT.	ADJUST EXISTING VALVE BOX
0+37	13' RT.	ADJUST EXISTING MANHOLE
0+39	32' RT.	ADJUST EXISTING VALVE BOX
1+65	4' LT.	ADJUST EXISTING MANHOLE
1+95	24' LT.	ADJUST EXISTING VALVE BOX
4+63	6' RT.	RECONSTRUCT EXISTING MANHOLE
5+05	3' LT.	ADJUST EXISTING VALVE BOX
7+71	4' LT.	ADJUST EXISTING VALVE BOX
7+85	26' RT.	ADJUST EXISTING VALVE BOX
7+86	6' LT.	ADJUST EXISTING VALVE BOX
-9+12	15' LT.	ADJUST EXISTING VALVE BOX
-13+17	25' LT.	ADJUST EXISTING VALVE BOX
13+24	10' RT.	ADJUST EXISTING VALVE BOX
13+36	4' RT. 6' Lt	ADJUST EXISTING VALVE BOX
-13+75	7' RT.	RECONSTRUCT EXISTING MANHOLE
17+05	0	ADJUST EXISTING MANHOLE
17+12	27' RT.	ADJUST EXISTING VALVE BOX
20+06	2' LT.	ADJUST EXISTING MANHOLE
22+07	2' LT.	RECONSTRUCT EXISTING MANHOLE
25+97-87	3' LT.	RECONSTRUCT EXISTING MANHOLE
32+30	21' RT.	ADJUST EXISTING VALVE BOX

RESIDENCE DRIVEWAYS				
STA. ①	OFFSET RT. LT.	WIDTH	REMARKS	
0+85	X	20'	PAVED	
7+08	X	20'	CURB CUT	
10+45		16'	PAVED	
10+65	X	20'	CURB CUT	
11+58	X	20'	CURB CUT	
18+40	X	20'	CURB CUT	
19+60		20'	PAVED	
19+60	X	20'	CURB CUT	
20+70	X	20'	UNPAVED	
31+00	X	24'	UNPAVED	
34+45	X	24'	UNPAVED	
43+50	X	30'	UNPAVED	
298+40	X	24'	PAVED	
350+00	X	20'	PAVED	
351+10	X	20'	PAVED	
351+70	X	20'	PAVED	
352+20	X	14'	PAVED	
352+70	X	14'	PAVED	
353+00	X	20'	UNPAVED	
369+80	X	24'	UNPAVED	

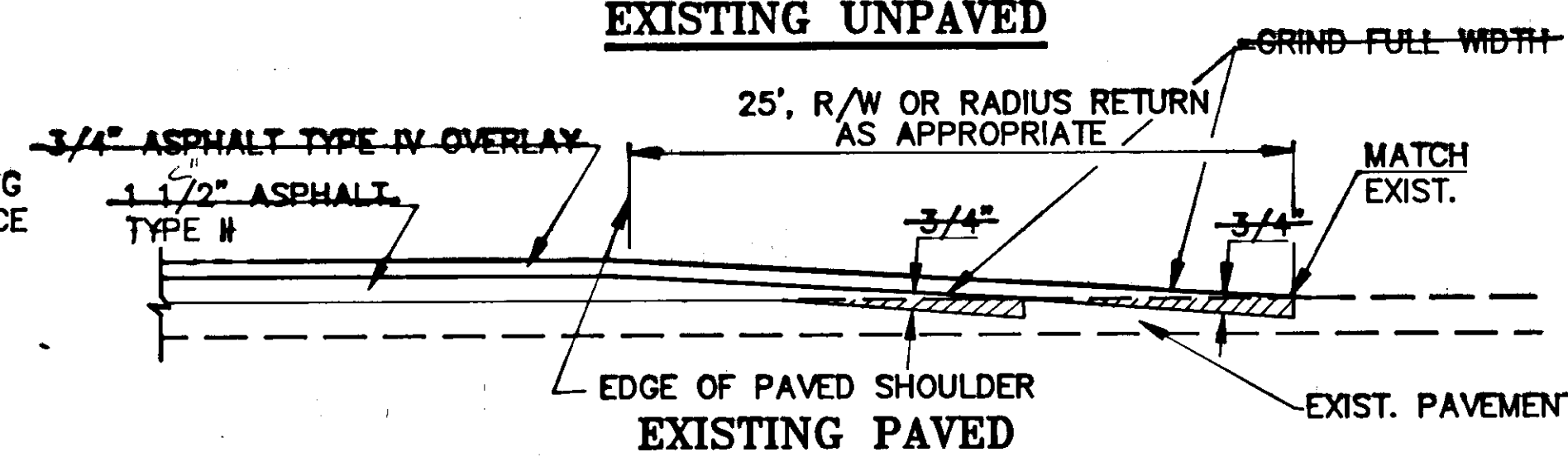
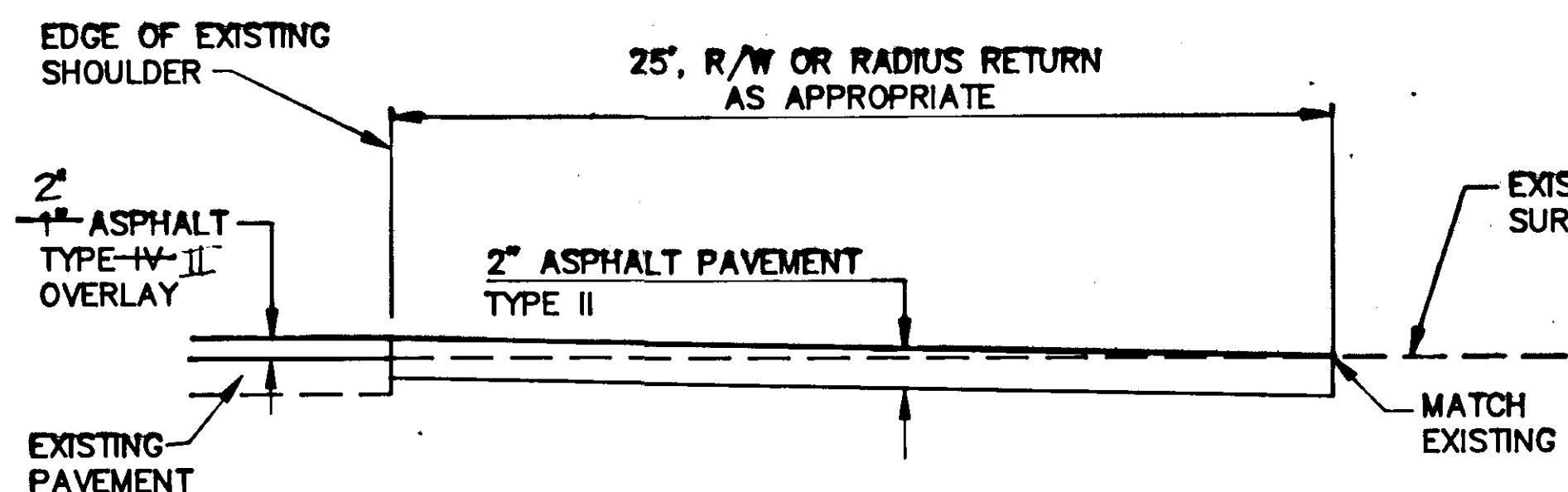
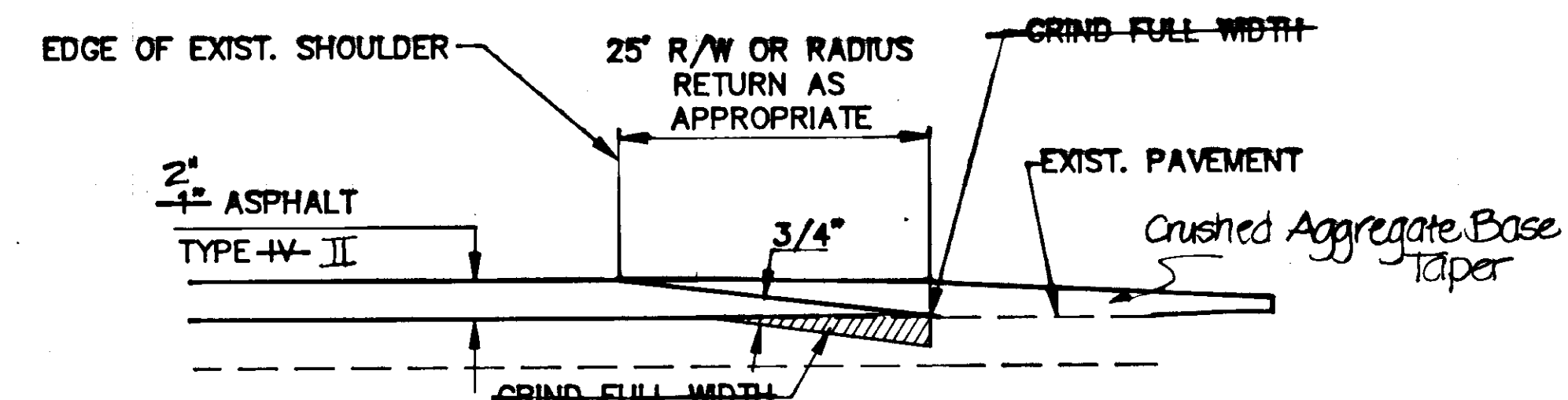
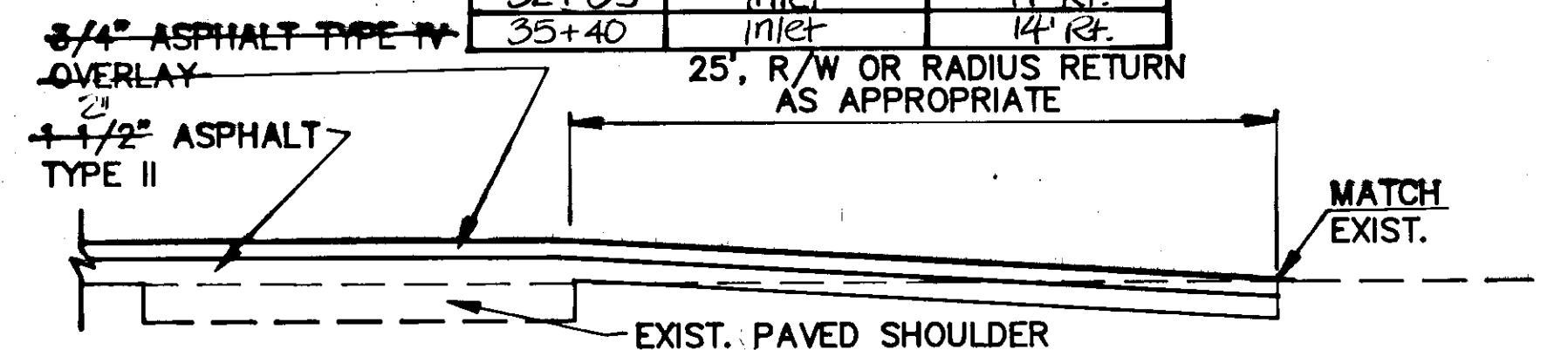
SERVICE DRIVEWAYS				
STA. ①	OFFSET RT. LT.	WIDTH	REMARKS	
2+40	X	30'	UNPAVED	
3+20	X	40'	PAVED	
3+40	X	40'	CURB CUT	
4+70	X	30'	UNPAVED	
6+00	X	40'	CURB CUT	
12+50	X	60'	CURB CUT	
14+75	X	32'	PAVED	
16+25	X	32'	PAVED	
25+00	X	26'	PAVED	
28+80	X	20'	PAVED	
33+55	X	30'	UNPAVED	
34+75	X	28'	PAVED	
35+25	X	40'	UNPAVED	
35+55	X	28'	PAVED	
36+00	X	30'	UNPAVED	
44+60	X	20'	UNPAVED	
53+50	X	20'	UNPAVED	
60+90	X	24'	PAVED	
76+70	X	24'	PAVED	
308+50	X	28'	UNPAVED	
353+70	X	60'	UNPAVED	
367+50	X	24'	PAVED	
371+00	X	24'	UNPAVED	
372+40	X	20'	UNPAVED	
373+40	X	20'	UNPAVED	

② SIDE STREET APPROACHES				
STA. ①	OFFSET RT. LT.	WIDTH	REMARKS	
1+90	X	X	"FOURTH ST." - PAVED	
2+75	X	X	"FOURTH ST." - PAVED	
5+30	X	X	"FIFTH ST." - PAVED	
7+80	X	X	"SIXTH ST." - PAVED	
8+15	X	X	"SIXTH ST." - PAVED	
9+20	X	X	"SEVENTH ST." - PAVED	
9+60	X	X	"SEVENTH ST." - PAVED	
13+40	X	X	"NINTH ST." - PAVED	
13+40	X	X	"NINTH ST." - PAVED	
16+90	X	X	"WEST HAMILTON DR." - PAVED	
17+10	X	X	"WEST HAMILTON DR." - PAVED	
23+20	X	X	"COLD STORAGE RD." - PAVED	
23+80	X	X	"HILLTOP ST." - PAVED	
26+00	X	X	"T&H ST." - PAVED	
26+70	X	X	"THOMPSON ST." - PAVED	
27+70	X	X	"SCHOOL DR." - PAVED	
29+40	X	X	"EASY ST." - PAVED	
33+00	X	X	"EASY ST." - PAVED	
33+00	X	X	"PORT BAGIAL ST." - PAVED	
40+50	X	X	"EAST HAMILTON DR." - PAVED	
49+50	X	X	"RESERVOIR ROAD" - PAVED	
50+80	X	X	"RALPH JAME ST." - PAVED	
145+70	X	X	"DUMP ROAD" - PAVED	
315+40	X	X	"SEALASKA DOCK" - PAVED	
348+60	X	X	"BAY VIEW ST." - PAVED	
350+60	X	X	"SPRING ST." - PAVED	
354+60	X	X	"BAY VIEW DR." - PAVED	
356+85	X	X	"ANCHORAGE ST." - PAVED	
366+00	X	X	"NO NAME ST." - UNPAVED	
367+00	X	X	"FIREWEED ST." - UNPAVED	
369+00	X	X	"HEMLOCK ST." - UNPAVED	
375+00	X	X	"BIG SALT ROAD" - PAVED	

Station	offset	Remark
5+43	15' Lt.	Adjust Valve Box
8+50	Rt.	Adjust Manhole
9+54	3' Lt.	Adjust Valve Box
11+90	0	Adjust Valve Box
13+25	20' Rt.	Adjust Manhole
14+50	20' Rt.	Adjust Manhole
15+50	20' Rt.	Adjust Manhole
15+25	0	Adjust Valve Box
16+45	20' Rt.	Adjust Manhole
35+15	5' Lt.	Adjust Manhole
9+54	6' Lt.	Adjust Valve Box

① STORM DRAIN INLETS		
0+41	INLET	26' RT.
0+49	INLET	18' RT.
0+83	INLET	18' RT. <i>Removed</i>
2+58	INLET	18' RT.
2+94	INLET	24' RT.
3+00	INLET	18' RT.
3+46	INLET	14' LT.
5+09	INLET	18' RT.
6+26	INLET	14' RT.
7+66	INLET	19' RT.
11+50	INLET	12' RT.
11+88	INLET	21' LT.
13+07	INLET	18' RT.
15+50	INLET	12' RT.
17+52-42	INLET	12' RT.
20+49	INLET	12' RT.
27+30	INLET	11' RT.
342+55	inlet	14' Lt.
1+04	inlet	14' Rt.
23+40	inlet	19' Rt.
28+90	inlet	14' Rt.
32+05	inlet	14' Rt.
35+40	inlet	14' Rt.

- ① ALL STATIONS SHOWN ON THIS SHEET ARE APPROXIMATE. FINAL LOCATION SHALL BE FIELD DETERMINED BY THE PROJECT ENGINEER.
- ② SIDE STREET APPROACHES SHALL BE PAVED TO THE RADIUS RETURNS.



**PAVEMENT TAPER FOR PAVED APPROACHES SERVICE OR RESIDENCE DRIVEWAYS IN OVERLAY AREAS**

**PAVEMENT DETAIL FOR EXISTING UNPAVED DRIVEWAYS AND APPROACHES IN OVERLAY AREAS**

**PAVEMENT DETAIL FOR APPROACHES AND DRIVEWAYS IN PAVEMENT REMOVAL AREAS**

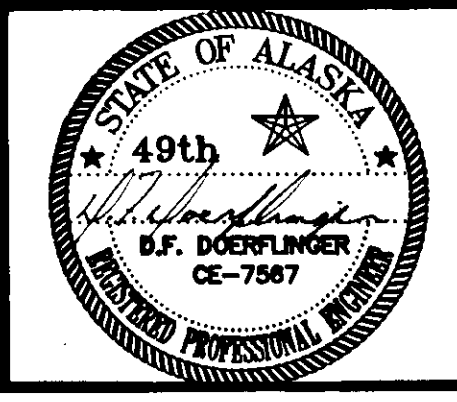
NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

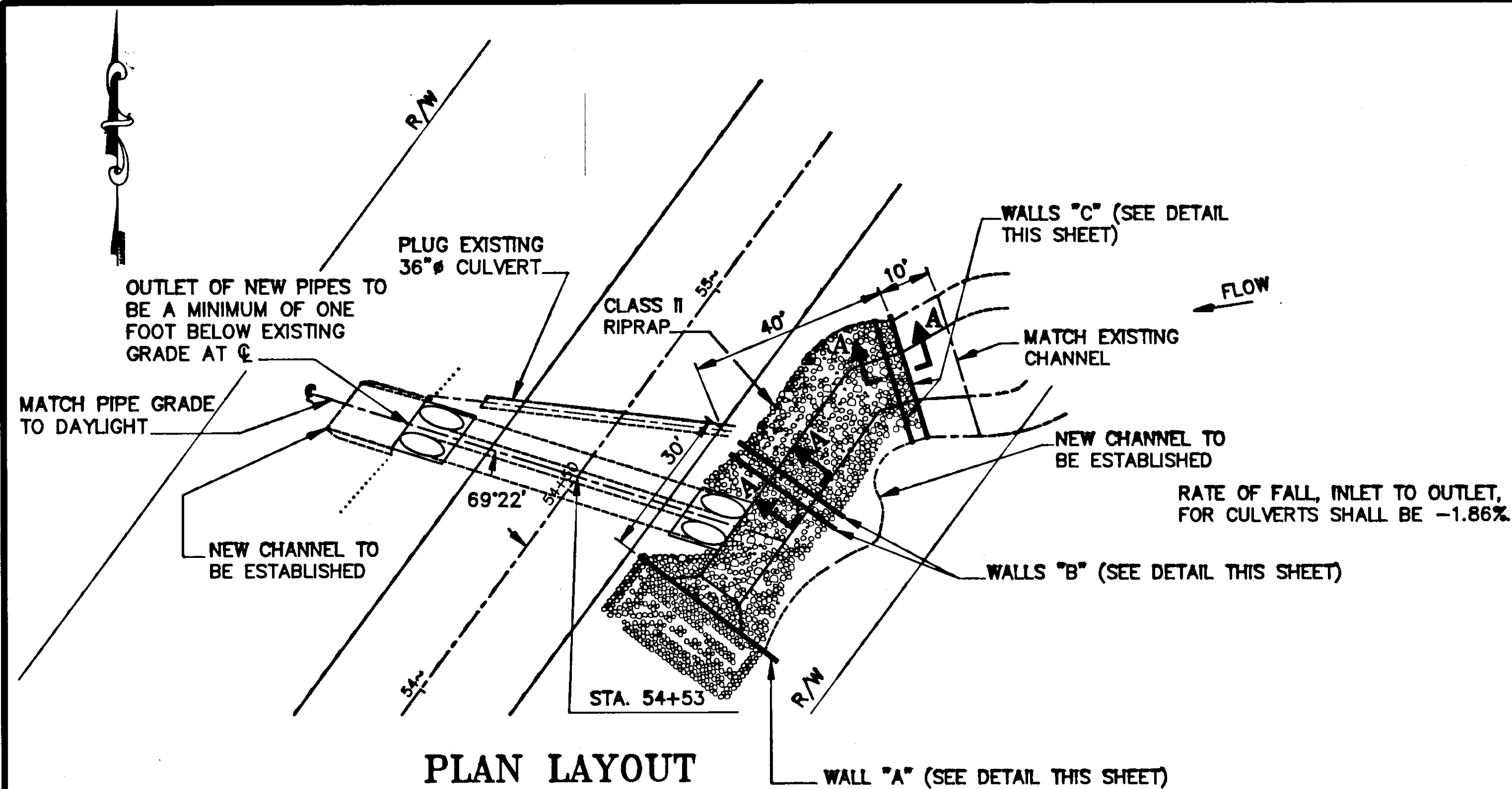
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1/10/96	AS BUILT

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KLAWOCK OVERLAY  
RS-0924(12)  
**APPROACH DETAILS**

DESIGNED BY:	D. STERLEY	PROJECT NO.	RS-0924(12)
DRAWN BY:	AUTOCADD/CSA	DATE:	9/90
CHECKED BY:	D. DOERFLINGER	SHEET	19 OF 29



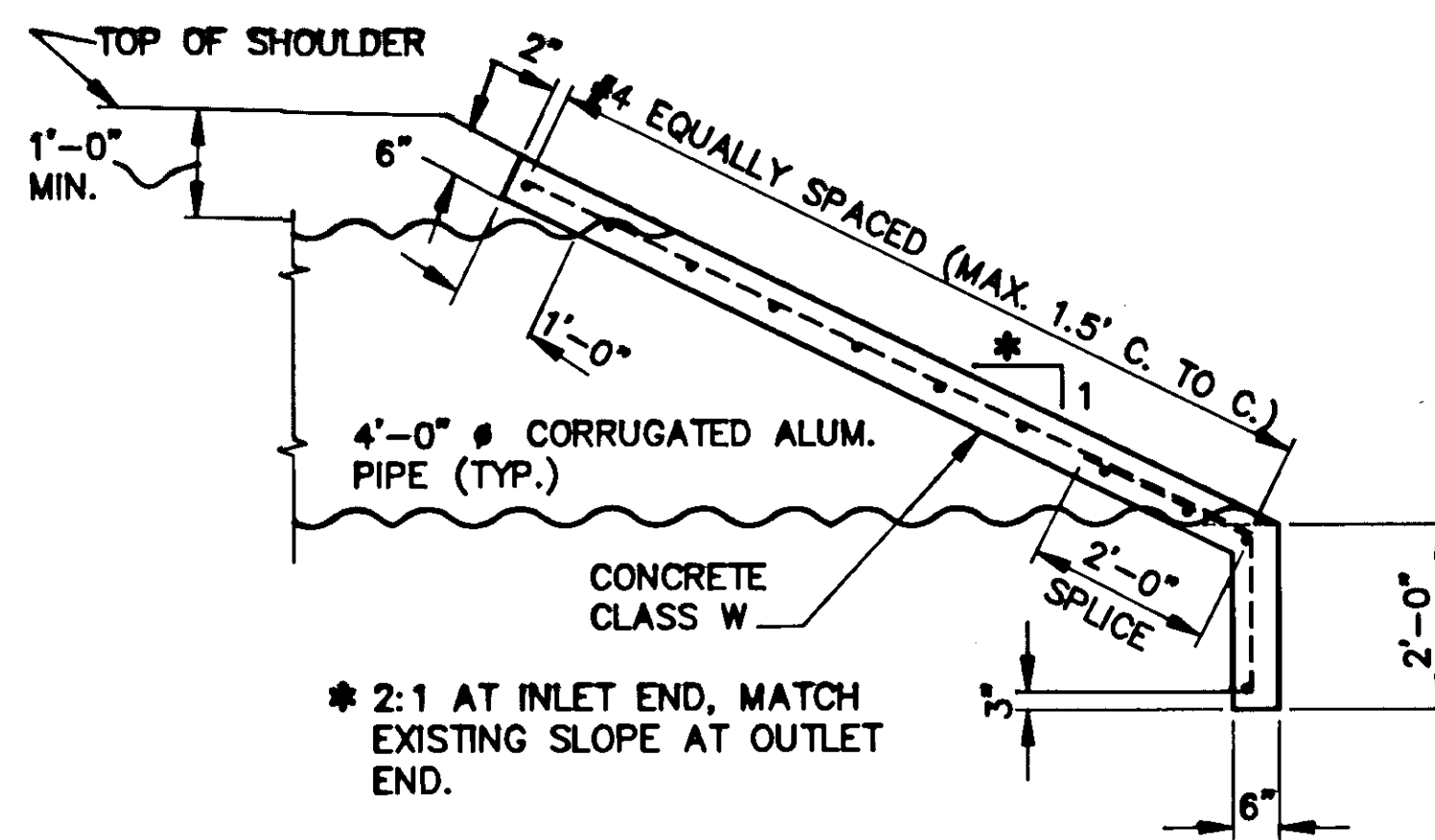


PLAN LAYOUT  
(GUARDRAIL OMITTED FOR CLARITY)

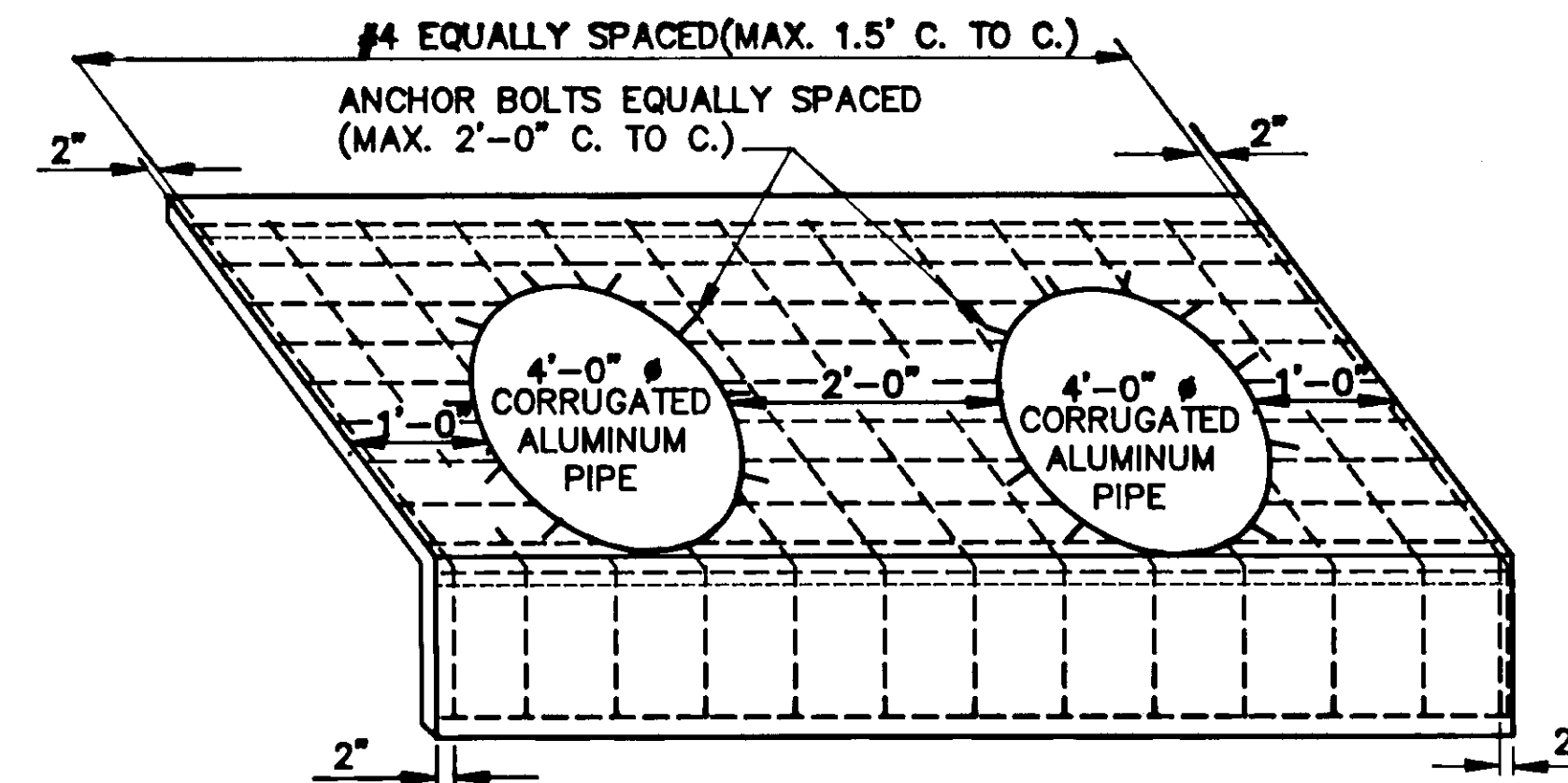
**ANCHOR BOLT NOTE :**

ANCHOR BOLTS OF THE SIZE SHOWN HEREON SHALL CONFORM WITH ASTM DESIGNATION A307 AND SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION A153 FOR EITHER STEEL OR ALUMINUM PIPES, OR SHALL CONFORM WITH ASTM DESIGNATION B211, ALLOY 2024-T4, TREATED WITH ZINC CHROMATE PAINT FOR ALUMINUM PIPES ONLY.

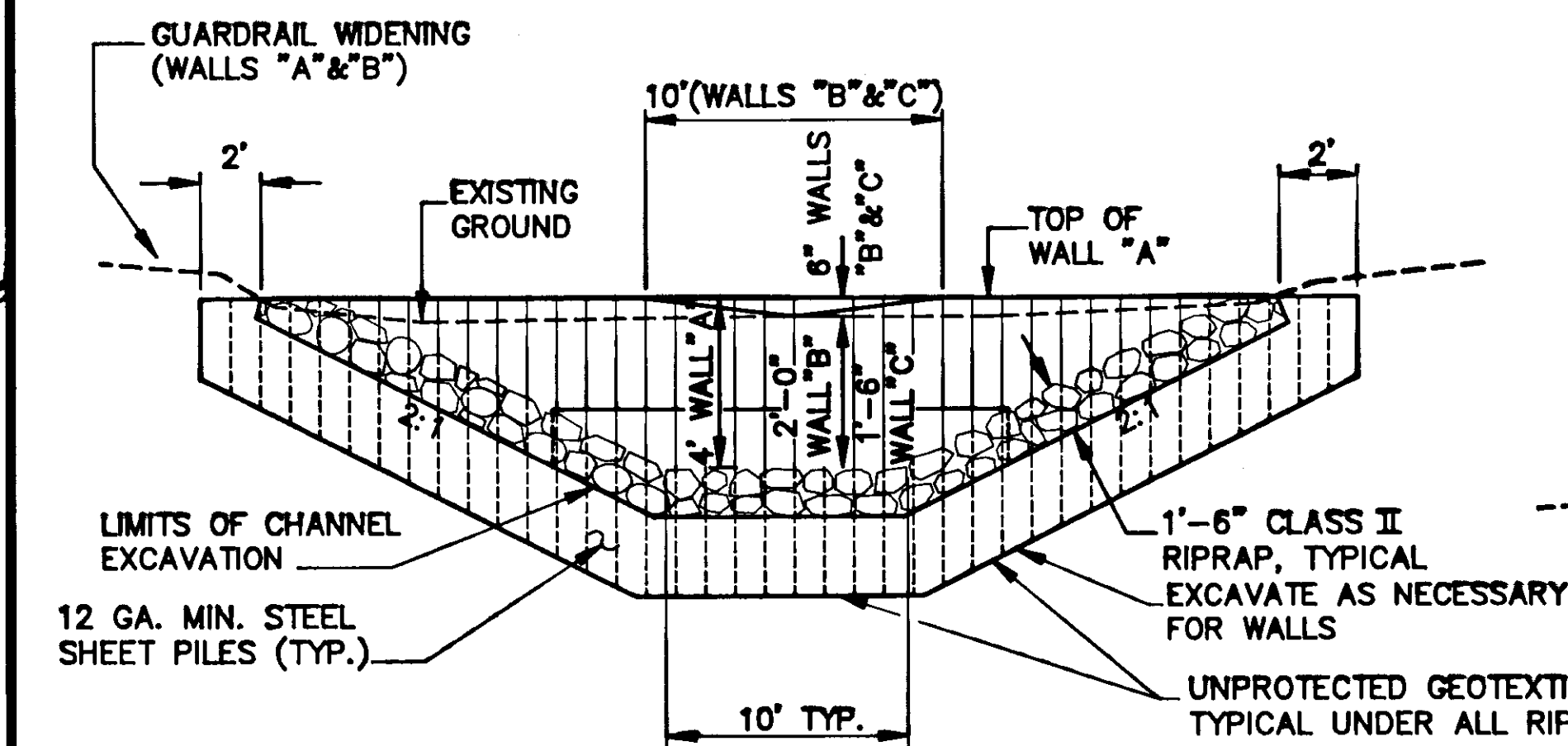
BASIS OF VERTICAL CONTROL FOR THIS CULVERT INSTALLATION CONSISTS OF A PK NAIL IN THE RIGHT EDGE OF PAVEMENT AT STA. "K" 54+57.24 WITH AN ASSUMED ELEVATION OF 44.69.



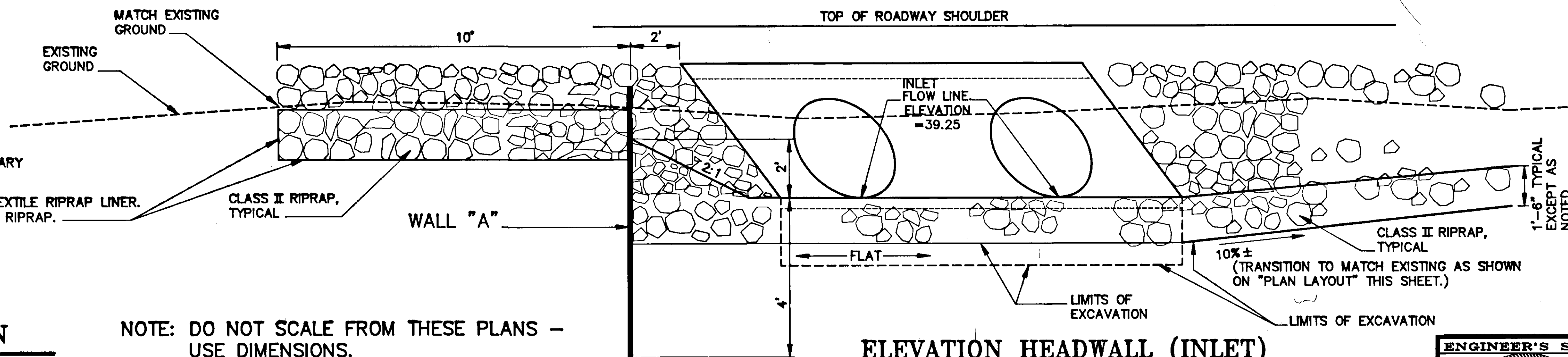
SECTION "A-A"  
FOR WALLS "B" & "C"



ANCHOR BOLT DETAILS



WALL ELEVATION & CHANNEL TYP. SECTION



ELEVATION HEADWALL (INLET)

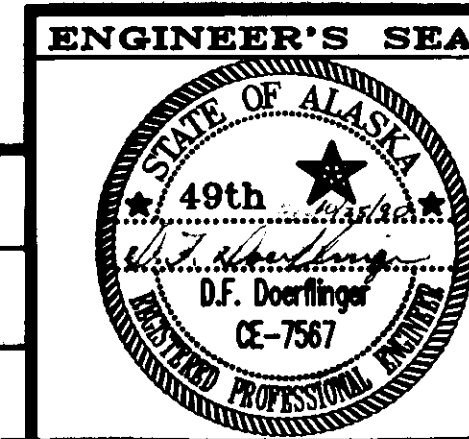
NOTE: DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS.

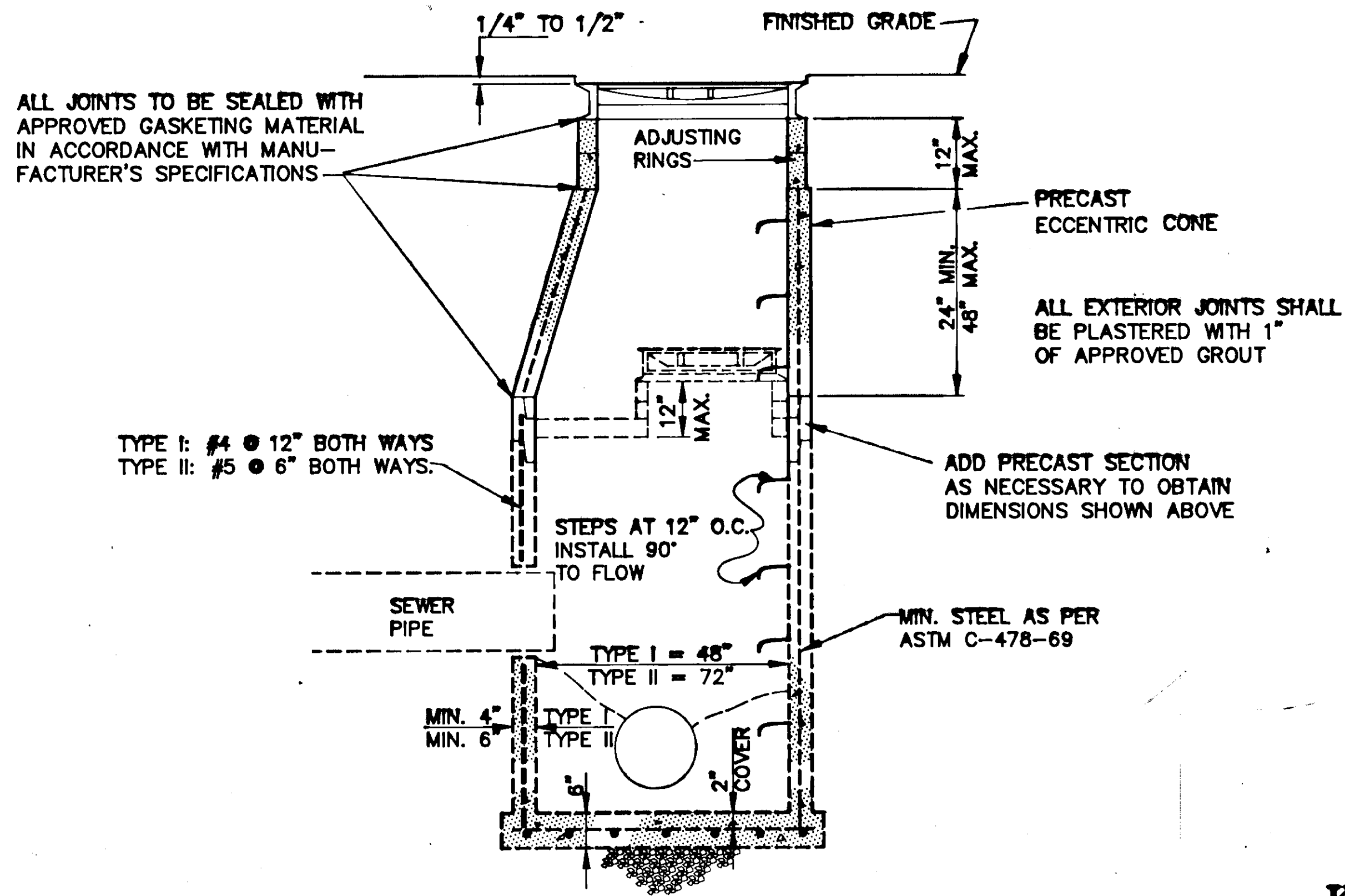
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1/10/96	AS BUILT

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

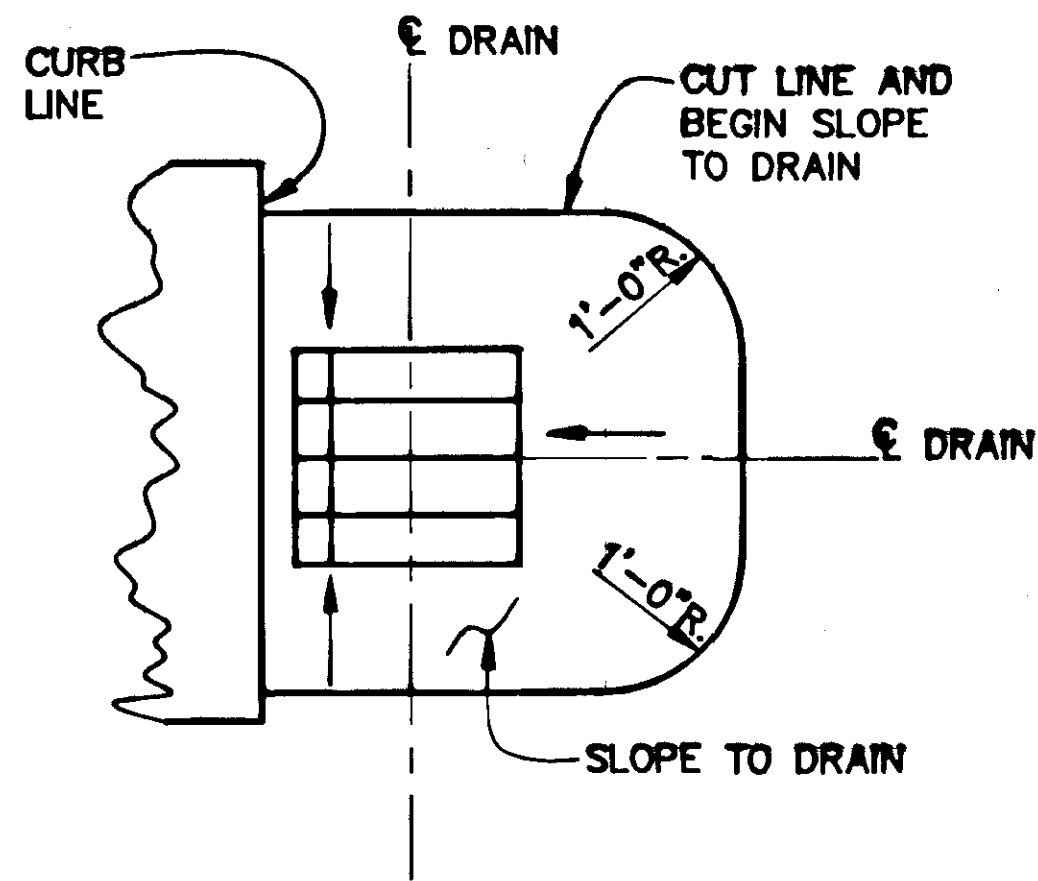
CRAIG TO KLAWOCK OVERLAY  
RS-0924 (12)  
CULVERT DETAILS AT "K" 54+41

DESIGNED BY: DOERFLINGER	PROJECT No. RS-0924(12)
DRAWN BY: AutoCAD / BWB	DATE: SEPT. 1990
CHECKED BY: DOERFLINGER	SHEET 20 OF 29

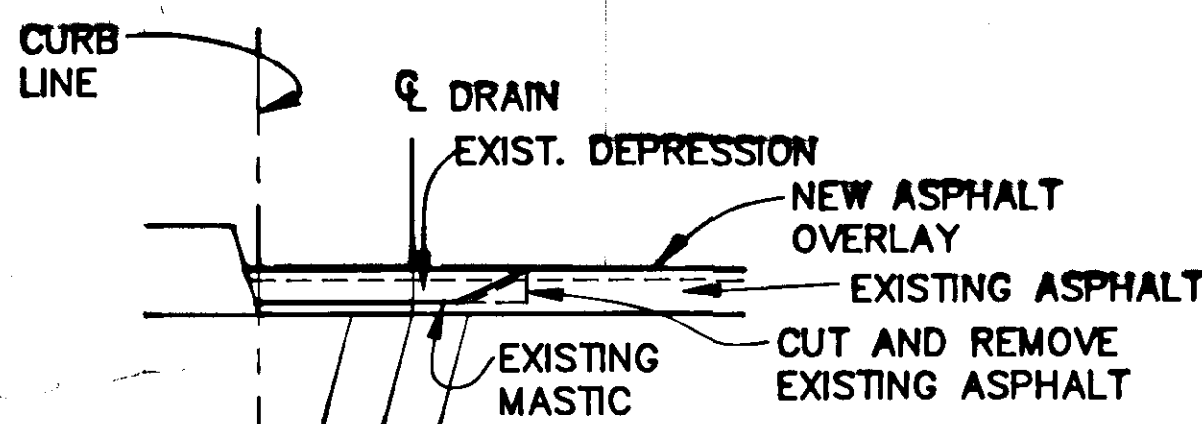




**MANHOLE RECONSTRUCTION DETAIL**



**PLAN**

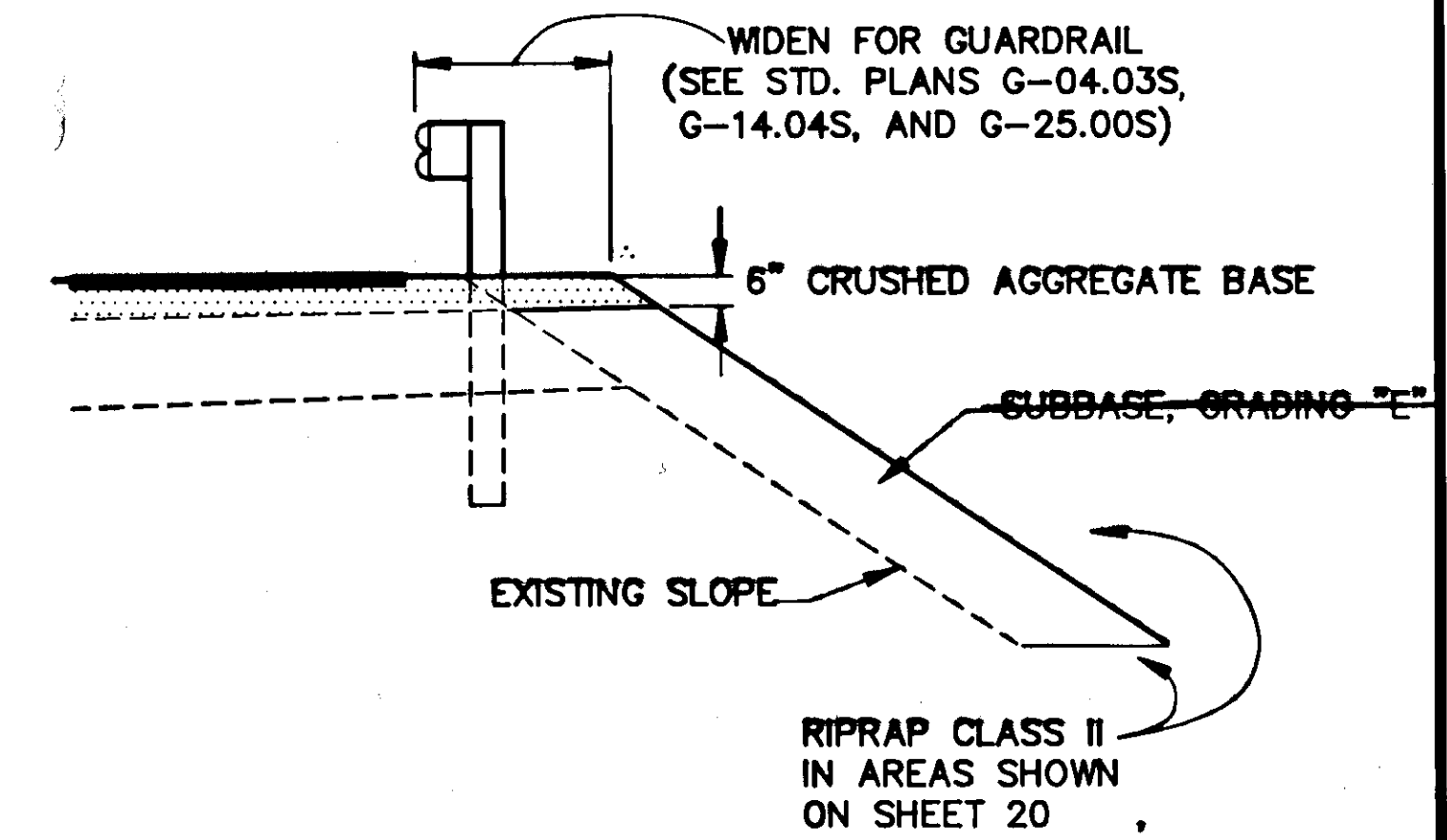
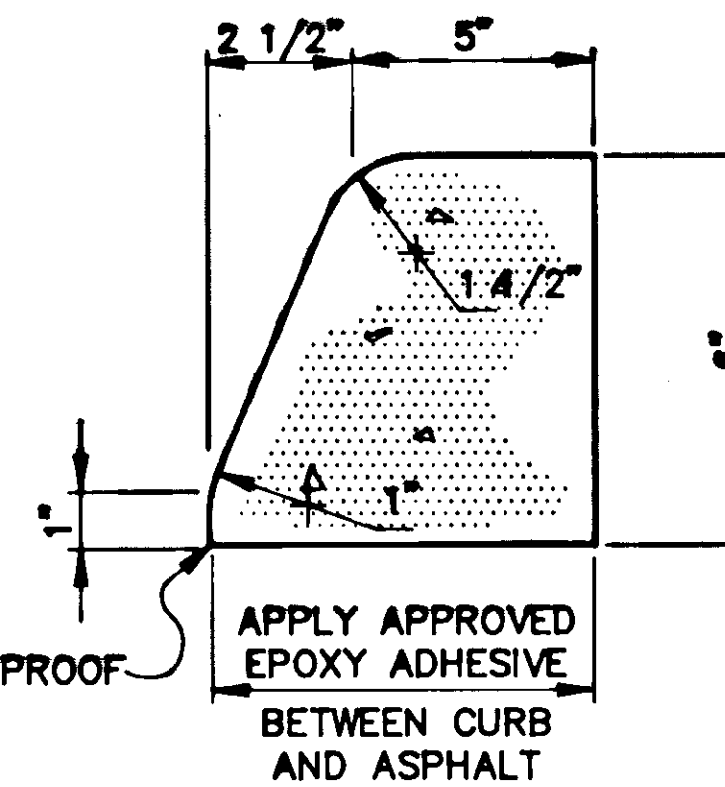


**ELEVATION**

**KLAWOCK RIVER BRIDGE  
ASPHALT DRAINAGE  
DETAIL**

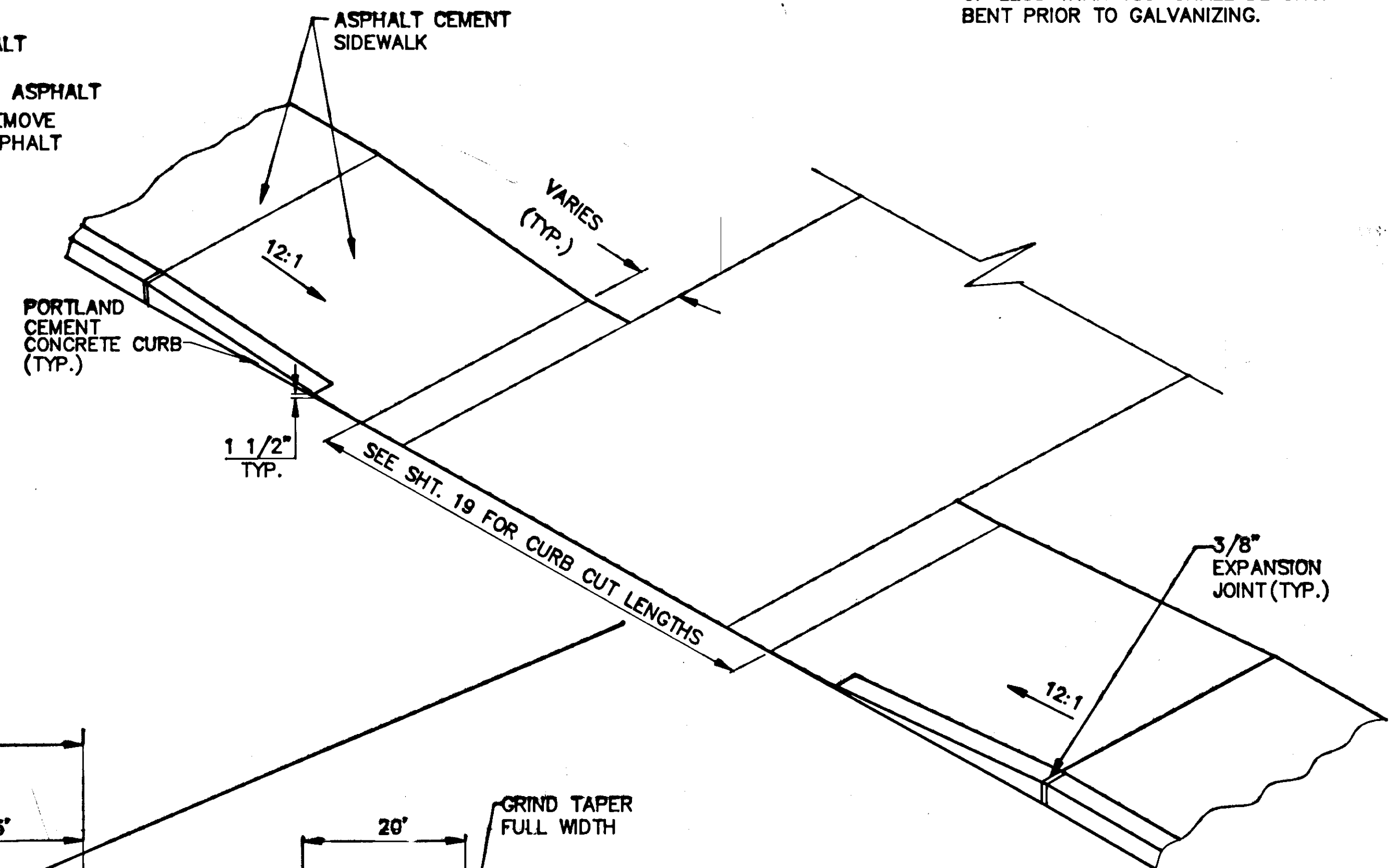
APPLY WATERPROOF SEALANT AT NOSING

**CURB, TYPE I**

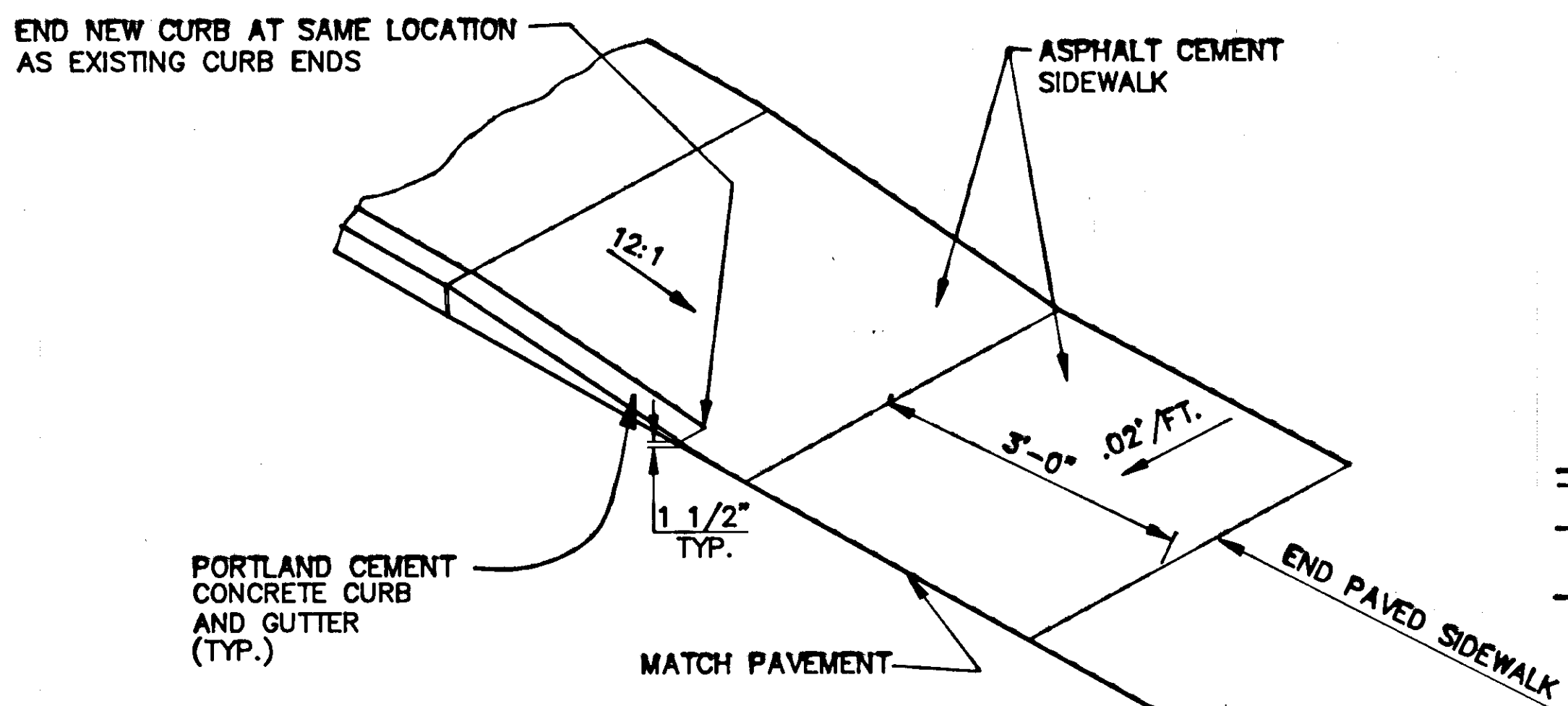


**GUARDRAIL WIDENING**

"K" 63+30 TO "K" 56+70  
NOTE:  
GUARDRAIL TO BE BENT TO A RADIUS OF LESS THAN 150' SHALL BE SHOP BENT PRIOR TO GALVANIZING.

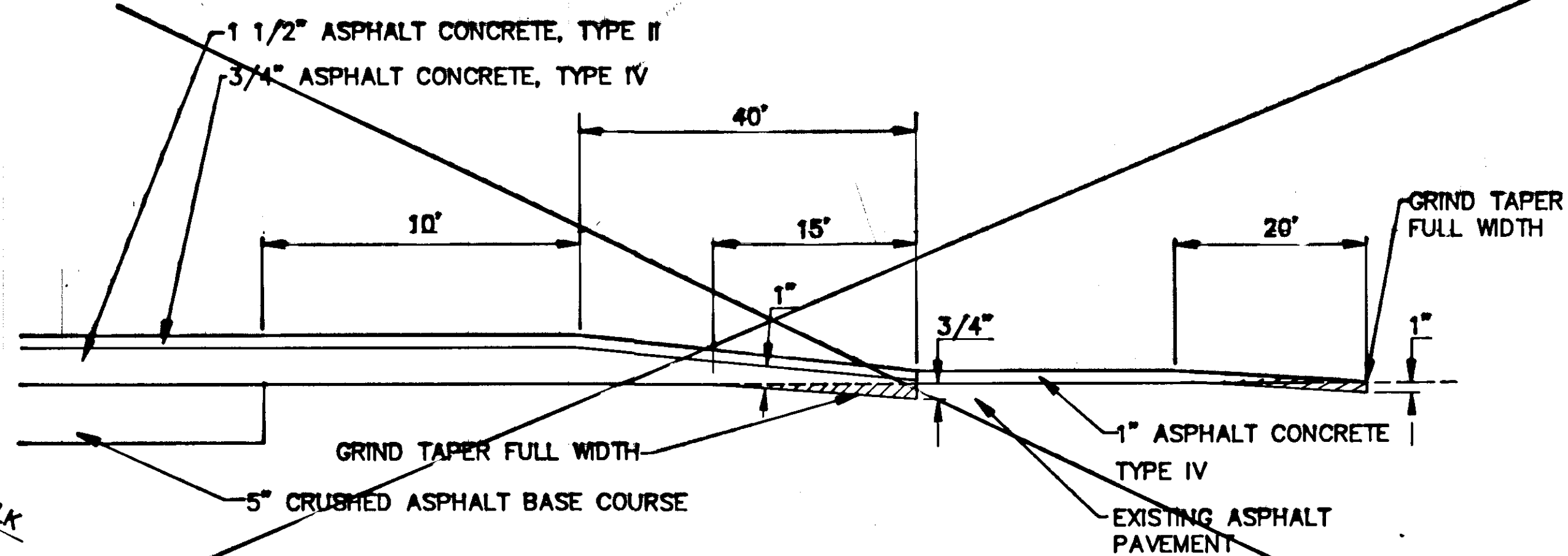


**CURB CUT DETAIL  
FOR DRIVEWAYS**



**CURB RAMP DETAIL**

TYPICAL AT RADIUS ENDS OF SIDE STREET APPROACHES



**"K" LINE PAVEMENT TAPER DETAIL**

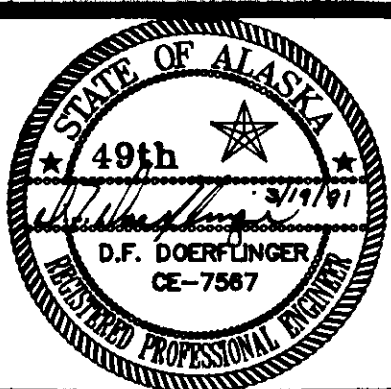
NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS-BUILT
RECORD OF REVISIONS		

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

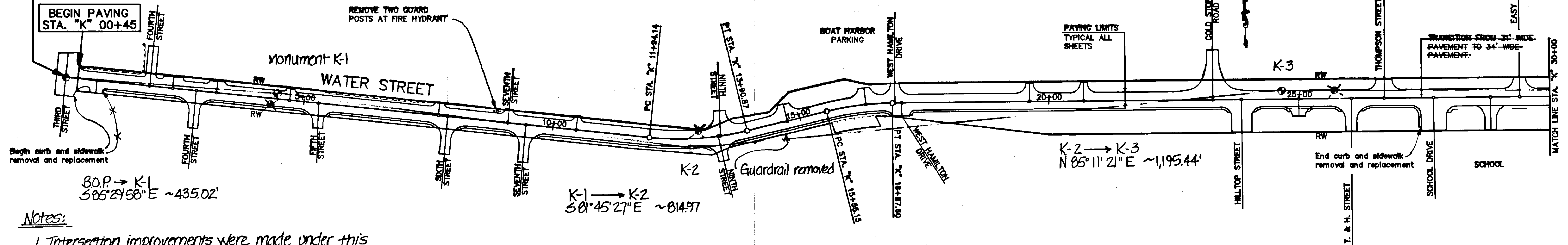
CRAIG TO KLAWOCK OVERLAY  
RS-0924(12)  
MISCELLANEOUS DETAILS

DESIGNED BY: D. DOERFLINGER	PROJECT NO. RS-0924(12)
DRAWN BY: AUTOCADD / R. Snyder	DATE: 9/90
CHECKED BY:	SHEET 21 OF 29



B.O.P.  
STA. "K" 00+21.18

BEGIN PAVING  
STA. "K" 00+45



B.O.P. → K-1  
S 85° 29' 58" E ~435.02'

K-1 → K-2  
S 81° 45' 27" E ~814.97

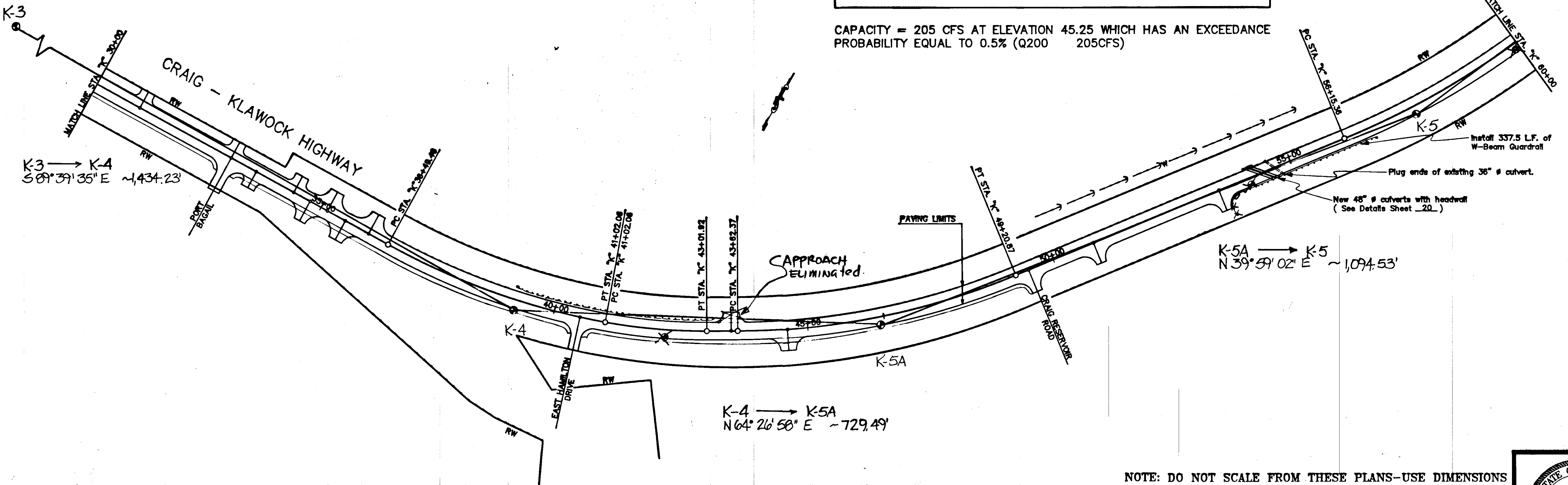
K-2 → K-3  
N 85° 11' 21" E ~1,195.44'

- Notes:
1. Intersection improvements were made under this contract at several sidestreet intersections with the mainline (Water Street); this work occurred at Third St, Fourth St (south side), Seventh St, West Hamilton Dr, Cold Storage Road, Hilltop St and Craig Reservoir Road.
  2. The existing concrete curb and asphaltic sidewalk was extended from the High School to the Chan Sest trailer park near Craig Reservoir Road.
  3. The existing centerline monument cases were removed under this contract and not paved over; the actual monuments were left in-place but buried.

NOTE: Existing Monuments are located at all P.C.'s, P.T.'s and P.C.C.'s.

HYDROLOGIC AND HYDRAULIC SUMMARY	
DOUBLE 4' CAP CULVERT	STA. "K" 54+41.00
DRAINAGE AREA	.5 SQUARE MILES
EXCEEDENCE PROBABILITY	1%      2%
DESIGN DISCHARGE	150 CFS    175 CFS
DESIGN HIGH WATER	43.25 FT.    44.15 FT.
ANTICIPATED BACKWATER = 0 FT.	

CAPACITY = 205 CFS AT ELEVATION 45.25 WHICH HAS AN EXCEEDANCE PROBABILITY EQUAL TO 0.5% (Q200 205CFS)



K-3 → K-4  
S 89° 39' 35" E ~1,434.23

K-4 → K-5A  
N 64° 26' 58" E ~729.49'

K-5A → K-5  
N 39° 59' 02" E ~1,094.53'

NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

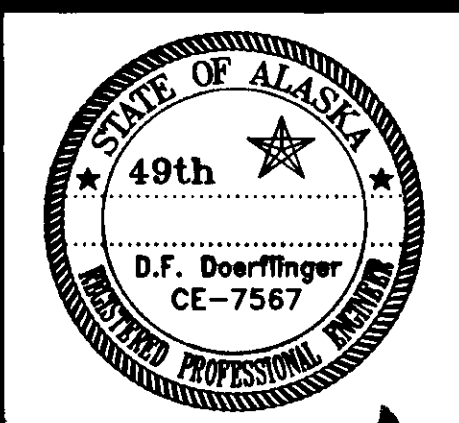
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	A6 BUILT

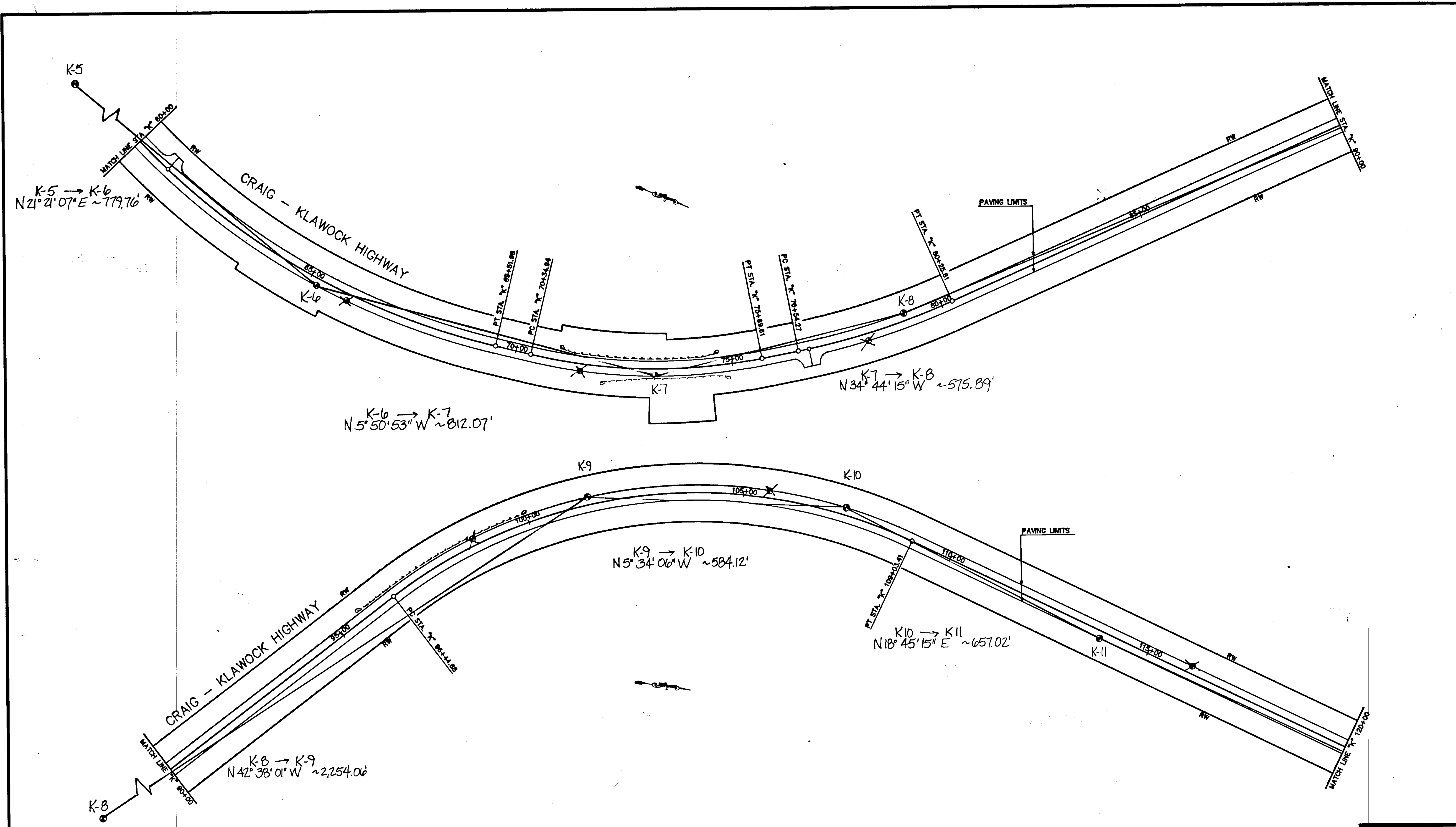
RECORD OF REVISIONS

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KLAWOCK OVERLAY  
RS-0924 (12)  
B.O.P. STA. "K" 00+21.18 TO STA. "K" 60+00

DESIGNED BY:	D. DOERFLINGER	PROJECT NO.	RS-0924 (12)
DRAWN BY:	AUTOCADD/R.S.	DATE:	9/90
CHECKED BY:	D. STERLEY	SHEET	22 OF 29





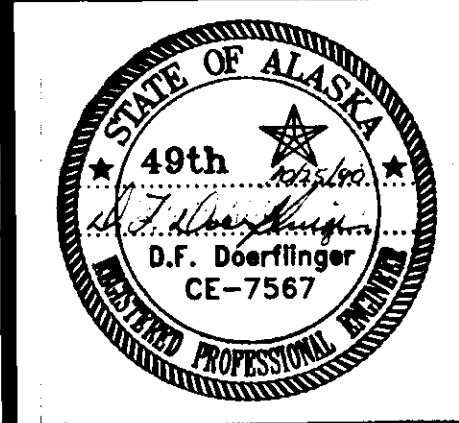
NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

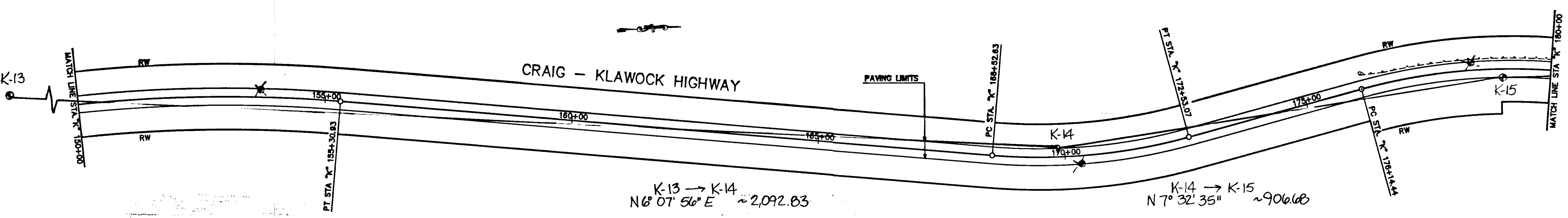
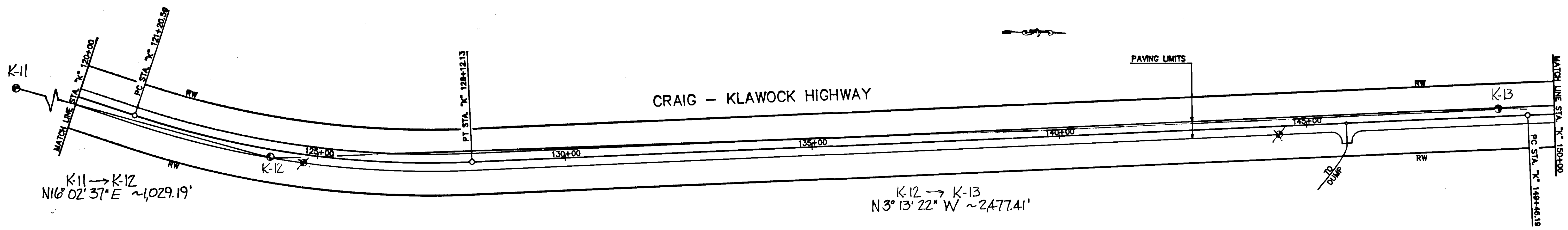
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS-BUILT
RECORD OF REVISIONS		

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KLAWOCK OVERLAY  
 RS-0924 (12)  
 STA. "K" 60+00 TO STA. "K" 120+00

DESIGNED BY:	D. DOERFLINGER	PROJECT NO.	RS-0924 (12)
DRAWN BY:	AUTOCADD/R.S.	DATE:	9/90
CHECKED BY:	D. STERLEY	SHEET	23 OF 29





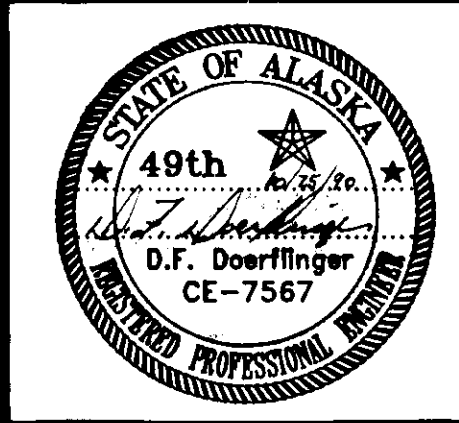
NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

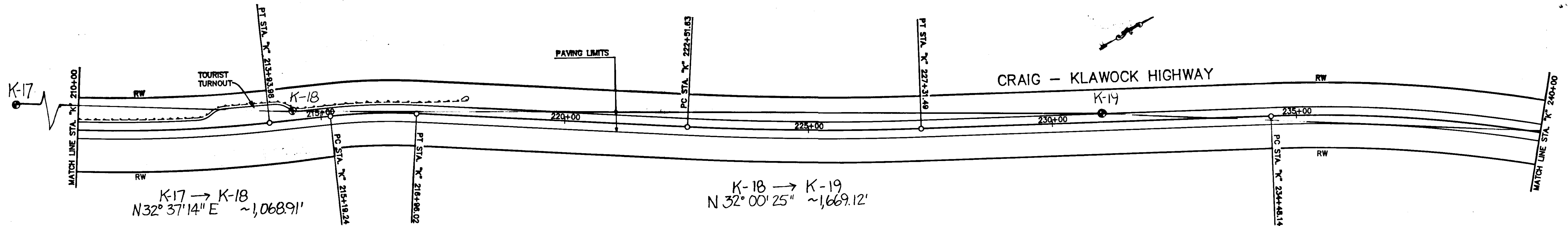
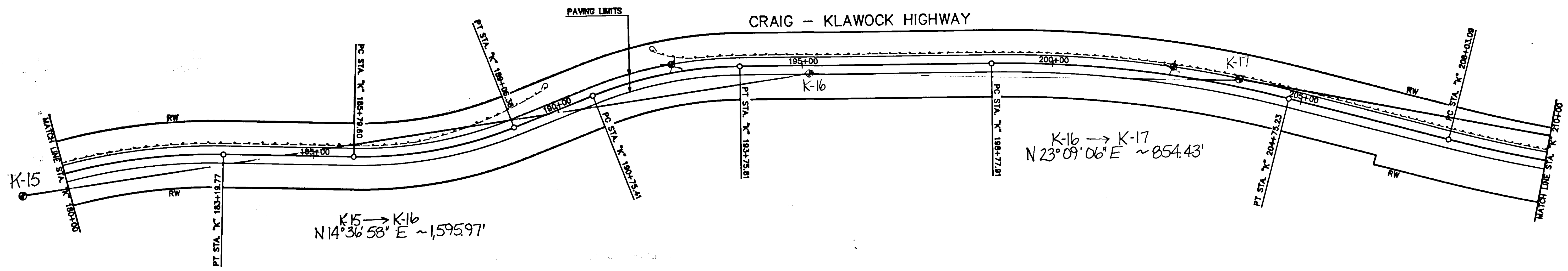
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS BUILT
RECORD OF REVISIONS		

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KLAWOCK OVERLAY  
 RS-0924 (12)  
 STA. "K" 120+00 TO STA. "K" 180+00

DESIGNED BY: D. DOERFLINGER	PROJECT NO. RS-0924 (12)
DRAWN BY: AUTOCADD/R.S.	DATE: 9/90
CHECKED BY: D. STERLEY	SHEET 24 OF 29





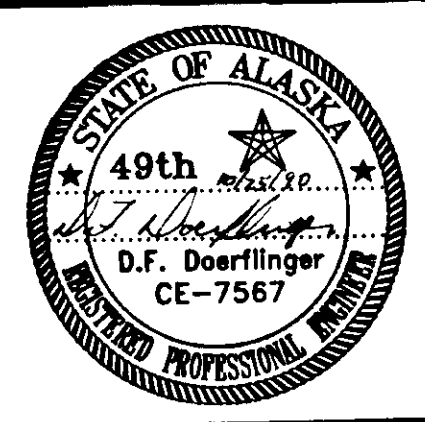
NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

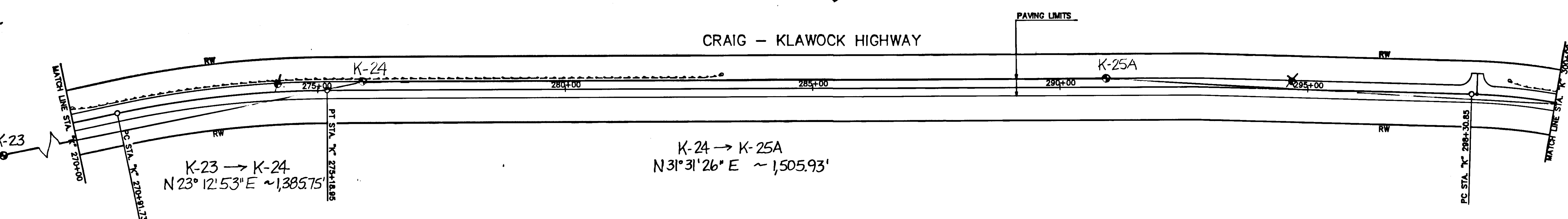
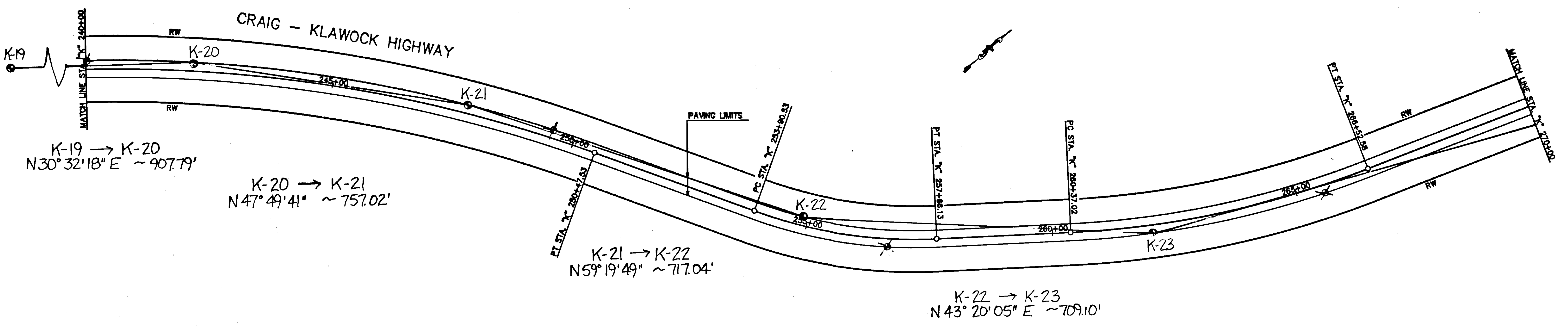
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS BUILT
RECORD OF REVISIONS		

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KLAWOCK OVERLAY  
 RS-0924 (12)  
 STA. "K" 180+00 TO STA. "K" 240+00

DESIGNED BY: D. DOERFLINGER	PROJECT NO. RS-0924 (12)
DRAWN BY: AUTOCADD/R.S.	DATE: 9/90
CHECKED BY: D. STERLEY	SHEET 25 OF 29





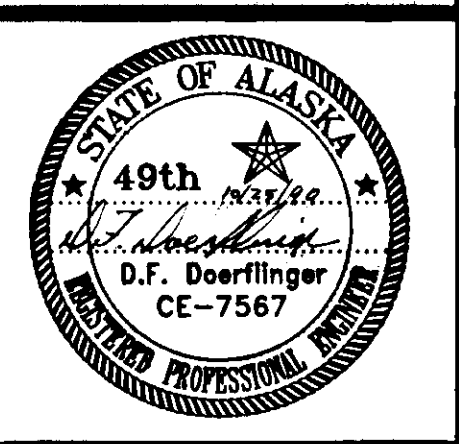
NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

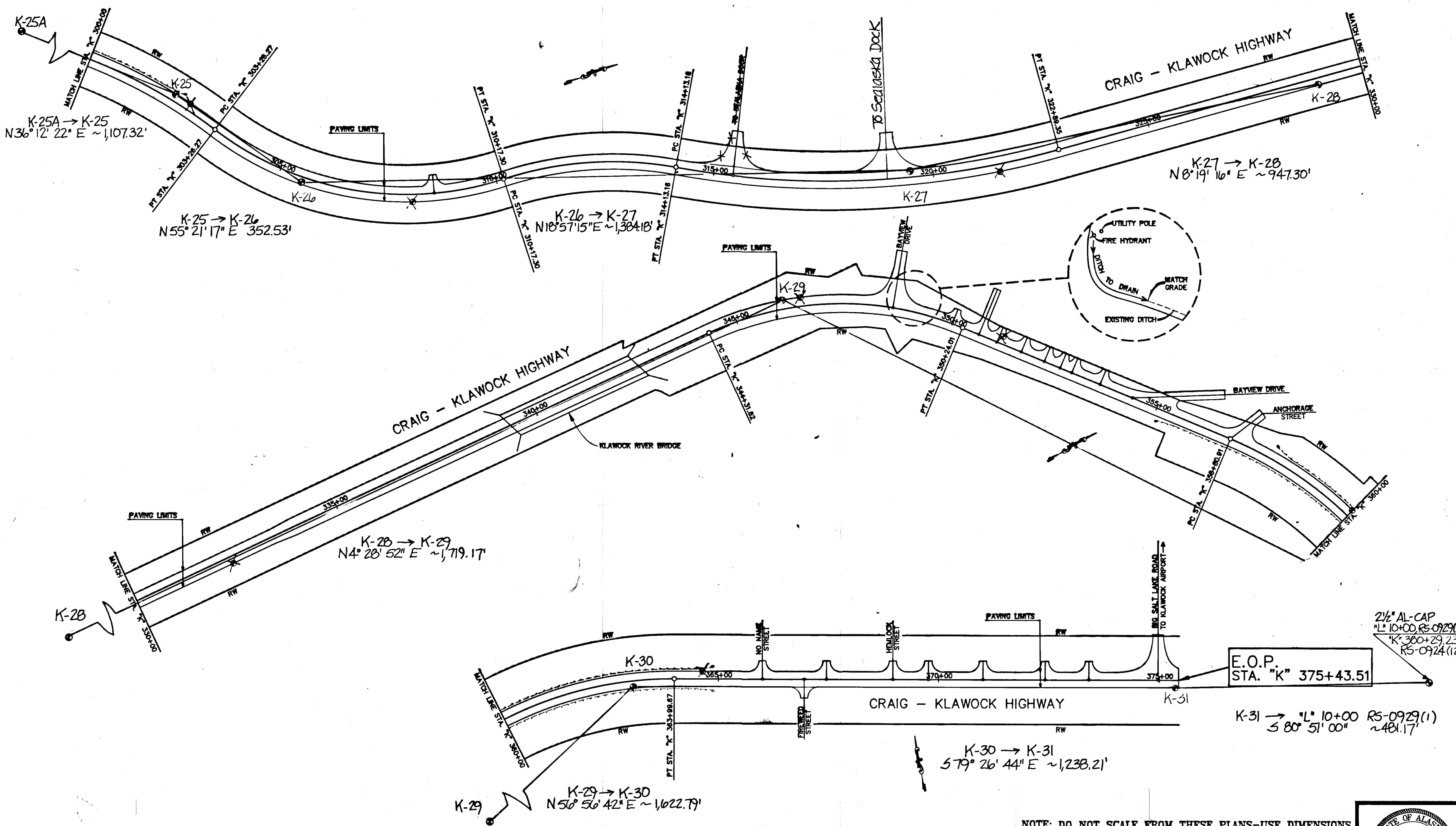
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS BUILT
RECORD OF REVISIONS		

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KLAWOCK OVERLAY  
 RS-0924 (12)  
 STA. "K" 240+00 TO STA. "K" 300+00

DESIGNED BY:	D. DOERFLINGER	PROJECT NO.	RS-0924 (12)
DRAWN BY:	AUTOCADD/R.S.	DATE:	9/90
CHECKED BY:	D. STERLEY	SHEET	26 OF 29





E.O.P. STA. "K" 375+43.51

2 1/2" AL-CAP  
 "L" 10+00, RS-0929(1)  
 "K" 380+29.23  
 RS-0924(12)

K-31 → "L" 10+00 RS-0929(1)  
 3 80° 51' 00" E ~481.17'

K-30 → K-31  
 5 79° 26' 44" E ~1,238.21'

K-29 → K-30  
 N 56° 56' 42" E ~1,622.79'

K-28 → K-29  
 N 4° 28' 52" E ~1,719.17'

K-26 → K-27  
 N 18° 57' 15" E ~1,384.18'

K-27 → K-28  
 N 8° 19' 16" E ~947.30'

K-25 → K-26  
 N 55° 21' 17" E 352.53'

K-25A → K-25  
 N 36° 12' 22" E ~1,107.32'

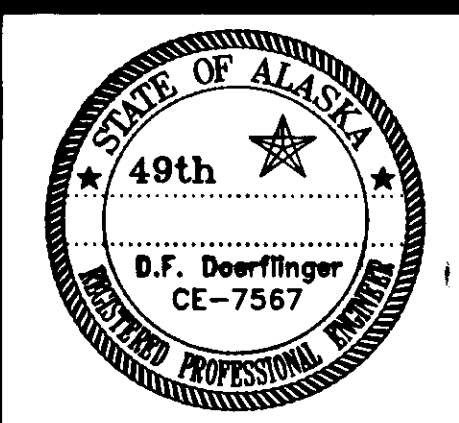
NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

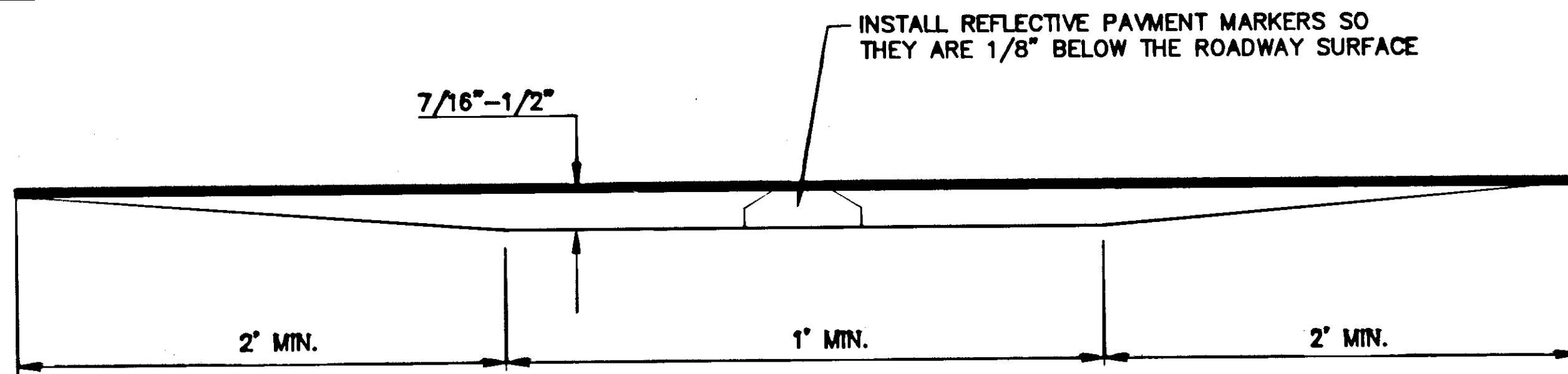
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS-BUILT
RECORD OF REVISIONS		

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

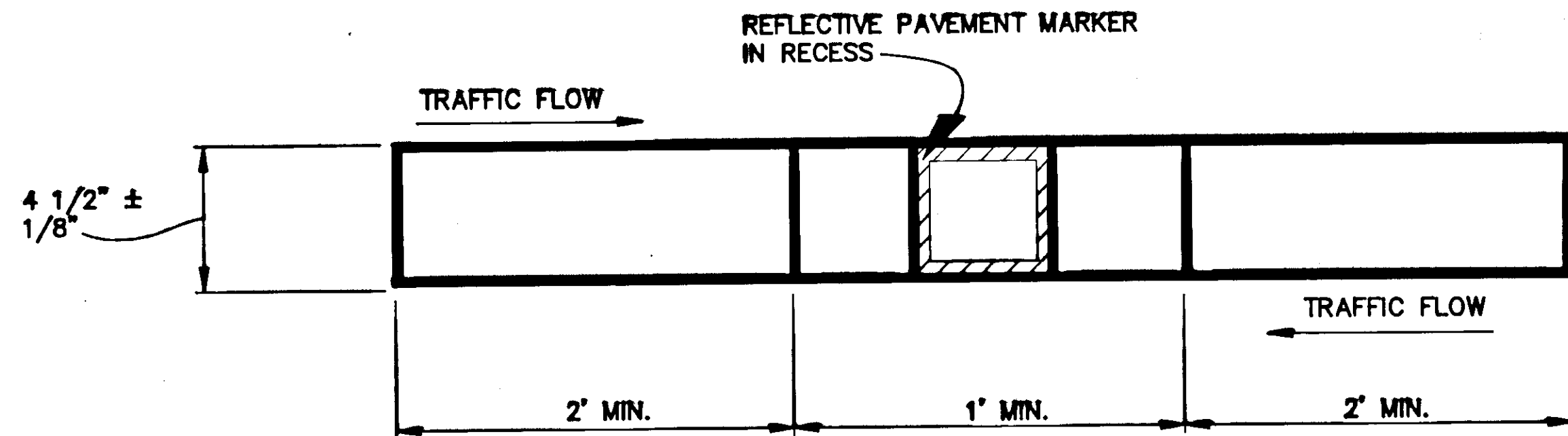
CRAIG TO KLAWOCK OVERLAY  
 RS-0924 (12)  
 STA. "K" 300+00 TO E.O.P. STA."K" 375+43.51

DESIGNED BY: D. DOERFLINGER	PROJECT NO. RS-0924 (12)
DRAWN BY: AUTOCADD/R.S.	DATE: 9/90
CHECKED BY: D. STERLEY	SHEET 27 OF 29

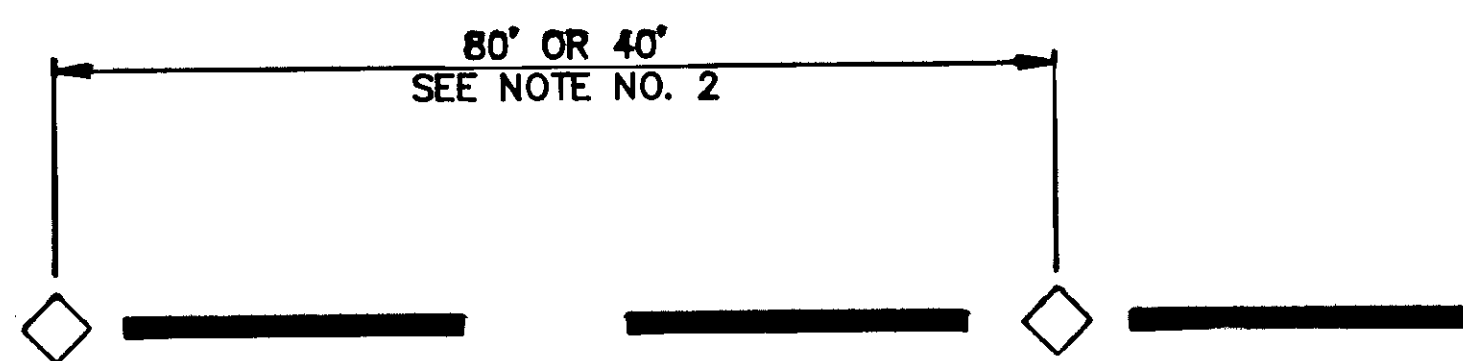




**SECTION X-X**



**PLAN**



**INSTALLATION  
REFLECTIVE PAVEMENT MARKER  
DETAILS**

**RECESSED PAVEMENT MARKER NOTES**

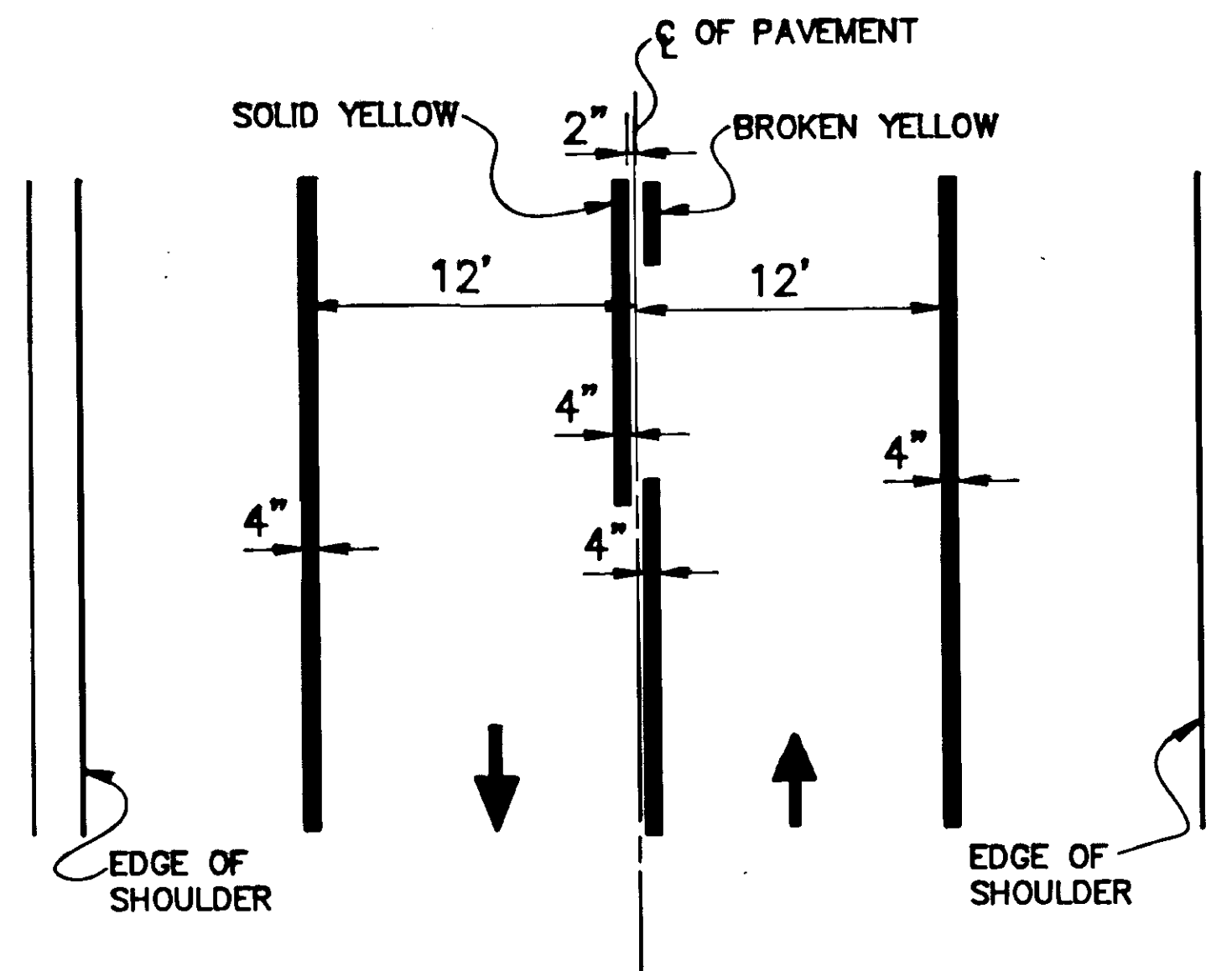
1. INSTALL REFLECTIVE PAVEMENT MARKERS BETWEEN STA. "K"0+45 TO "K"375+43
2. R.P.M.'S SHALL BE PLACED BETWEEN THE DASHED CENTER STRIPE OR BETWEEN DOUBLE NO PASSING STRIPES ON CURVES AND SHALL BE SPACED 80' ON TANGENT AND 40' ON CURVES
3. INSTALLATION OF R.P.M.'S SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.

**PAINTED PAVEMENT MARKING  
QUANTITIES**

DOUBLE YELLOW	5.1 MILES
SINGLE YELLOW WITH WHITE DASH	1.7 MILES
WHITE DASH	.5 MILES

**STRIPING NOTE**

1. PAVEMENT MARKINGS SHALL CONSIST OF CENTERLINE AND SHOULDER, IN ACCORDANCE WITH STANDARD DRAWING T-21.01, AND THE ALASKA TRAFFIC MANUAL, EXCEPT AS DETAILED ON THIS SHEET.



**STRIPING PLACEMENT  
WHEN RECESSED PAVEMENT MARKERS  
ARE INSTALLED**

**LANE CLOSURE, EXTENDED DURATION NOTES**

1. FLOOD LIGHTS SHALL BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
2. STEADY BURN WARNING LIGHTS SHALL BE USED TO MARK CHANNELIZING DEVICES AT NIGHT AS NEEDED.
3. CHANNELIZING DEVICES ARE TO BE EXTENDED TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.

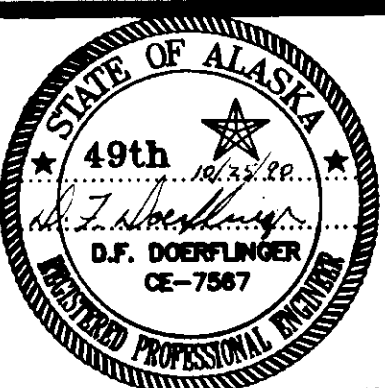
NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

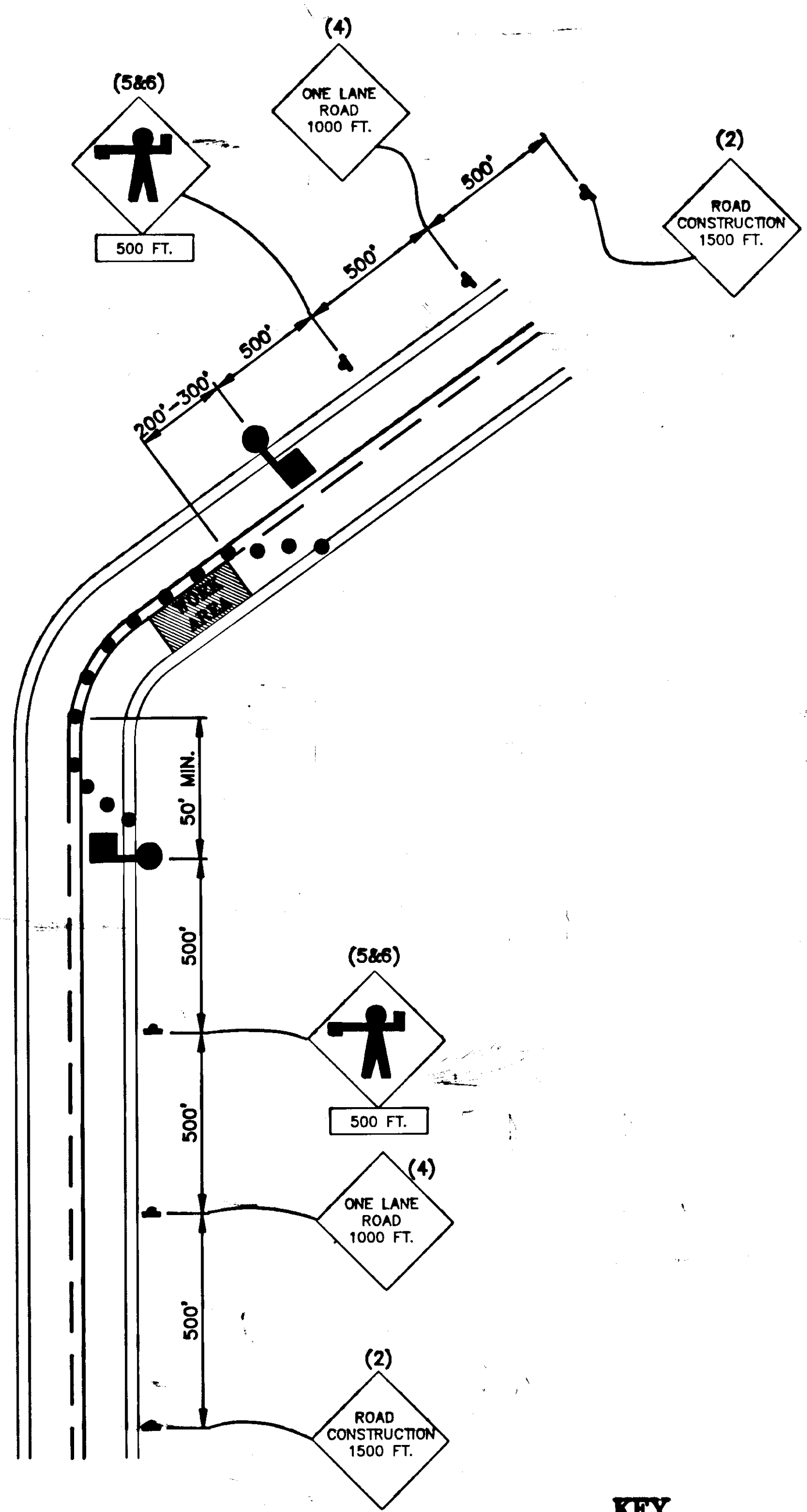
BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS BUILT
RECORD OF REVISIONS		

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KLAWOCK  
RS-0924(12)  
REFLECTIVE PAVEMENT MARKER DETAILS

DESIGNED BY: D. STERLEY	PROJECT NO. RS-0924(12)
DRAWN BY: AUTOCADD/CSA	DATE: 9/90
CHECKED BY: D. DOERFLINGER	SHEET 28 OF 29



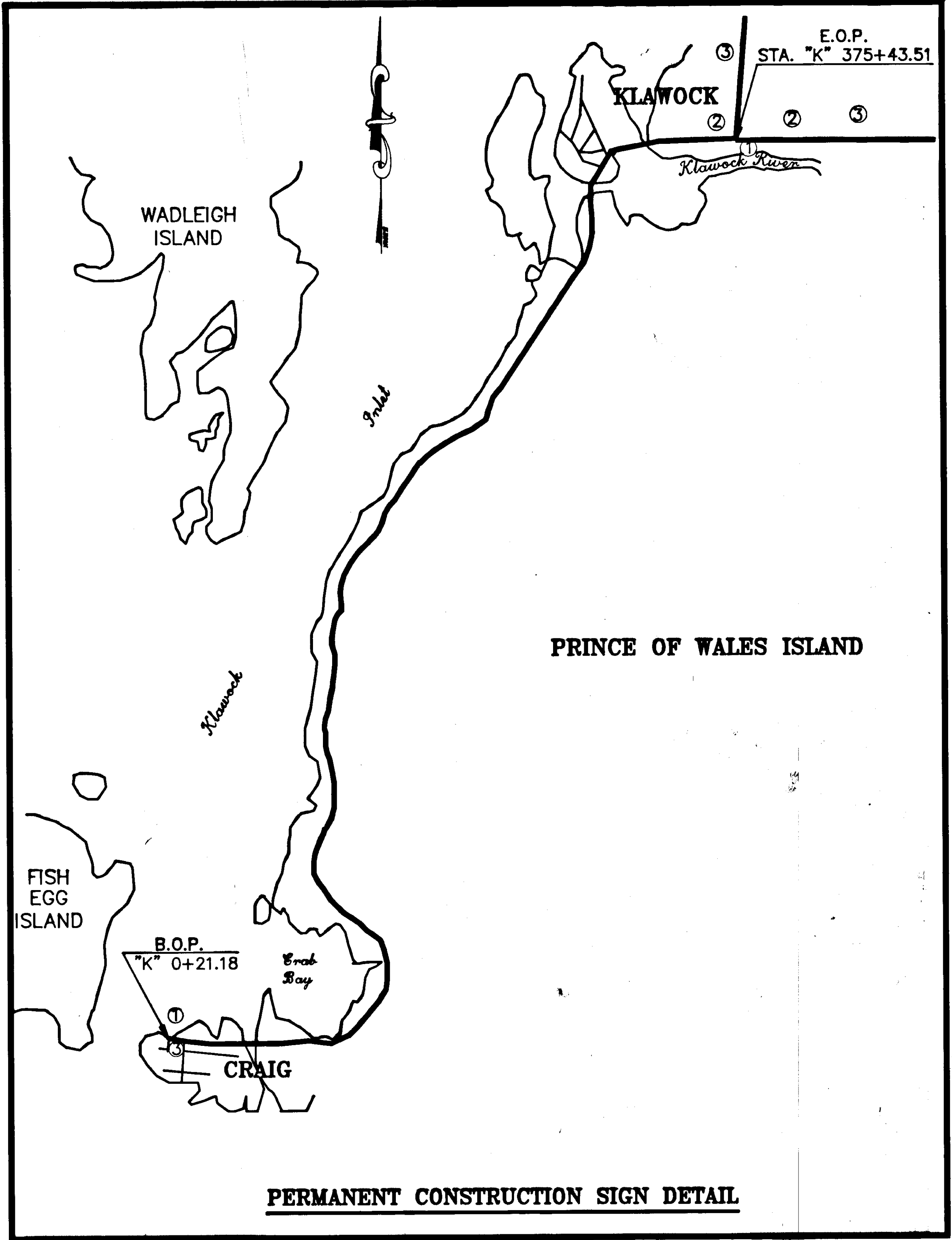


TYPICAL ONE LANE ROAD

**GENERAL NOTES**

1. IF ENTIRE WORK AREA IS VISIBLE FROM ONE STATION, SINGLE FLAGGER MAY BE USED.
2. WARNING LIGHTS SHOULD BE USED TO MARK CHANNELIZING DEVICES AT NIGHT AS NEEDED.
3. CHANNELING DEVICES ARE TO BE EXTENDED TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.
4. CONTRACTOR SHALL MAINTAIN SINGLE LANE TRAFFIC THRU THE WORK SITE AT ALL TIMES.

CONSTRUCTION SIGNING				
NO.	CODE	DESCRIPTION	SIZE	QUANT.
4	CW20-4B	ONE LANE ROAD 1,000 FT.	48"X48"	2
5	CW20-7	FLAGGER SYMBOL	36"X36"	3
6	CW20-7	FLAGGER PLAQUE	24"X24"	3
PERMANENT CONSTRUCTION SIGNS				
①	G20-2	END CONSTRUCTION	60"X24"	2
②	CW20-IE	ROAD CONSTRUCTION 1500 FT.	48"X48"	2
③	CW20-IF	ROAD CONSTRUCTION AHEAD	48"X48"	3



PERMANENT CONSTRUCTION SIGN DETAIL

NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

BY:	DATE:	DESCRIPTION OF CHANGE:
KK	1-10-96	AS - GUILI

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

CRAIG TO KLAWOCK OVERLAY  
 RS-0924(12)  
 TRAFFIC CONTROL PLAN

DESIGNED BY:	D. STERLEY	PROJECT NO.	70190
DRAWN BY:	AUTOCADD/CSA	DATE:	9/90
CHECKED BY:	D. DOERFLINGER	SHEET	29 OF 29

