

NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
			ALASKA	SDRER00537	2023	A1	9
			CDS ROUTE:	296401	MILEPOINT:	1.049 TO	1.117

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
&
PUBLIC FACILITIES

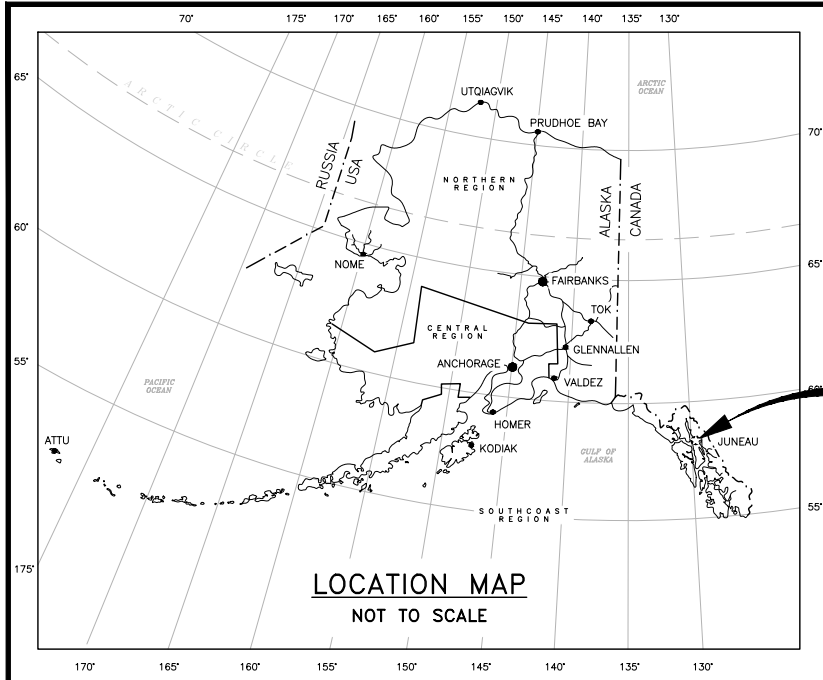
PROPOSED HIGHWAY PROJECT

SDRER00537

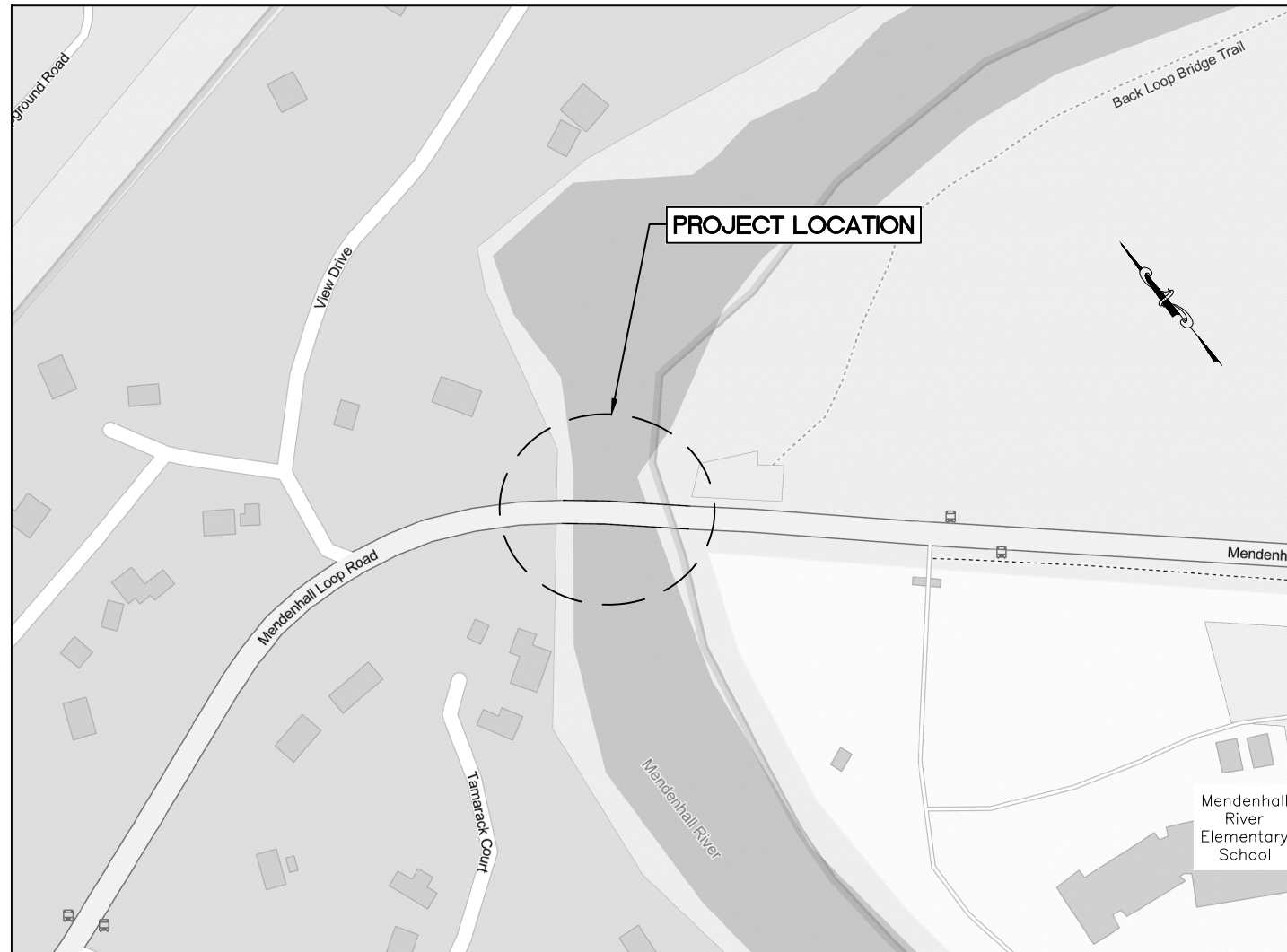
JNU MENDENHALL RIVER FLOOD ER 08-2023

REPAIR EMBANKMENT AND ABUTMENT ARMORING

INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
A1	TITLE SHEET
A2	LEGEND & SHEET LAYOUT INDEX
A3	SURVEY CONTROL SHEET
C1	ESTIMATE OF QUANTITIES & GENERAL NOTES
D1	SUMMARIES
F1	PLAN & PROFILE
N	BRIDGE DESIGN
Q1	EROSION SEDIMENT CONTROL PLANS
T1	TRAFFIC CONTROL PLANS (and/or DEVICES)



PROJECT LOCATION:
JUNEAU, AK



MENDENHALL "BACK LOOP" ROAD
BRIDGE NUMBER 0217

AS-BUILT
Contractor: Colaska dba SECON
Project Engineer: Travis Watkins
Begin Date: 10/17/23
End Date: 10/21/23

THE FOLLOWING STANDARD PLANS APPLY TO THIS PROJECT:
C-06.00

PROJECT SUMMARY	
WIDTH OF PAVEMENT	N/A
LENGTH OF GRADING	N/A
LENGTH OF PAVING	N/A
LENGTH OF PROJECT	100'

BRETT WELLS, P.E., PROJECT MANAGER

Record Drawings have been reviewed by the Project Engineer, and represent to the best of my knowledge, the Project as constructed.

PE

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
&
PUBLIC FACILITIES

APPROVED BY: DATE: 8/29/2023

ACCEPTED FOR CONSTRUCTION: DATE: 8/29/2023

Christopher Goins, P.E., C.M.
Regional Director, Southcoast Region

NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
			ALASKA	SDRER00537	2023	A2	9

	RECOVERED	SET
BLM MONUMENT		
GLO MONUMENT		
USC&GS MONUMENT		
PRIMARY MONUMENT		
CENTERLINE MONUMENT IN CASING		
PRIMARY R.O.W. MONUMENT		
BEARING OBJECT		
MISCELLANEOUS MONUMENT		
LINE OF SIGHT MONUMENT		
CONCRETE R.O.W. MONUMENT		
BENCHMARK		
REBAR AND CAP		
REBAR		
IRON PIPE		
PK NAIL		
SPIKE		
HUB AND TACK		
CONSTRUCTION CENTERLINE		
MISCELLANEOUS CENTERLINE		
STATION EQUATION	$\frac{L}{100} = \frac{L^2}{48+97.23 \text{ POT BK} = 10+00}$ $\frac{O}{100} = \frac{O^2}{48+97.23 \text{ PC AHD}}$	
PROJECT RIGHT-OF-WAY LINE		
EXISTING RIGHT-OF-WAY LINE		
EXISTING PROPERTY LINE		
CONTROLLED ACCESS LINE		
UTILITY EASEMENT LINE		
TEMPORARY EASEMENT LINE (TCP OR TCE)		
ACCESS OR SECTION LINE EASEMENT		
PROPOSED CUT SLOPE LIMIT		
PROPOSED FILL SLOPE LIMIT		
SECTION LINE		
1/4 SECTION LINE		
1/16 SECTION LINE		
TOWNSHIP & RANGE LINE	T. 2 N. T. 1 N.	T. 2 E. T. 1 E.

	EXISTING	PROPOSED
SANITARY SEWER (FLOW DIRECTION →)	---SS---	→→→SS→
FUEL LINE	---O---	→→→O→
GAS LINE	---G---	→→→G→
WATER LINE	---W---	→→→W→
METER, VALVE, FIRE HYDRANT		
EXISTING STORM DRAIN (FLOW DIRECTION →)	---SD---	
PROPOSED STORM DRAIN		
FIBER OPTIC LINE	---FO---	
DIRECT BURIAL TELEPHONE CABLE	---T---	---T---
DIRECT BURIAL ELECTRIC CABLE	---E---	---E---
ELECTRIC LINE (OVERHEAD)	---E---	---E---
POWER POLE LINE		
JOINT USE POWER & TELEPHONE		
TELEPHONE POLE LINE		
POLE ANCHOR		
STUB POLE (POWER OR TELEPHONE)		
TELEPHONE DUCT	===T===	===T===
TELEPHONE PEDESTAL		
BURIED CABLE MARKER		
PIPELINE MARKER OR VALVE		
CATCH BASIN OR DROP INLET		
MANHOLE		
SANITARY SEWER CLEAN OUT		

	EXISTING	PROPOSED
ROADWAY/PAVEMENT EDGE	-----	=====
FENCE	--X--X--X--X--	-X--X--X--X--
CURB AND GUTTER	=====	=====
DETECTABLE WARNINGS		
GUARDRAIL
CULVERT PIPE		
SIGN		
MAILBOX		
RAILROAD TRACKS	=====	=====
RAILROAD DEVICES		
TREE LINE	~~~~~	~~~~~
WATER BOUNDARY	~~~~~	~~~~~
ORDINARY HIGH WATER LINE	~~~~~	~~~~~
FLOW CENTERLINE	~~~~~	~~~~~
FLOW DIRECTION		
WETLANDS		
EXISTING BUILDINGS		
POST OR BOLLARD	•	•
WELL OR MONITORING WELL		
SEPTIC PIPE		
FUEL TANK FILL PIPE/VENT		
SATELLITE DISH		
TEST HOLE		
CONIFER TREE		
DECIDUOUS TREE		
GRAVE		
THERMOSIPHON		
PARKING METER		
VEHICLE PLUG-IN		
DELINEATOR/GUIDE MARKER		

	EXISTING	PROPOSED
JUNCTION BOX, TYPE IA		
JUNCTION BOX, TYPE II		
JUNCTION BOX, TYPE III		
SIGNAL FACE, VEHICULAR		
SIGNAL FACE, BACKPLATE		
SIGNAL FACE, LEFT TURN, BACKPLATE		
SIGNAL FACE, PEDESTRIAN		
LOOP DETECTOR		
VIDEO DETECTOR		
RADAR DETECTOR		
OPTICOM DETECTOR		
PEDESTRIAN PUSH BUTTON		
SIGNAL POST W/O MAST ARM		
SIGNAL POLE W/MAST ARM		
SIGNAL CONTROLLER		
LOAD CENTER		
LUMINAIRE		
RIGID METAL CONDUIT		

H = HOUSE
 G = GARAGE
 M = MERCHANT/STORE
 B = BARN
 S = SHED
 P = PRIVY
 SS = SERVICE STATION
 W = WAREHOUSE

ABBREVIATIONS:

APPROX	APPROXIMATELY	SQFT	SQUARE FOOT
C	CENTERLINE	STA	STATION
CY	CUBIC YARD	T	TANGENT
E	EAST, EASTING	TCE	TEMPORARY CONSTRUCTION EASEMENT
ELEV	ELEVATION	TS	TUBE STEEL
FT	FOOT, FEET	TYP	TYPICAL
H	HORIZONTAL	V	VERTICAL
HW/D	HEADWATER TO DIAMETER RATIO	VPC	VERTICAL POINT OF CURVATURE
IE	INVERT ELEVATION	VPI	VERTICAL POINT OF INTERSECTION
IN	INCH, INCHES	VPT	VERTICAL POINT OF TANGENCY
L	LENGTH OF CURVE	W	WEST
LCL	LEFT OF CENTERLINE	WWR	WELDED WIRE REINFORCEMENT
LT	LEFT	Ø	DIAMETER
LVC	LENGTH OF VERTICAL CURVE		
MAX	MAXIMUM		
MIN	MINIMUM		
N	NORTH, NORTHING		
NO.	NUMBER		
NTS	NOT TO SCALE		
OC	ON CENTER		
PC	POINT OF CURVATURE		
POT	POINT ON TANGENT		
PST	PERFORATED STEEL TUBE		
PT	POINT OF TANGENCY		
PVI	POINT OF VERTICAL INTERSECTION		
R	RADIUS		
RCL	RIGHT OF CENTERLINE		
RT	RIGHT		
S	SOUTH		

Record Drawings have been reviewed by the Project Engineer, and represent to the best of my knowledge, the Project as constructed.

PE

LEGEND & ABBREVIATIONS



DRAWING LOCATION: G:\Jnu\SDRER00537\SV\C3D\Mendenhall River SCS_081723.dwg
 DATE: 8/23/2023 14:57
 TIME: 14:57
 LAYOUT: A2
 SCALE:
 XREFS:
 DESIGNED: JRAPOL
 CHECKED: E PEDERSEN
 DRAFTER: JRAPOL

NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
			ALASKA	SDRER00537	2023	A3	9

MONUMENT NOTES:

- IF ANY PAIR OF CONTROL POINTS DISAGREES FROM PUBLISHED VALUE BY MORE THAN 1:10,000 HORIZONTALLY OR VERTICALLY THEN A THIRD NETWORK POINT MUST BE TIED TO ASCERTAIN WHICH POINT IS IN ERROR OR HAS BEEN DISTURBED.
- WHETHER LISTED OR NOT, ALL PROPERTY MONUMENTS, PROPERTY MARKERS, CORNERS OR ACCESSORIES WHICH WILL BE DISTURBED OR BURIED SHALL BE REFERENCED PRIOR TO BEING DISTURBED, AND RE-ESTABLISHED IN THEIR ORIGINAL HORIZONTAL POSITION AND A RECORD OF MONUMENT FORM IN ACCORDANCE WITH (A.S.34.65.040) AND (A.S.19.10.260) SHALL BE SUBMITTED TO THE CONSTRUCTION ENGINEER FOR REVIEW PRIOR TO RECORDING. COORDINATE VALUES LISTED ARE FOR INFORMATIONAL PURPOSES AND SHOULD BE USED TO RESET MONUMENTS ONLY AS A LAST RESORT.
- WHEN POSSIBLE ALL ORIGINAL PRIMARY MONUMENTS SHALL BE SAVED AND RESET IN THEIR ORIGINAL HORIZONTAL POSITION AND A RECORD OF MONUMENT FORM IN ACCORDANCE WITH (A.S.34.65.040) AND (A.S.19.10.260) SHALL BE SUBMITTED TO THE CONSTRUCTION ENGINEER FOR REVIEW PRIOR TO RECORDING.
- RIGHT OF WAY LOCATION IS SHOWN FOR GRAPHICAL ORIENTATION PURPOSES ONLY. REFER TO ALASKA DOT&PF RIGHT OF WAY MAPS FOR RIGHT OF WAY INFORMATION.
- HORIZONTAL AND VERTICAL CONTROL MUST BE FIELD VERIFIED BY THE CONTRACTOR. DISCREPANCIES WILL BE REPORTED TO DOT&PF CONSTRUCTION PROJECT ENGINEER.

Record Drawings have been reviewed by the Project Engineer, and represent to the best of my knowledge, the Project as constructed.

PE

HORIZONTAL CONTROL

Horizontal Control for this project is based on the DOT/PF 2000 Juneau Grid

The DOT/PF Juneau Grid-2000 System is a local ground coordinate system based at USC&GS first order control station EDDIE (Destroyed). It relates to AKSPC zone 1 NAD83 (1992) through the following parameters:
 Zone = NAD83 (1992) AKSPC ZONE 1
 Grid Scale = 0.999928875
 Convergence = -0°45'27.26"
 Translation about USC&GS point EDDIE (Destroyed) as follows:
 AKSPC Northing = 2383469.17 FT US
 AKSPC Easting = 2512570.06 FT US
 Local Northing = 500000.00 FT US
 Local Easting = 500000.00 FT US

Project Specific Horizontal Control

- 116: BRASS CAP MONUMENT IN MENDENHALL RIVER BRIDGE NORTH SIDEWALK NORTHWEST CORNER
- 48: BRASS CAP MONUMENT IN MONUMENT CASE IN NORTH SHOULDER OF BACK LOOP ROAD
- 49: BRASS CAP MONUMENT IN MONUMENT CASE IN SOUTH SHOULDER OF BACK LOOP ROAD

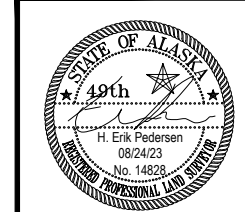
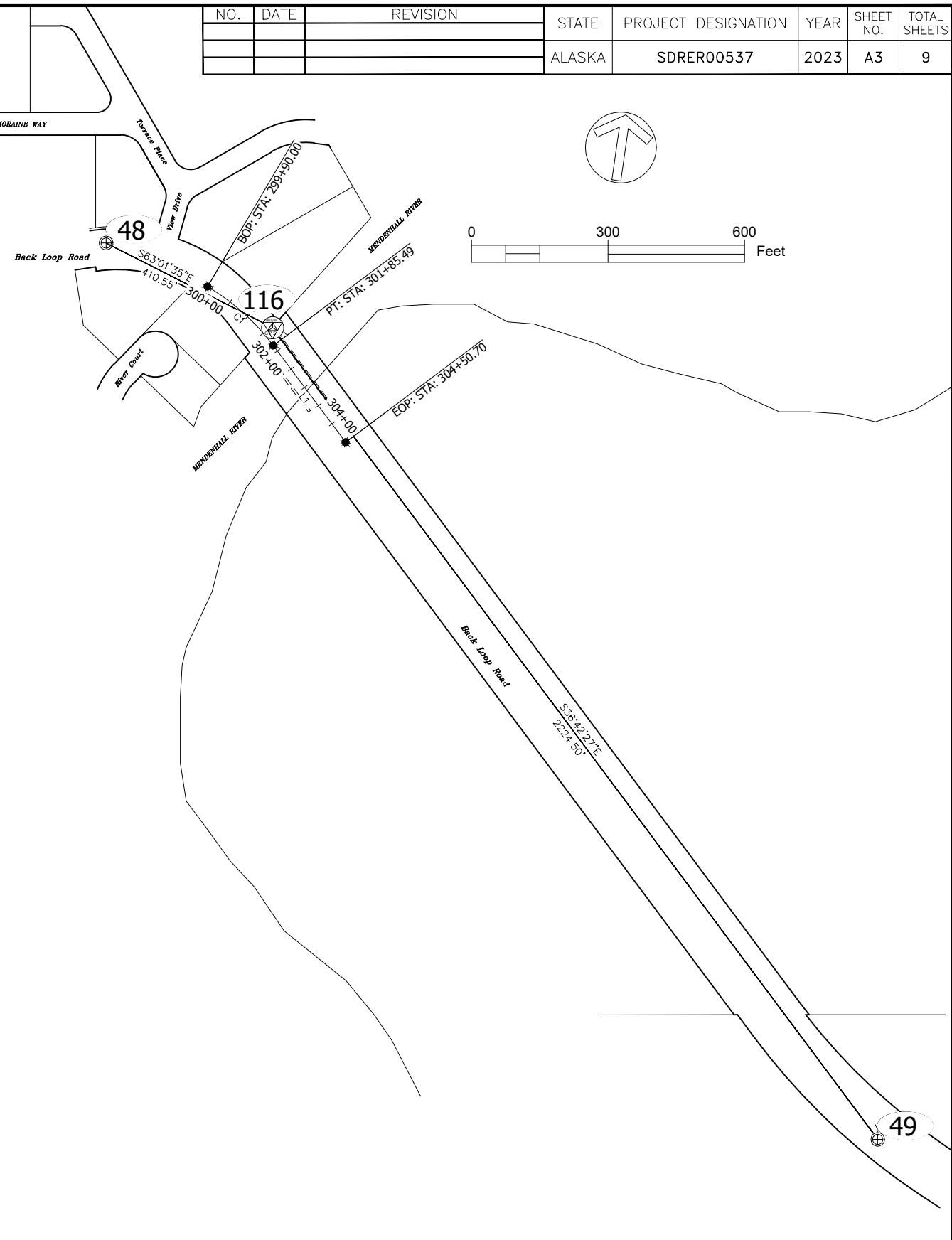
Project Specific Vertical Control

- 116: BRASS CAP MONUMENT IN MENDENHALL RIVER BRIDGE NORTH SIDEWALK NORTHWEST CORNER

The Vertical Datum for JNU Grid-2000 is Mean Lower Low Water = 0.00' Gastineau Channel - Stephens Pass tidal datum based on NOAA NOS tidal benchmark series 9452210. The tidal epoch is 1960-1978, time period 1994-1998, published 11/1999.

Point #	Northing	Easting	Elevation	Description	Station	Offset
48	517817.41	496611.82	75.14 ±	BC_SH_MON	N/A	N/A
49	515847.82	498307.35	56.65 ±	BC_SH_MON	N/A	N/A
116	517631.20	496977.70	71.97	BC_95-J-16	301+54.49	23.17L

SEGMENT	STATION	NORTHING	EASTING	DISTANCE	BEARING	STATION	RADIUS	LENGTH	DELTA
C1	299+90.00	517721.55	496834.75			301+85.49	495.37	195.49	22°36'38"
L1	301+85.49	517591.96	496979.41	265.21	S36° 50' 25"E	304+50.70			



STATE OF ALASKA
 DEPARTMENT OF TRANSPORTATION
 AND PUBLIC FACILITIES
 6860 Glacier Highway Juneau Ak. 99801
 (907) 465-1763

**JNU MENDENHALL RIVER FLOOD
 ER 8-2023**

SURVEY CONTROL


NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
			ALASKA	SDRER00537	2023	C1	9

ESTIMATE OF QUANTITIES			
ITEM NUMBER	PAY ITEM	PAY UNIT	PLAN QUANTITY
201.0009.0000	CLEARING AND GRUBBING	LS	ALL REQUIRED
611.0002.0001	RIPRAP, CLASS I	TON	1,355.00
611.0002.0004	RIPRAP, CLASS IV	TON	1,913.00
640.0001.0000	MOBILIZATION AND DEMOBILIZATION	LS	ALL REQUIRED
641.0006.0000	WITHHOLDING	CS	ALL REQUIRED
643.0002.0000	TRAFFIC MAINTENANCE	LS	ALL REQUIRED
643.0025.0000	TRAFFIC CONTROL	CS	ALL REQUIRED
658.0001.0000	EROSION, SEDIMENT, AND POLLUTION CONTROL WITHOUT CGP COVERAGE	LS	ALL REQUIRED
658.0002.0000	ESCP CHANGES BY DIRECTIVE	CS	ALL REQUIRED

BASIS OF ESTIMATE		
ITEM No.	PAY ITEM	ESTIMATING FACTOR
611.0002.0001	RIPRAP, CLASS I	3375 LB/CUYD
611.0002.0004	RIPRAP, CLASS IV	2400 LB/CUYD

AS-BUILT QUANTITIES				
No.	Item	Unit	QTY	
201.0009.0000	Clearing and Grubbing	LS	1	
611.0002.0001	Riprap, Class I	TON	1,224.60	
611.0002.0004	Riprap, Class IV	TON	1,740.00	
640.0001.0000	Mobilization and Demobilization	LS	1	
641.0006.0000	Withholding	CS	0	
643.0002.0000	Traffic Maintenance	LS	1	
643.0025.0000	Traffic Control	CS	0	
658.0001.0000	Erosion, Sediment, and Pollution Control Without CGP Coverage	LS	1	
658.0002.0000	ESCP Changes by Directive	CS	0	
ITEMS ADDED BY CHANGE ORDER				
603.2041.0000	Pipe Repair	LS	1	

Record Drawings have been reviewed by the Project Engineer, and represent to the best of my knowledge, the Project as constructed.

PE 

ESTIMATE OF QUANTITIES



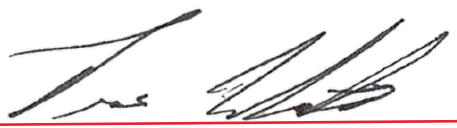
NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
			ALASKA	SDRER00537	2023	D1	9

[201.0009.0000] CLEARING AND GRUBBING					
STATION		OFFSET		AREA (AC)	NOTES
START	END	START	END		
301+31	301+97	0 LT	152 LT	0.159	
301+31	301+89	0 RT	125 RT	0.109	
TOTAL:				0.268	

[611.0001.0001] RIPRAP, CLASS 1				
OFFSET	THICKNESS (FT)	VOLUME (CY)	WEIGHT (T)	NOTES
LT	VARIES	777	1315	
RT	VARIES	24	40	
TOTAL:			1355	

[611.0001.0004] RIPRAP, CLASS 4				
OFFSET	THICKNESS (FT)	VOLUME (CY)	WEIGHT (T)	NOTES
LT	VARIES	1169	1403	
RT	VARIES	425	510	
TOTAL:			1913	

Record Drawings have been reviewed by the Project Engineer, and represent to the best of my knowledge, the Project as constructed.

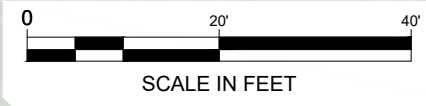
PE 

SUMMARY TABLES

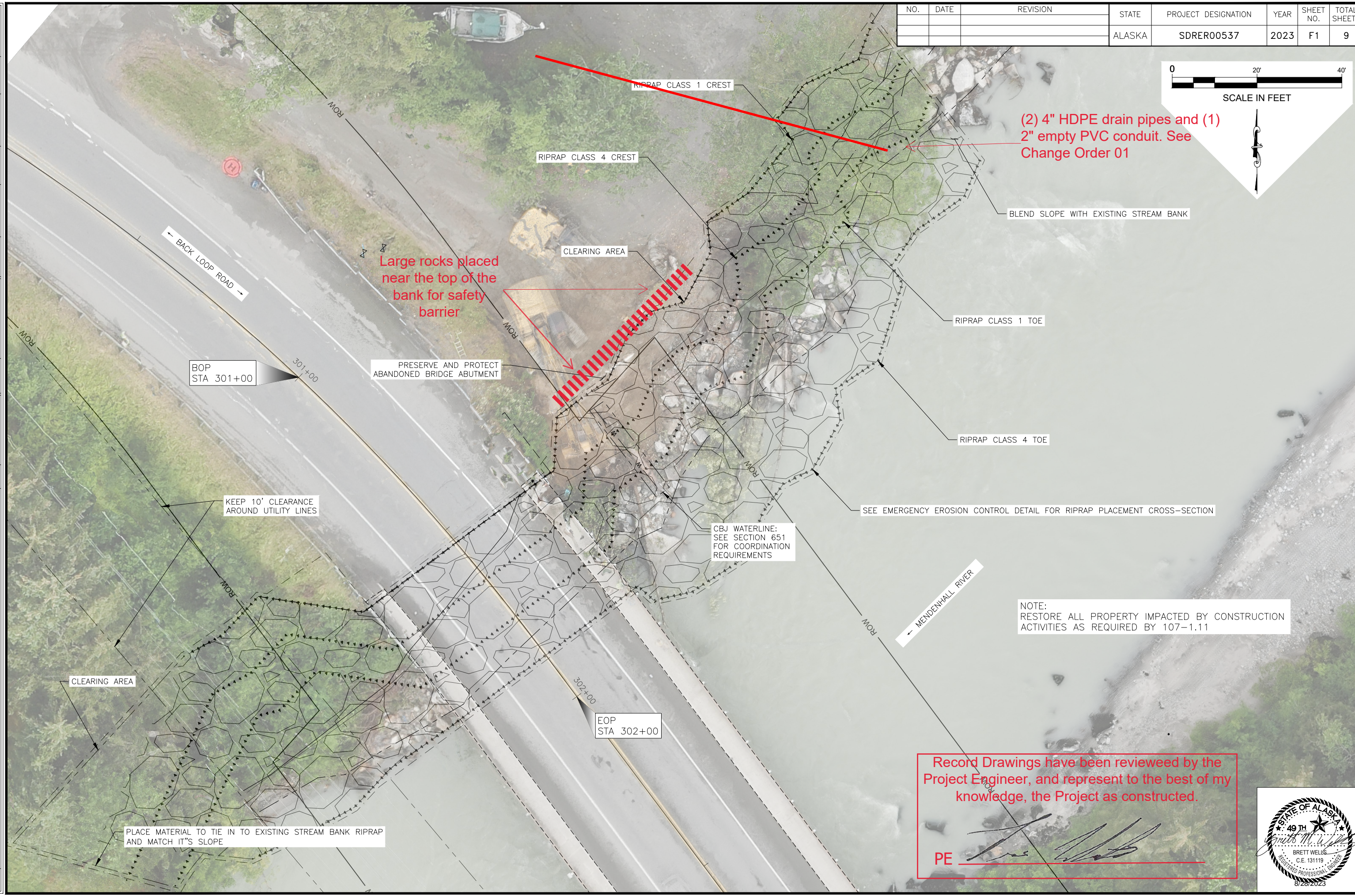


FIRM STATE OF ALASKA DEPARTMENT OF TRANSPORTATION & PUBLIC FACILITIES
 FILE Q:\nu\SDRER00537\PlanSet\INU_SSHWY00537_F01_PLAN_PROFILE.dwg
 ADDRESS 6860 GLACIER HWY, JUNEAU, AK 99811
 DATE 8/28/2023 15:29 LAYOUT F1 PLAN
 PHONE (907) 465-1763 DESIGNED BW
 CERTIFICATE OF AUTH # (907) 465-1763 CHECKED BW
 DRAFTED BW, WU

NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
			ALASKA	SDRER00537	2023	F1	9



(2) 4" HDPE drain pipes and (1) 2" empty PVC conduit. See Change Order 01



NOTE:
 RESTORE ALL PROPERTY IMPACTED BY CONSTRUCTION
 ACTIVITIES AS REQUIRED BY 107-1.11

Record Drawings have been reviewed by the
 Project Engineer, and represent to the best of my
 knowledge, the Project as constructed.

PE *[Signature]*



STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	TBD	2023	N	T1Shts

GENERAL NOTES

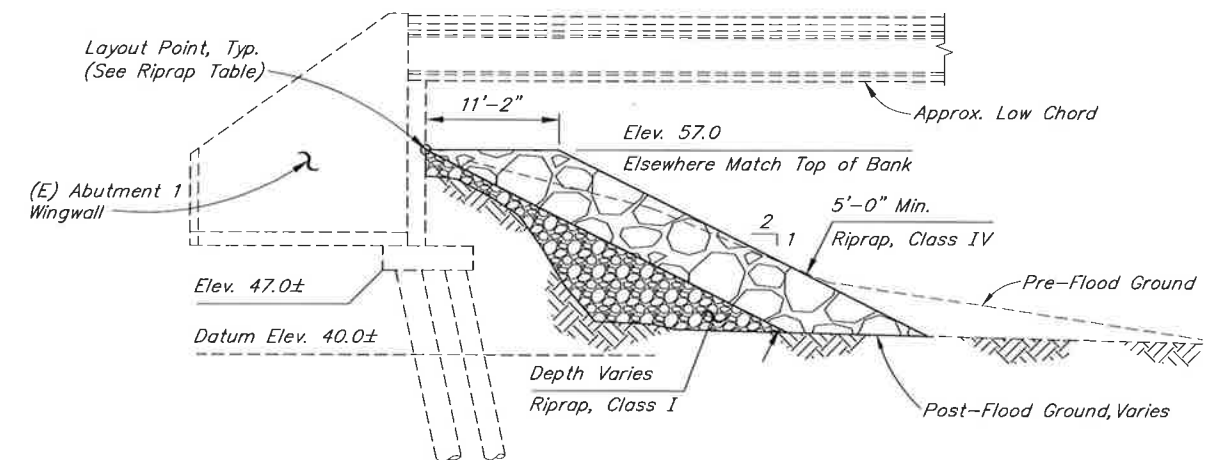
CONSTRUCTION:..... State of Alaska Standard Specifications for Highway Construction, 2020 with Special Provisions.

SURVEY DATA:..... Layout stationing and elevations are based upon 1985 Asbuilt drawings for Bridge No. 217 Mendenhall River. Survey control data available through the Alaska DOT&PF Southcoast Region Survey Office

Existing stations, elevations and dimensions are based on as-built plans, and those plans may not show existing dimensions and conditions. Where dimensions of the proposed work depend on the existing bridge dimensions, field-verify the controlling dimensions and adjust proposed dimensions of the work to fit existing conditions.

POINT	STATION	OFFSET	ELEVATION
①	301+32.8	166.5' Left	M.T.B.
②	301+37.9	133.6' Left	M.T.B.
③	301+43.5	96.8' Left	M.T.B.
④	301+49.9	62.0' Left	M.T.B.
⑤	301+57.0	27.4' Left	57.0'
⑥	301+55.5	0.0' Center	57.0'
⑦	301+53.7	28.0' Right	57.0'
⑧	301+32.0	45.0' Right	M.T.B.

M.T.B. = Match the original ground at the top of bank.



RIPRAP SECTION

No Scale

ITEM NO.	ITEM	PAY UNIT	ESTIMATING UNIT	SUBST.	SUPERST.	TOTAL QUANTITY
611.0001.0001	Riprap, Class I	TONS	CY	1,355	---	1,355
611.0001.0004	Riprap, Class IV	TONS	CY	1,913	---	1,913

Item numbers are for reference only. Quantities shown are not necessarily the pay quantities nor the total quantity of the particular item.

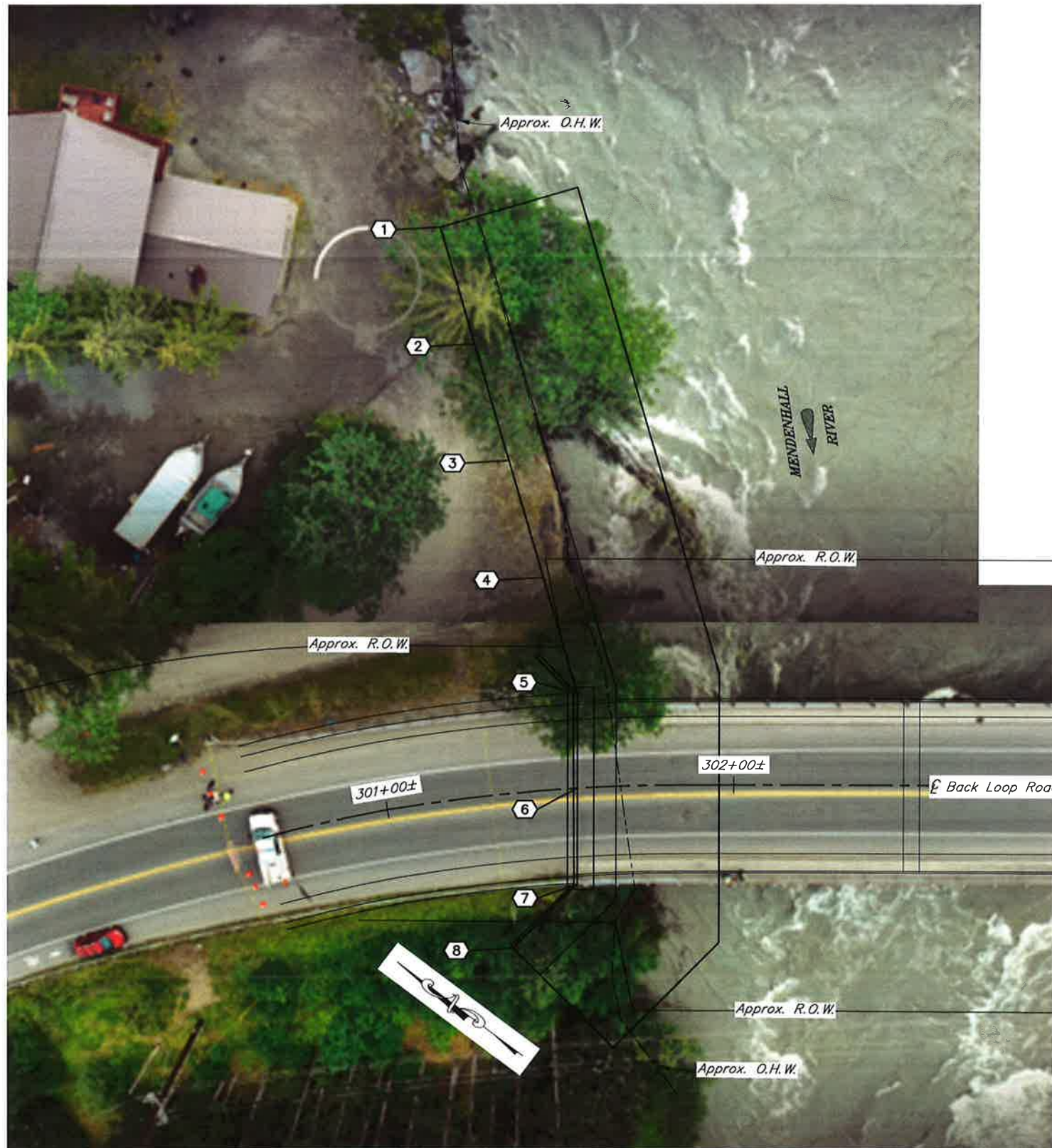
Riprap Conversion Factors: Class I = 1.6875 Ton/CY and Class IV = 1.2 Ton/CY

Record Drawings have been reviewed by the Project Engineer, and represent to the best of my knowledge, the Project as constructed.

PE

NOTES:

1. Stations and Elevations based on 1985 asbuilt drawings.
2. Place the riprap carefully to prevent damage to the bridge superstructure.
3. Fill Area = 0.25 acres

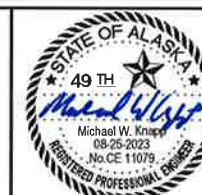


RIPRAP LAYOUT

No Scale

HYRAULICS BY: Michael Knapp	CHECKED: Luke Boles
DRAWN BY: Javier De Leon	CHECKED: Michael Knapp
QUANTITIES BY: Michael Knapp	CHECKED: Luke Boles

STATE OF ALASKA
 DEPARTMENT OF TRANSPORTATION
 AND PUBLIC FACILITIES
 BRIDGE SECTION
 3132 Channel Drive
 Juneau, Alaska 99801
 907-465-2975



MENDENHALL RIVER BRIDGE
 BACK LOOP ROAD
EMERGENCY EROSION CONTROL



BRIDGE NO. 217
 DWG. NO. 1

NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
			ALASKA	SDRER00537	2023	Q1	9



ESCP GENERAL NOTES:

1. CONSTRUCTION ENTRANCE/EXIT MUST BE ESTABLISHED TO MINIMIZE OFF-SITE IMPACTS.
2. INSTALL PERIMETER CONTROL BMP WHEN WORKING WITHIN 25 FEET OF SURFACE WATERS AND ALONG WETLANDS WHERE A 25 FOOT VEGETATIVE BUFFER IS NOT RETAINED.
3. IF EXCAVATION DE-WATERING WILL OCCUR WITHIN 1,500FT OF AN ADEC IDENTIFIED CONTAMINATED SITE, THEN THE PROJECT MUST COMPLY WITH THE ADEC EXCAVATION DE-WATERING GENERAL PERMIT.
4. INLET / OUTLET PROTECTION REQUIRED FOR ALL CULVERTS, CROSSING CULVERT PROTECTION IS SHOWN ON THE ESCP SHEETS, DRIVEWAY CULVERTS ARE NOT SHOWN FOR VISUAL CLARIFICATION.
5. AREAS OF DISTURBANCE, TEMPORARY AND PERMANENT STABILIZATION, WILL BE MARKED AS WORK PROCEEDS AND ADDED TO THE LEGEND.
6. REFER TO APPENDIX B OF THE CONTRACT FOR ENVIRONMENTAL PERMIT INFORMATION.
7. REFER TO THE SUPPLEMENTAL INFORMATION OF THE CONTRACT FOR THE ESCP TEMPLATE.
8. A PORTABLE TOILET WILL BE PLACED IN THE PARKING LOT THAT IS ON THE EAST SIDE OF THE BRIDGE. THIS UNIT MUST BE SECURED BY STAKES INTO THE GROUND AND BE A MINIMUM OF 50FT FROM THE WATERS OF THE U.S.

LEGEND:	
	EXISTING SURFACE FLOW
	SEDIMENT BARRIER
	CONSTRUCTION ENTRANCE AND EXIT
	WETLANDS
	RIP RAP

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PE

ESCP

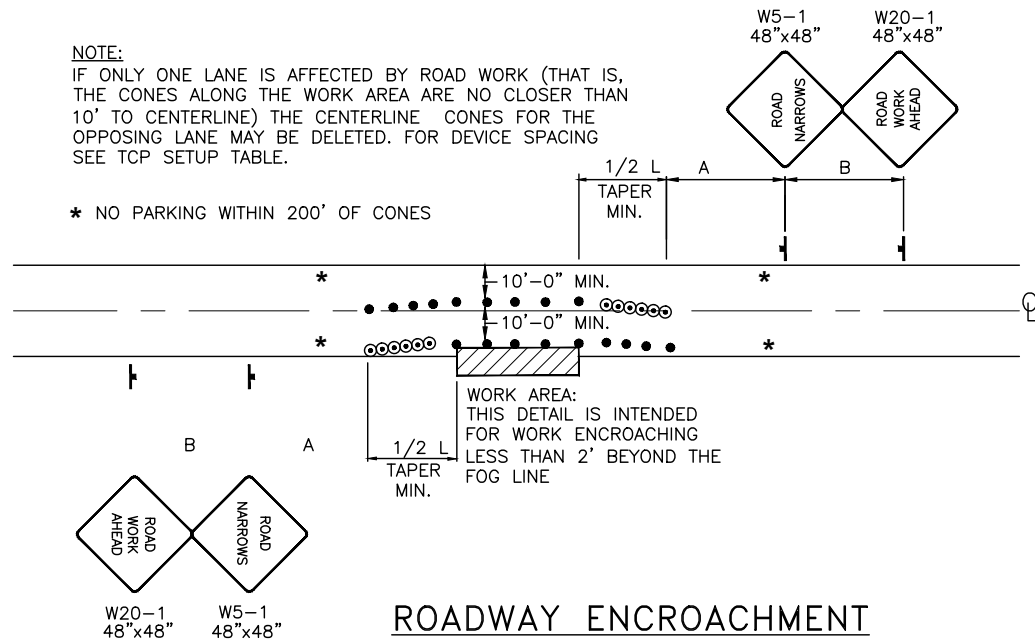
NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
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SPEED (MPH)	MIN MERGING TAPER LENGTH (L) IN FEET WIDTH OF OFFSET (W) IN FEET			MIN NUMBER OF DEVICES WIDTH OF OFFSET (W) IN FEET			MAX DEVICE SPACING IN FEET		BUFFER SPACE (FT)
	10'	11'	12'	10'	11'	12'	ALONG TAPER	ALONG TANGENT	
25 OR BELOW	105	115	125	6	6	6	25	50	155
30	150	165	180	6	7	7	30	60	200
35	205	225	245	7	8	8	35	70	250
40	270	295	320	8	9	9	40	80	305
45	450	495	540	11	12	13	45	90	360

NOTE:

IF ONLY ONE LANE IS AFFECTED BY ROAD WORK (THAT IS, THE CONES ALONG THE WORK AREA ARE NO CLOSER THAN 10' TO CENTERLINE) THE CENTERLINE CONES FOR THE OPPOSING LANE MAY BE DELETED. FOR DEVICE SPACING SEE TCP SETUP TABLE.

* NO PARKING WITHIN 200' OF CONES



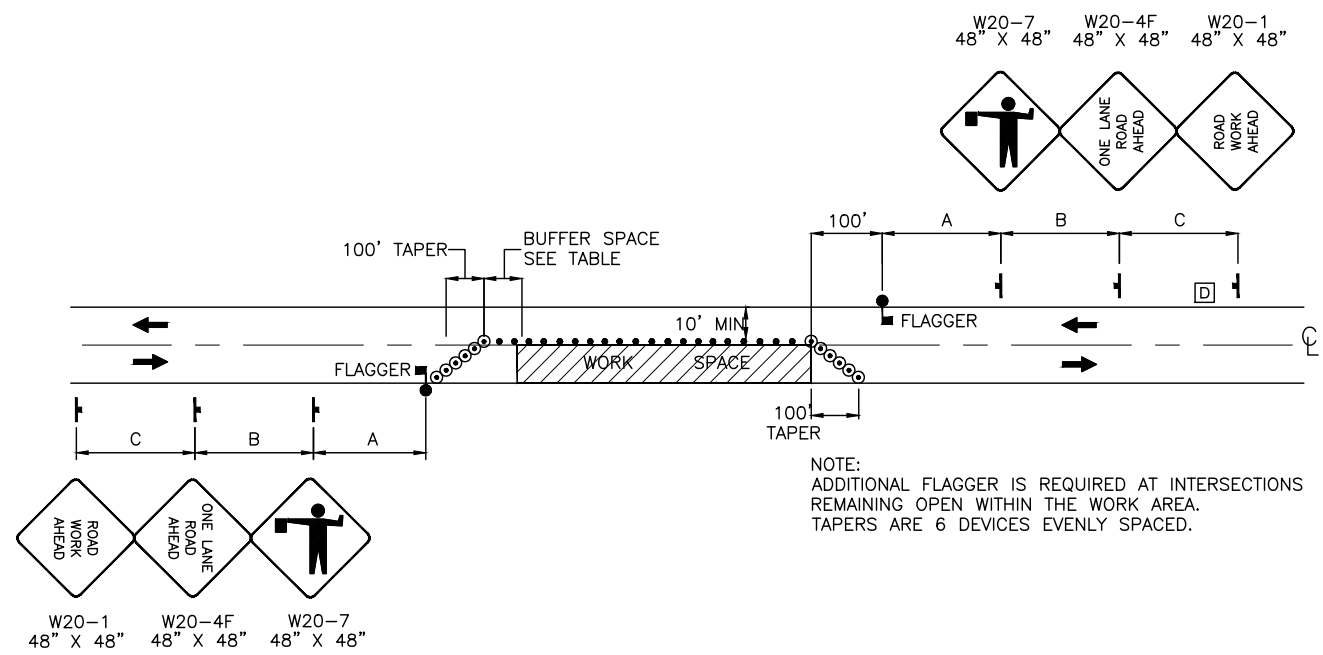
ROADWAY ENCROACHMENT

TRAFFIC CONTROL NOTES:

- IT IS THE INTENT OF THIS TRAFFIC CONTROL PLAN (TCP) TO ILLUSTRATE SOME, NOT ALL, OF THE TRAFFIC CONTROL SETUPS WHICH WILL BE REQUIRED ON THIS PROJECT. PLANS FOR CONFIGURATIONS NOT COVERED BY THE TCP SHALL BE CREATED BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER FOR APPROVAL PER SUB SECTION 643-1.03.
- TEMPORARY DRIVING LANES SHALL HAVE A MINIMUM WIDTH OF 10'-0".
- DRIVEWAYS MAY BE CLOSED DURING ACTUAL WORK ON A GIVEN DRIVEWAY, PROVIDED THAT THE CLOSURE DOES NOT EXCEED 8 HOURS FOR GRADING AND PAVING ACTIVITIES OR 4 DAYS FOR CONCRETE WORK, AND THE AFFECTED RESIDENTS HAVE BEEN GIVEN 24 HOURS NOTICE OF THE CLOSURE. PROVIDE RAMPAS AS REQUIRED WHERE CURB AND GUTTER HAS BEEN CONSTRUCTED AND ADJOINING PAVEMENT AND/OR SIDEWALK HAS NOT BEEN CONSTRUCTED, PROTECT IN-PLACE WORK FROM DAMAGE DURING TEMPORARY ACCESS PERIODS. FOR CONCRETE CURBS, AND SIDEWALKS, ALLOW CURING TIME PER SECTIONS 608 AND 609 BEFORE OPENING TO VEHICULAR TRAFFIC.
- MAINTAIN A MINIMUM OF ONE LANE TO BE OPEN FOR TRAFFIC AT ALL TIMES.
- CONSTRUCTION SIGNS SHALL BE PLACED SUCH THAT THEY DO NOT OBSCURE EXISTING TRAFFIC SIGNS.
- VERTICAL EDGES GREATER THAN 2" ARE NOT ALLOWED DURING NON-WORKING HOURS. LIMIT SHOULDER EXCAVATION TO AN AREA THAT CAN BE COMPLETELY BACKFILLED BY EN OF WORK SHIFT.
- WORK AT THE EMERGENCY RESPONSE SERVICES DRIVEWAYS MUST ONLY BLOCK ONE ENTRANCE AT A TIME.
- CONSTRUCTION SIGNING SHALL BE IN PLACE ONLY WHEN THE CONDITIONS EXIST FOR WHICH THE SIGNS ARE INTENDED.
- TRAFFIC CONTROL DEVICES, IF USED AT NIGHT, SHALL BE LIT IN ACCORDANCE WITH THE ALASKA TRAFFIC MANUAL.
- IT IS THE INTENT OF THIS TRAFFIC CONTROL PLAN (TCP) TO ILLUSTRATE SOME, NOT ALL, OF THE TRAFFIC CONTROL SETUPS WHICH WILL BE REQUIRED ON THIS PROJECT. ALL TCPs SHALL BE CREATED BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER FOR APPROVAL. WHERE APPROPRIATE, THEY SHALL INCORPORATE APPLICABLE DETAILS FROM THESE SHEETS.
- ALL TRAFFIC CONTROL PLANS SUBMITTED BY THE CONTRACTOR SHALL BE NUMBERED. ALL TRAFFIC CONTROL PLANS THAT USE A TYPICAL APPLICATION AS DESCRIBED IN THE MUTCD SHALL REFERENCE THE TYPICAL APPLICATION. EXAMPLE: TCP 3, MUTCD TA-10.

TRAFFIC CONTROL LEGEND

	ARROW BOARD
	BARRIER: CONCRETE
	BARRIER: PEDESTRIAN
	BARRIER: TYPE III BARRICADE
	CONE
	CONSTRUCTION SIGN
	DRUMS
	FLAGGER
	TEMPORARY SIGNAL
	TRAFFIC DIRECTION
	WARNING DEVICE: HIGH LEVEL
	WARNING LIGHT
	WORK SPACE



TWO LANE ROADWAY-SINGLE LANE CLOSURE

Record Drawings have been reviewed by the Project Engineer, and represent to the best of my knowledge, the Project as constructed.

PE

TCP