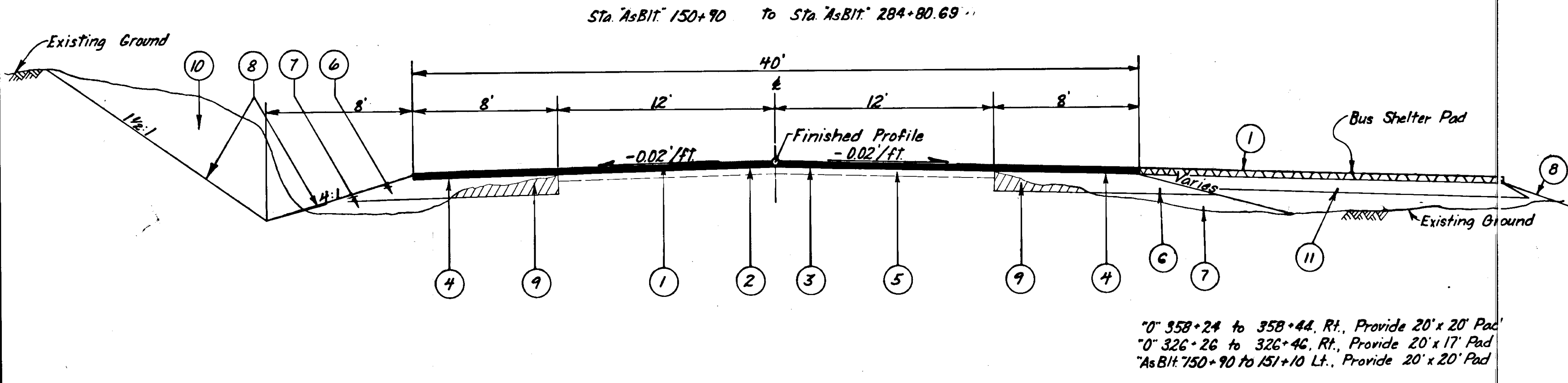


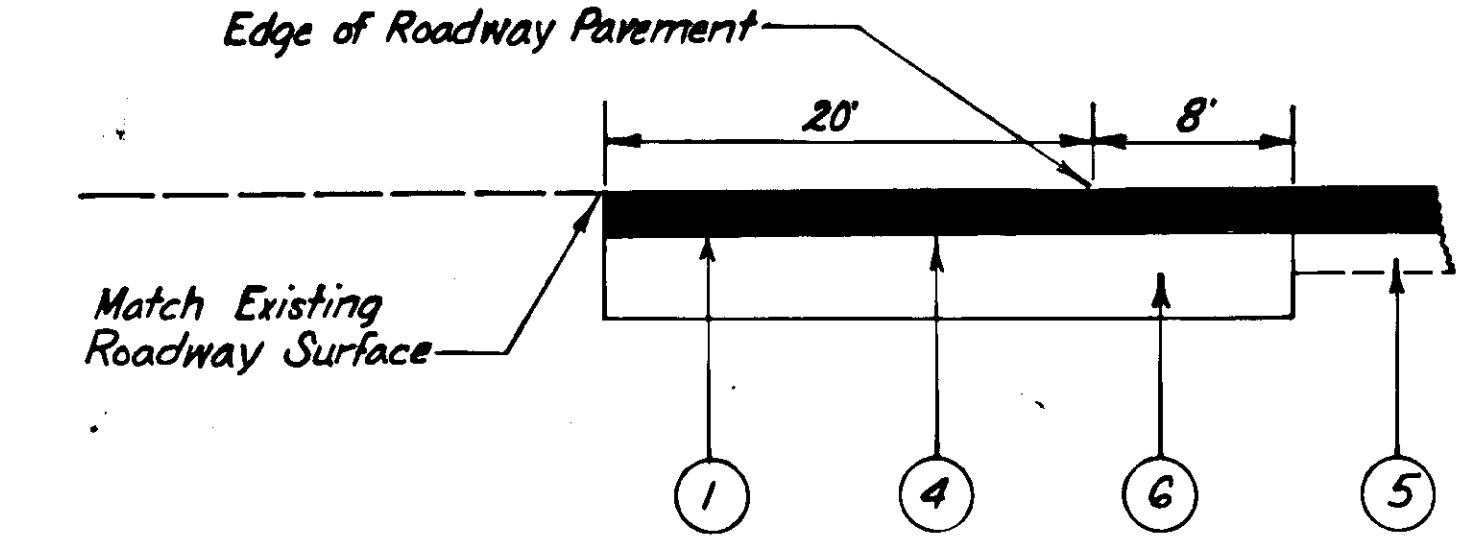
STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	RS-0966(12)	1983	2	9

PROPOSED TYPICAL SECTIONS

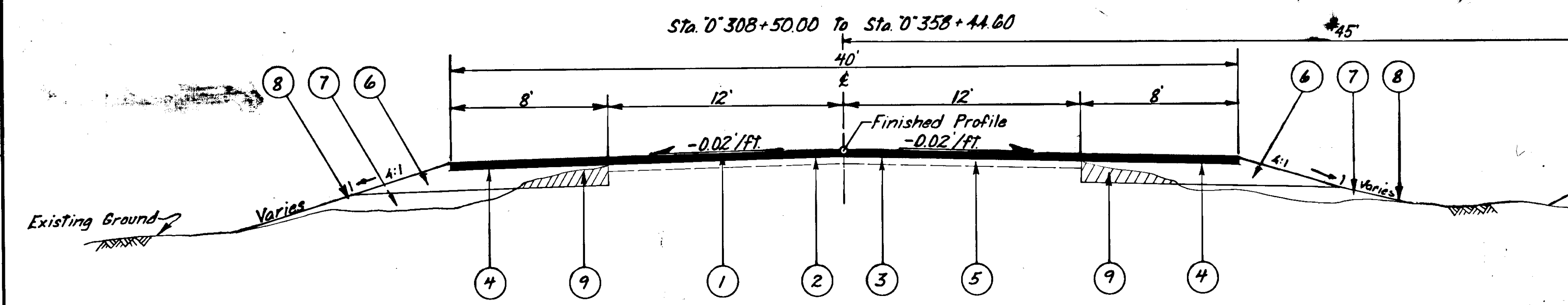


*0' 358+24 to 358+44, Rt., Provide 20' x 20' Pad
 *0' 326+26 to 326+46, Rt., Provide 20' x 17' Pad
 *AsBit 750+90 to 151+10 Lt., Provide 20' x 20' Pad

- GENERAL NOTES**
- Grades & alignment shown on the plans are subject to minor revisions.
 - Culvert lengths & locations are approximate only and are subject to minor revisions.
 - All waste material shall be placed outside the R/W limits as approved by the engineer.
 - Selective tree removal shall include all trees 6" diameter and larger. Removal of trees less than 6" diameter and all brush, clearing, and grubbing shall not be measured for pay but shall be considered incidental to other items of work.
 - One lane of traffic shall be maintained at all times. Traffic control, signing and delineation shall be in accordance with Standard Dwg. C-10.01.
 - All in-water construction activity in Little Auke Creek and the two unnamed streams between stations 170 and 180 shall be limited to the month of June.
 - To replace lost spawning area in Little Auke Creek, a minimum of 10 inches of clean gravel (1 to 2 inches in diameter and not crushed) shall be placed in the streambed for a distance of 10-15 feet both upstream and downstream of the extended culvert. This work shall be considered incidental to other items and no separate payment will be made therefore.
 - Power poles at the following Stations will be replaced by others: Station 160+70, 32' Lt., Station 169+31, 35' Lt., Station 221+24, 33' Lt., Station 223+75, 29' Lt., Station 233+28, 35' Lt., Station 292+98, 33' Lt., Station 206+53, 22' Rt., Station 238+98, 24' Rt.

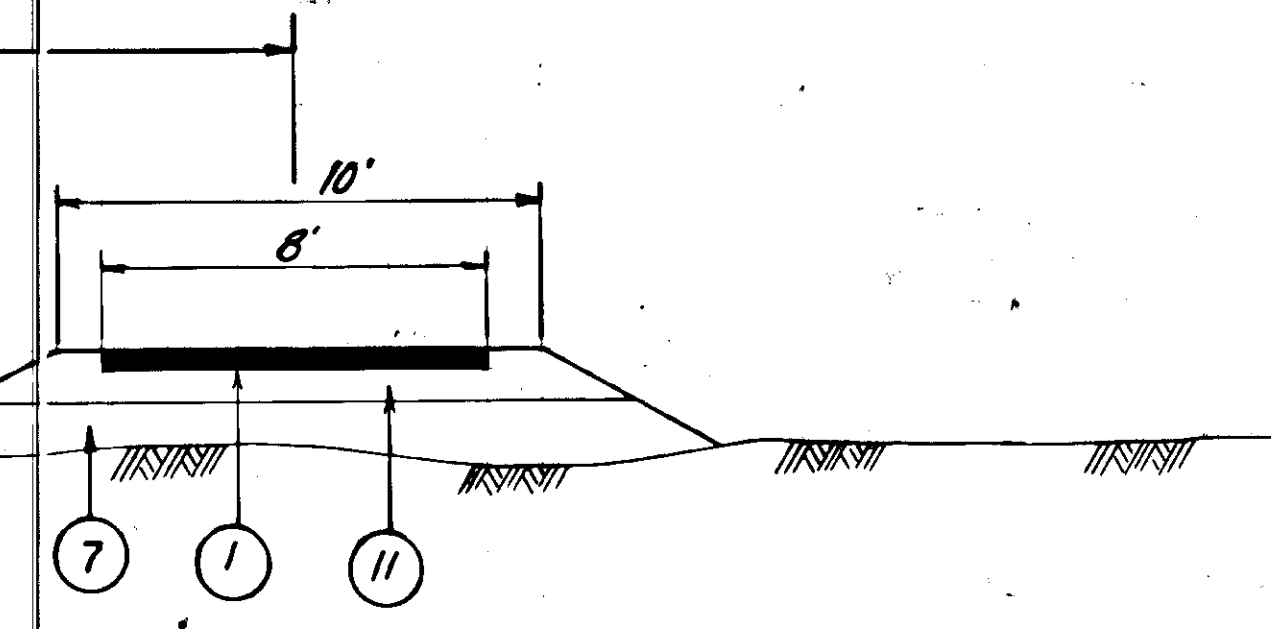


UNPAVED INTERSECTION & APPROACH DETAIL

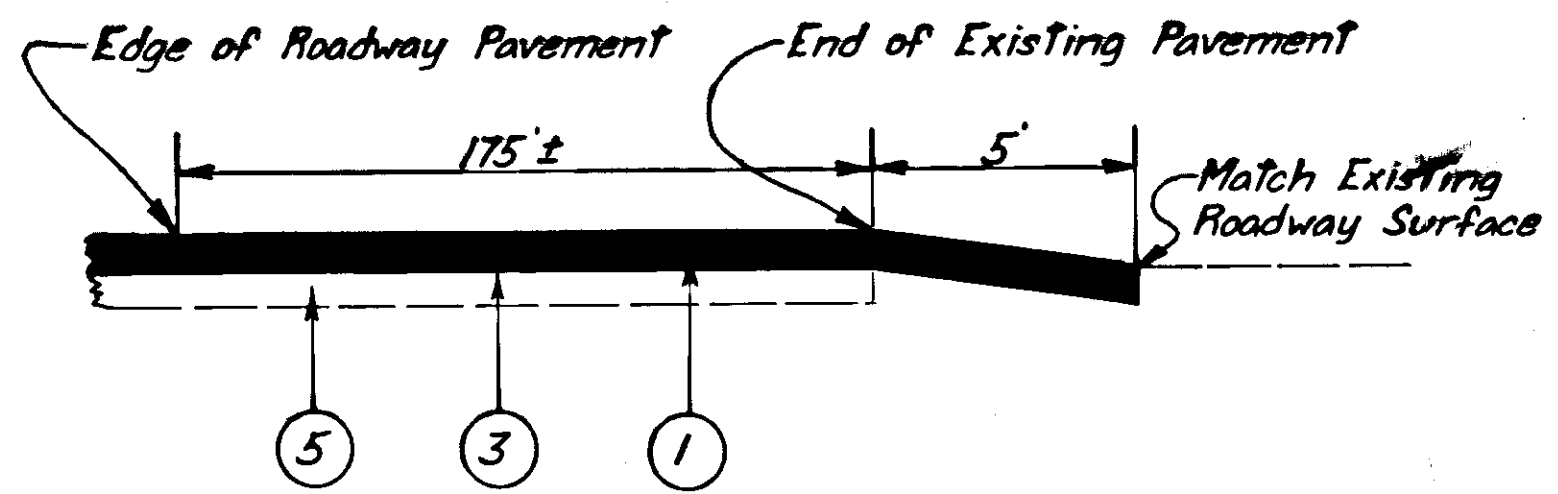


* Note: Remove the Existing Pavement from Sta. 0' 327+90 to 0' 358+44.60. Replace with 2" x 24' wide Hot Asphalt Pavement. Shape, compact and prime existing base course prior to placing new asphalt.

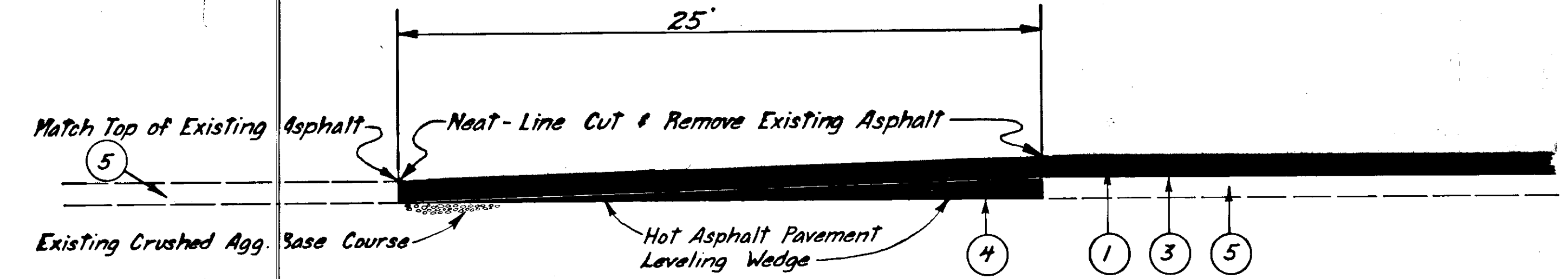
PEDESTRIAN ACCESS
 Asbit 313+33 to Match Existing Loop Road Pedestrian Access
 * Offset is 33' from 313+33 to 320+43 (See Detail on sheet 6)



LABELING INDEX	
1	1 1/2" Hot Asphalt Pavement
2	1" Leveling Course
3	CSS-1 Emulsified Asphalt for Tack Coat
4	MC-30 Liquid Asphalt for Prime Coat
5	Existing Asphalt or new 2" Asphalt
6	6" Crushed Aggregate Base Course
7	Subbase Grading E
8	Seeding, Class I
9	Linear Grading
10	Unclassified Excavation
11	4" Crushed Aggregate Base Course



PAVEMENT OVERLAY & TRANSITION DETAIL AT MONTANA CREEK ROAD INTERSECTION



PAVEMENT TRANSITION DETAIL



ESTIMATE OF QUANTITIES

ITEM NO.	ITEM	UNIT	QUANTITY
110(1)	Mobilization	L.S.	All Required
111(1)	Temporary Erosion & Pollution Control	C.S.	"
114(1)	Construction Surveying by the Contractor	L.S.	"
115(1)	Traffic Maintenance	L.S.	"
116(1)	Furnishing & Maintaining Field Office	L.S.	"
116(2)	Furnishing & Maintaining Field Laboratory	L.S.	"
201(5)	Selective Tree Removal	EA	250
202(1)	Removal of Structures & Obstructions	L.S.	All Required
202(4)	Removal of Culvert Pipe	L.F.	1486
202(9)	Single Mailbox Installation	EA	37
202(10)	Multiple Mailbox Installation	L.F.	12
202(2)	Removal of Pavement	S.Y.	8,613
203(3)	Unclassified Excavation	C.Y.	7,747
304(1)	Subbase Grading E	Ton	47,450
203(8)	Linear Grading	Sta.	176.1
301(1)	Crushed Aggregate Base Course	Ton	45,546
401(1)	Asphalt Concrete, (Type II)	Ton	40,403
401(2)	AC-5 Asphalt Cement	Ton	624
402(1)	CSS-1 Asphalt for Tack Coat	Ton	12.2
403(1)	MC-30 Liquid Asphalt for Prime Coat	Ton	50.3
501(1)	Class "A" Concrete	L.S.	All Required
503(1)	Reinforcing Steel	L.S.	All Required
509(1)	Deck Protection	L.S.	All Required
603(2)	6'-4" x 4'-9" Corrugated Steel Pipe Arch	L.F.	16
603(22-18)	18" Pipe	L.F.	2272
603(22-24)	24" Pipe	L.F.	346
603(22-30)	30" Pipe	L.F.	40
603(22-36)	36" Pipe	L.F.	76
603(27)	58" x 36" Pipe Arch	L.F.	12
604(3)	Reconstruct Existing Manholes	EA	#
604(4)	Adjust Existing Manholes	EA	#
605(6)	Modified Underdrain	L.F.	550
606(4)	Removal & Reconstruction of Guard Rail	L.F.	650
606(6)	End Anchorages	EA	12
614(1)	Survey Monuments	EA	5
614(2)	Monument Cases	EA	5
614(4)	Adjust Existing Monument Cases	EA	24
615(1)	Standard Signs	S.F.	262
615(5)	Guide Markers	EA	124
618(1)	Seeding	M.S.F.	475
618(4)	Water for Maintenance	M.Gal.	25
634(1)	Reinforcing Fabric	S.Y.	4554
639(1)	Approaches	EA	105
670(1)	Painted Traffic Markings	L.S.	All Required

CULVERT EXTENSION SUMMARY

STATION	DIAMETER	LEFT EXTENSION	RIGHT EXTENSION
AsBIT 162+48	24" R	6' 8"	8' 12"
168+68	18"	8' 16"	12' 10"
172+02	18"	6' 5"	X
172+18	18"	X	6'
175+84	36" H	8' X	8'
177+62	24"	8'	8'
180+85	18"	10'	12'
179+05	18"	12' 8"	6' 18"
189+01	18"	11'	6' 13"
192+11	18"	2' 21"	2' 18"
197+42	6'-4" x 4'-9"	2'-4"	2'-4"
200+80	18"	10' 8"	6' 10"
217+98	18"	8' 14"	6' 10"
221+31	18"	6'	4'
221+48	18"	8'	X
224+41	18"	8' 10"	2'
227+41	18"	8'	4'
231+00	24"	10' X	10'
231+16	24"	X 10'	12' X
243+42	24"	12'	8' 6"
261+51	18"	3'	6'
284+37	58" x 36" arch	4'	8'
AsBIT 287+02	18"	10'	8'
310+03	24"	6' X	20'
310+16	24"	X	2'
314+75	24"	6'	18'
315+13	24"	X	30' 25'
325+10	24"	12'	6'
318+82	24"	11' X	28' 40'
325+08	30"	16' X	X 10'
325+45	30"	X	24'

APPROACH CULVERT SUMMARY

STATION	LEFT	RIGHT	REMOVE & DISPOSE	INSTALL		REMARKS	APPROACH CULVERT SUMMARY ADDITIONS				
				18"	24"		STATION	LT.	RT.	12"	18"
AsBIT 150+98		X	34'	40'	51'		150+94		X		30'
151+43	X		40'	50'	40'	G.H.E.A.	152+25	X			50'
152+42		X	30'	38'	40'		166+11	X			40'
154+01		X	35'	44'	35'		322+09		X	18'	
151+00		X		30'		Bus Pad	358+24		X	22'	
154+49	X		43'	48'	106'		220+61	X		26'	
156+21	X		20'	38'	36'		230+44	X		40'	
157+00	X		28'	38'			236+32	X		40'	
158+11	X		118'	128'	126'	Bus Barn	336+40		X	50'	
150+90	X			30'			332+50		X	60'	
159+17	X		29'	46'			333+50		X	50'	
161+19	X		20'	36'			340+00		X	30'	
163+01	X		20'	38'	40'		336+40		X	50'	
163+57	X		5'	6'	16'	Pedestrian Access	338+48		X	30'	
155+62	X			40'			341+55		X	30'	
164+18	X		20'	40'	30'		343+35		X	30'	
165+44	X		18'	38'	28'		338+00		X	40'	
167+69		X	28'	36'							
169+62	X		22'	42'	36'						
172+77	X		44'	50'	46'						
176+39	X		60'	64'		New Road					
180+50	X		26'	32'							
185+56	X		20'	36'							
192+97	X		24'	38'							
193+77	X		30'	40'							
196+61	X		20'	38'	28'						
198+45	X		17'	34'							
199+60				38'							
200+38	X		30'	34'	52'	WILSON MURPHY EXIST. CONCRETE HEADWALLS GOAT HILL RD.					
210+50		X	46'	56'							
215+36	X		20'	38'	30'						
216+95	X		29' Wood	40'	30'	Exist. Wood Culvert					
204+50					82'	Cross Culvert					
220+12	X		20'	40'	28'						
221+73	X		20'	38'	28'						
223+17	X		28'	42'	28'						
225+19	X		20'	38'	28'						
223+75	X			20'							
229+01	X		20'	40'	28'						
233+25	X										
264+25	X		24'	38'							
266+83	X		20'	38'	28'						
267+56	X		20'	38'	28'						
268+95	X	X	22'	40'	28'						
269+44	X		36'	44'	28'						
270+11	X		34'	48'		Powers St.					
272+41	X		24'	38'	28'						
272+54	X	X	36'	48'		Steelhead St.					
288+09	X		28'	40'							
AsBIT 289+69	X		28'	40'							
321+66	X		20'	38'	30'						
323+99	X		20'	38'	50'						
331+63	X		20'	38'	30'						
340+80	X		30'	42'	40'	F.A.A. Rd.					
345+33		X	30'	48'	30'						
350+35		X	44'	54'	50'	Conifer Lane					
352+93	X		20'	38'	30'						
353+64	X		20'	38'	25'						
355+28	X		30'	42'	36'						
356+18	X		22'	40'	30'						
357+20	X		30'	40'	46'	Drudge Lake Ave.					

BASIS OF ESTIMATE

ITEM NO.	ITEM	FACTOR
203(58)	Borrow, (Type A)	1.98 Tons/Cubic Yard
301(1)	Crushed Agg. Base Course	1.96 Tons/Cubic Yard
401(1)	Asphalt Concrete (Type I)	114.4 lbs./Sq. Yd./Inch Depth
401(2)	AC-5 Asphalt Cement	6% of Item 401(1)
402(1)	CSS-1 Asphalt for Tack	0.1 Gal./Sq. Yd., 25.3 Gal./Ton *
403(1)	MC-30 Liquid Asph. for Prime	0.25 Gal./Sq. Yd., 25.6 Gal./Ton

* Diluted

REINFORCING FABRIC SUMMARY

EAST BOUND LANE		WEST BOUND LANE	
STATION	WIDTH	STATION	WIDTH
157+94	50'	193+81	160'
159+28	25'	194+50	161'
161+52	80'	195+60	163'
161+74	69'	194+50	164'
162+80	80'	197+52	165'
164+85	80'	197+52	168'
165+10	80'	200+30	168'
165+65	80'	200+24	169'
168+07	80'	201+29	171'
170+36	80'	201+29	171'
172+10	90'	201+46	171'
172+48	80'	210+91	180'
176+68	80'	214+28	182'
179+88	100'	220+88	185'
182+98	107'	221+13	186'
185+08	104'	223+28	189'
186+28	105'	223+24	189'
188+40	107'	227+47	190'
190+98	106'	227+24	194'
192+08	109'	242+70	194'
196+60		194+95	
197+85		197+85	
198+20		199+40	
199+90		200+00	

ESTIMATE OF ROADWAY QUANTITIES

ITEM NO.	ITEM	QUANTITIES
202(1)	Removal of Structures and Obstructions	Existing headwalls at 197+42
509(1)	Deck Protection	159 Sq. Yd. Lake Cr. Bridge



APPROACH SUMMARY

STATION	WIDTH	RAD.	REMARKS	STATION	WIDTH	RAD.	REMARKS	STATION	WIDTH	RAD.	REMARKS
AsBlt 151+75	14'		Lt.	AsBlt 196+61	14'		Rt.	AsBlt 275+95	14'		Rt.
" 152+67	24'		Lt.	" 198+45	14'		Lt.	" 280+28	24'	25'	Lt. Montana Ck. Rd.
" 153+90	20'		Rt.	" 199+57	24'		Rt.				
" 154+00	24'		Lt.	" 200+38	24'		Lt.				
" 154+00	14'		Rt.	200+75	201+90	24'	Rt.				
" 154+80	14'		Lt.	" 202+17	24'	20'	Lt.	267+70	14'		Rt.
" 156+05	14'		Lt.	" 210+50	24'	25'	Rt. Goat Hill Rd.	269+00	14'		Rt.
" 158+11	3-24'		Lt.	" 215+36	14'		Lt.	269+25	14'		Lt.
" 159+17	14'		Lt.	" 217+30	24'	25'	Rt. Goat Hill Rd.				
" 161+19	14'		Lt.	" 217+30	14'		Lt. & Rt.				
" 163+01	24'		Lt.	" 218+98	14'		Lt. & Rt.	" 0 316+07	14'		Rt.
" 157+00	14'		Rt.	220+10	221+30	14'	Rt.	" 0 321+66	14'		Lt.
" 163+57	14'		Rt.	220+80	220+20	14'	Lt.	" 0 323+99	24'	20'	Lt.
" 164+18	14'		Lt.	221+80	222+25	14'	Lt.	" 0 326+06	24'	25'	Rt. Mint Way
" 165+05	20'	14'	Rt.	223+08	223+08	14'	Rt.	" 0 327+90	24'	25'	Rt. Glendale St.
" 165+44	14'		Lt.	" 223+55	14'		Lt.	" 0 330+87	24'	25'	Rt. Glendale St.
" 167+69	14'		Rt.	" 225+40	16'		Lt.	" 0 331+63	24'	25'	Rt. New St.
" 169+62	14'		Lt.	" 226+20	16'		Rt.	" 0 336+63	24'	25'	Rt. New St.
" 172+77	14'		Lt.	230+80	228+80	20'	Lt.	" 0 337+20	16'		Rt.
" 176+39	24'	25'	Lt. New Road	" 230+10	14'		Rt.	" 0 339+00	16'		Rt.
" 177+14	16'		Lt.	" 230+30	16'		Rt.	" 0 340+29	14'		Rt.
" 179+46	14'		Rt.	" 231+00	14'		Rt.	" 0 340+80	14'	25'	Lt. F.A.A. Rd.
" 180+50	14'		Lt.	" 231+50	14'		Rt.	" 0 341+69	16'		Rt.
" 181+68	14'		Rt.	245+50	244+20	14'	Rt.	" 0 343+27	16'		Rt.
" 182+16	14'		Lt.	253+75	248+80	24'	Lt.	331+60	14'		Lt.
" 184+11	24'		Rt. Lakashore Ct.	" 250+45	16'		Lt.	" 0 345+33	16'		Rt.
" 185+06	20'		Rt.	" 252+00	16'		Lt.	" 0 348+68	20'		Lt. Paved
" 185+56	14'		Lt. Paved	" 255+70	24'	26'	Rt. Wren Dr.	" 0 349+52	20'		Lt.
" 186+75	14'		Rt.	" 261+80	14'		Rt.	" 0 350+35	24'	25'	Rt. Connifer Lane
" 189+55	14'		Rt.	" 263+50	14'		Rt.	" 0 351+94	24'	25'	Lt. Lake Ave.
" 190+13	24'	25'	Lt. Windfall Ave.	" 264+65	14'		Rt.	" 0 352+93	14'		Lt.
" 192+30	14'		Rt.	266+85	266+65	14'	Rt.	" 0 353+64	14'		Lt. Paved
" 192+97	14'		Lt.	" 270+11	24'	25'	Rt. Powers St.	" 0 355+28	20'		Lt.
" 193+77	14'		Lt. Paved	" 272+01	14'		Rt.	" 0 356+18	20'		Lt.
" 193+93	14'		Rt.	" 272+41	14'		Lt.	" 0 357+20	24'	25'	Lt. Dredge Lake Rd.
" 195+67	14'		Rt.	" 272+54	24'	25'	Rt. Steelhead St.				
" 196+61	14'		Lt.	" 273+13	14'		Lt.				
				" 275+77	24'		Lt.				

MONUMENT SUMMARY

STATION	MON.	CASE	POINT	REMARKS	STATION	MON.	CASE	POINT	REMARKS
AsBlt 150+90.00		X		Adjust	AsBlt 253+95.20		X	PT.S	Adjust
" 151+83.30		X	P.C.		" 263+01.50		X	P.S.T.	
" 160+09.60		X	P.T.		" 270+18.70		X		
" 169+06.50		X	P.C.		" 277+35.90		X	P.C.	
" 171+13.40		X	P.T.		" 282+27.20		X	P.T.	Adjust
" 172+94.10		X	P.C.		AsBlt 284+80.69 Bk	X	X	P.T. - Bk	Install
" 178+23.70		X	P.T.		" 0 284+80.69 Ahd.			P.C. Ahd.	
" 190+00.00		X	P.O.T.						
" 201+74.80		X	P.C.						
" 206+64.80		X	P.T.						
" 209+14.20		X	P.C.						
" 217+49.90		X	P.T.						
" 222+54.10		X	P.C.						
" 225+44.00		X	P.T.		" 0 320+49.44	X	X	P.T.	Install
" 228+89.70		X	P.C.		" 0 332+39.53	X	X	P.C.	
" 233+36.20		X	P.T.		" 0 352+69.85	X	X	P.T.	
" 235+26.60		X	P.C.		" 0 358+71.91	X	X	P.C.	
" 243+98.30		X	P.S.T.						

MISCELLANEOUS SUMMARY TABLES

GUIDE MARKER SUMMARY

STATION TO STATION	OFFSET	TYPE	QUANTITY	SPACING
AsBlt 286+33 - AsBlt 300+58	L	B	28	25'
" 286+33 - " 300+58	R	B	28	26'
" 300+58 - " 301+33	L	C	3	25'
" 300+58 - " 301+33	R	C	3	25'
" 303+98 - " 0 308+73	R	B	28	10'
" 303+98 - " 0 308+73	L	B	28	10'
" 303+73 - " 0 304+48	R	C	3	25'
" 303+73 - " 0 304+48	L	C	3	25'

GUIDE MARKER SUMMARY NOTES

- All type C markers shall be triple disc.
- All type C markers shall be mounted directly behind the guardrail.
- All type B markers shall be mounted 2' outside the edge of the shoulder but no closer than 6' to the edge of traveled way.
- Use existing object markers located at both ends of Mendenhall River Bridge.

FILL SLOPE SUMMARY

LEFT			RIGHT		
STATION	TO STATION	SLOPE	STATION	TO STATION	SLOPE
AsBlt 150+90	AsBlt 171+00	4:1	AsBlt 150+90	AsBlt 161+00	4:1
" 172+00	" 175+00	2:1	" 172+00	" 172+00	3:1
" 175+50	" 178+00	3:1	" 173+50	" 173+50	1 1/2:1
" 180+00	" 203+00	4:1	" 174+00	" 175+50	2:1
" 203+50	" 207+00	1 1/2:1	" 176+00	" 204+00	4:1
" 208+00	" 236+50	4:1	" 204+50	" 207+00	1 1/2:1
" 237+00	" 262+00	3:1	" 208+00	" 216+00	4:1
" 263+00	" 278+00	4:1	" 216+50	" 233+00	3:1
" 279+00	" 284+80	3:1	" 234+00	" 236+50	4:1
" 0 308+50	" 0 358+44.6	4:1	" 237+00	" 239+00	3:1
			" 240+00	" 243+93.3	2:1
			" 246+00	" 253+95.2	3:1
			" 255+00	" 280+00	4:1
			" 281+00	" 284+80	3:1
			" 0 308+50	" 0 332+16.5	3:1
			" 0 333+16.5	" 0 358+44.6	4:1

GUARDRAIL SUMMARY NOTES

- Stations shown include terminal end sections.
- At Little Auke Creek remove the last two posts & install a breakaway terminal end section as per Standard Drawing G-14.01.
- At Lake Cr. remove the last two posts & install a breakaway terminal end section as per Std Dwg G14.01.

GUARDRAIL SUMMARY

FROM STATION	OFFSET	TO STATION	OFFSET	REMARKS
AsBlt 196+83.43	24' Rt.	AsBlt 197+83.43	22' Rt.	Remove & Relocate
AsBlt 197+00.57	22' Lt.	AsBlt 198+00.57	24' Lt.	
AsBlt 204+00.00	20' Lt.	AsBlt 207+37.50	20' Lt.	
AsBlt 204+62.50	20' Rt.	AsBlt 207+75.00	20' Rt.	

MANHOLE SUMMARY

STATION	OFFSET	REMARKS
AsBlt 149+31	15.5' Rt.	Adjust
151+78	20' Rt.	"
152+79	20' Rt.	"
153+99	20' Rt.	Reconstruct
155+77	20' Rt.	"
157+72	18.5' Rt.	"
160+00	21.2' Rt.	Adjust
162+35	26' Rt.	Reconstruct
166+20	20' Rt.	"
328+05	42' Rt.	Adjust
331+77	48' Rt.	"
333+20	48' Rt.	"
335+80	45' Rt.	"
339+79	45' Rt.	"
343+82	46' Rt.	"
325+70	Lt.	Adjust



SIGN SCHEDULE

STATION	OFFSET		CODE NO.	LEGEND	SIGN PANEL		POST			FACING TRAFFIC	REMARKS
	LT.	RT.			SIZE	AREA	NO. of POSTS	LENGTH	EMBED.		
"Y" 0+90		20	RI-2	YIELD			1	13'	3'	E.B.	U.E.S.
AsBit. 152+75	33'		DI-3	Ferry Terminal Tea Harbor ← Juneau			2	16'	3'	W.B.	U.E.S.
AsBit. 153+50	33'		MIO-2	7	6" x 12 3/4"	.53	1	14'	3'	E.B. & W.B.	
AsBit. 155+80	33'		W3-1	Symbol	36" x 36"	9.0	1	16'	3'	W.B.	
AsBit. 157+80	33'		RI-1	STOP AHEAD	18" x 24"	3.0				W.B.	MOUNT BELOW SYMBOL
AsBit. 160+92		33'	RI-1	STOP			1	13'	3'	S.B.	U.E.S.
		33'	W7-1	Symbol			1	17.5'	3'	E.B.	U.E.S.
		33'		HILL	24" x 24"	4.0				E.B.	MOUNT BELOW SYMBOL
AsBit. 161+50	33'		W7-1	Symbol			1	16'	3'	W.B.	U.E.S.
		33'		HILL						W.B.	U.E.S.
AsBit. 166+06		33'	W1-4L		36" x 36"	9.0	1	19'	3'	E.B.	
		33'	W1-13	45 M.P.H.	24" x 24"	4.0				E.B.	M.B.C.S.
AsBit. 176+17	33'		RI-1	STOP			1	15.5'	3'	S.B.	U.E.S.
AsBit. 181+23	33'		W1-4L		36" x 36"	9.0	1	18.5'	3'	W.B.	
		33'	W1-13	45 M.P.H.	24" x 24"	4.0				W.B.	M.B.C.S.
AsBit. 183+01		18'	TI-1	Lake Creek	42" x 24"	7.0	2	15'	3'	E.B.	
AsBit. 183+49		18'	TI-1	Lake Creek	42" x 24"	7.0	2	15'	3'	W.B.	
AsBit. 184+11		33'	D3-1	LAKESHORE CT.			1	15'	3'	E.B. & W.B.	U.E.S.
		33'	RI-1	STOP						N.B.	U.E.S., M.B.S.S.
AsBit. 189+10	33'		MIO-2	6			1	13'	3'	E.B. & W.B.	U.E.S.
AsBit. 190+13	33'		D3-1	WINDFALL AVE.			1	15'	3'	E.B. & W.B.	U.E.S.
		33'	RI-1	STOP	30" x 30"	6.25				S.B.	M.B.S.S.
AsBit. 198+74		33'	W1-4R				1	19'	3'	E.B.	U.E.S.
		33'	W1-13	35 M.P.H.						E.B.	U.E.S., M.B.C.S.
AsBit. 210+50		33'	D3-1	GOAT HILL RD.			1	13.5'	3'	E.B. & W.B.	U.E.S.
		33'	RI-1	STOP	30" x 30"	6.25				N.B.	M.B.S.S.
AsBit. 215+83		33'	D3-1	GOAT HILL RD.			1	15'	3'	E.B. & W.B.	U.E.S.
		33'	RI-1	STOP						N.B.	U.E.S., M.B.S.S.
AsBit. 219+54		33'	W1-2R		36" x 36"	9.0	1	19'	3'	E.B.	
		33'	W1-13	45 M.P.H.	24" x 24"	4.0				E.B.	M.B.C.S.
AsBit. 220+49	33'		W1-4R		36" x 36"	9.0	1	15.5'	3'	W.B.	
		33'	W1-13	45 M.P.H.	24" x 24"	4.0				W.B.	M.B.C.S.
AsBit. 225+89		33'	W1-4L		36" x 36"	9.0	1	18.5'	3'	E.B.	
		33'	W1-13	35 M.P.H.	24" x 24"	4.0				E.B.	M.B.C.S.
AsBit. 228+44		33'	W1-2L		36" x 36"	9.0	1	15'	3'	W.B.	
		33'	W1-13	45 M.P.H.	24" x 24"	4.0				W.B.	M.B.C.S.
AsBit. 244+20	33'		MIO-2	5	6" x 12 3/4"	.53	1	15'	3'	E.B. & W.B.	
AsBit. 270+04		33'	D7-1L	Symbol			1	14'	3'	E.B.	U.E.S.
		33'		CAMPING						E.B.	MOUNT BELOW SYMBOL
		33'								E.B.	MOUNT BELOW CAMPING
AsBit. 270+11		33'	D3-1	POWERS ST.			1	14'	3'	E.B. & W.B.	U.E.S.
		33'	RI-1	STOP						N.B.	U.E.S., M.B.S.S.
AsBit. 272+54		33'	D3-1	STEELHEAD ST.			1	14'	3'	E.B. & W.B.	U.E.S.
		33'	RI-1	STOP	30" x 30"	6.25				N.B.	M.B.S.S.
AsBit. 274+35		33'	W1-2R		36" x 36"	9.0	1	15'	3'	E.B.	
		33'	W1-13	45 M.P.H.	24" x 24"	4.0				E.B.	M.B.C.S.
AsBit. 275+04		33'	DI-2	Montana Creek Rd ← Mendenhall Lake	120" x 30"	25.0	2	12'	3'	E.B.	Framed
AsBit. 280+26		33'	D3-1	MONTANA CREEK RD.	42" x 9"	2.63	1	17'	3'	E.B. & W.B.	
		33'	RI-1	STOP	30" x 30"	6.25				S.B.	M.B.S.S.

SIGN SCHEDULE (Cont.)

STATION	OFFSET		CODE NO.	LEGEND	SIGN PANEL		POST			FACING TRAFFIC	REMARKS
	LT.	RT.			SIZE	AREA	NO. of POSTS	LENGTH	EMBED.		
"0" 313+45		39	R5-3	NO MOTOR VEHICLES	12" x 18"	1.5'	1	9'	3'	W.B.	
"0" 325+94		51	RI-1	STOP	18" x 18"	3'	1	9'	3'	E.B.	
"0" 326+18		39	RI-1	STOP	18" x 18"	3'	1	9'	3'	W.B.	
"0" 326+28		33	D3-1	MINT WAY	24" x 9"	1.5'	1	16'	3'	E.B. & W.B.	
		51	RI-1	STOP						N.B.	U.E.S., M.B.S.S.
"0" 327+78		39	RI-1	STOP	18" x 18"	3'	1	9'	3'	E.B.	
"0" 328+02		33	RI-1	STOP	18" x 18"	3'	1	9'	3'	E.B.	
"0" 327+90		33	D3-1	GLENDALE ST.			1	15'	3'	E.B. & W.B.	U.E.S.
		51	RI-1	STOP	30" x 30"	6.25				N.B.	M.B.S.S.
"0" 330+75		39	RI-1	STOP	18" x 18"	3'	1	9'	3'	E.B.	
"0" 330+99		33	RI-1	STOP	18" x 18"	3'	1	9'	3'	E.B.	
"0" 330+87		33	D3-1	GLENDALE ST.			1	16'	3'	E.B. & W.B.	U.E.S.
		51	RI-1	STOP						N.B.	U.E.S., M.B.S.S.
"0" 350+23		51	RI-1	STOP	18" x 18"	3'	1	9'	3'	E.B.	
"0" 350+47		39	R5-3	NO MOTOR VEHICLES	12" x 18"	1.5'	1	9'	3'	N.W.	
		33	RI-1	STOP	18" x 18"	3'	1	9'	3'	W.B.	
"0" 350+35		33	D3-1	CONNIFER LANE			1	15'	3'	E.B. & W.B.	U.E.S.
		33	RI-1	STOP						N.B.	U.E.S., M.B.S.S.
"0" 351+94		33	D3-1	LAKE AVE.			1	15.5'	3'	E.B. & W.B.	U.E.S.
		33	RI-1	STOP						S.B.	U.E.S., M.B.S.S.
"0" 352+33		33	W3-1	Symbol	36" x 36"	9.0	1	17.5'	3'	E.B.	
		33		STOP AHEAD	18" x 24"	3.0				E.B.	MOUNT BELOW SYMBOL
"0" 353+44		33	DI-2	← Mendenhall Glacier Airport →	120" x 30"	25.0	2	16'	3'	E.B.	Framed
"0" 357+20		33	D3-1	DREDGE LAKE AVE.	36" x 9"	2.25	1	14'	3'	E.B. & W.B.	
		33	RI-1	STOP	30" x 30"	6.25				S.B.	M.B.S.S.
"0" 358+69		33	RI-1	STOP	30" x 30"	6.25	1	14'	3'	E.B.	

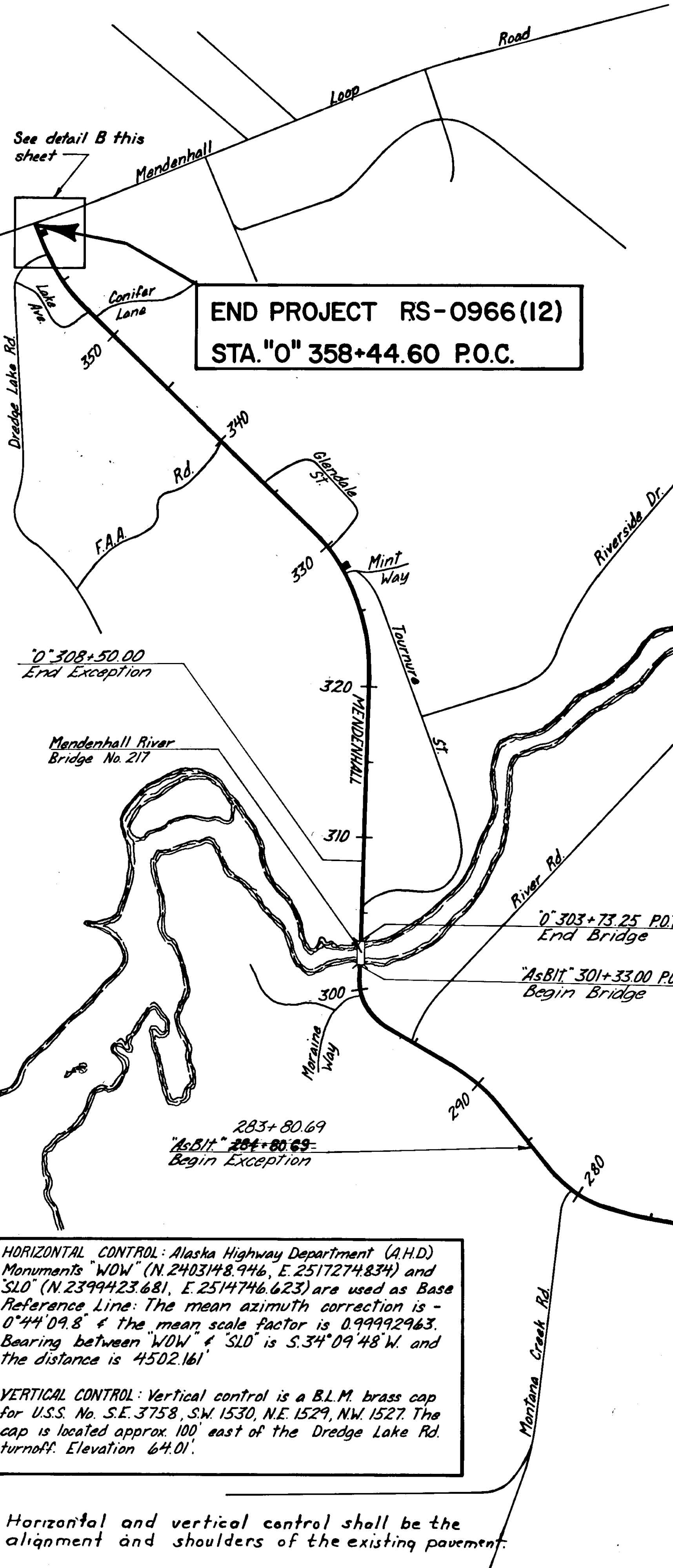
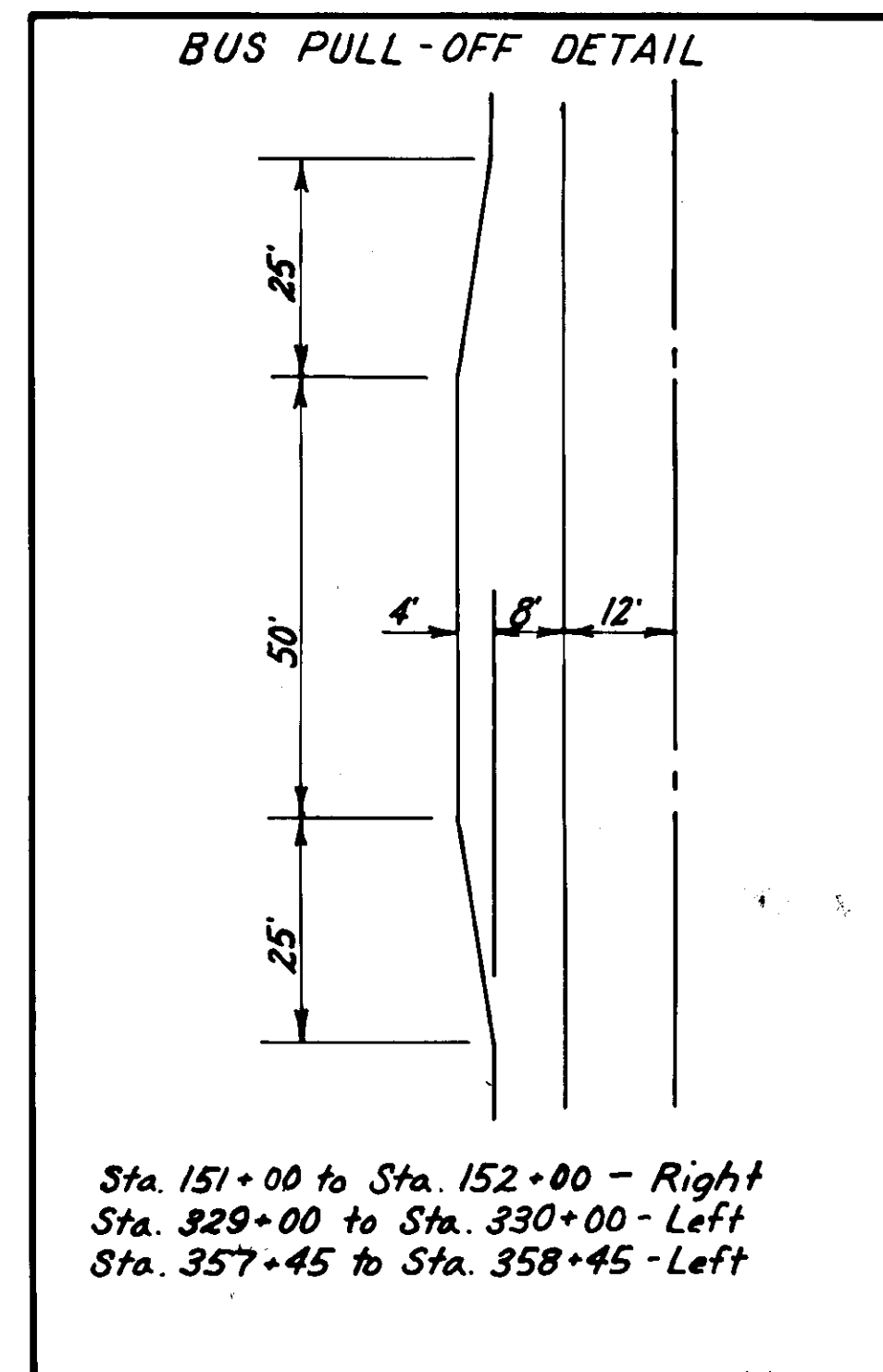
SIGNING SCHEDULE NOTES

1. U.E.S. = Use Existing Sign
2. M.B.C.S. = Mount Below Curve Sign
3. M.B.S.S. = Mount Below Street Sign
4. Sign locations & post lengths are approximate only & are subject to minor revisions.
5. All sign posts shall be telescoping perforated galvanized square steel posts; the 2" size shall be used above ground & the 2 1/4" size shall be used below ground for the sleeve.
6. All posts shall be installed with sleeve type embedment in accordance with Standard Drawing S-30.70, except that the 2 1/4" size shall be used for the entire embedment depth.
7. Post lengths are from the cut-off in the sleeve to the top of the post. See Standard Drawings S-05.00 & S-30.00.
8. All signs shall be .063" thick.
9. All signs to be replaced shall be dismantled by the contractor and stockpiled at the State of Alaska, D.O.T. & P.F. Maintenance Station as directed by the engineer. This work shall be considered incidental to Pay Item 615(1) Standard Signs, no separate payment shall be made therefore.
10. All new signs shall be unframed unless otherwise noted.
11. Relocation of existing signs shall not be measured for payment but shall be considered incidental to item 615(1) Standard Signs.

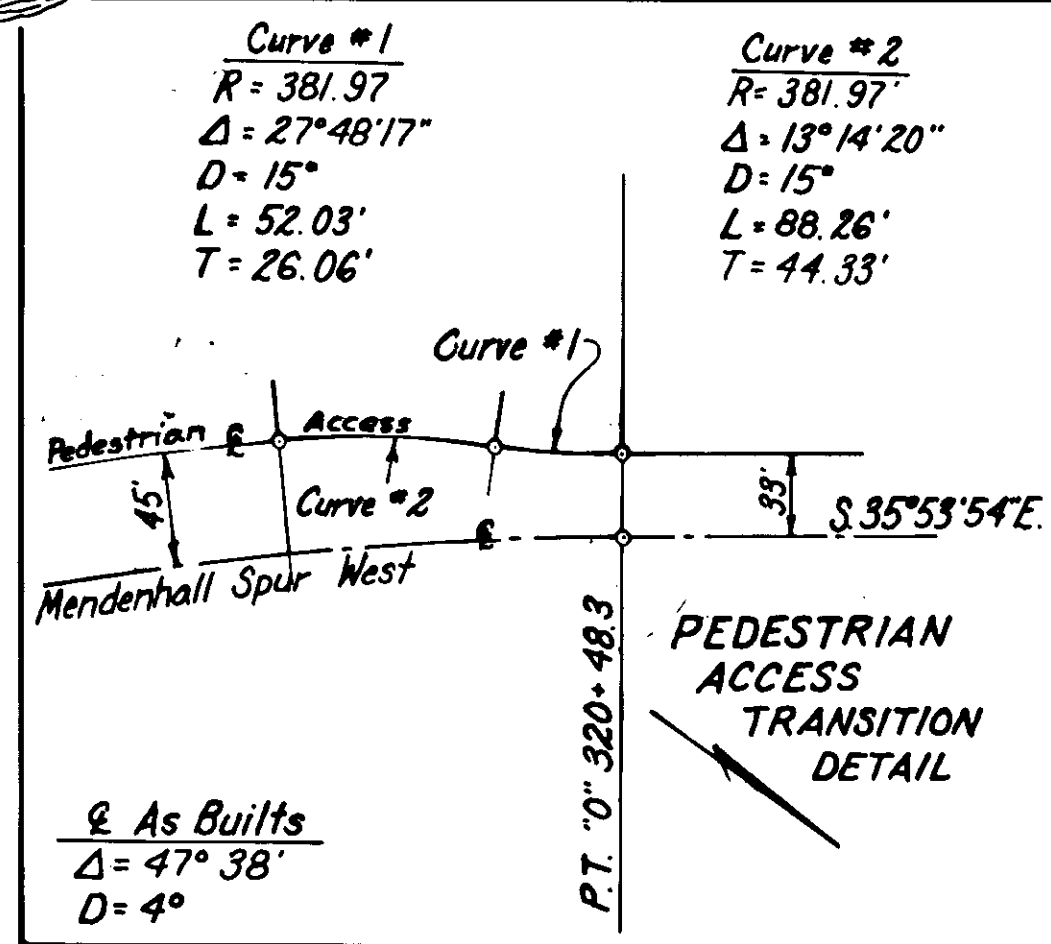
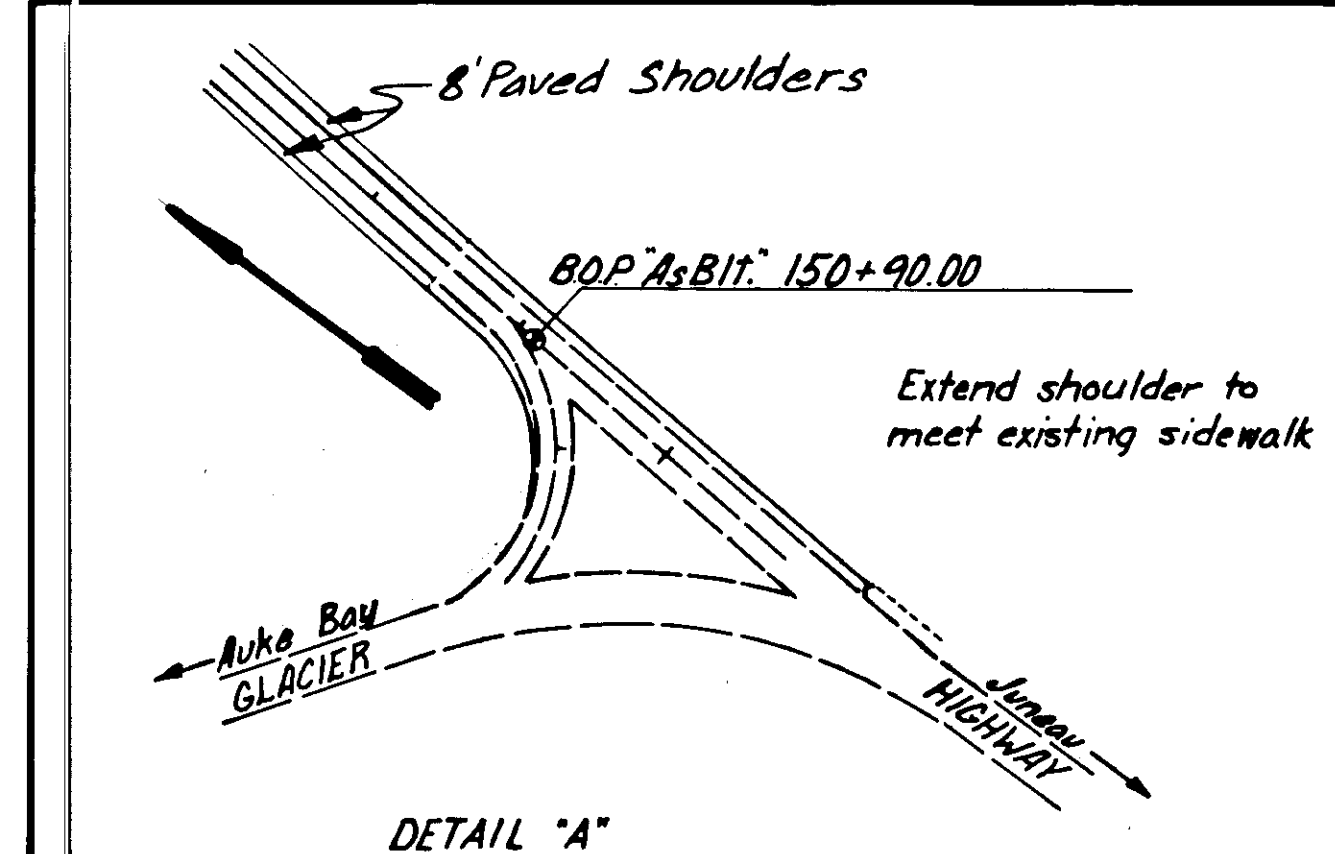
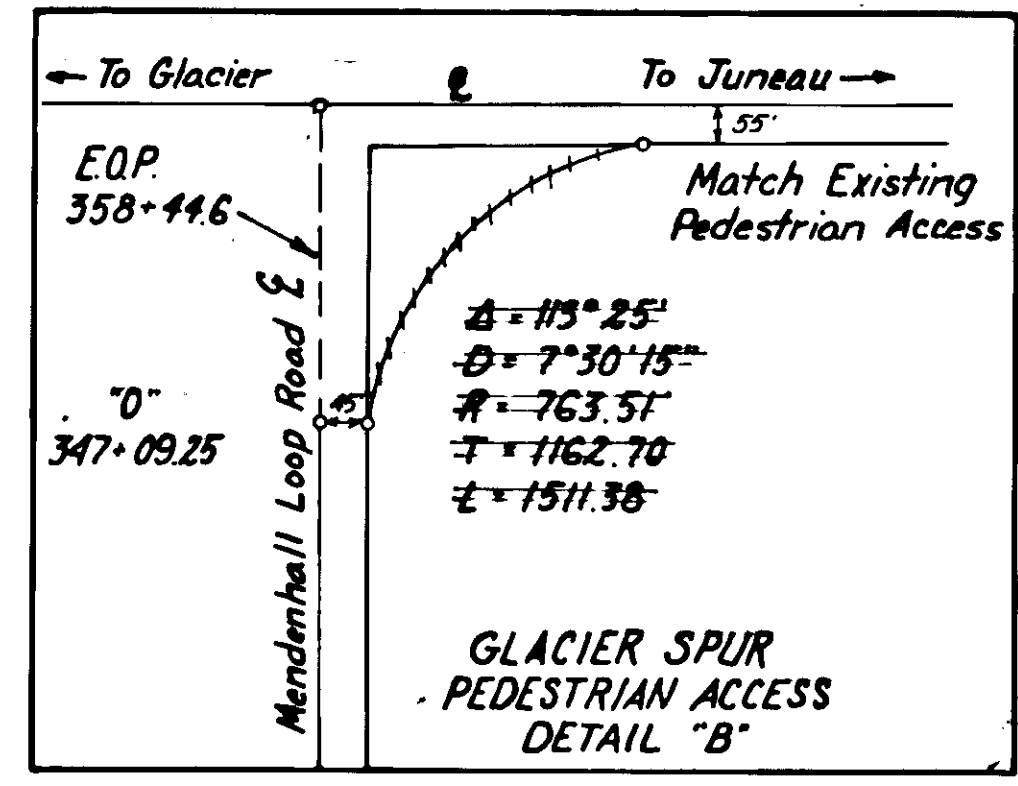


STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	RS-0966(12)	1983	6	9

STRIP MAP



END PROJECT RS-0966(12)
STA. "0" 358+44.60 P.O.C.

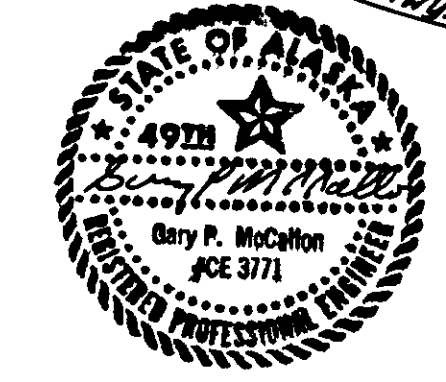


HORIZONTAL CONTROL: Alaska Highway Department (A.H.D.) Monuments "WDW" (N. 2403148.946, E. 2517274.834) and "SLO" (N. 2399423.681, E. 2514746.623) are used as Base Reference Line. The mean azimuth correction is -0° 44' 09.8" & the mean scale factor is 0.99992963. Bearing between "WDW" & "SLO" is S. 34° 09' 48" W. and the distance is 4502.161'

VERTICAL CONTROL: Vertical control is a B.L.M. brass cap for U.S.S. No. S.E. 3758, S.W. 1530, N.E. 1529, N.W. 1527. The cap is located approx. 100' east of the Dredge Lake Rd. turnoff. Elevation 64.01'.

Horizontal and vertical control shall be the alignment and shoulders of the existing pavement.

BEGIN PROJECT RS-0966(12)
STA. "As Bit" 150+90

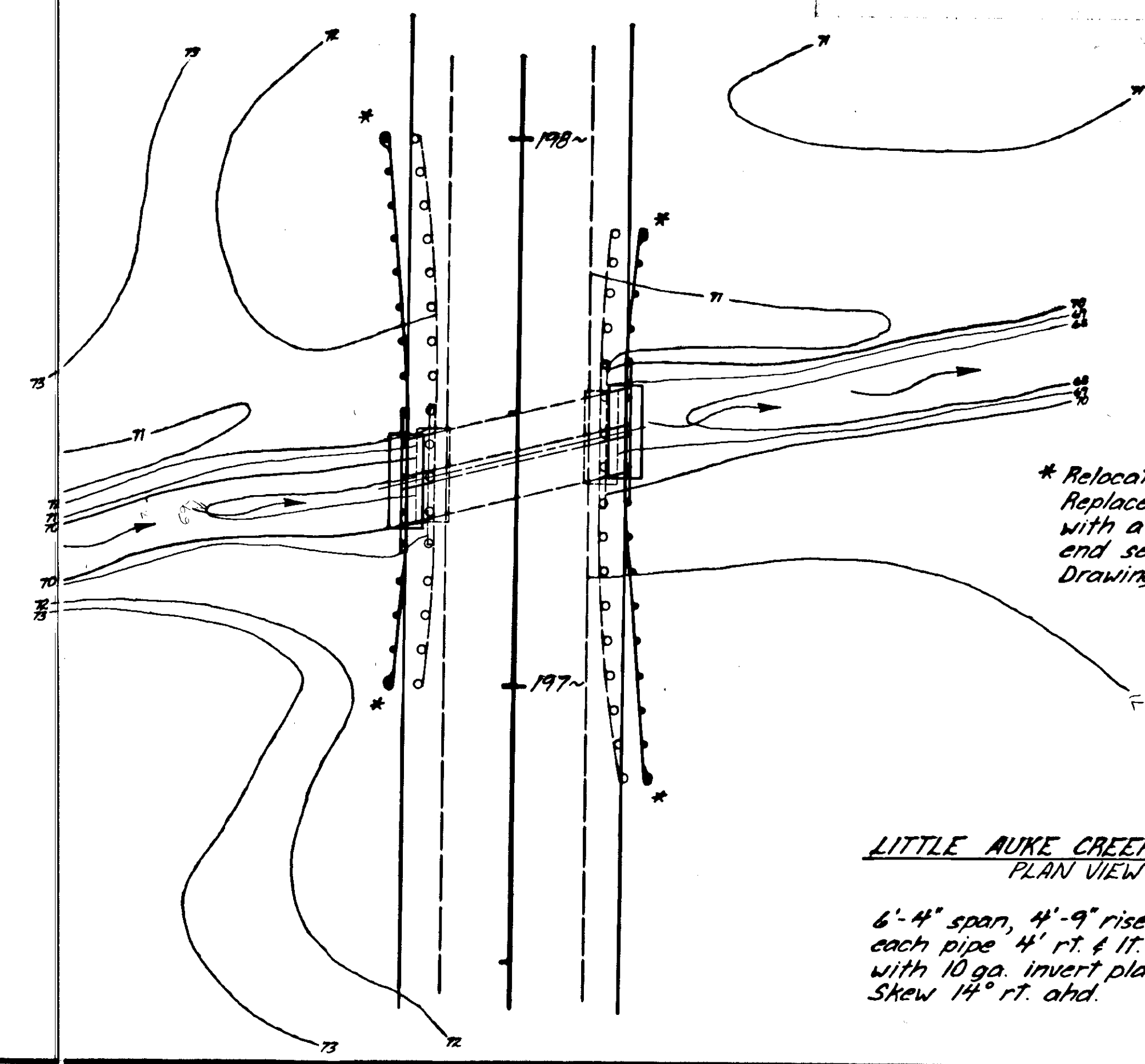
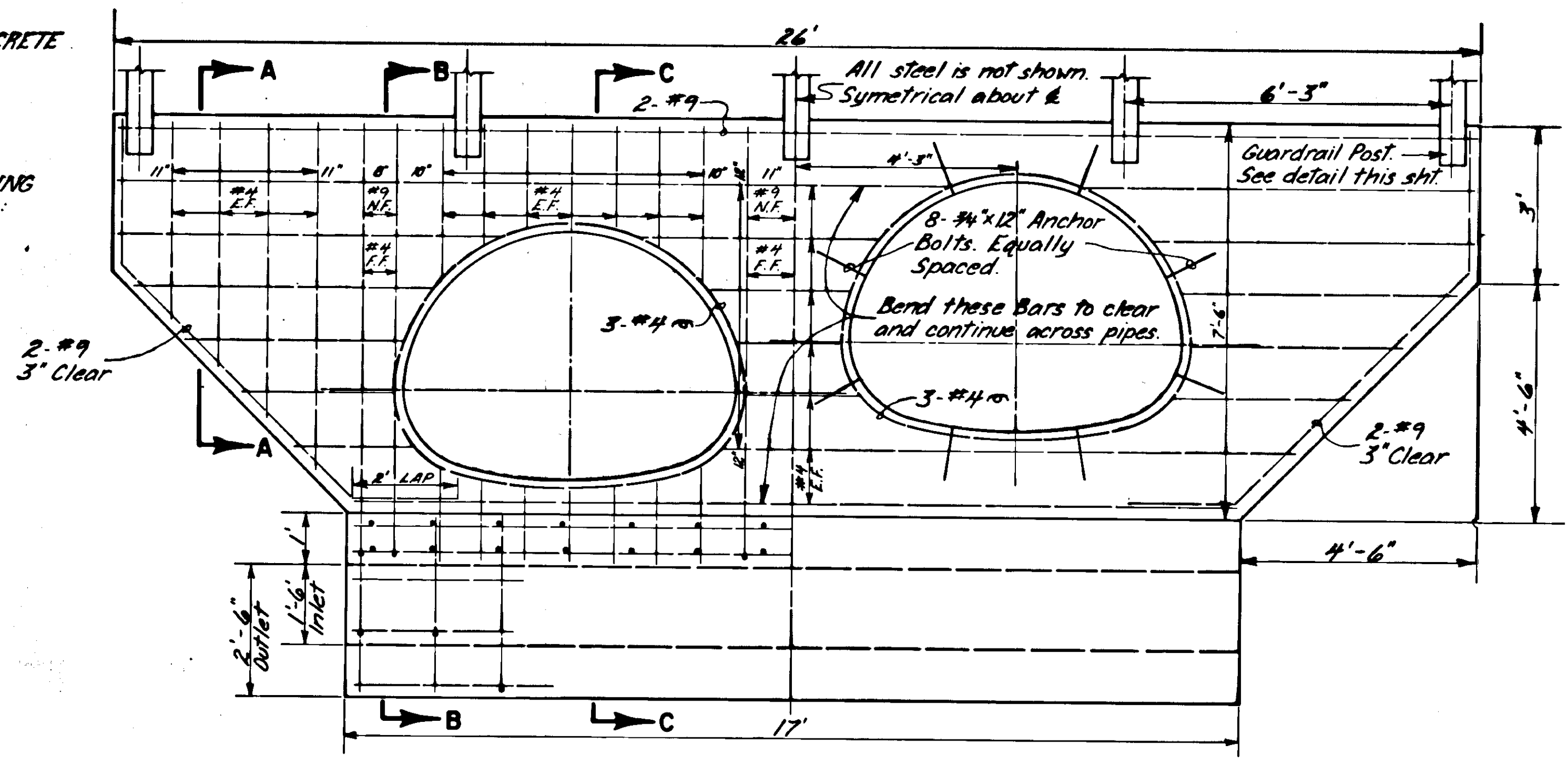


HEADWALL DETAILS

STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	RS-0966(12)	1983	7	9

CUBIC YARDS OF CONCRETE PER HEADWALL:
 INLET = 8.6 cu. yds.
 OUTLET = 9.2 cu. yds.

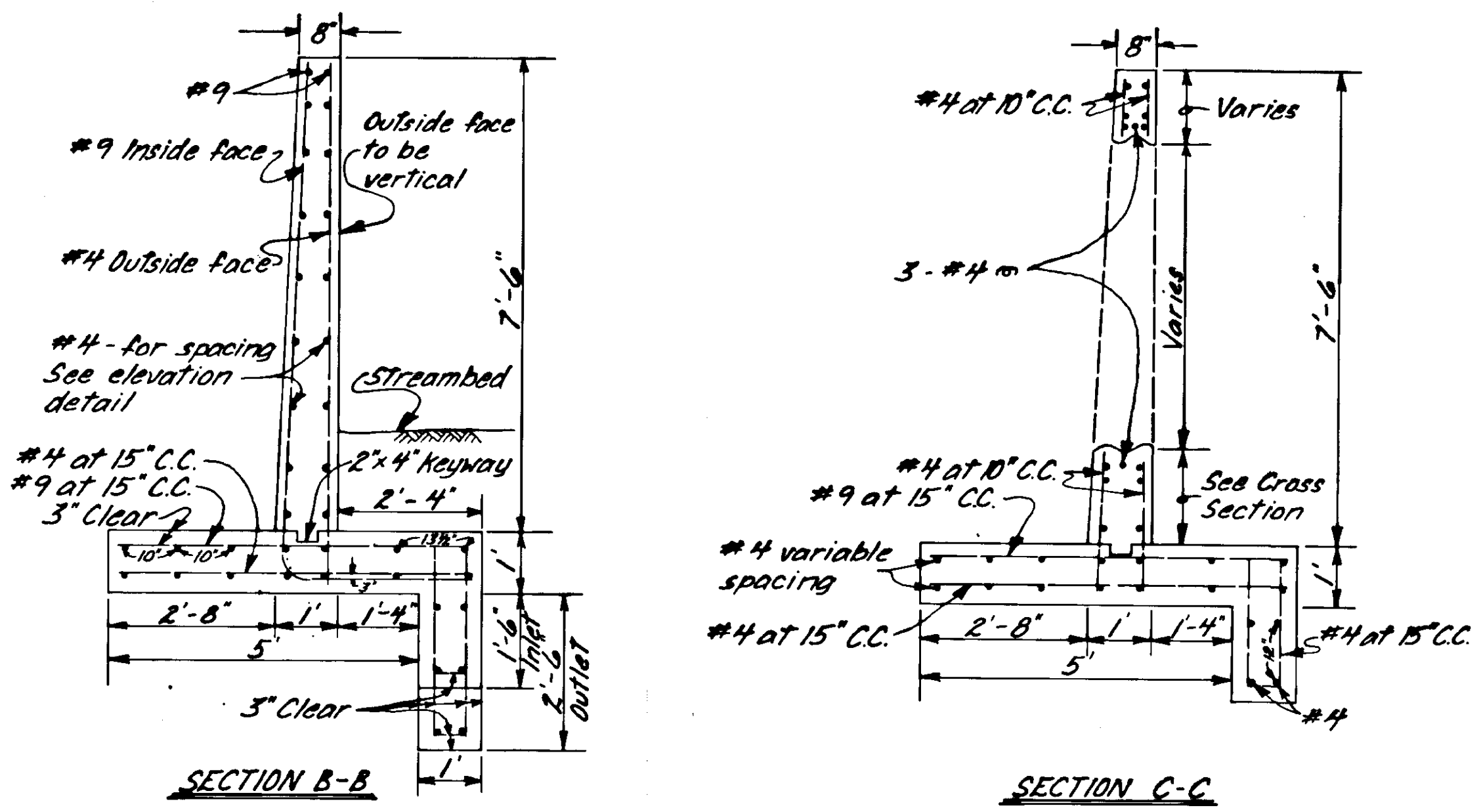
POUNDS OF REINFORCING STEEL PER HEADWALL:
 INLET = 1,538 lbs.
 OUTLET = 1,579 lbs.



LITTLE AUKE CREEK CULVERT
 PLAN VIEW

6'-4" span, 4'-9" rise, extend each pipe 4' r.t. & l.t. 12 ga. with 10 ga. invert plates. Skew 14° r.t. abd.

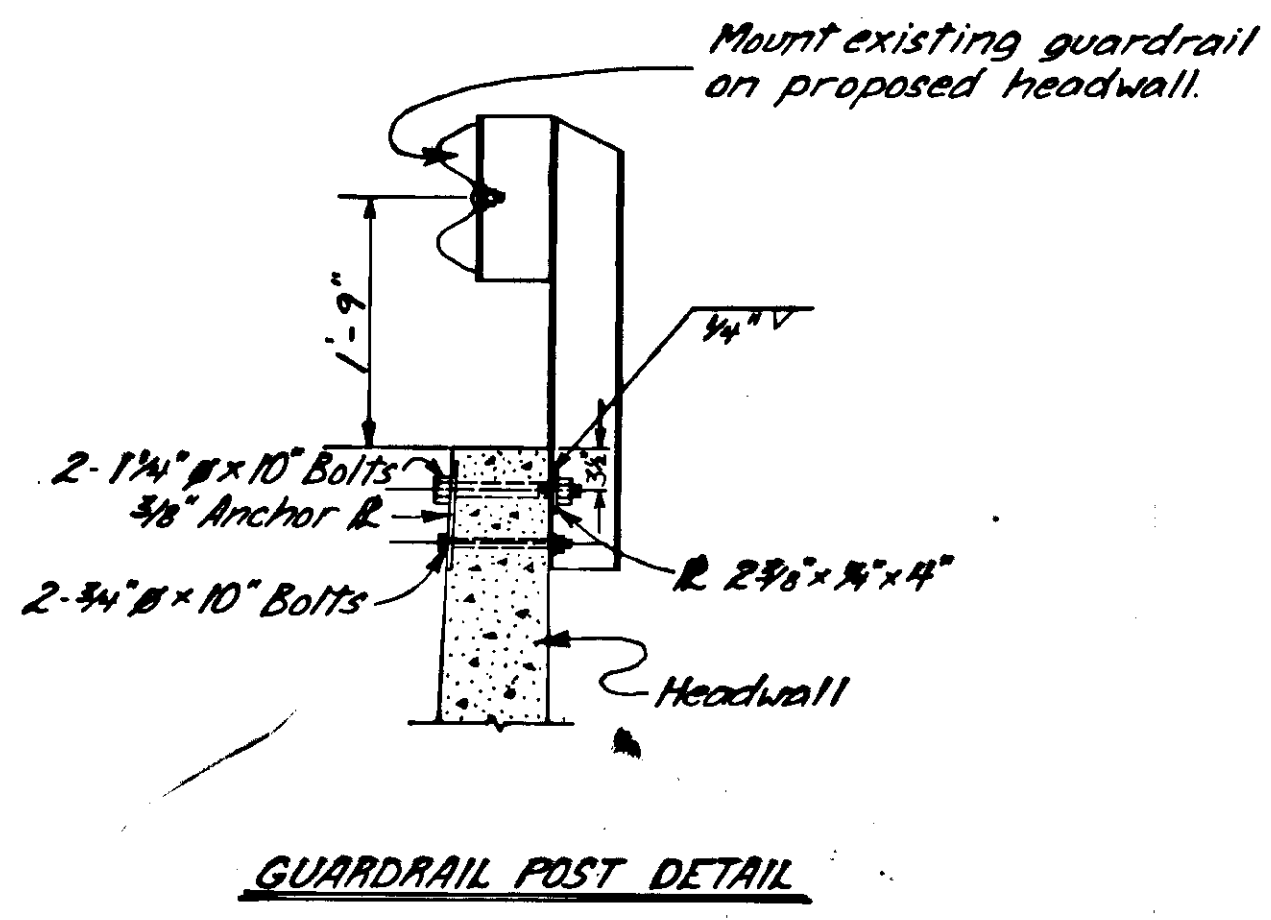
ELEVATION



SECTION A-A

SECTION B-B

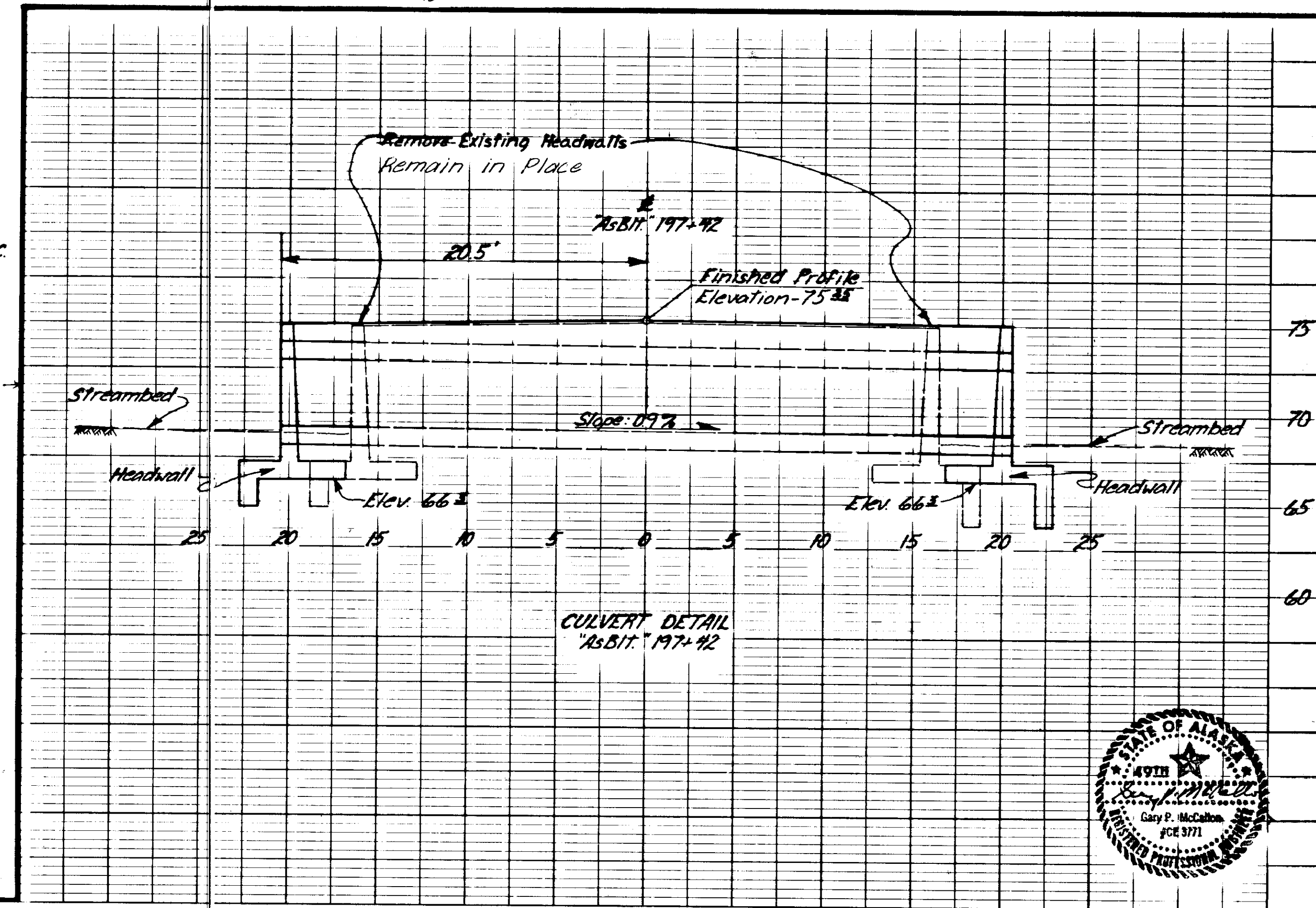
SECTION C-C



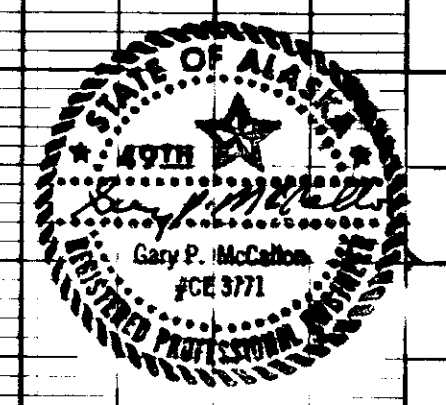
GUARDRAIL POST DETAIL

HEADWALL NOTES

1. All exposed corners shall be chamfered 3/4".
2. Clear distance for all reinforcing steel shall be 2" unless otherwise noted.
3. All concrete shall be Class "A", minimum compressive strength 3000 p.s.i. at 28 days.
4. All bar splices shall be lapped 24 bar diameters.

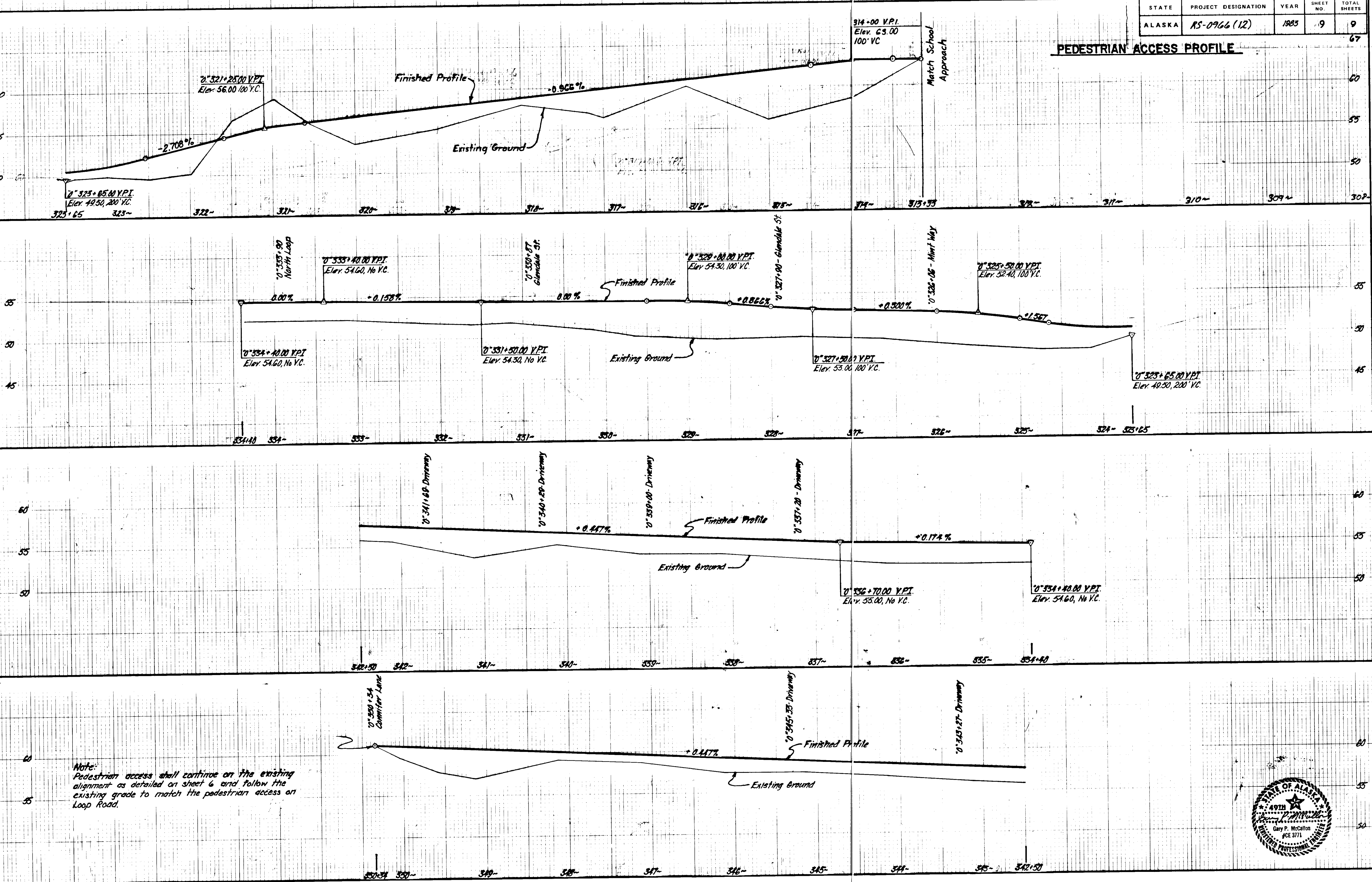


CULVERT DETAIL
 'AS BUILT' 197-192



STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
ALASKA	RS-0966 (12)	1983	9	9

PEDESTRIAN ACCESS PROFILE



Note:
Pedestrian access shall continue on the existing alignment as detailed on sheet 6 and follow the existing grade to match the pedestrian access on Loop Road.

