

STATE	PROJECT	SHEET NO.
ALASKA	X 30061	1

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

PLAN AND PROFILE
PROPOSED HIGHWAY PROJECT

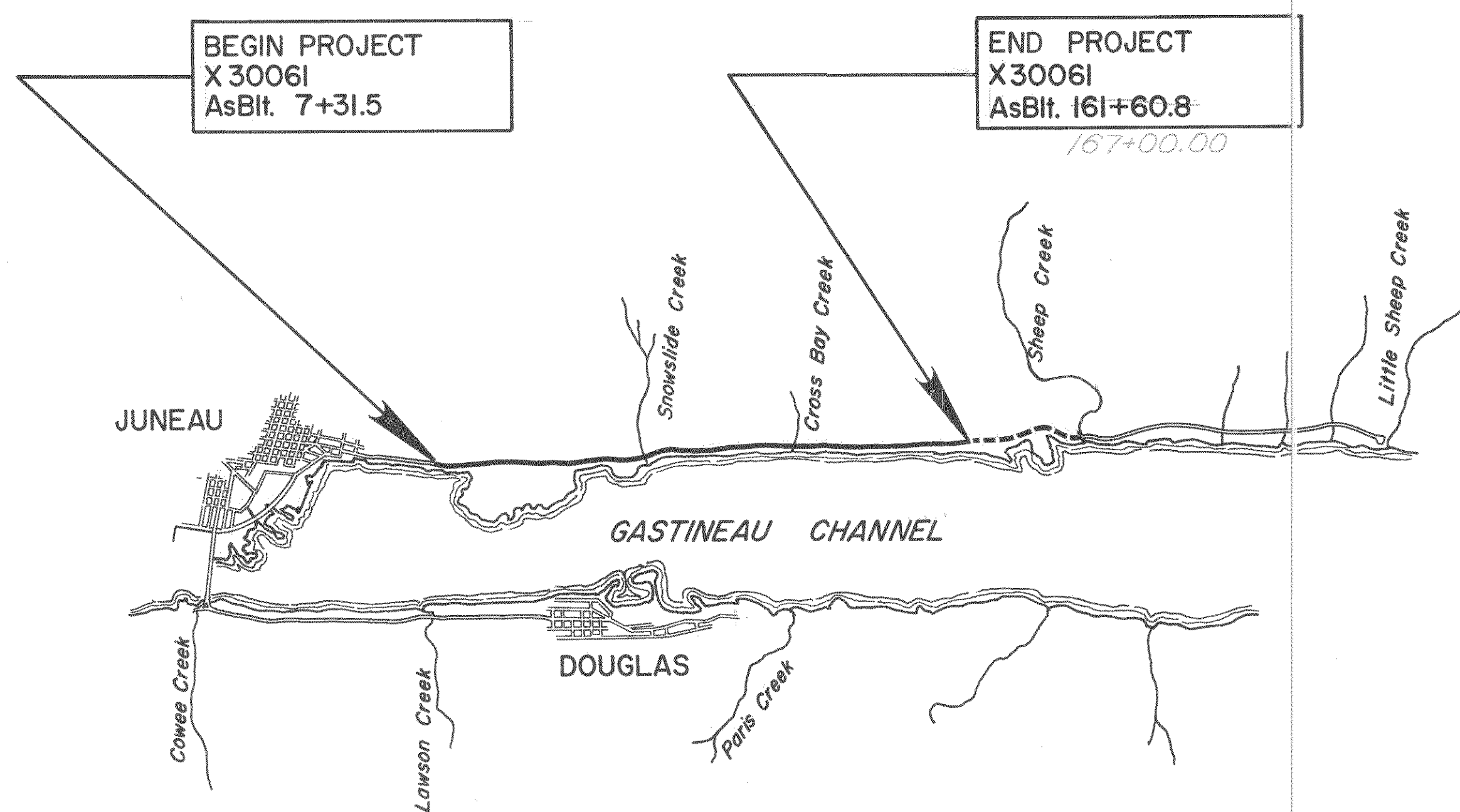
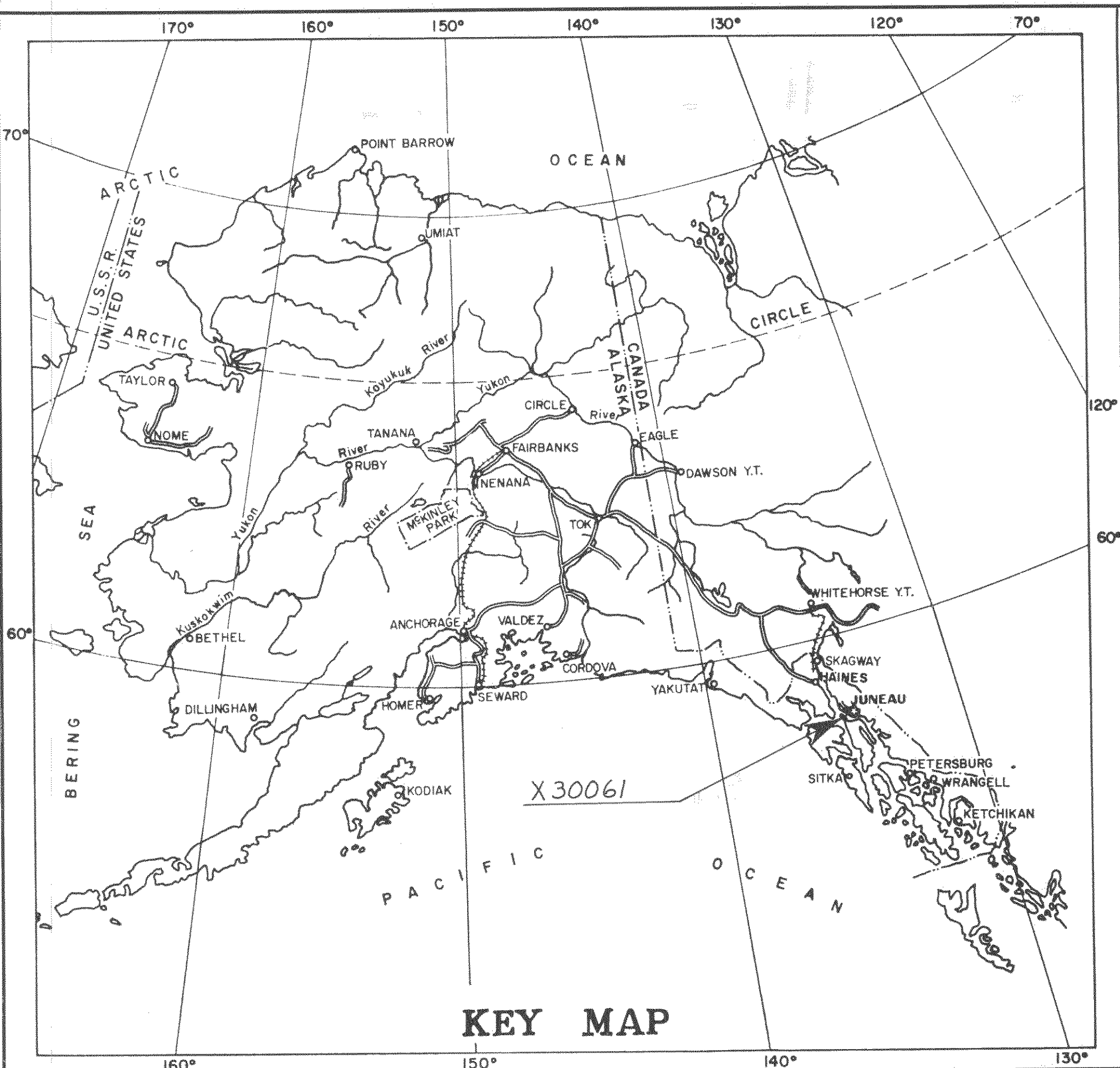
X30061
THANE ROAD

JUNEAU SOUTH CITY LIMITS TO SHEEP CREEK
GRAVEL SURFACING
AS BUILT PLANS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	TYPICAL SECTION & ESTIMATE OF QUANTITIES

The following standard drawings apply to this project: A-1, C-00.01, C-10.00, C-11.01, D-02.02,



PROJECT SUMMARY

Width of Surfacing = 22'
Length of Project = 15,429.3 = 2.922 mi.
Change Length = 15,968.5 = 3.024 mi.

Contractor: Berg Const Co, Inc
Proj. Engr: Phil Speer
Beginning Date: Oct. 30, 1976
Completion Date: Nov. 26, 1976

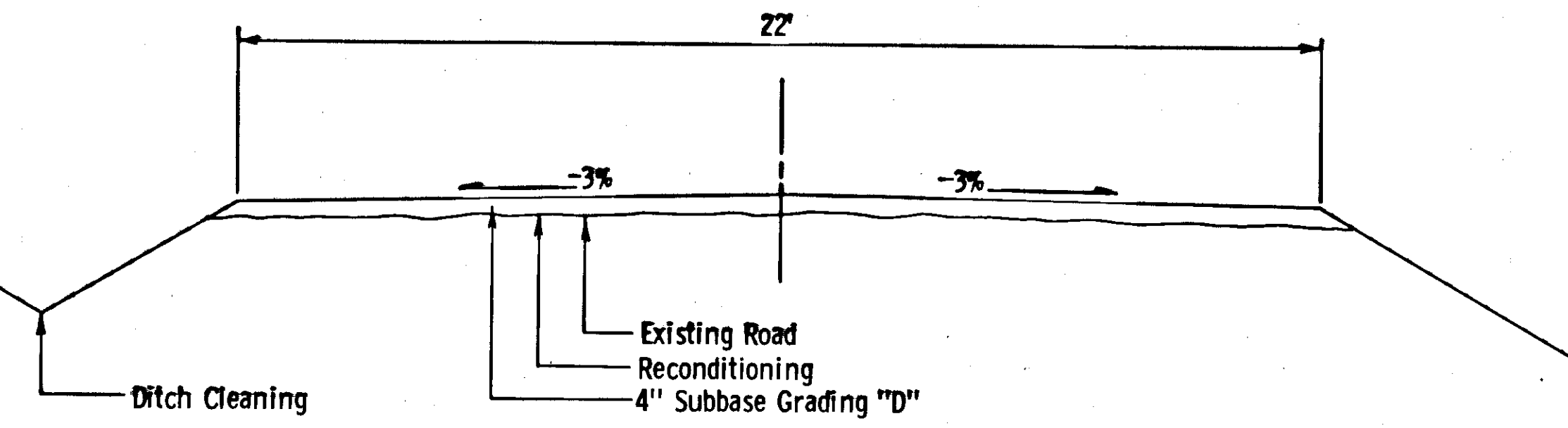
STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

APPROVED
Walter K. Williams Date 7/17/76
SOUTHEASTERN DISTRICT ENGINEER

STATE OF ALASKA
DEPARTMENT OF HIGHWAYS

APPROVED
Phil Speer Date 7/9/76
COMMISSIONER OF HIGHWAYS

TYPICAL SECTION



AsBlt. 7+31.5 to AsBlt. 161+60.8

ESTIMATE OF QUANTITIES			
Item No.	Item	Unit	Quantity
110(1)	Mobilization	L. S.	All Req'd.
113(1)	Flagging	M. H.	700 14
201(7)	Ditch Cleaning	L. F.	15,429 15,769
205(5)	Culvert Bedding & Backfill (EWO No. 1)	Ton	462
303(1)	Reconditioning	Mi.	2.922 3.024
304(1)	Subbase Grading "D"	Ton	8,015 8,854
603(22E)	18" Pipe Conduit	L. F.	84 181
603(22G)	24" Pipe Conduit	L. F.	368 290
603(22H)	30" Pipe Conduit	L. F.	6 7
603(22I)	36" Pipe Conduit	L. F.	18 0

BASIS OF ESTIMATE	
Item No.	Estimating Factor
304(1)	1.87 Ton / Cu. Yd.

GENERAL NOTES

- Super elevations shall match existing or as directed by the Engineer.
- Culvert work shall consist of removing and replacing damaged ends as directed by the Engineer.
- Culvert stationing is approximate

CULVERT SUMMARY

Station	18"	24"	30"	36"	Remarks
14+70 18+00	6'	60'			Remove and Replace See Gen. Note No. 2
21+57 21+40	6'			*	See General Note No. 2
22+07 23+85	X6'				See General Note No. 2
25+26 24+50	6'	56'			Remove and Replace See Gen. Note No. 2
26+32 43+50	X-	3'			See General Note No. 2
46+10		6'			See General Note No. 2
47+44 48+30	6'	50'			Remove and Replace See Gen. Note No. 2
49+55 49+50	X4'				See General Note No. 2
50+61 53+00	X6'				See General Note No. 2
51+13 75+00	X3'				See General Note No. 2
54+30 86+10		54' 3'			Remove and Replace See Gen. Note No. 2
87+40	5'				See General Note No. 2
65+92 89+00	5'		*		See General Note No. 2
75+42 95+90	X	36'			See General Note No. 2 Remove & Replace
78+06 99+50	X4'				See General Note No. 2
80+70 109+74		48'		*	See General Note No. 2 Remove & Replace
83+34 111+00	X1/2'				See General Note No. 2
112+25			7'		See General Note No. 2
88+62 117+25		X7'			See General Note No. 2
91+26 119+80	X4'				See General Note No. 2
99+18 128+00	10'	50'			Remove and Replace See Gen. Note No. 2
109+74 129+80	8'	50'			Remove and Replace See Gen. Note No. 2
110+80 133+00	X4'				See General Note No. 2
135+00	4'				See General Note No. 2
114+49 138+00	6'	*			See General Note No. 2
115+55 139+00	6'			*	See General Note No. 2
135+61 141+50	6'	X			See General Note No. 2
138+35 143+25	X	36'			See General Note No. 2 Remove & Replace
141+42 146+80	X	36'			See General Note No. 2 Remove & Replace
149+00	4'				See General Note No. 2
144+06 150+25	6'	X			See General Note No. 2
147+23 152+75	6'	X			See General Note No. 2
154+62 153+85	6'	X			See General Note No. 2
155+61 155+80	6'	X			See General Note No. 2
159+90 158+00	X6'				See General Note No. 2
158+60		38'			Remove & Replace
160+96 160+50	X	36'			See General Note No. 2 Remove & Replace
161+00	6'				See General Note No. 2
161+60	8'				See General Note No. 2
162+50	16'				See General Note No. 2
164+75		36'			Remove & Replace
Totals =					181' 290' 7' 0'