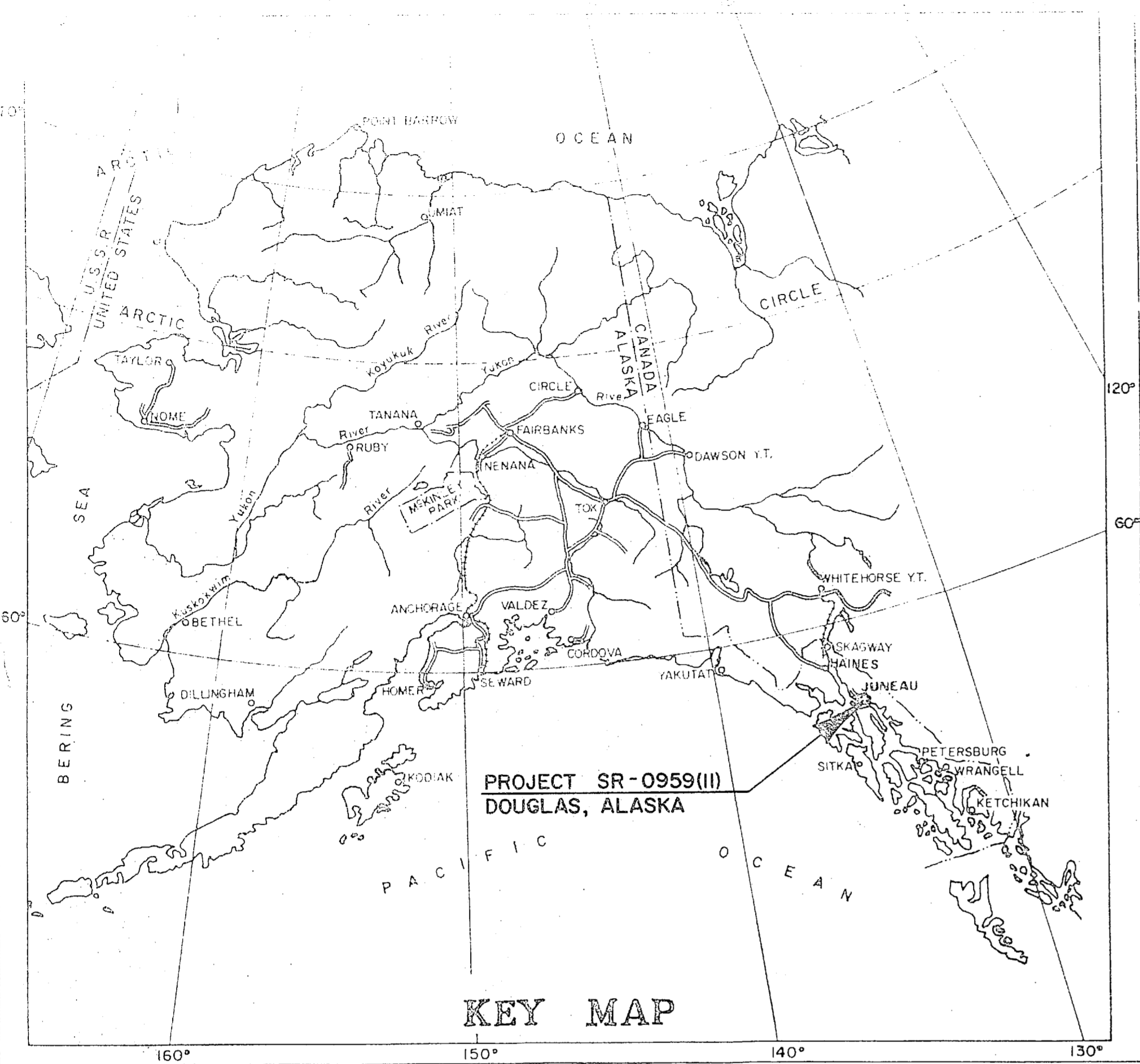


STATE OF ALASKA
 DEPARTMENT OF TRANSPORTATION
 &
 PUBLIC FACILITIES

PLAN AND PROFILE
 PROPOSED HIGHWAY PROJECT
 SR-0959(II) & B-56532
 NORTH DOUGLAS HIGHWAY
 KOWEE CREEK TO HELIPOINT
 PAVING

INDEX OF SHEETS	
1	TITLE SHEET
2	TYPICAL SECTION & ESTIMATE OF QUANTITIES
3	MISC. SUMMARIES & GUARD RAIL DETAIL
4	SIGNING SCHEDULE & MAIL BOX SUMMARY
5-7	PLAN SHEETS
8	TRAFFIC CONTROL PLAN

The following Standard Plans shall apply to this project: A-1, C-00.00, C-10.01, C-11.01, G-04.00 S, G-04.00 W, G-14.00 S, G-14.00 W, G-18.00, I-40.00, M-20.00, M-23.00, S-00.00, S-05.00, & S-30.00 T-21.00



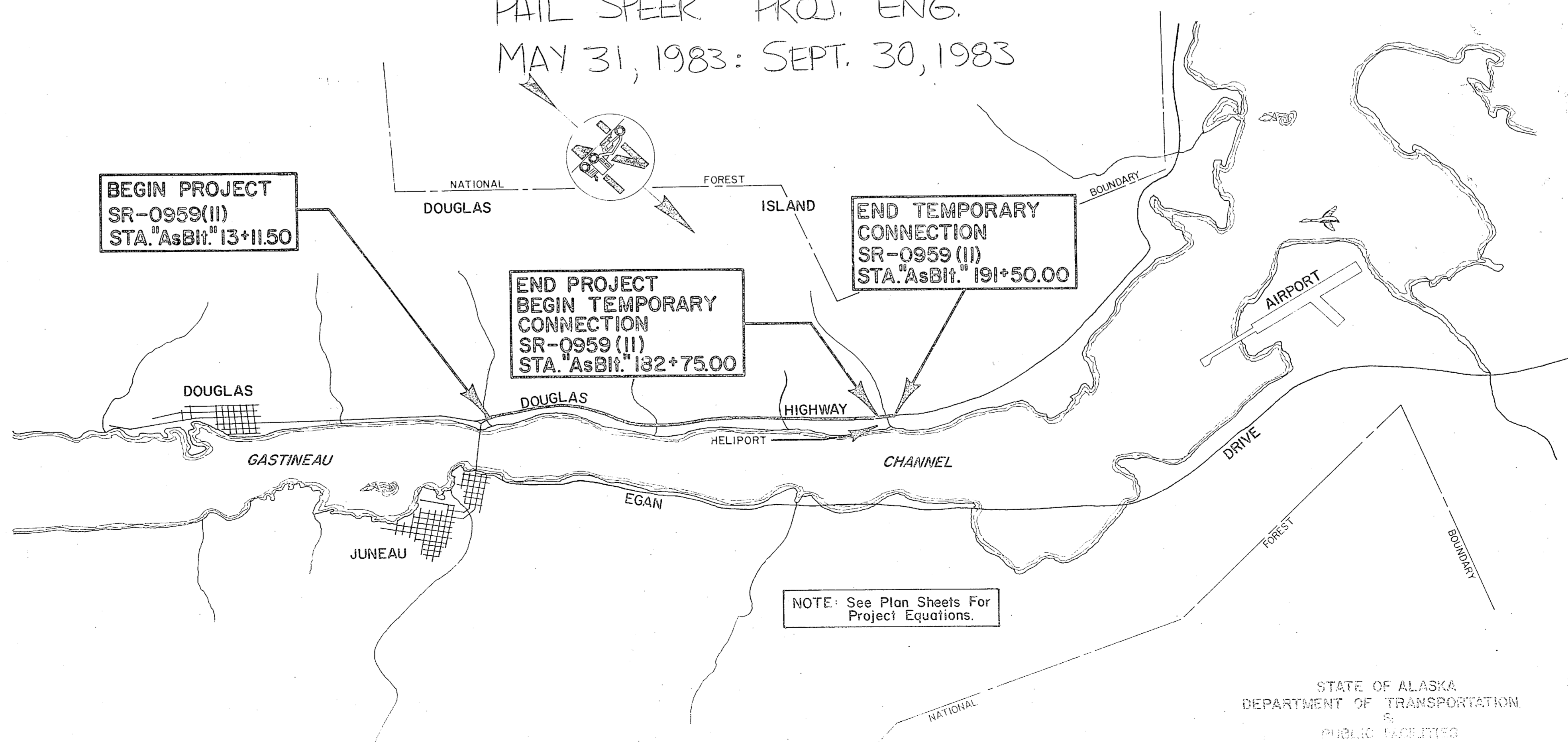
"AS BUILT PLANS"
 ASSOC. SAND AND GRAVEL
 PHIL SPEER PROJ. ENG.
 MAY 31, 1983: SEPT. 30, 1983

PROJECT SUMMARY

- WIDTH OF PAVEMENT = 32'
- LENGTH OF PAVEMENT = 17,854.4' = 3.38 mi. ✓
- LENGTH OF PROJECT = 16,981.1' = 3.22 mi. ✓
- LENGTH OF TEMP. CONN. = 873.3' = 0.17 mi. ✓

DESIGN DESIGNATION

- A.D.T. 1981 = 2,227
- A.D.T. 2000 = 3,309
- D.H.V. 14% = 463
- T. = 2%
- T.I. = 65%
- V. = 50 MPH



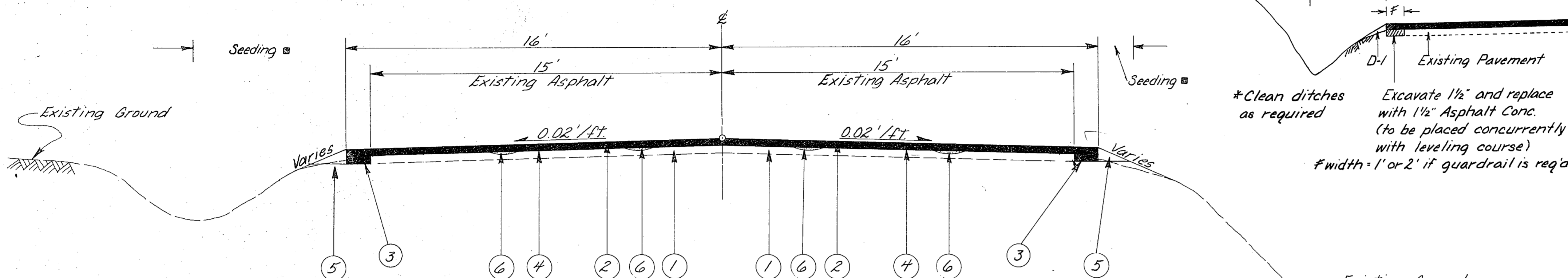
Completed 1/22/83

TYPICAL SECTION OF IMPROVEMENT

B.O.P. Sta. "AsBit" 13+11.50 To E.O.P. Sta. "AsBit" 182+75.00

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	SR-0959(11)	1983	2	8

TYPICAL SECTION 8 ESTIMATE OF QUANTITIES



GENERAL NOTES

- Approach stationing shown in these plans is approximate only & is subject to minor revisions.
- Mail box stationing shown in these plans is approximate only & is subject to minor revisions.
- Guard rail stationing shown in these plans is approximate only & is subject to minor revisions.
- All vertical saw cuts made in the existing pavement shall be made in a neat, workmanlike manner and shall not be paid for directly, but shall be considered incidental to other items of work.
- Those existing approaches needing shaping and regrading shall be paid for under Item 639 (1), Approaches. If additional material is required it shall be paid for under Item 301 (1), Crushed Aggregate Base Course, Grading D-1.
- Where one foot of widening cannot be achieved in areas not requiring guardrail, the hand-laid rock embankment detail shall be used, with the distance from centerline to edge of pavement to be 16 feet.
- All existing pavement from the B.O.P. to the E.T.C. shall receive a complete, uniform coverage of CSS-1 Asphalt for Tack Coat, as directed by the engineer.
- The 500 L.F. of 18" pipe shown in the estimate of quantities shall be installed at approaches or driveways, as directed. If removal of existing approach or driveway culverts is required, no separate payment shall be made, as this shall be considered incidental to Item 603(22-18), 18" Pipe.
- All centerline & shoulder stripes shall be painted traffic markings. Solid shoulder stripes shall be offset 11' from the centerline.
- Several (approx 8) private utility conduit crossings (water, sewer, and/or electrical) exist within the project limits. Prior to placing embankment, cleaning ditches, or excavation, the contractor shall locate these conduits. For more information concerning these crossings, contact the S.E. Region Utilities Engineer.

NOTE: Seeding shall be for width of ditch clearing or embankment placement, or as directed.

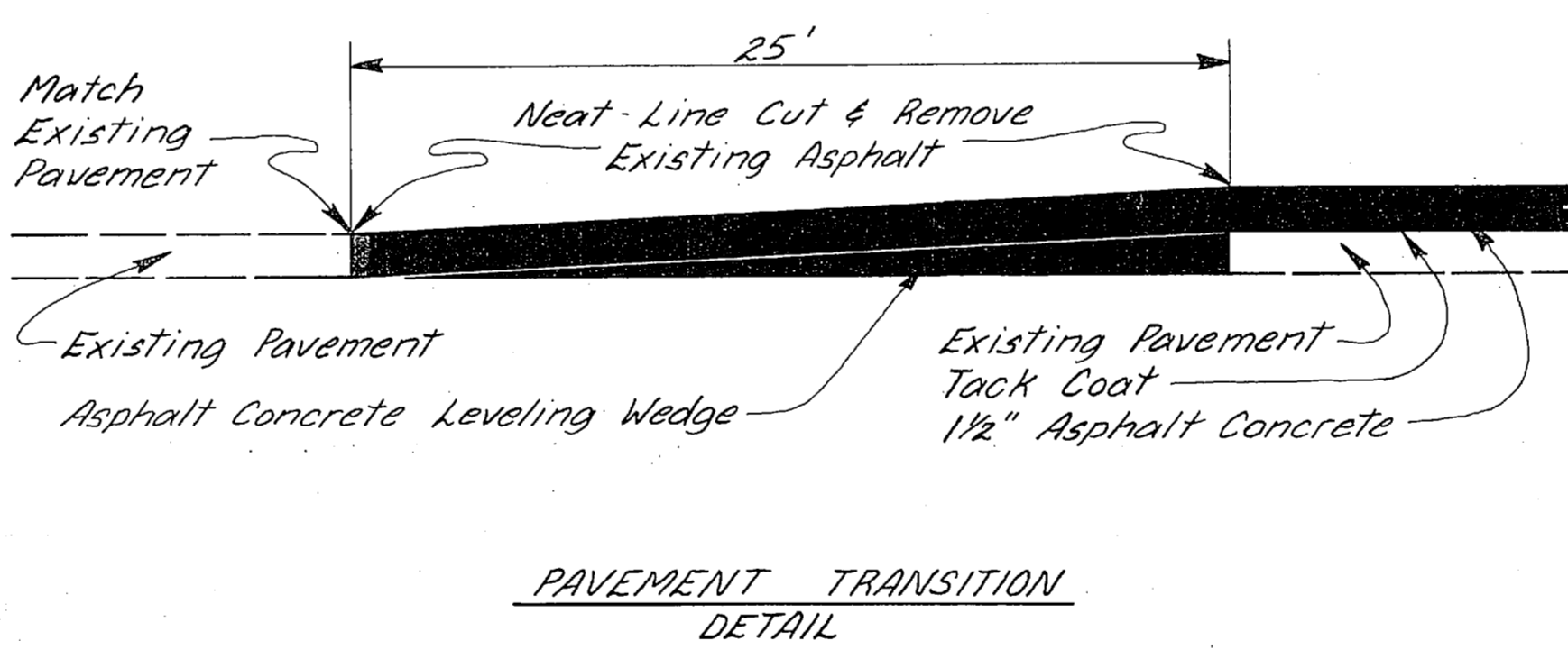
NOTE: From E.O.P. Sta. "AsBit" 182+75.00 to "AsBit" 182+87.00, the proposed width of 32' shall be tapered at 12:1 to match the existing width of 30'. From Sta. "AsBit" 182+87.00 to End Temporary Connection Sta. "AsBit" 191+50.00, overlay the existing pavement at the existing width. In this section there is no widening required.

LABELING INDEX

1	Existing Asphalt	5	Crushed Aggregate Base Course
2	1/2" Asphalt Concrete - Type II	6	3/4" Asphalt Concrete Leveling Course
3	Linear Grading - Cut 1/2" below top of existing pavement for width of widening or to daylight as required.		
4	CSS-1 Asphalt for Tack Coat		

ESTIMATE OF QUANTITIES

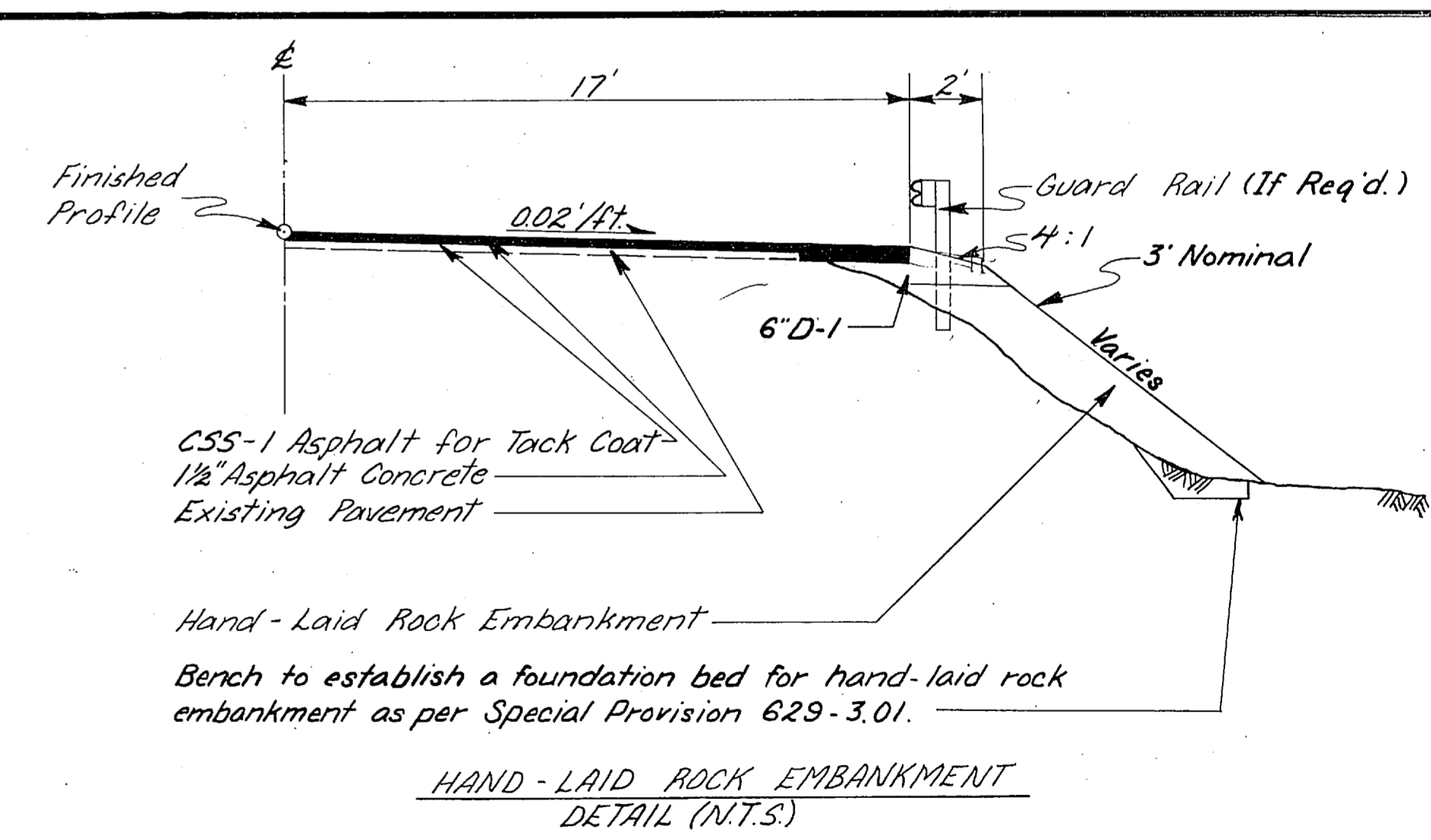
ITEM NO.	ITEM	UNIT	QUANTITY
110(1)	Mobilization	L.S.	All Required
111(1)	Temporary Erosion & Pollution Control	C.S.	All Required
113(1)	Flagging	Man Hr.	603.5 300
114(1)	Construction Surveying by the Contractor	L.S.	All Required
115(1)	Traffic Maintenance	L.S.	All Required
116(1)	Furnishing & Maintaining a Field Office	L.S.	All Required
116(2)(LA)	Furnishing & Maintaining a Field Laboratory	L.S.	All Required
	CHANGE ORDER # 1		
202(9)	Single Mail Box Installation	Each	4 93
202(10)	Multiple Mail Box Installation	L.F.	49 92.4
203(8)	Linear Grading	Station	170 ✓
301(1)	Crushed Aggregate Base Course	Ton	800 2391.23
401(1)	Asphalt Concrete, Type II	Ton	9,200 103 19.05
401(2)	AC-5 Asphalt Cement	Ton	552 639.12
402(2)	CSS-1 Asphalt for Tack Coat	Ton	53 33.74
603(22-18')	18 Inch Pipe	L.F.	500 72
606(1)	Beam Type Guard Rail, Type I Post	L.F.	3 617.5
606(4)	Removal and Disposal of Guard Rail	L.F.	1,837.5 ✓
606(6)	End Anchorages	Each	30 31
614(3)	Adjust Existing Monument Cases	Each	16 18
615(1)	Standard Signs	Sq. Ft.	112.15 40
618(1)	Seeding	M.S.F.	153.53 392
629(1)	Hand-Laid Rock Embankment	C.V.M.	104 500
639(1)	Approaches	Each	116 445
670(1)	4" Painted Traffic Markings (Approx. 54,100 Lineal Ft.)	L.S.	All Required ✓



BASIS OF ESTIMATE

ITEM NO.	ITEM	ESTIMATING FACTOR
301(1)	Crushed Agg. Base Course	1.96 Tons / Cu. Yd.
401(1)	Asphalt Concrete, Type II	1140 Lbs / Sq. Yd. / in. Depth
401(2)	AC-5 Asphalt Cement	6% of Item 401(1)
402(2)	CSS-1 Asphalt for Tack Coat	0.10 Gal./Sq. Yd. * 240 Gal./Ton

* Diluted Mixture



NOTES

- No rock embankment shall be placed outside of the R/W.
- This detail shall apply between Sta. "AsBit" 41+60± to "AsBit" 41+90± (Left); Sta. "AsBit" 128+50 and "AsBit" 131+00 and between Sta. "AsBit" 131+65 and "AsBit" 132+45 or as directed by the engineer.

APPROACH SUMMARY

STATION	LT.	RT.	WIDTH	REMARKS
As Bit 14+42		X	14'	✓
" 16+80		X	20'	✓
" 18+27		X	25' 14"	25' x 5' TAPER SOUTH 36' x 5' TAPER NORTH
" 18+43	X		14'	✓
" 18+75		X	14'	✓
" 19+54		X	14'	✓
" 20+17		X	14'	✓
" 20+65		X	14'	✓
" 20+70	X		20'	✓
" 21+18		X	14'	✓
" 21+86	X		14'	✓
" 22+39		X	20'	✓
" 22+81		X	14'	✓ PART OF 22+81 20'+14'
" 23+08		X	20'	✓
" 23+92	X		14'	✓
" 25+30	X		14'	✓
" 25+40		X	20'	✓
" 26+72		X	14'	✓
" 28+09	X		14'	✓
" 28+15		X	20'	✓
" 28+91		X	14'	✓
" 30+10		X	20'	✓ 36'
" 38+50	X		20'	✓
" 41+24		X	14'	✓
" 41+24	X		14'	✓
" 42+83	X		14'	✓
31+89	X		14'	✓
" 43+51		X	14'	✓
" 44+89	X		14'	✓
" 45+00		X	20'	✓ Anchor Apts. 35' x 10' TAPER
" 45+60		X	20'	✓ Anchor Apts. 40'
" 46+73		X	14'	✓
" 33+36		X	14'	✓
" 47+47	X		14'	✓
" 48+26		X	30'	✓
" 48+69		X	14'	✓
" 49+64		X	20'	✓
" 49+74	X		14'	✓
40+71	X		50'	✓ 5' WIDE TAPER
" 50+64	X		20'	✓ 21'
" 51+06		X	14'	✓
" 51+27	X		14'	✓
" 52+49		X	20'	✓
" 52+49	X		14'	✓
52+11	✓			25' x 15' WIDE
" 53+97	X		14'	✓
" 54+18	X		14'	✓
" 55+34		X	14'	✓
" 55+50	X		14'	✓
" 56+34		X	14'	✓
" 57+40	X		14'	✓
" 57+87	X		14'	✓
" 58+56	X		14'	✓
" 59+19		X	14'	✓
" 60+09		X	14'	✓
66+64	X		20'	✓ A.M.C. Jeep Dealer
" 67+38	X		20'	✓ A.M.C. Jeep Dealer 31'
" 69+54	X		20'	✓ Gravel Pit
" 72+13		X	20'	✓
" 77+46		X	20'	✓
" 81+79	X		20'	✓
58+76	X		17'	✓
" 82+48		X	20'	✓
" 83+11	X		14'	✓
" 83+48		X	20'	✓
" 83+96	X		14'	✓
" 84+49		X	14'	✓
71+05	X		14'	✓
" 87+44		X	14'	✓
As Bit 87+65	X		14'	✓
76+04	X		14'	✓
92+28	X		14'	✓

STATION	LT.	RT.	WIDTH	REMARKS
As Bit 88+66	X		14'	✓
" 89+55		X	14'	✓
" 90+03	X		14'	✓
" 90+77	X		40'	✓
" 91+03		X	30'	✓
92+61		X	224'	✓ 10' WIDE TAPER
" 92+67		X	14'	✓
" 93+78		X	14'	✓
" 94+31	X		20'	✓
" 94+31		X	14'	✓
" 95+36	X		20'	✓
92+80	X		14'	✓
" 96+31		X	14'	✓
" 97+37	X		14'	✓
" 97+90		X	14'	✓
" 100+75	X		20'	✓
" 102+28		X	14'	✓
96+07	X		14'	✓
" 102+44	X		14'	✓
" 105+66	X		14'	✓
" 106+56		X	20'	✓
" 108+88	X		20'	✓
" 109+62	X		20'	✓
" 110+73	X		14'	✓
" 111+67	X		14'	✓
" 112+21	X		14'	✓
" 113+79	X		14'	✓
" 114+48		X	20'	✓ 20' TAPER 10' WIDE SOUTH OF APPROACH
" 116+69	X		14'	✓
99+21	X		14'	✓
" 116+75		X	14'	✓
" 118+12		X	14'	✓
" 118+96		X	20'	✓
" 119+97	X		14'	✓
" 121+08		X	14'	✓
" 103+56		X	14'	✓
" 122+40	X		14'	✓
" 123+56	X		14'	✓
" 124+30	X		20'	✓
" 124+72	X		14'	✓
" 125+19	X		14'	✓
109+77	✓		14'	✓
" 125+77	X		14'	✓
" 127+20	X		14'	✓
" 127+94	X		20'	✓
" 129+26	X		14'	✓
" 130+63	X		14'	✓
116+80	✓		14'	✓ 5' WIDE TAPER
" 131+53	X		14'	✓
" 131+90	X		20'	✓
" 132+59	X		14'	✓
" 134+12	X		14'	✓
" 135+01	X		14'	✓
119+34	X		14'	✓
" 135+86	X		20'	✓
" 136+55	X		14'	✓
" 137+50	X		14'	✓
" 140+03	X		14'	✓
" 140+19	X		14'	✓
136+08	X		14'	✓
" 141+35	X		20'	✓
" 141+98	X		14'	✓
" 142+14	X		14'	✓
" 143+83	X		20'	✓
" 145+52	X		14'	✓
135+81	X		14'	✓ 5' TAPER
" 148+48	X		14'	✓
" 149+69	X		14'	✓
" 153+81	X		14'	✓
" 154+08	X		14'	✓
" 154+08	X		14'	✓
149+84	X		14'	✓
" 156+24	X		14'	✓
As Bit 156+35	X		14'	✓
154+84	X		14'	✓
161+59	X		14'	✓

STATION	LT.	RT.	WIDTH	REMARKS
As Bit 156+45		X	14'	✓
" 157+51		X	14'	✓
" 158+62		X	30'	✓
" 158+62	X		14'	✓
" 160+57		X	14'	✓
123+05	X		14'	✓ 5' WIDE TAPER
" 160+99	X		14'	✓
" 162+63	X		14'	✓
" 163+32	X		14'	✓
" 163+89	X		14'	✓
" 164+90	X		14'	✓
" 165+06	X		14'	✓
" 167+54	X		14'	✓
" 167+91	X		14'	✓
" 169+18	X		14'	✓
" 169+76	X		20'	✓
" 170+87	X		14'	✓
" 174+14	X		25' 34"	✓ Bonnie Brae Estates
" 174+56	X		14'	✓
" 182+32	X		14'	✓ Helipoint
As Bit 183+06	X		25' 34"	✓

TOTAL NUMBER OF APPROACHES = 145 166

MONUMENT SUMMARY

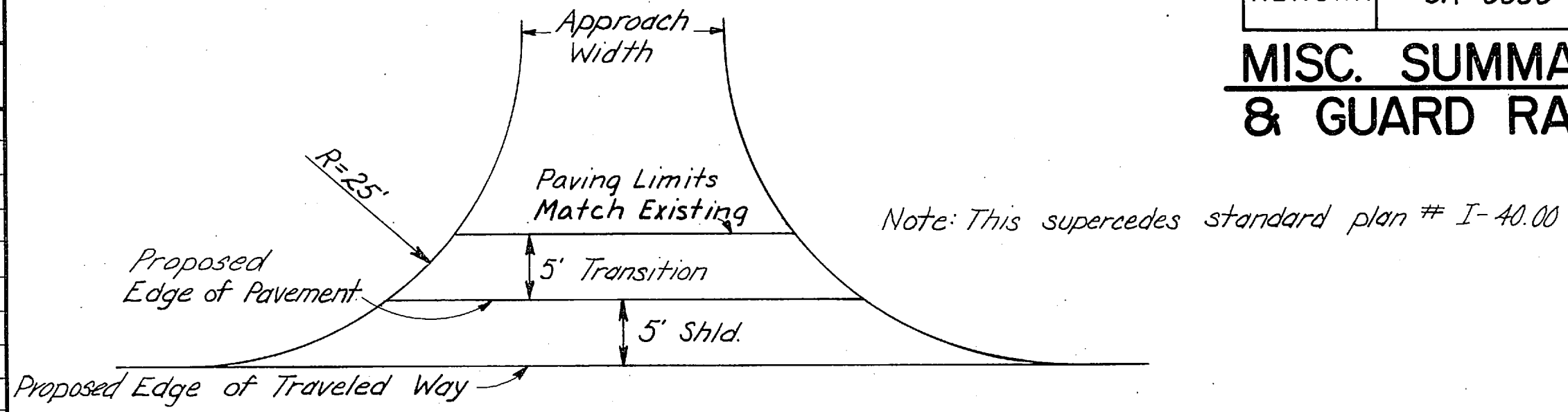
STATION	POINT	REMARKS
As Bit 21+41.40	Point	Adjust Case
" 33+57.31	P.C.	"
" 42+37.48	P.T.	"
" 62+42.34	P.C.	"
" 67+24.62	P.T.	"
" 70+26.11	P.C.	"
" 74+43.05	P.T.	"
" 77+86.90	P.I.	"
" 83+88.29	P.C.	"
" 88+23.59	P.T.	"
" 94+04.36	P.C.	"
" 99+38.22 Bk =	P.T.	"
" 99+20.62 Ahd.		"
" 105+99.64	P.O.T.	"
" 122+70.81	P.O.T.	"
" 145+35.00	P.I.	"
" 163+50.00	P.O.T.	"
" 179+98.44	Point	"
As Bit 190+98.34 Bk =	P.O.T.	Adjust Case
As Bit 191+00.00 Ahd.		"

TOTAL NUMBER OF MONUMENTS = 18 16

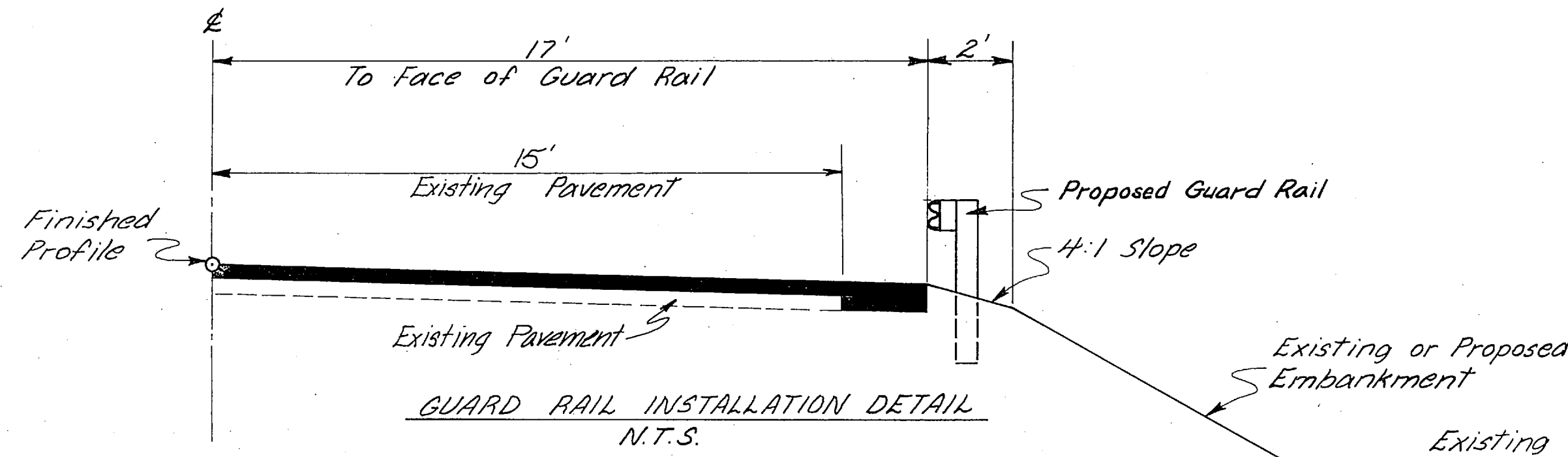
GUARD RAIL REMOVAL SUMMARY

STATION	TO	STATION	LENGTH		REMARKS
			LEFT	RIGHT	
As Bit 63+58.00		As Bit 66+70.50	312.5'	✓	
" 77+50.50		" 80+75.50	325.0'	✓	
" 83+65.00		" 84+40.00	75.0'	✓	
" 84+30.50		" 86+55.50	225.0'	✓	
" 91+30.00		" 92+05.00	75.0'	✓	
" 94+46.00		" 95+71.00	125.0'	✓	
" 94+46.00		" 94+96.00	50.0'	✓	
" 137+60.00		" 139+10.00	150.0'	✓	
" 137+75.00		" 139+25.00	150.0'	✓	
" 183+37.00		" 184+62.00	125.0'	✓	
As Bit 182+85.00		As Bit 185+10.00	225.0'	✓	

TOTAL LENGTH TO BE REMOVED = 1,837.5 L.F.



APRON DETAIL FOR EXISTING PAVED APPROACHES *
* See standard plan I-40.00 for unpaved approaches



- NOTES
- The 4' offset dimension, for Standard Plans G45-G4W Break-Away Cable Terminal, should be referenced to the back of rail rather than the edge of pavement.
 - All existing steel guardrail posts & beams shall be stockpiled at the D.O.T. Maintenance Yard at 7 mile Glacier Hwy. The Contractor shall dispose of wood posts & blocks.
 - All Guardrail Radii less than 150 feet shall be sharp formed.

GUARD RAIL INSTALLATION SUMMARY

STATION	TO	STATION	LENGTH		TERMINAL END SECTIONS	REMARKS
			LEFT	RIGHT		
As Bit 60+58.00		As Bit 71+45.50	1087.5'	✓	2	
" 72+22.00		" 76+34.50	425.0'	✓	2	R=25', L=37.5'
" 77+50.00		" 80+75.00	337.5'	✓	2	R=25', L=37.5'
" 84+65.00		" 86+90.00	237.5'	✓	2	R=25', L=37.5'
" 91+30.00		" 92+05.00	75.0'	✓	3	
" 94+46.00		" 94+96.00	62.5'	✓	2	R=25', L=37.5'
" 94+46.00		" 95+71.00	125.0'	✓	2	
" 112+25.00		" 114+25.00	225.0'	✓	2	R=25', L=37.5'
" 111+80.00		" 113+17.50	150.0'	✓	2	R=25', L=37.5'
" 128+50.00		" 131+00.00	250.0'	✓	2	Hand-Laid Rock
" 131+65.00		" 132+40.00	108.0'	✓	2	Hand-Laid Rock, R=25', L=37.5'
" 137+75.00		" 139+25.00	150.0'	✓	2	
" 137+60.00		" 139+10.00	162.5'	✓	2	R=25', L=37.5'
" 183+37.00		" 184+62.00	125.0'	✓	2	
" 182+85.00		" 185+10.00	225.0'	✓	2	

TOTAL LENGTH TO BE INSTALLED = 3,777.5' L.F. TOTAL OF TERMINAL SECTIONS = 30-3800.0 L.F.

* Indicates that the existing approach is paved. (Use apron detail, this sheet)

Bary P. ...

SIGNING SCHEDULE & MAIL BOX SUMMARY

NO.	STATION	OFFSET		CODE NO.	LEGEND	SIZE	AREA S.F.	SIGN THICKNESS	NO OF POSTS	TYPE	SIZE	LENGTH	EMBED	FACING TRAFFIC	REMARKS
		LT.	RT.												
1	"As Bit" 16+50	32'		R2-1	Speed Limit 30	30"x36"	7.5	.080	1	Tube	2"	15'	3.5'	S.B.	
2	" 16+50		28'	R2-1	Speed Limit 45	30"x36"	7.5	.080	1	"	2"	15'	3.5'	N.B.	
3	" 49+30		28'	R8-1	No Parking on Pavement or Within 8'	24"x30"	5.0	.080	1	"	2"	15'	3.5'	N.B.	Mount Below Sign 3
4	" 49+30		28'	R8-1A		12"x24"	2.0	.080	1	"	2"	15'	3.5'	N.B.	
5	" 56+65		26'	M10-1	2	6"x24"	1.0	.080	1	"	2"	15'	3.5'	N.B./S.B.	
6	" 59+75		32'	R2-1	Speed Limit 45	30"x36"	7.5	.080	1	"	2"	15'	3.5'	N.B.	
7	" 79+00		22'	W1-2L		36"x36"	9.0	.080	1	"	2"	15'	3.5'	N.B.	Mount Below Sign 7
8	" 79+00		22'	W13-1	35 M.P.H.	24"x24"	4.0	.080	1	"	2"	15'	3.5'	N.B.	
9	" 80+90		20'	R8-1	No Parking on Pavement	24"x30"	5.0	.080	1	"	2"	15'	3.5'	N.B.	Mount Below Sign 9
10	" 80+90		20'	R8-1A	Within 8'	12"x24"	2.0	.080	1	"	2"	15'	3.5'	N.B.	
11	" 100+10		29'	R2-1	Speed Limit 45	30"x36"	7.5	.080	1	"	2"	15'	3.5'	N.B.	
12	" 100+10		28'	R2-1	Speed Limit 45	30"x36"	7.5	.080	1	"	2"	15'	3.5'	S.B.	
13	" 103+55		29'	R8-1	No Parking on Pavement	24"x30"	5.0	.080	1	"	2"	15'	3.5'	S.B.	Mount Below Sign 13
14	" 103+55		29'	R8-1A	Within 8'	12"x24"	2.0	.080	1	"	2"	15'	3.5'	S.B.	
15	" 109+45		26'	M10-1	3	6"x24"	1.0	.080	1	"	2"	15'	3.5'	N.B./S.B.	
16	" 148+50		23'	R8-1	No Parking on Pavement	24"x30"	5.0	.080	1	"	2"	15'	3.5'	N.B.	Mount Below Sign 16
17	" 148+50		23'	R8-1A	Within 8'	12"x24"	2.0	.080	1	"	2"	15'	3.5'	N.B.	
18	" 162+25		26'	M10-1	4	6"x24"	1.0	.080	1	"	2"	15'	3.5'	N.B./S.B.	
19	" 173+90		35'	R1-1	Stop	30"x30"	6.25	.080	1	"	2"	15'	3.5'	E.B.	Bonnie Brae Estates
20	" 180+90		24'	D1-1	Helipoint	18"x16"	2.88	1.25	2	"	2"	15' & 17'	3.5'	N.B.	Frame Reg'd
21	" 185+10		25'	R2-1	Speed Limit 45	30"x36"	7.5	.080	1	"	2"	15'	3.5'	N.B.	
22	"As Bit" 185+10		28'	R2-1	Speed Limit 45	30"x36"	7.5	.080	1	Tube	2"	15'	3.5'	S.B.	
						112.25	S.F.								
						109.95	S.F.								

SIGNING NOTES

1. Sign locations & post lengths are approximate only & are subject to minor field revisions.
2. All sign posts shall be telescoping, perforated, & galvanized steel posts; the 2" size shall be used above ground & the 2 1/4" size shall be used below ground for the sleeve.
3. All posts shall be installed with the sleeve type embedment in accordance with Standard Drawing S-30.00, except that the 2 1/4" size shall be used for the entire embedment depth.
4. All signs shall be unframed. Except #20.
5. All existing signs shall be removed by the contractor & shall be returned to the State of Alaska, D.O.T. & P.F. Maintenance Yard, as directed by the engineer. This work shall be considered incidental to other items of work & no separate payment shall be made therefore.

MAIL BOX & NEWS PAPER TUBE SUMMARY

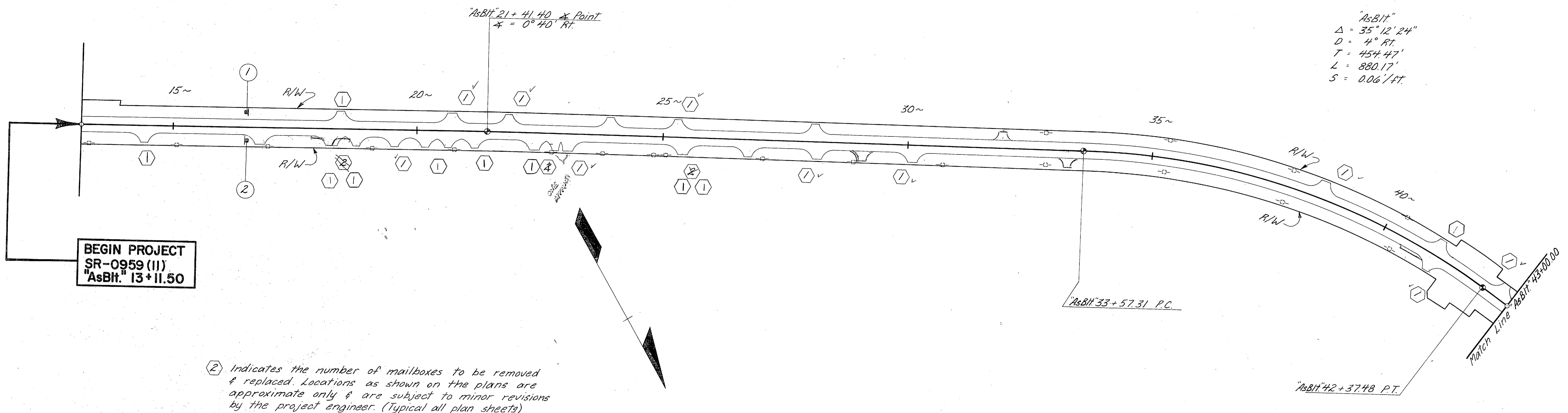
STATION	BOXES ON		STATION	BOXES ON		STATION	BOXES ON		NO. OF MAIL BOXES	NO. OF NEWS TUBES		
	LT.	RT.		LT.	RT.		LT.	RT.				
"As Bit" 18+50		X	14+10	X	1	45+40	X	1				
" 19+74		X	18+70	X	1	46+93	X	1				
" 20+85		X	19+00	X	1	47+27	X	1				
" 21+00	X		19+02	X	1	48+49	X	1				
" 21+38		X	19+74	X	1	49+44	X	1				
" 22+06	X		20+45	X	1	50+44	X	1				
" 22+69		X	21+00	X	1	51+07	X	1				
" 23+38	X		21+38	X	1	52+29	X	1				
" 25+50	X		22+06	X	1	52+29	X	1				
" 25+60		X	22+15	X	1	53+77	X	2				
" 27+95		X	22+17	X	1	55+54	X	1				
" 29+90		X	25+50	X	1	55+70	X	1				
" 38+70	X		25+60	X	1	57+60	X	1				
" 41+44	X		25+62	X	1	58+56	X	1				
" 41+44	X		27+95	X	1	59+89	X	1				
" 42+63	X		29+40	X	1	67+38	X	1				
" 43+71		X	29+90	X	1	72+26	X	1				
" 45+09	X		38+50	X	1	76+90	X	1				
" 45+40		X	41+44	X	1	81+50	X	1				
" 46+93		X	41+44	X	1	82+66	X	14				
" 47+27	X		42+63	X	1	83+28	X	6				
" 48+49		X	43+71	X	1	84+16	X	2				
" 49+44		X	45+09	X	1	84+69	X	10				
" 50+44		X										
" 51+07	X											
" 52+29		X										
" 52+29	X											
" 53+77		X										
" 55+54		X										
"As Bit" 55+70	X											
										TOTAL NUMBER OF EACH	113	40

MAIL BOX NOTES

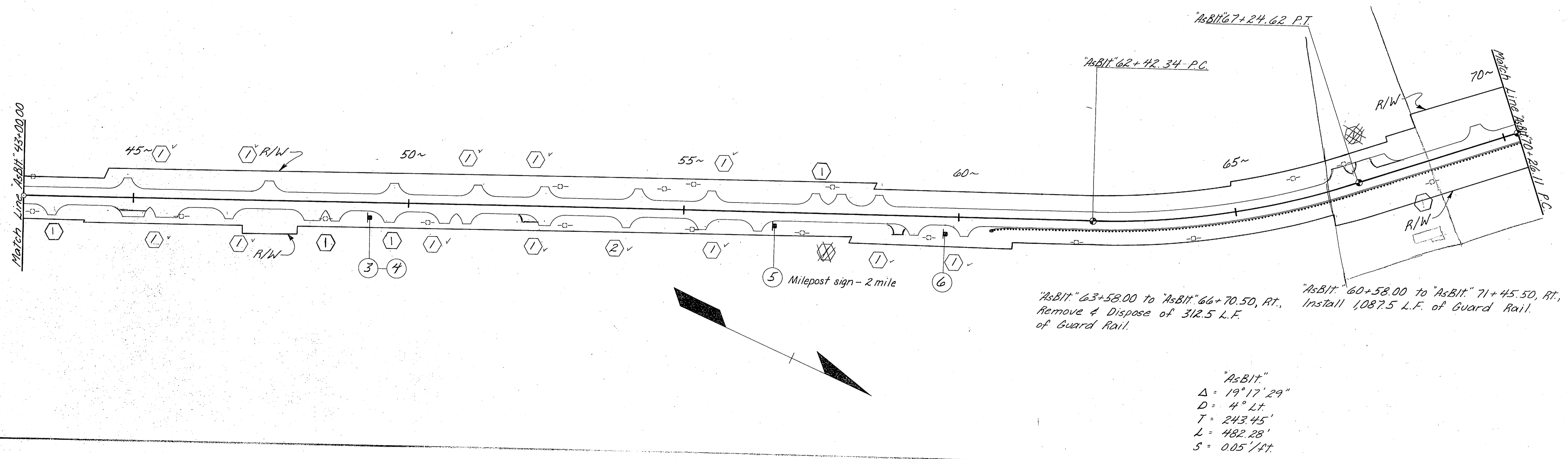
1. Newspaper tube installation shall be considered incidental to Item 202(9), Single Mailbox Installation. No separate payment shall be made.

Berry Ann Meehan

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	SR-0959(II)	1983	5	8



AsBIT
 $\Delta = 35^\circ 12' 24''$
 $D = 4^\circ \text{ RT}$
 $T = 454.47'$
 $L = 880.17'$
 $S = 0.06'/\text{ft}$



Barry P.M. Malin

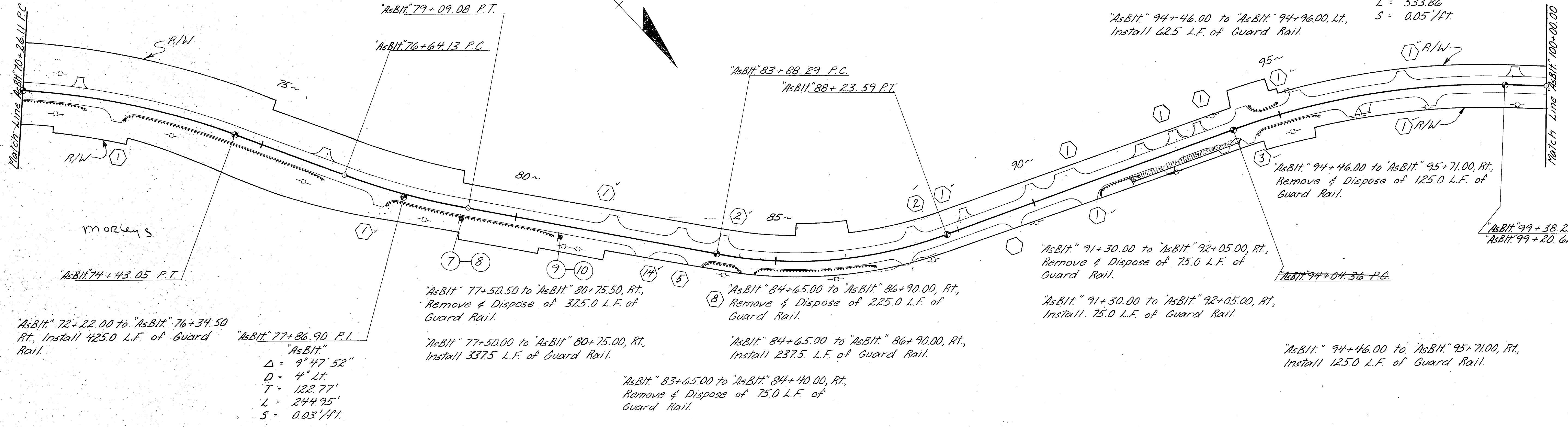
STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	SR-0959(11)	1983	6	8

"AsBIT"
 $\Delta = 16^{\circ}40'39"$
 $D = 4^{\circ} \text{ Rt.}$
 $T = 209.96'$
 $L = 416.94'$
 $S = 0.04'/\text{ft.}$

"AsBIT"
 $\Delta = 30^{\circ}28'15"$
 $D = 7^{\circ} \text{ Lt.}$
 $T = 222.92'$
 $L = 435.30'$
 $S = 0.06'/\text{ft.}$

"AsBIT" 94+46.00 to "AsBIT" 94+96.00, Lt,
 Remove & Dispose of 50.0 L.F. of
 Guard Rail.
 "AsBIT" 94+46.00 to "AsBIT" 94+96.00, Lt,
 Install 62.5 L.F. of Guard Rail.

"AsBIT"
 $\Delta = 21^{\circ}21'15"$
 $D = 4^{\circ} \text{ Rt.}$
 $T = 270.06'$
 $L = 533.86'$
 $S = 0.05'/\text{ft.}$



"AsBIT" 72+22.00 to "AsBIT" 76+34.50
 Rt, Install 425.0 L.F. of Guard
 Rail.

"AsBIT" 77+86.90 P.I.
 "AsBIT"
 $\Delta = 9^{\circ}47'52"$
 $D = 4^{\circ} \text{ Lt.}$
 $T = 122.77'$
 $L = 244.95'$
 $S = 0.03'/\text{ft.}$

"AsBIT" 77+50.50 to "AsBIT" 80+75.50, Rt,
 Remove & Dispose of 325.0 L.F. of
 Guard Rail.

"AsBIT" 77+50.00 to "AsBIT" 80+75.00, Rt,
 Install 337.5 L.F. of Guard Rail.

"AsBIT" 84+65.00 to "AsBIT" 86+90.00, Rt,
 Remove & Dispose of 225.0 L.F. of
 Guard Rail.

"AsBIT" 84+65.00 to "AsBIT" 86+90.00, Rt,
 Install 237.5 L.F. of Guard Rail.

"AsBIT" 83+65.00 to "AsBIT" 84+40.00, Rt,
 Remove & Dispose of 75.0 L.F. of
 Guard Rail.

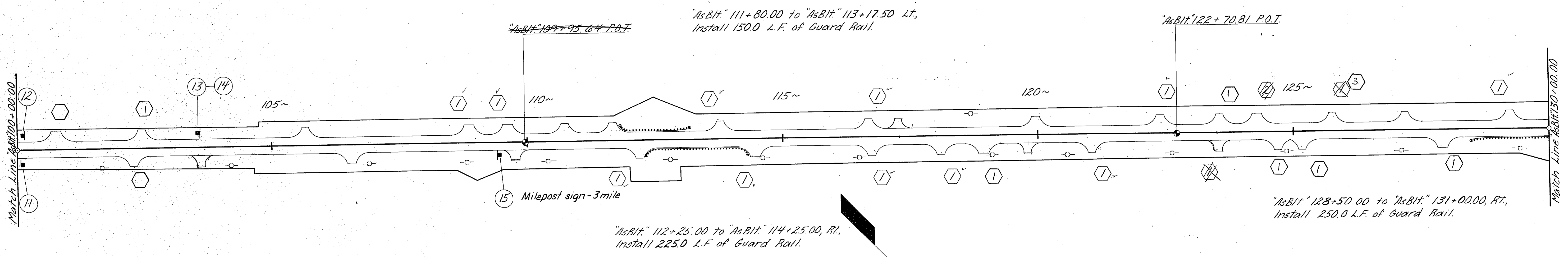
"AsBIT" 91+30.00 to "AsBIT" 92+05.00, Rt,
 Remove & Dispose of 75.0 L.F. of
 Guard Rail.

"AsBIT" 91+30.00 to "AsBIT" 92+05.00, Rt,
 Install 75.0 L.F. of Guard Rail.

"AsBIT" 94+46.00 to "AsBIT" 95+71.00, Rt,
 Remove & Dispose of 125.0 L.F. of
 Guard Rail.

"AsBIT" 94+46.00 to "AsBIT" 95+71.00, Rt,
 Install 125.0 L.F. of Guard Rail.

"AsBIT" 99+38.22 P.T. Bk =
 "AsBIT" 99+20.62 P.O.T. Ahd.



"AsBIT" 111+80.00 to "AsBIT" 113+17.50 Lt,
 Install 150.0 L.F. of Guard Rail.

"AsBIT" 122+70.81 P.O.T.

"AsBIT" 128+50.00 to "AsBIT" 131+00.00, Rt,
 Install 250.0 L.F. of Guard Rail.

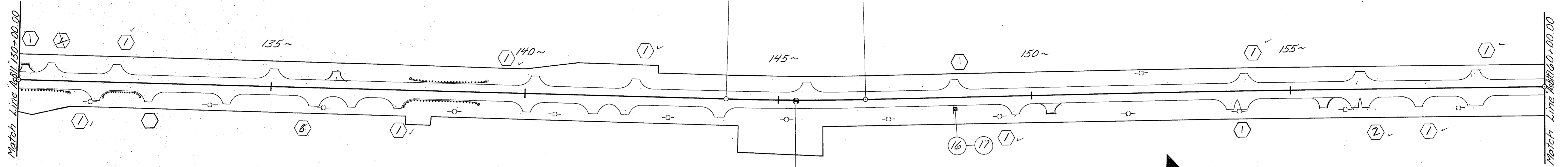
"AsBIT" 112+25.00 to "AsBIT" 114+25.00, Rt,
 Install 225.0 L.F. of Guard Rail.

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	SR-0959(II)	1983	7	8

"AsBit." 137+75.00 to "AsBit." 139+25.00, Lt.,
Remove & Dispose of 150.0 L.F. of
Guard Rail.

"AsBit." 137+75.00 to "AsBit." 139+25.00, Lt.,
Install 150.0 L.F. of Guard Rail.

"AsBit." 143+97.51 P.C.
"AsBit." 146+71.40 P.T.
"AsBit." 145+35.50 P.I.

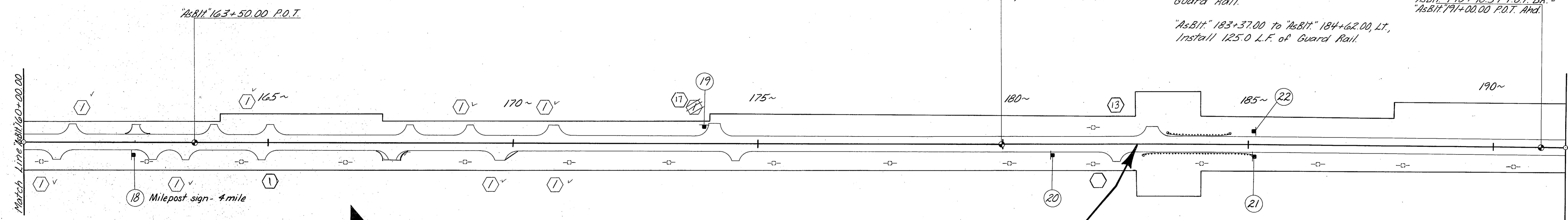
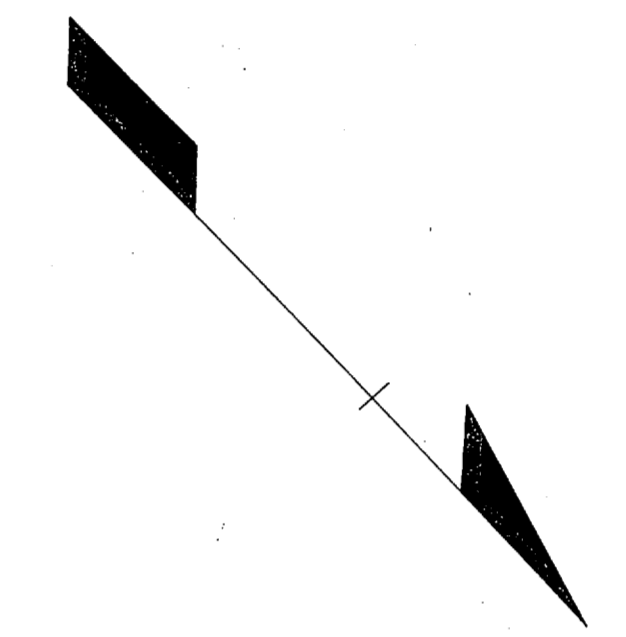


"AsBit." 131+65.00 To "AsBit." 132+40.00, Rt.,
Install 100.0 L.F. of Guard Rail.

"AsBit." 137+60.00 to "AsBit." 139+10.00, Rt.,
Remove & Dispose 150.0 L.F. of
Guard Rail.

"AsBit." 137+60.00 to "AsBit." 139+10.00, Rt.,
Install 162.5 L.F. of Guard Rail.

"AsBit."
 $\Delta = 2^{\circ} 44' 20''$
 $D = 1^{\circ} Lt.$
 $T = 136.97'$
 $L = 237.89'$
 $S = 0.02' / ft.$



"AsBit." 163+50.00 P.O.T.

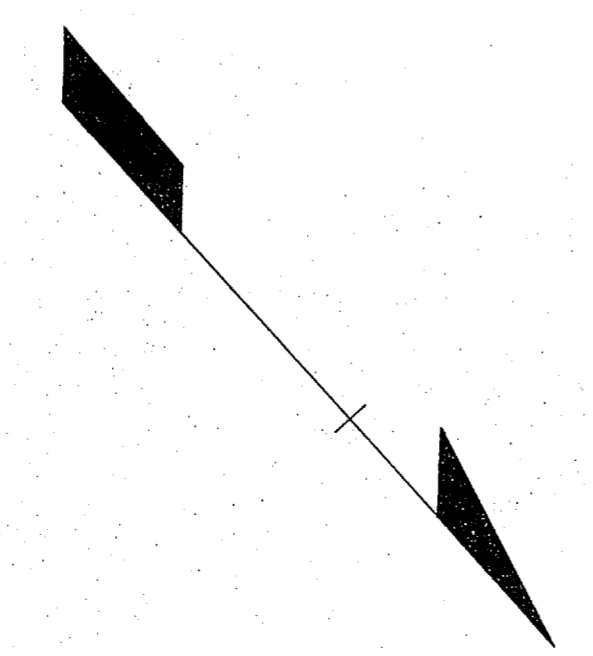
"AsBit." 179+98.44 Point
 $\Delta = 0^{\circ} 07' 30'' Rt.$

"AsBit." 183+37.00 to "AsBit." 184+62.00, Lt.,
Remove & Dispose 125.0 L.F. of
Guard Rail.

"AsBit." 183+37.00 to "AsBit." 184+62.00, Lt.,
Install 125.0 L.F. of Guard Rail.

"AsBit." 190+98.34 P.O.T. Bk. =
"AsBit." 191+00.00 P.O.T. And.

18 Milepost sign - 4 mile



**END PROJECT, BEGIN
TEMPORARY CONNECTION
SR-0959 (II)
"AsBit." 182+75.00**

"AsBit." 182+85.00 to "AsBit." 185+10.00, Rt.,
Remove & Dispose of 225.0 L.F. of
Guard Rail.

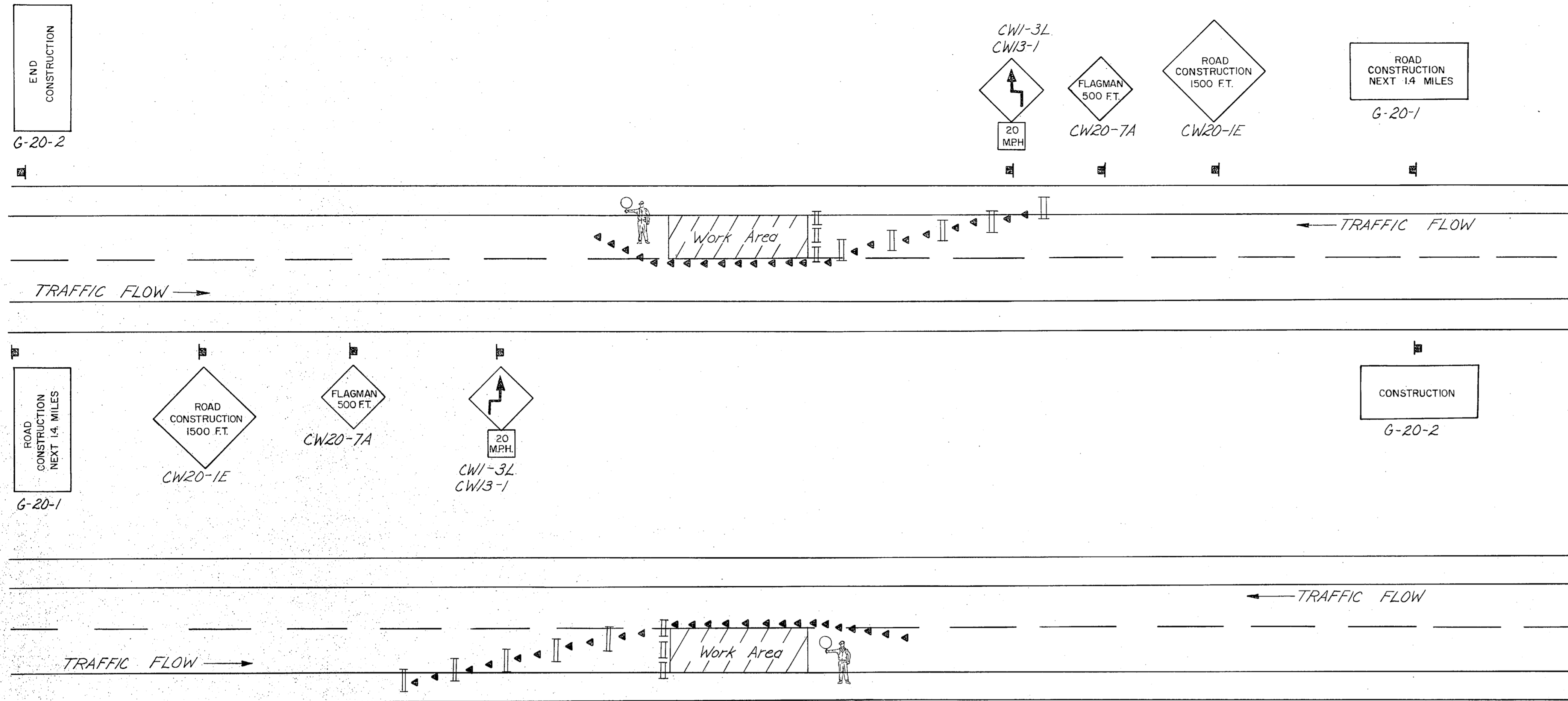
"AsBit." 182+85.00 to "AsBit." 185+10.00, Rt.,
Install 225.0 L.F. of Guard Rail.

**END TEMPORARY
CONNECTION
SR-0959(II)
"AsBit." 191+50.00**

Barry P. M. Miller

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	SR-0959(11)	1983	8	8

TRAFFIC CONTROL PLAN



NOTES:

1. A flagperson shall be required to allow construction vehicles in & out of traffic & to direct all one-way traffic operations.
2. Flashing warning lights shall be utilized during dusk to dawn hours to delineate areas where work is in progress.
3. No shoulder drop-offs greater than 1 1/2" shall be allowed during non-working hours.
4. The contractor shall designate an employee who shall be responsible for installing & maintaining all construction signing & traffic control devices.
5. The contractor shall schedule his work so as to provide for unimpeded two-way traffic during non-working hours. Lane closures or detours shall not be allowed during non-working hours.

Bary P. M. Miller