

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
&
PUBLIC FACILITIES

PLAN AND PROFILE
PROPOSED HIGHWAY PROJECT
GLACIER HIGHWAY
TEE HARBOR TO SHRINE
OF SAINT TERESE
RELOCATION OF GUARDRAIL, PAVING

STATE	PROJECT	SHEET NO.	TOTAL SHEETS
ALASKA	RS-M-0961(16)	1	4

INDEX OF SHEETS

1	TITLE SHEET
2	TYPICAL SECTIONS & ESTIMATE OF QUANTITIES
3	SUMMARY TABLES
4	PLAN SHEET

PROJECT SUMMARY

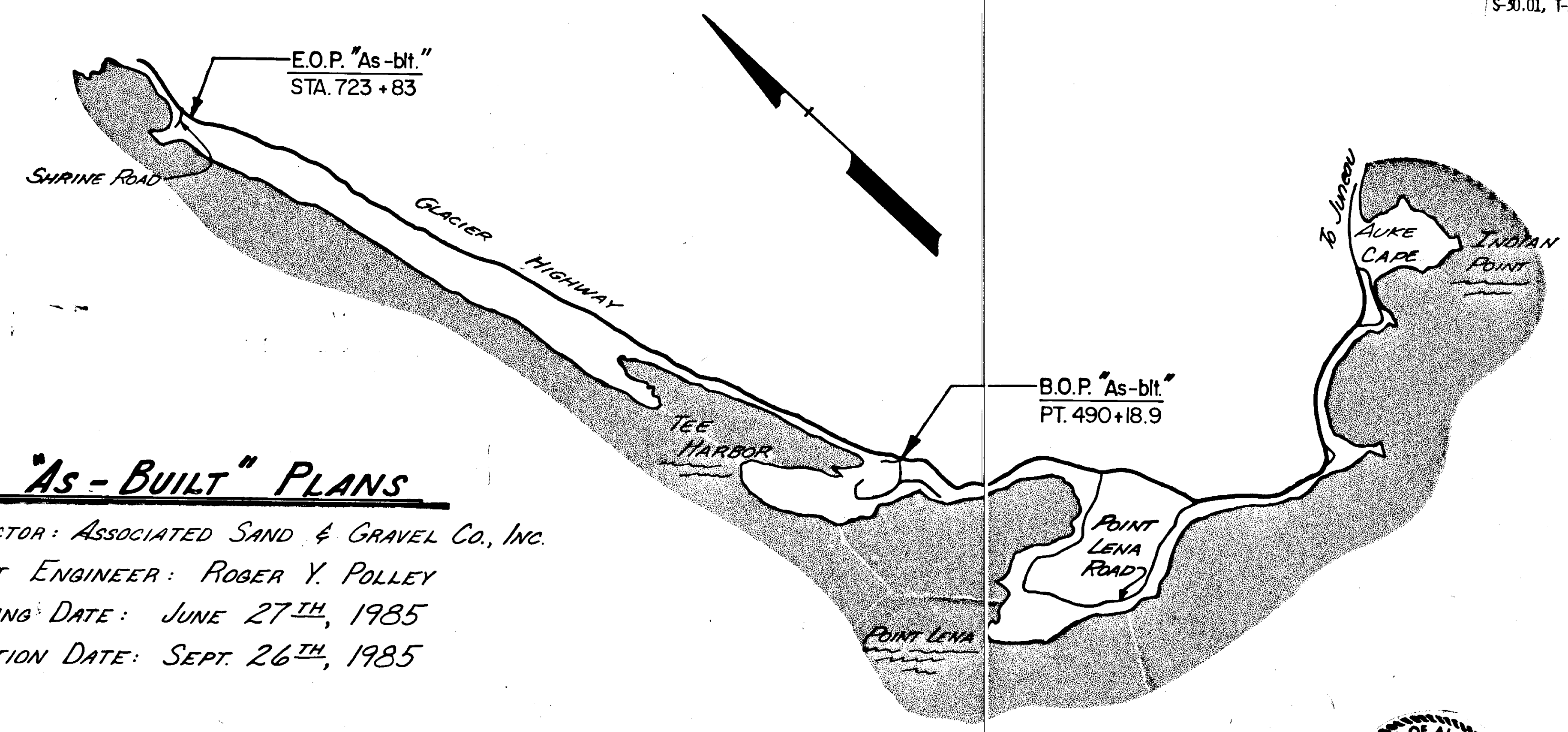
WIDTH OF PAVEMENT = 26 ft.
LENGTH OF PAVING = 4.42 mi. (23,3564')
LENGTH OF PROJECT = 4.42 mi. (23,3564')

DESIGN DESIGNATION

TEE HARBOR TO THE SHRINE
ADT - (1985) 626
ADT - (2005) 1,026
DHV - 133
D - 60/40
T - 4.5
TI - 6.0
V - 50 m.p.h.

RS-M-0961(16) (B-30122)

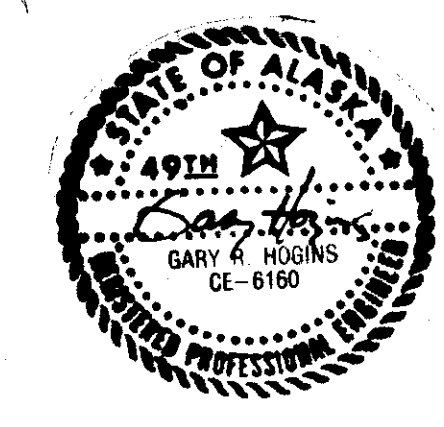
THE FOLLOWING STANDARD DRAWINGS APPLY TO THIS PROJECT = C-01.01, C-02.00, C-03.01, G-04.01M, G-12.00, G-14.02M, I-40.00, M-20.01, M-23.00, S-00.00, S-05.00, S-30.01, T-21.00



"As-BUILT" PLANS

CONTRACTOR: ASSOCIATED SAND & GRAVEL CO., INC.
PROJECT ENGINEER: ROGER Y. POLLEY
BEGINNING DATE: JUNE 27TH, 1985
COMPLETION DATE: SEPT. 26TH, 1985

NOTE: Equations are shown on the plan sheets.



STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
&
PUBLIC FACILITIES
APPROVED
William K. Wilson Date 4-22-85

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
&
PUBLIC FACILITIES
APPROVED
Gary R. Hogins Date 4-23-85

GENERAL NOTES

- (1) Material utilized to construct re-located guardrail flares will be paid for under Item 203(5c)-Borrow. Placement will be as directed by the Engineer.
- (2) Saw cuts for pavement transition shall be incidental to Item 401(1) - Asphalt Concrete Type II and No separate payment will be made.
- (3) Payment for the removal and disposal of the existing asphalt for the transitions is incidental to other items of work and shall not be paid for directly.
- (4) The scenic pullouts will not be paved. Feather the mainline paving in four (4) feet.
- (5) One lane of traffic shall be maintained at all times. Traffic control, signing and delineation shall be in accordance with standard drawing C-03.01.
- (6) Shoulder cuts made as a result of linear grading greater than 2" in depth, shall not be allowed to remain after work hours.
- (7) Project identification signing shall be as per the Manual on Uniform Traffic Control Devices.

398.5
3
18
106.2
226
6315
10,073.5
676.5
21.9
4350
33,343
32

ESTIMATE OF QUANTITIES

ITEM NO.	ITEM	UNIT	QUANT.
110(2)	Mobilization & Demobilization	L.S.	All Req'd
111(1)	Temporary Erosion 2nd Pollution Control	C.S.	All Req'd
115(3)	Temporary Pavement Marking	L.S.	All Req'd
113(1)	Flagging	M.H.	7200
114(1)	Construction Surveying by Contractor	L.S.	All Req'd
115(1)	Traffic Maintenance	L.S.	All Req'd
116(1)	Furnishing & Maintaining Field Office	L.S.	All Req'd
116(2)	Furnishing & Maintaining Field Laboratory	L.S.	All Req'd
202(9)	Single mail-box installation	Each	5
202(10)	Multiple mail-box installation	L.F.	77
203(5c)	Borrow, Type A	C.Y.V.M.	300
203(8)	Linear grading	Sta.	233.5
301(1)	Crushed Aggr. Base Course	Ton	6800
401(1)	Asphalt Concrete - Type II	Ton	10,000
401(2)	Asphalt Cement - AC-5	Ton	500
402(1)	CSS-1 Asphalt for tack coat	Ton	18.5
109(1)	DBE and WBE Adjustments	L.S.	All Req'd
606(6)	End Anchorage	Each	4
606(8)	Removal & Relocation of Guardrail	L.F.	437.5
615(1)	Standard Signs	S.F.	78.67
634(1)	Reinforcing Fabric	S.Y.	3550
639(1)	Approachs	Each	15
670(7)	Painted Traffic Markings	L.S.	All Req'd

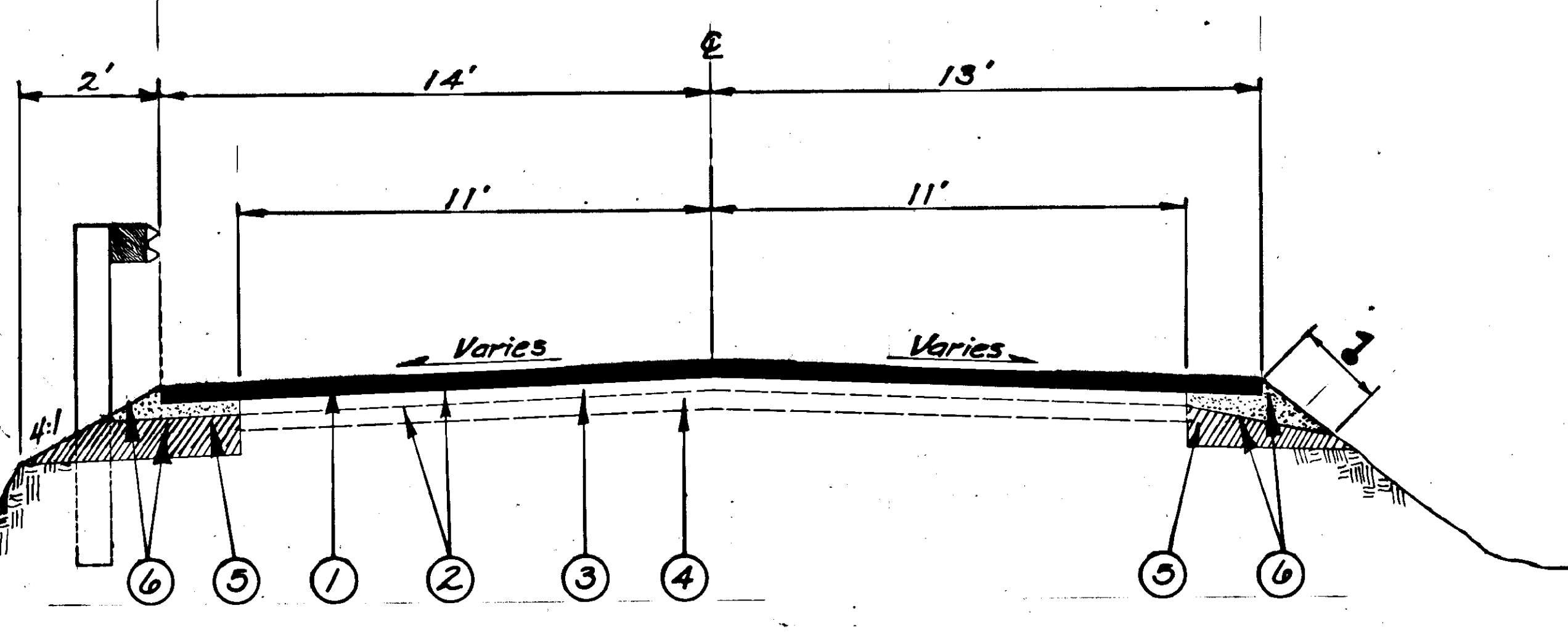
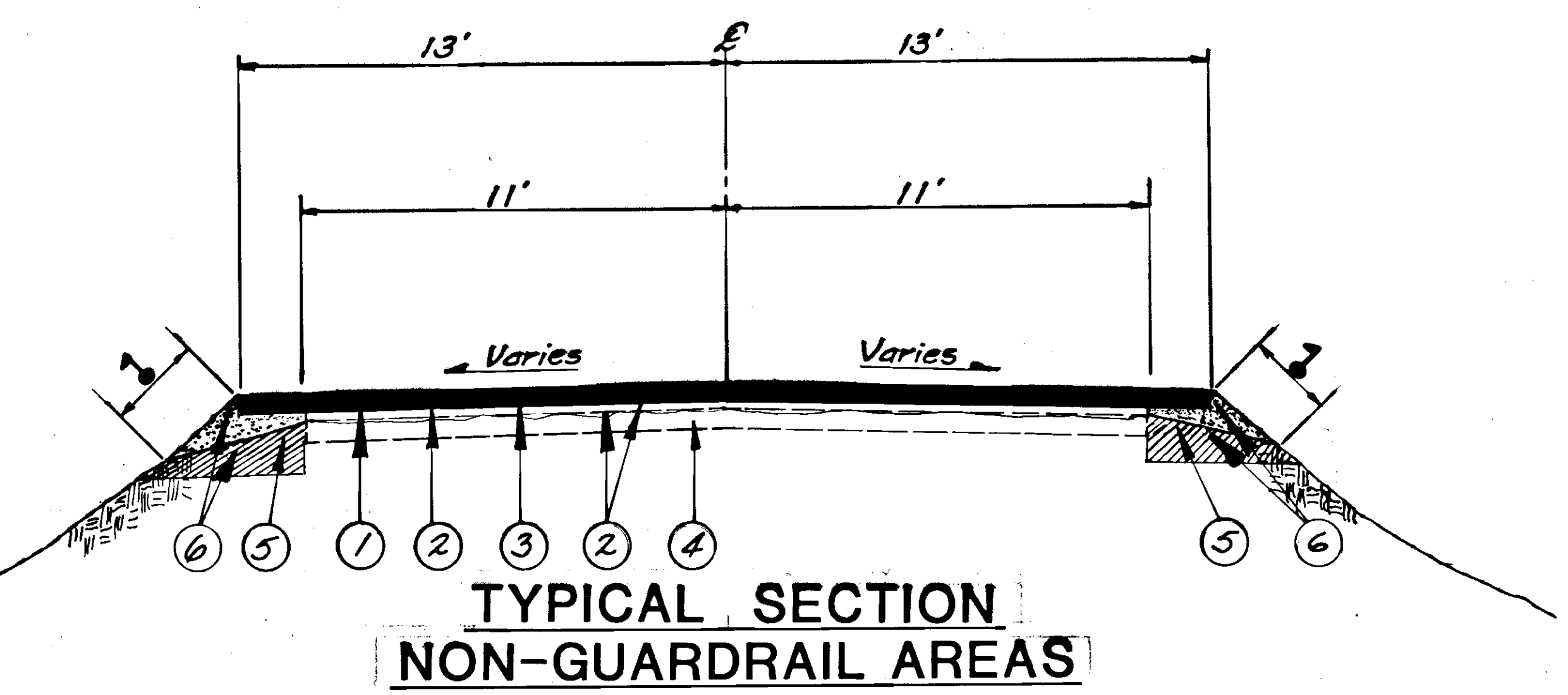
LABELING INDEX

1	1/2" asphalt concrete, type II	6	Crushed aggregate base course (6")
2	CSS-1 asphalt for tack coat	7	Borrow, Type A
3	1" leveling course		
4	Existing pavement		
5	Linear grading		

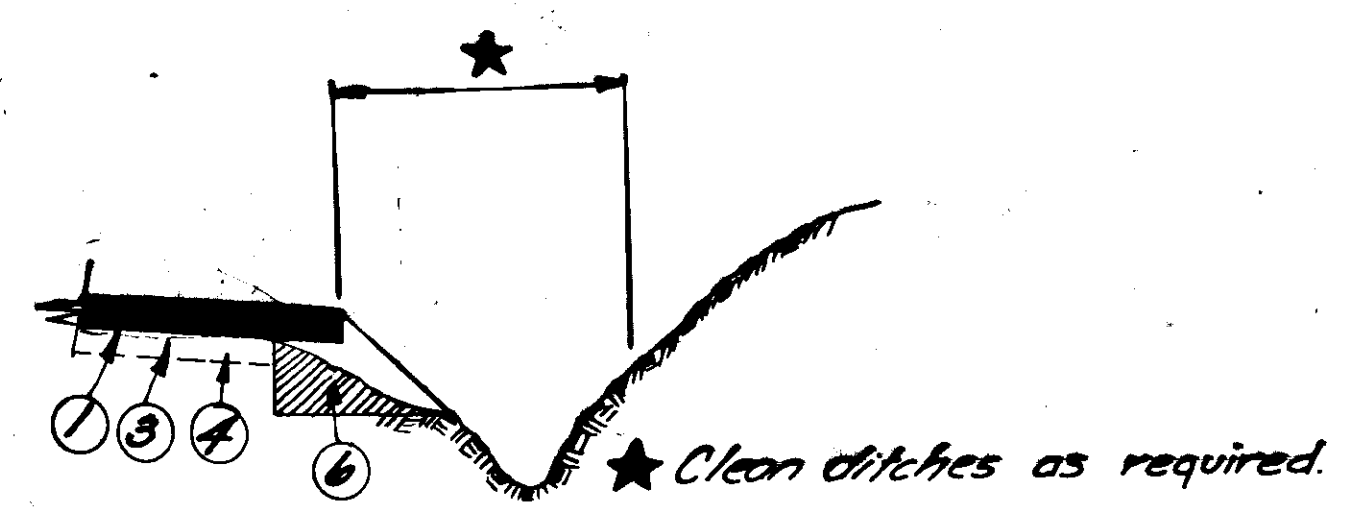
BASIS OF ESTIMATE

ITEM NO.	ESTIMATING FACTORS
301(1)	1.96 Tons per Cu. Yd.
401(1)	114.0 lbs per Sq. Yd./Inch of depth
401(2)	6% of item 401(1)
402(1)	0.10 gal. per sq. yd./240 gal. per Ton

NOTE:
Slope will vary.

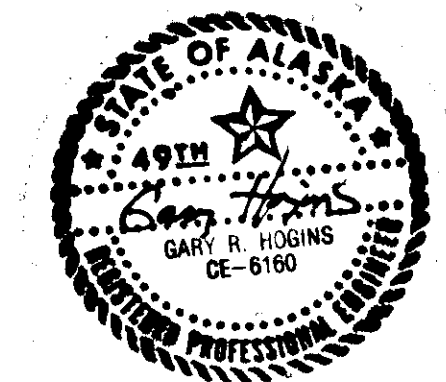
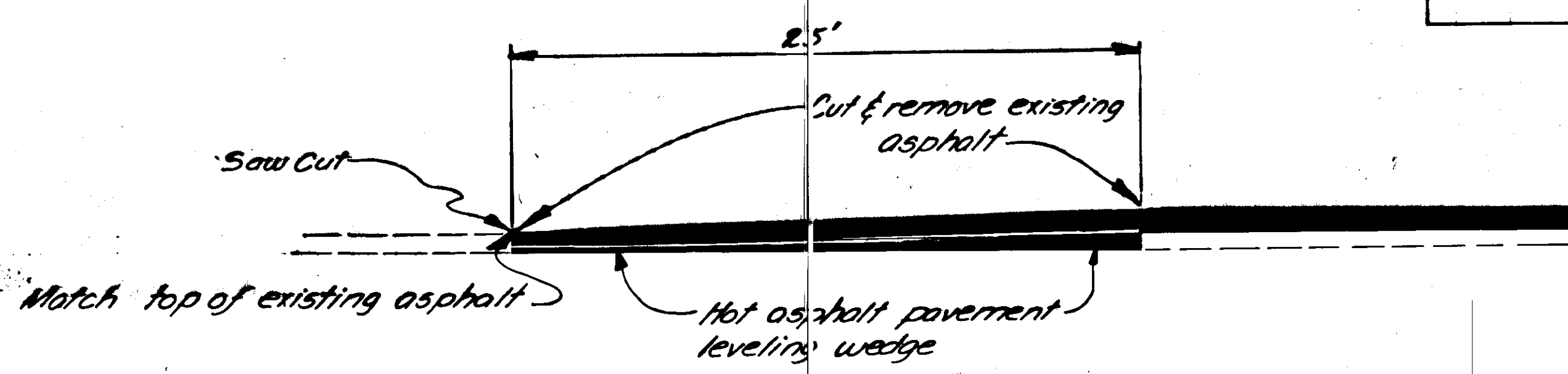
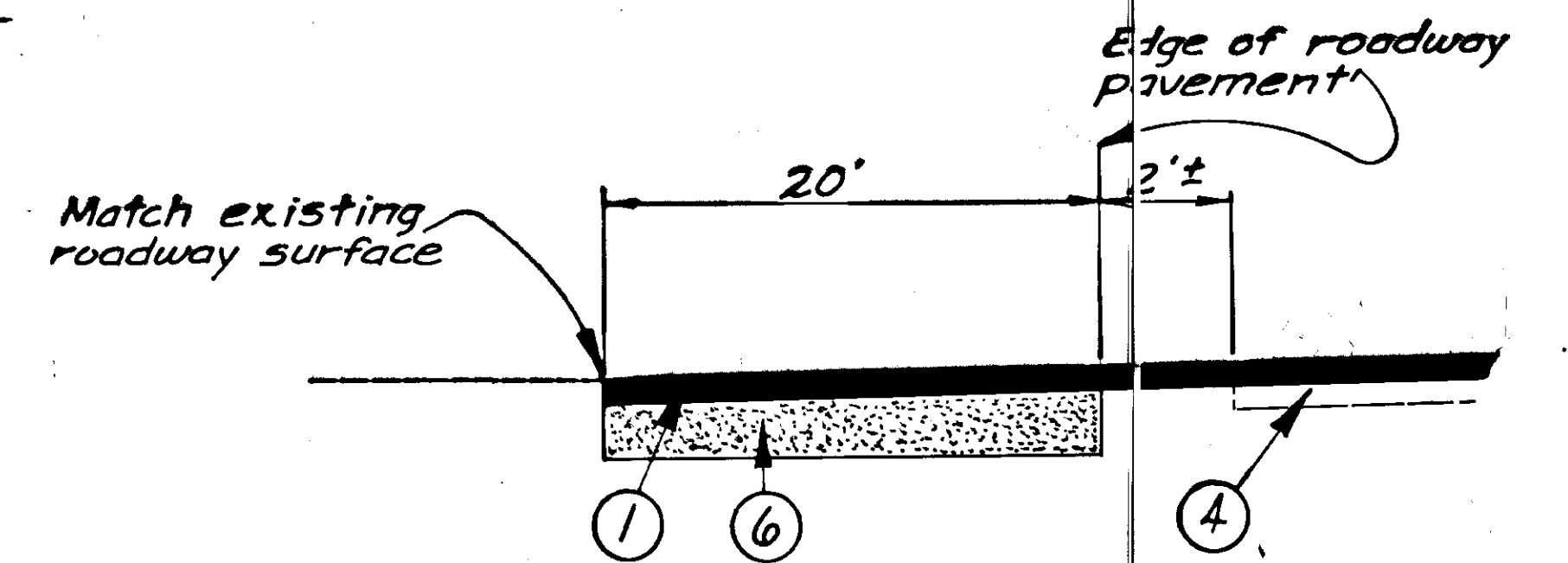


See Sht. 3, Relocation Summary



NOTE:
Excavation will be required 6" from top of leveling course and daylighting on the foreslope. This excavation will be back-filled and compacted with crushed aggregate base course.

LINEAR GRADING
(applies to both lt. & rt. shoulders)



BEGIN STATION	END STATION
589+51 LT	502+70 LT
507+60 LT	508+60 LT*
Turnout LT	
511+29 LT*	533+91 LT*
Turnout LT	
537+09 LT*	540+75 LT*
Turnout LT	
542+66 LT*	544+73 LT

Approach Summary

492+81 RT	568+70 LT	607+20 LT
505+61 LT	567+30 RT	611+20 LT
508+21 LT	570+50 LT	614+36 LT
552+00 RT	571+50 LT	617+25 LT
553+75 RT	571+20 LT	620+00 LT
555+00 LT	574+50 RT	645+00 RT
556+45 LT	586+70 LT	691+00 LT
557+60 LT	576+30 LT	32 approaches
558+60 LT	577+00 LT	Existing approaches were
559+90 LT	577+30 RT	drilled and poured to existing
560+45 LT	577+00 RT	widths and 20' deep.
563+00 LT	580+20 RT	Stout approaches to 25'
564+00 LT	582+80 RT	
564+20 RT	584+45 LT	

* Tie into existing rail & posts on turnout rail or posts unless otherwise noted.
The existing end terminal to be stockpiled at the D.C. yard, 7 mile Glacier Hg.

Guardrail Summary

543+58 to 545+70	18 panels x 13.5 = 243 LF
537+57 to 541+67	33 panels x 12.5 = 412.5 LF
512+13 to 534+32	182 panels x 12.5 = 2275 LF
508+37 to 508+42	9 panels x 12.5 = 112.5 LF
490+26 to 503+50	106 panels x 12.5 = 1325 LF

* Stationing approx. only actual footage via panel count

H SUMMARY

REMARKS
Paved
Paved
concrete

SIGNING SCHEDULE

NO.	STATION	FRAM. LT.	FRAM. RT.	CODE NO.	LEGEND	SIGN PANEL THICKNESS			NO. OF POSTS	EMBANKMENT	FACING TRAFFIC	REMARKS
						SIZE	UNFRAM.	FRAM. AREA SF.				
1	493+50	18		DI-1	Tee Harbor	12x80	0.100	6.67	2		SB	UPPER CASE 1/4" LOWER CASE 1/4"
2	520+00		27	WI-3L	45 M.P.H.	36x36	0.080	9.0	1		NB	
3	520+00		27	WI-1	45 M.P.H.	24x24	0.080	4.0	1		NB	
4	523+43		27	WI-2	19	6x12	0.080	0.5	1		NB	
5	548+43	27		WI-5L	45 M.P.H.	36x36	0.080	9.0	1		SB	
6	548+43	27		WI-1	45 M.P.H.	24x24	0.080	4.0	1		SB	
7	574+67		27	WI-2	20	6x12	0.080	0.5	1		NB	
8	588+40		27	WI-8	ROCKS	30x30	0.080	6.25	1		NB	
9	611+40	27		WI-8	ROCKS	30x30	0.080	6.25	1		SB	
10	611+40		27	WI-2	21	6x12	0.080	0.5	1		NB	
11	612+70		27	WI-2L		36x36	0.080	9.0	1		NB	
12	627+90	27		WI-2R		36x36	0.080	9.0	1		SB	
13	655+75		27	WI-2	22	6x12	0.080	0.5	1		NB	
14	714+91		27	WI-2	23	6x12	0.080	0.5	1		NB	
15	722+30		27	WI-2R		36x36	0.080	9.0	1		NB	
16	722+30		27	WI-1	50 M.P.H.	24x24	0.080	4.0	1		NB	

SIGNING NOTES:

- Sign locations are approximate only and are subject to minor revisions.
- All sign posts shall be square telescoping perforated galvanized steel posts. The 2" size shall be used above ground and the 2 1/4" size shall be used for the entire embedment length.
- All posts shall be installed with the sleeve type embedment in accordance with Standard Drawing S-30.01.
- Existing signs to be replaced shall be removed and stockpiled as directed at the D.O.T./P.F. maintenance yard, 7 mile Glacier Highway.

SINGLE MAILBOX INSTALLATION

STATION	LEFT	RIGHT	REMARKS
509+39	✓		1 Newspaper Tube (N.T.)
550+74	✓		1 + 1 N.T.
555+82	✓		1 N.T. only
557+00	✓		1 + 1 N.T.
575+31	✓		1 N.T. only

MULTIPLE MAILBOX INSTALLATION

STATION	LT.	RT.	REMARKS
552+54		✓	18'

NOTE: Stationing is As-Built

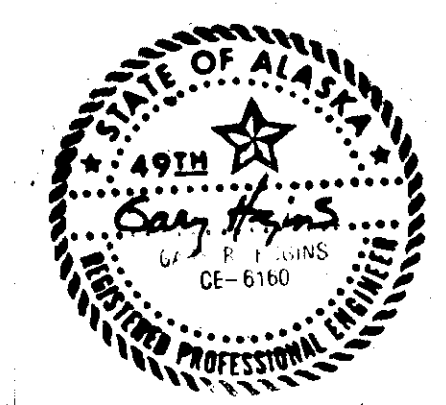
REINFORCING FABRIC

BEGIN STATION	END STATION	RT.
490+19	491+24	6'*
497+19	499+50	6'*
501+75	509+19	12'
509+19	510+19	6'*
510+69	528+46	12'
528+46	536+48	6'*
536+48	541+37	12'
541+37	542+94	6'*
542+94	553+54	12'
553+54	555+21	6'*
555+21	579+19	12'
579+19	586+70	6'*
586+70	596+53	12'
596+53	597+59	6'*
597+59	599+72	12'
599+72	600+72	6'*
600+72	605+21	12'
605+21	607+54	6'*
607+54	608+54	12'
608+54	610+65	6'*
610+65	626+48	12'
626+48	627+38	6'*
627+38	632+37	12'
632+37	638+72	6'*
638+72	640+39	12'
640+39	641+24	6'*
641+24	642+94	12'
642+94	644+24	6'*
644+24	668+53	12'
668+53	668+84	6'*
668+84	675+61	12'
675+61	677+24	6'*
677+24	679+35	12'
679+35	688+47	6'*
715+06	719+06	6'*

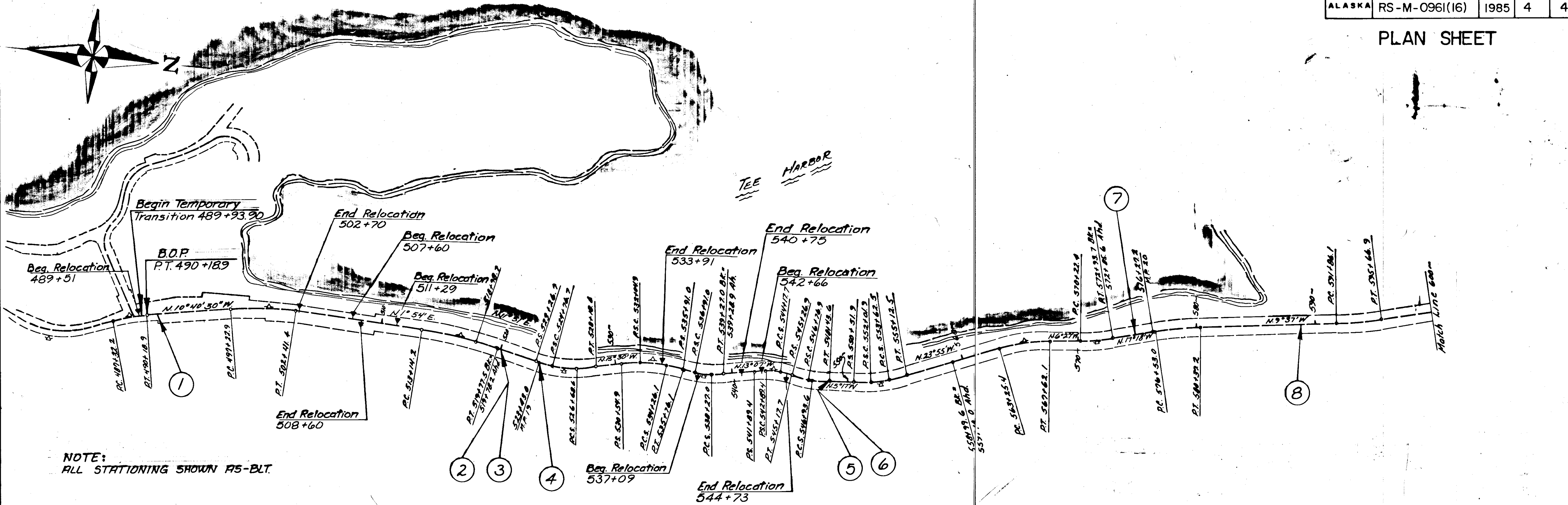
REINFORCING FABRIC

BEGIN STATION	END STATION	RT.
490+01	499+50	12'
499+56	500+96	12'
503+67	504+81	6'*
506+89	515+13	12'
517+93	520+89	12'
521+32	522+32	12'
528+20	533+99	6'*
536+48	546+08	12'
546+84	547+34	12'
553+24	553+54	12'
555+21	559+66	12'
560+32	568+99	12'
570+08	577+26	12'
577+99	580+41	12'
582+97	586+00	12'
588+54	592+31	6'*
604+98	610+65	12'
613+20	644+64	12'
620+91	621+64	12'
630+11	632+37	12'
633+24	633+74	12'
634+11	640+39	6'*
642+24	659+99	6'*
665+23	666+23	12'
669+24	675+61	6'*
677+24	677+64	12'
707+03	709+10	6'*
721+72	722+72	12'

* Reinforcing fabric shall be placed on outside edge of existing pavement.



PLAN SHEET



NOTE:
ALL STATIONING SHOWN AS-BLT.

LEGEND

- ① Sign Location, Refer to Sign Summary.
- ▼ Guardrail Relocation, Refer to Guardrail Summary.

