

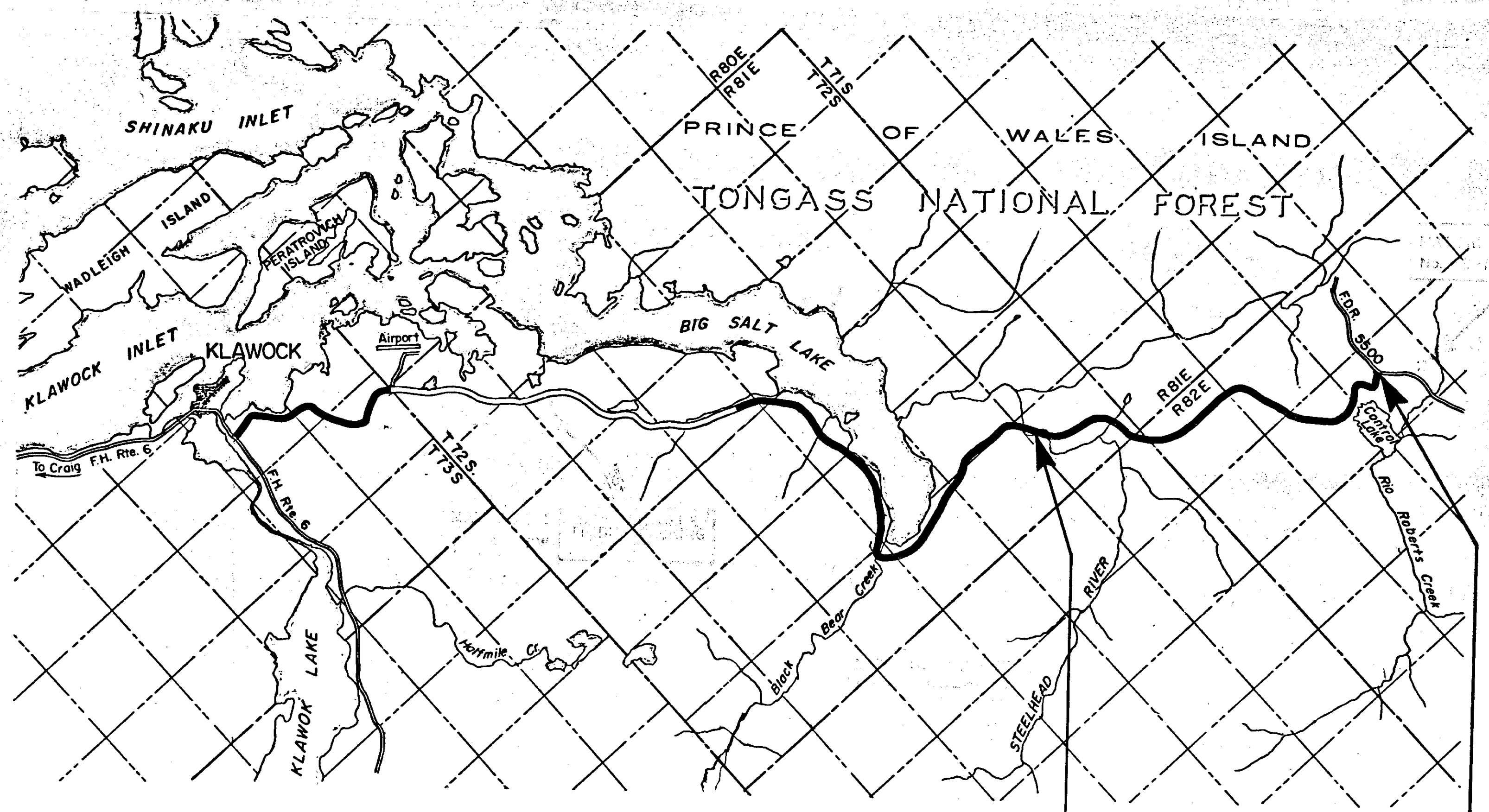
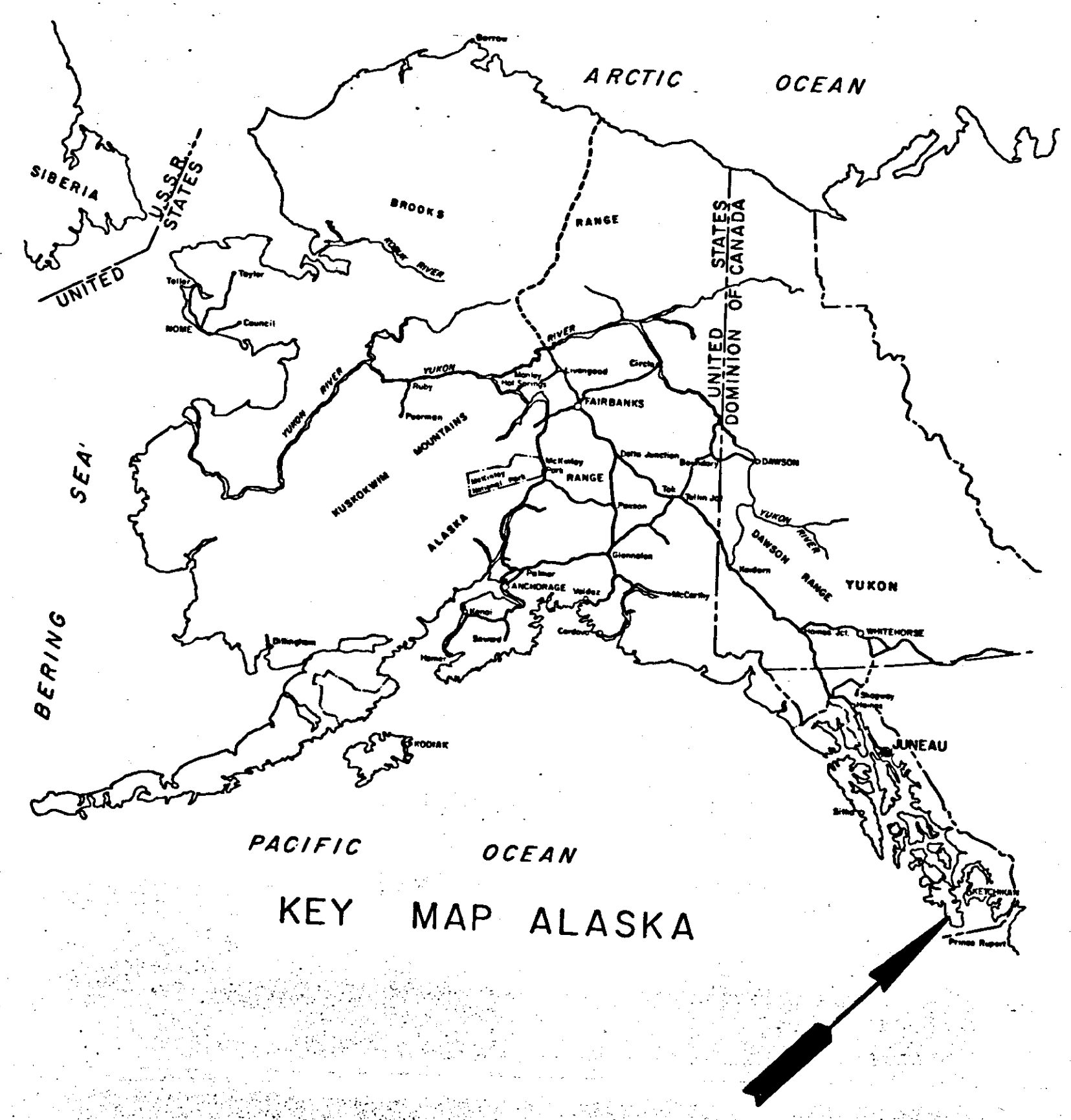
# AS CONSTRUCTED PLANS

PREPARED BY  
 U.S. DEPARTMENT OF TRANSPORTATION  
 FEDERAL HIGHWAY ADMINISTRATION  
 WESTERN DIRECT FEDERAL DIVISION  
 FOR

ALASKA FOREST HIGHWAY PROJECT 9-1(3)  
 & FEDERAL LANDS PROJECT FLH 9-1(1)

## BIG SALT HIGHWAY LENGTH 4.735 MILES CLASS 2 TONGASS NATIONAL FOREST ALASKA

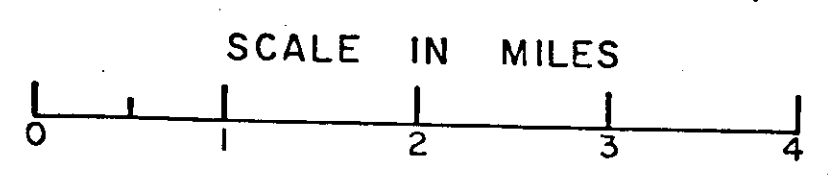
INDEX TO SHEETS	
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	TYPICAL SECTION
3	TABLE OF SPECIAL DITCHES
4-8	STRAIGHT LINE GRAPH



**CONVENTIONAL SYMBOLS**

BOUNDARY LINE	=====
TOWNSHIP OR RANGE LINE	-----
SECTION LINE	-----
QUARTER SECTION LINE	-----
TEENTH LINE	-----
NATIONAL PARK OR FOREST BOUNDARY	=====
LINE TO BE CONSTRUCTED	-----
MIT OF SLOPE LINE TOP OF CUT	-----
MIT OF SLOPE LINE TOE OF FILL	-----
PROPOSED RIGHT OF WAY LINE	-----
EXISTING RIGHT OF WAY LINE	-----
LIMITED ACCESS RIGHT OF WAY	-----
PAVELED WAY	-----
RAILROAD	-----
EXISTING FENCE	-----
EXISTING CULVERT	-----
CULVERT TO BE CONSTRUCTED	-----
EXISTING BRIDGE	-----
PROPOSED BRIDGE	-----
POWER POLES (EXISTING & PROPOSED)	-----
TELEPHONE/TELEGRAPH POLES (EXIST. & PROP.)	-----
WATER USE POLES (EXISTING & PROPOSED)	-----

*Description of Project*  
 Improvement: Grading of  
 Project Began: 9/28/81  
 Completed: 8/01/82  
 Engineer: Larry Addms  
 Plans: 100-222  
 Prepared: Mark Eastman  
 Checked: John Buckoff 12/21/82  
 Reviewed: Raymond L. Scholt 12/21/82



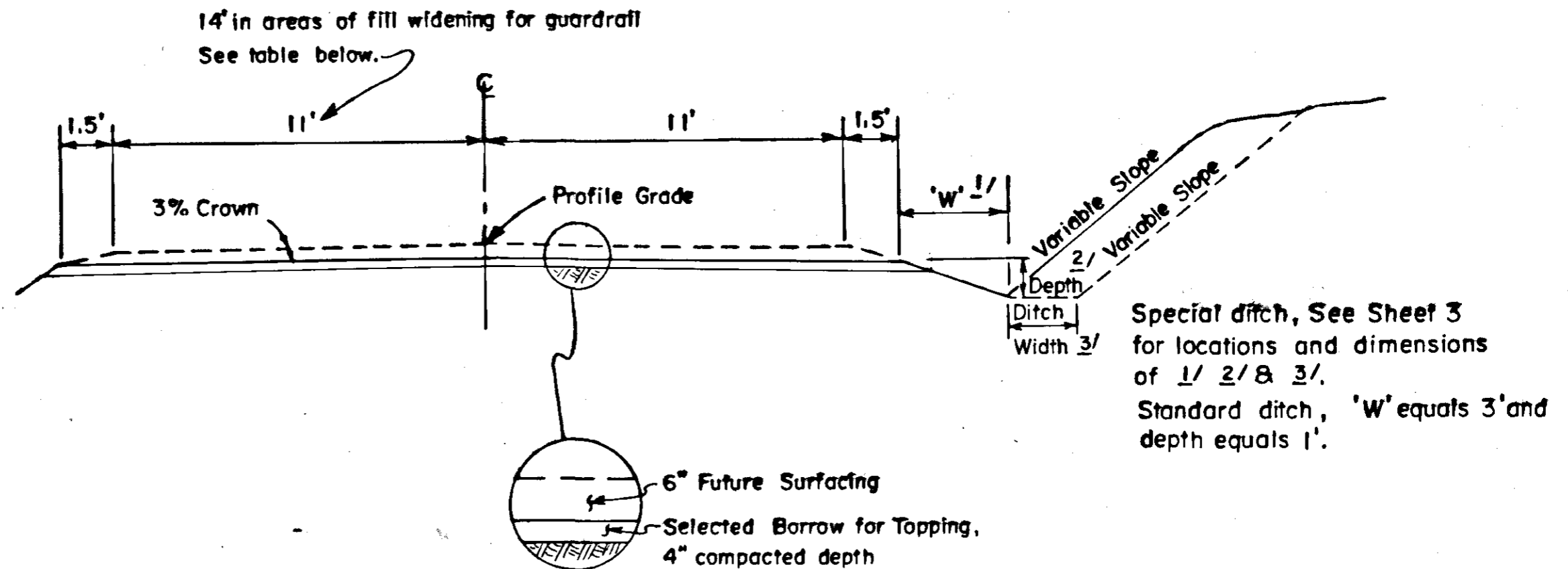
12+ MILES TO KLAWOCK

650+00 BEGIN PROJECT  
 FH 9-1(3) & FLH 9-1(1)

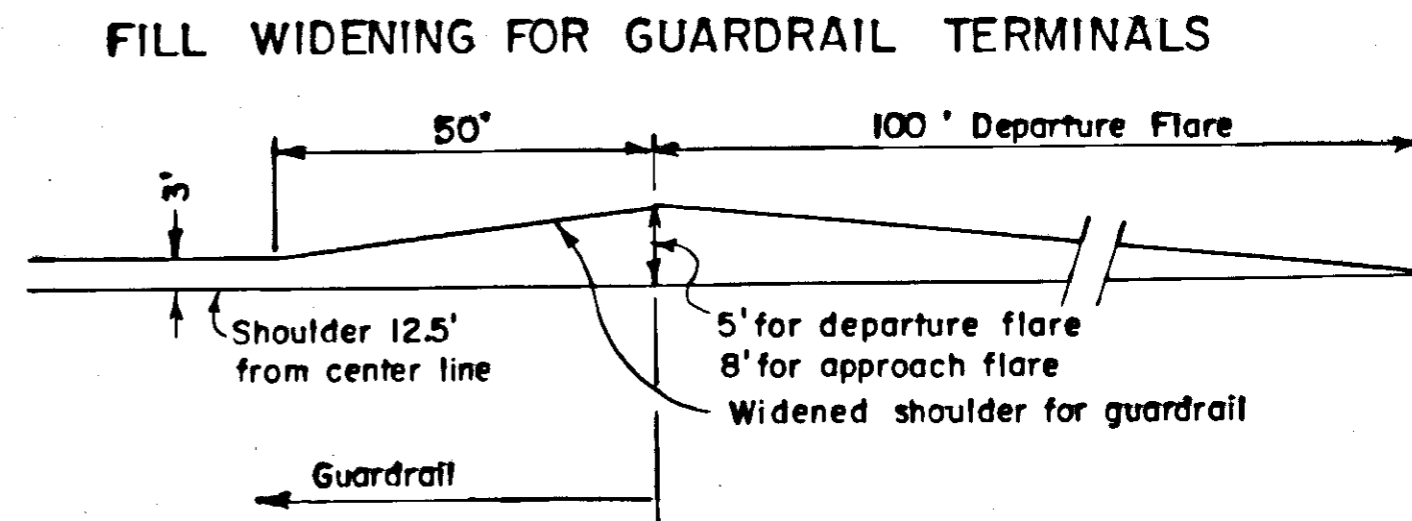
900+00 END PROJECT  
 FH 9-1(3) & FLH 9-1(1)

Plans Completed January 1982  
 Recommended for Approval  
 DATE: \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

Maximum superelevations on curves is at the rate "e" as indicated under the horizontal curve data on the Plan Sheets.

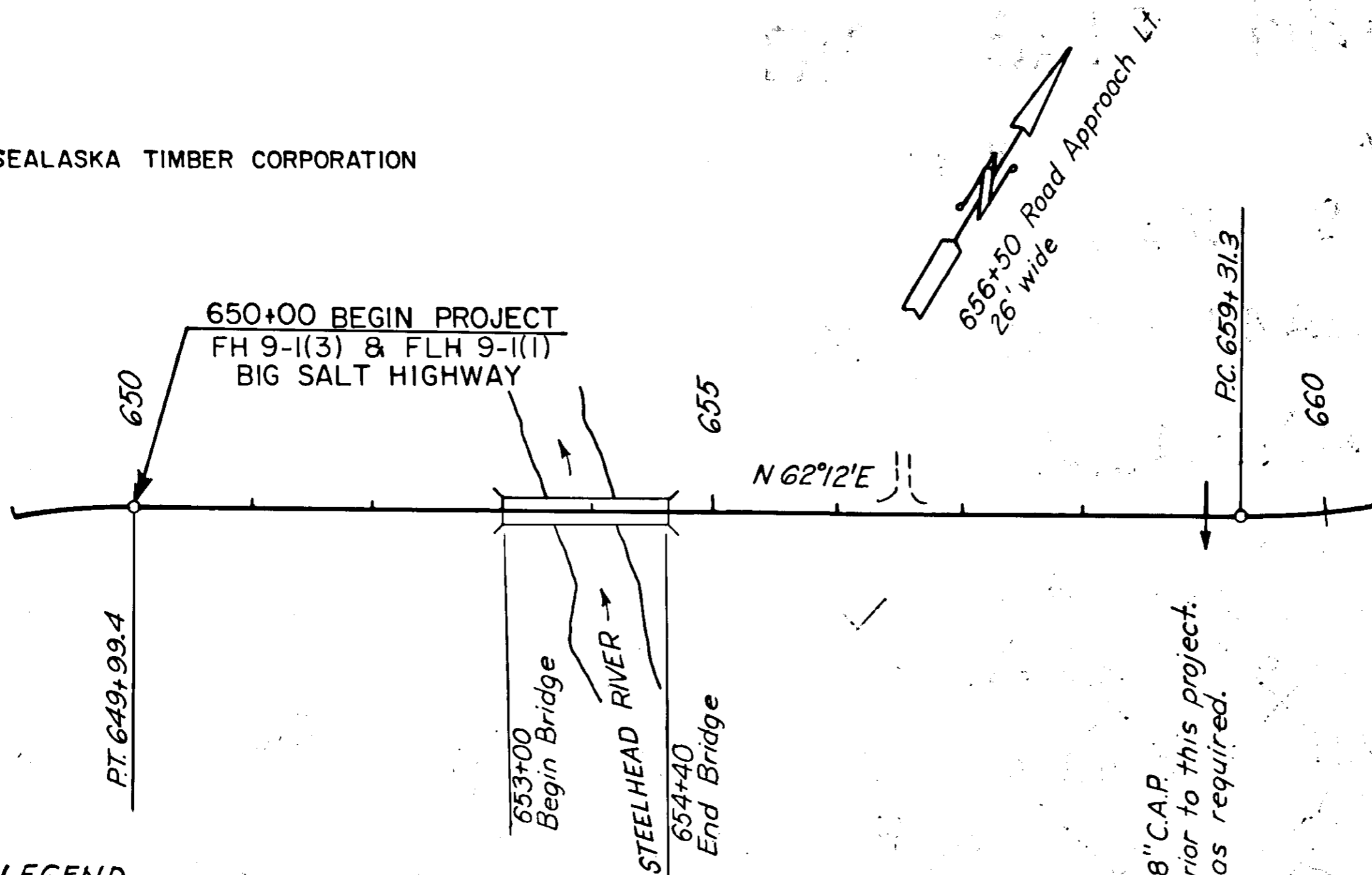


FILL WIDENING FOR GUARDRAIL					
STATION TO STATION	SIDE	TERMINAL TYPE	STATION TO STATION	SIDE	TERMINAL TYPE
652+00 - 653+00	Left	B-Bridge	728+00 - 731+00	Right	B-A
652+00 - 653+00	Right	B-Bridge	729+00 - 730+50	Left	A-B
654+50 - 655+50	Left	Bridge-A	749+00 - 760+50	Right	A-A
654+50 - 655+00	Right	Bridge-A	785+50 - 788+00	Right	B-A
694+00 - 698+50	Right	A-B	891+50 - 899+00	Right	B-A
705+00 - 710+50	Right	B-A			



SPECIAL						DITCH						LISTINGS					
STA.	STA.	Width in Feet			LT/RT	STA.	STA.	Width in Feet			LT/RT	STA.	STA.	Width in Feet			LT/RT
		1/	2/	3/				1/	2/	3/				1/	2/	3/	
650+00	652+00	9	3	--	Left	737+00	--	6	2	2	Left	815+50	--	6	2	--	Left
650+00	653+00	6	2	2	Right	741+50	--	6	2	4	Left	816+00	--	6	2	3	Left
660+00	661+00	9	3	--	Right	742+00	742+50	6	2	--	LT & RT	816+50	--	6	2	11	Left
664+83	--	6	2	--	Left	747+00	--	6	2	--	Left	817+00	--	6	2	--	Left
666+00	--	6	2	--	Left	747+50	749+00	6	2	4	Left	818+06	--	6	3	7	Left
676+50	680+50	6	2	--	Left	754+18	--	6	2	2	Left	818+50	819+00	6	2	--	Left
681+50	--	6	2	--	Left	754+50	758+25	6	2	--	Left	821+00	--	6	2	3	Left
683+00	--	6	2	--	Left	759+00	--	12	4	3	Left	821+50	823+00	6	2	--	Left
690+50	691+00	6	2	--	Left	760+50	--	6	2	--	Right	825+00	--	6	2	--	Left
691+50	--	6	2	2	Left	766+50	--	6	2	2	Left	826+00	--	6	2	2	Left
692+00	--	6	2	2	Right	768+00	768+50	6	2	2	Left	826+50	--	6	2	--	Left
692+00	693+50	6	2	2	Left	769+00	769+50	6	2	4	Left	829+00	832+00	6	2	4±	Left
694+00	--	6	2	3	Left	770+00	--	6	2	2	Left	835+50	--	6	2	--	Left
698+50	702+00	6	2	--	Left	772+50	--	6	2	2	Left	837+50	--	6	2	2	Left
705+00	--	6	2	--	Left	773+00	--	6	2	--	Left	838+00	--	6	2	--	Left
706+00	707+00	6	2	--	Left	774+00	--	6	2	8	Left	838+58	--	6	2	4	Left
710+50	--	6	2	6	Left	777+00	--	6	2	--	Left	839+00	--	6	2	--	Left
711+00	714+00	6	2	--	Left	779+00	--	6	2	2	LT & RT	839+50	--	6	2	2	Left
715+00	715+50	6	2	--	Left	780+00	780+50	6	2	--	Left	841+00	--	9	3	--	Left
718+00	718+50	6	2	--	Left	782+32	--	12	4	--	Right	848+50	849+00	6	2	--	Left
719+00	--	6	2	9.5	Left	786+50	--	6	2	2	Left	851+00	--	6	2	2	Left
719+50	720+00	6	2	2	Left	788+00	789+00	6	2	2	Left	853+50	--	6	2	2	Left
720+50	--	6	2	--	Left	789+00	791+50	8	2	--	Left	854+50	855+00	6	2	--	Left
721+17	--	6	2	2	Right	792+50	795+00	6	2	2	Left	858+00	--	6	2	--	Left
723+50	--	6	2	--	Left	795+00	--	6	2	--	Right	864+00	--	12	4	2	Left
725+50	726+50	6	2	--	Left	796+00	796+50	6	2	2	Left	865+00	--	9	3	--	Left
727+50	--	6	2	--	Left	797+00	--	6	2	3.5	Left	865+50	866+50	6	2	2	Left
734+00	--	6	2	2	Left	803+00	806+50	6	2	--	Left	884+50	885+50	6	2	2	Left
735+00	--	6	2	--	Right	812+00	813+00	6	2	2	Left	887+00	--	6	2	2	Left
736+00	--	6	2	2	Left	813+50	814+00	6	2	--	Left	888+00	--	6	2	2	Left
736+00	--	6	3	2	Right	815+16	--	6	2	6.5	Left	891+00	892+50	6	2	4	Left
												893+00	--	6	2	--	Left

SEALASKA TIMBER CORPORATION



LEGEND

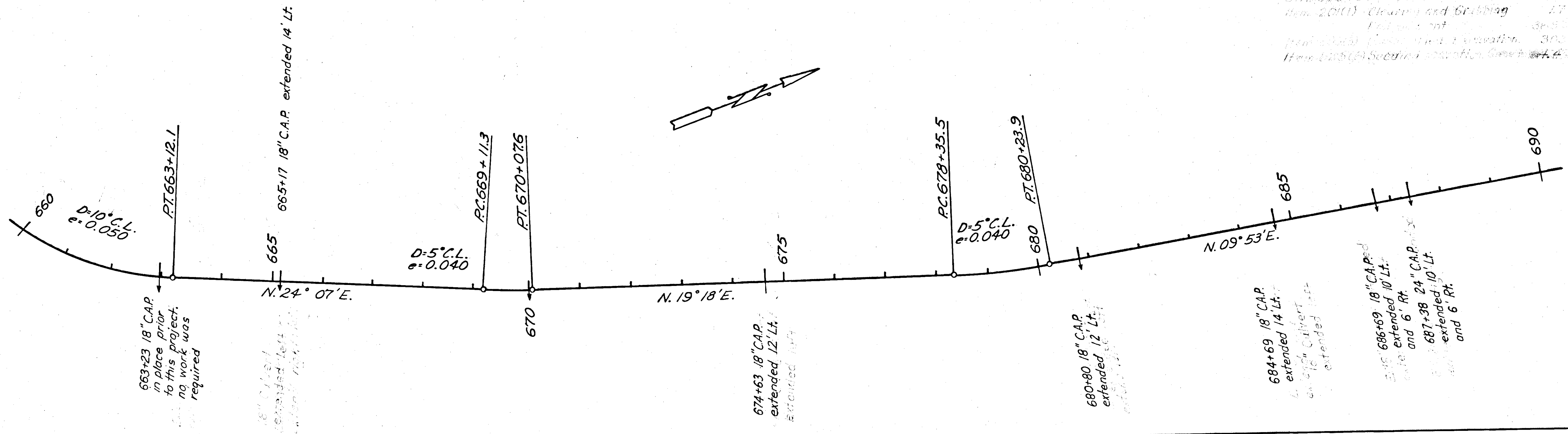
C.A.P. = Corrugated Aluminum  
Culvert Pipe

NOTE:

The alignment shown on these plans is design alignment and is not necessarily the way that the road was constructed.

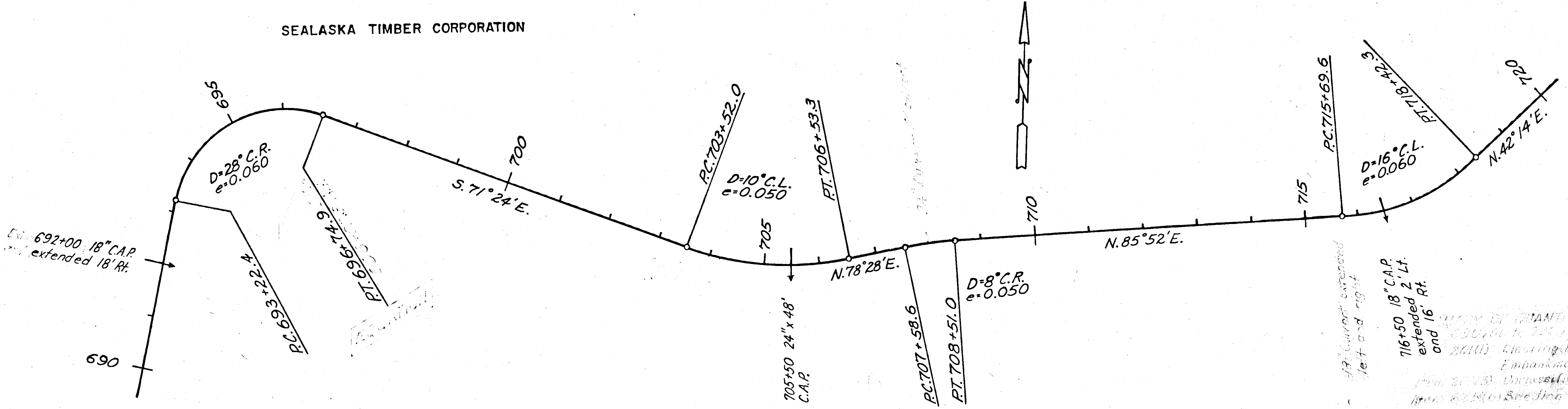
SOUTH TONGASS NATIONAL FOREST  
 SEALASKA TIMBER CORPORATION

SUMMARY OF QUANTITIES  
 STA. 660+00 TO 690+00  
 Item 20(1) Clearing and Grubbing 17 Acres  
 Embankment 36,570 Yds.  
 Item 20(2) Unclassified Excavation 303 Yds.  
 Item 20(3) Seeding 24 Units

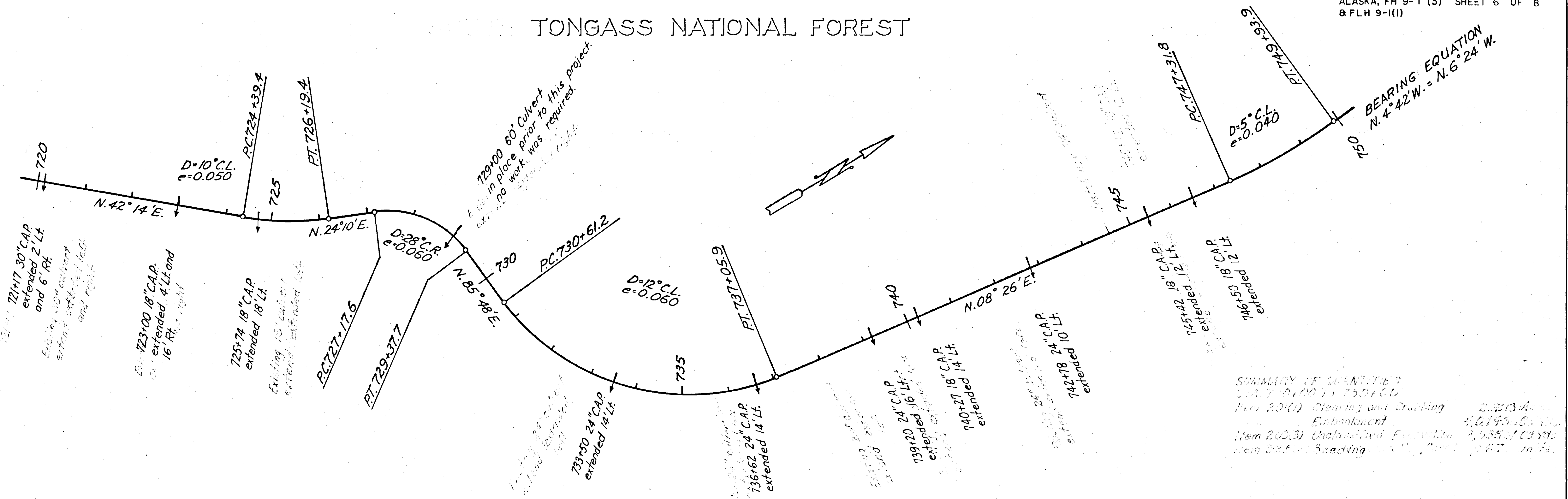


SOUTH TONGASS NATIONAL FOREST  
 SEALASKA TIMBER CORPORATION

SUMMARY OF QUANTITIES  
 STA. 690+00 TO 720+00  
 Item 20(1) Clearing and Grubbing 4 Acres  
 Embankment 3,570 Yds.  
 Item 20(2) Unclassified Excavation 249 Yds.  
 Item 20(3) Seeding 24 Units



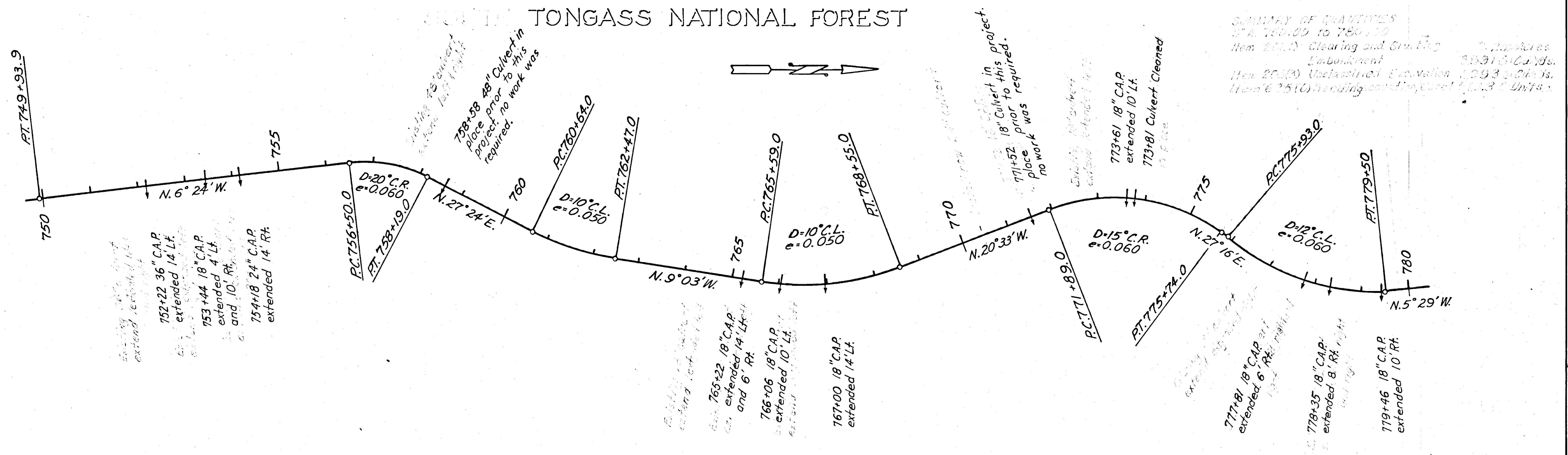
# TONGASS NATIONAL FOREST



SUMMARY OF QUANTITIES STA. 720+00 TO 750+00

Item 201(1) Clearing and Grubbing	2,2213 Acres
Embankment	4,01436 Cu Yds.
Item 201(3) Unclassified Excavation	2,55521 Cu Yds.
Item 205(1) Seeding	1673 Units.

# TONGASS NATIONAL FOREST



SUMMARY OF QUANTITIES STA. 750+00 TO 780+00

Item 201(1) Clearing and Grubbing	2,2213 Acres
Embankment	3,53156 Cu Yds.
Item 201(3) Unclassified Excavation	2,59300 Cu Yds.
Item 205(1) Seeding	1673 Units.