

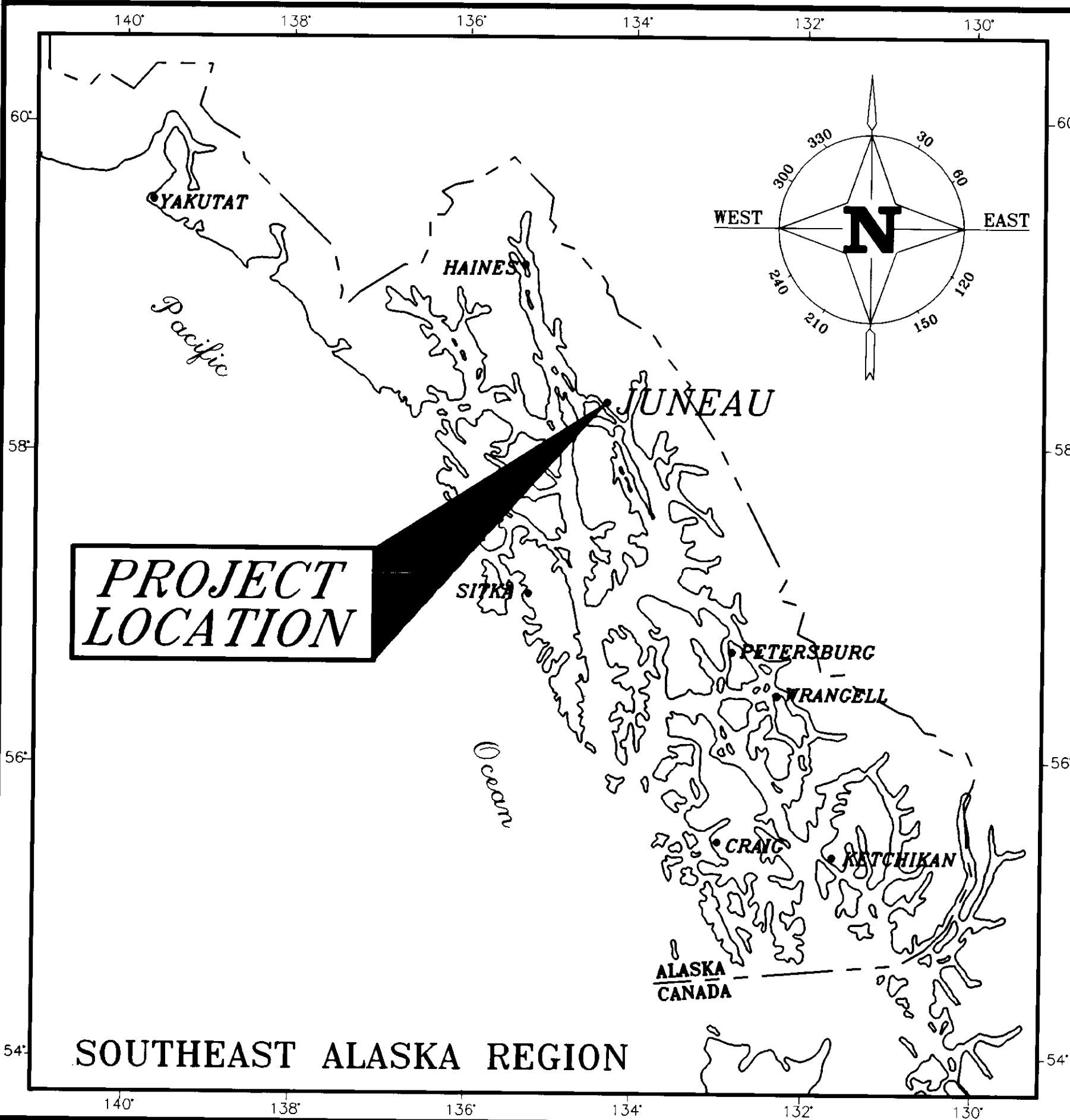
STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND
PUBLIC FACILITIES
SOUTHEAST REGION
DESIGN AND CONSTRUCTION DIVISION

JUNEAU, ALASKA

GLACIER HIGHWAY
ENGINEER'S CUT-OFF TO
FRITZ COVE ROAD
PAVEMENT REHABILITATION

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	TYPICAL SECTIONS
3	ESTIMATE OF QUANTITIES & SUMMARIES
4	TRAFFIC CONTROL PLAN
5	MISCELLANEOUS DETAILS
6-7A	ENVIRONMENTAL PLANS LEFT TURN LANES - FRITZ COVE RD. INTER.
7	FOUNDATION CONTROL DETAILS



GRINDING AND RESURFACING
NH-093-3(13) PROJECT NO. 71808

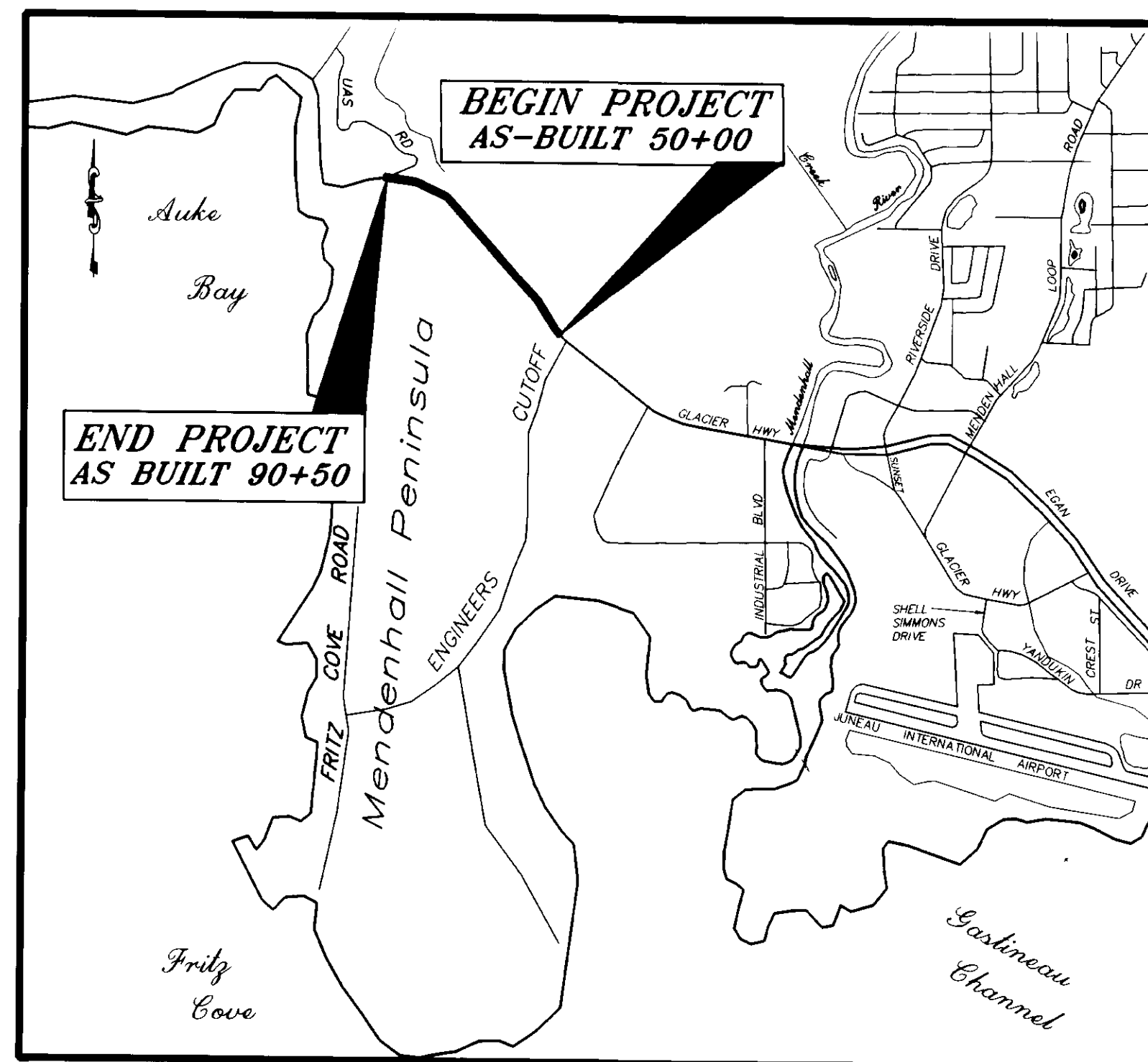
THE FOLLOWING STANDARD DRAWINGS APPLY TO THIS PROJECT:
A-1, C-01.03, C-02.01, G-4.04S, G-04.05W G-14.04S,
G-14.04W, I-20.11 M-16.01, M-20.11, M-23.11, S-00.00,
S-05.00, S-20.00, S-30.01 T-20.00, T-21.01

DESIGN DESIGNATION

ADT 1993	8300
ADT 2006	10070
DHV 15% (2005)	1500
% TRUCKS	4.3%
V	50/40 MPH
ESAL	680,000

PROJECT SUMMARY

LENGTH OF PROJECT 4,350 4,050 FEET (0.77 MI.)
LENGTH OF PAVING 4,350 4,050 FEET (0.77 MI.)
WIDTH OF PAVING 40 FEET



VICINITY MAP

AS-BUILT PLANS

CONTRACTORS: SECON INC. / RED SAM (J.V.)
ORIGINAL CONTRACT AMOUNT: \$ 825,157.00
PROJECT ENGINEER: MARK FIGLEY
START DATE: JULY 1995
FINISH DATE: SEPTEMBER 1995

AS-BUILT

BY: B.P. DATE: 1-6-97

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND
PUBLIC FACILITIES
SOUTHEAST REGION DESIGN SECTION

APPROVED [Signature] Date 4/18/95
Regional Preconstruction Engineer

APPROVED [Signature] Date 4/18/95
Director, S.E. Region Design & Construction

PROJECT NUMBER:
71808

DATE:
1995

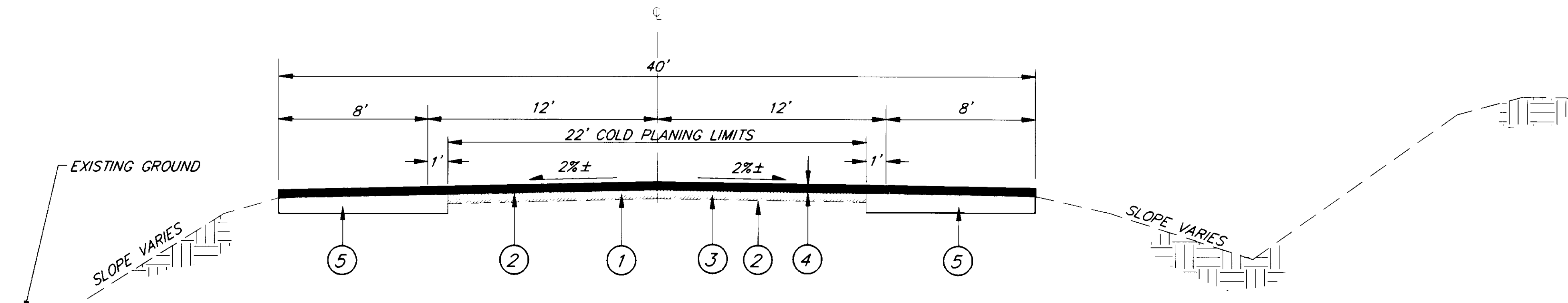
SHEET 1 OF 7

ENGINEER'S SEAL
STATE OF ALASKA
49th
Tracy W. Moore
CE-4932

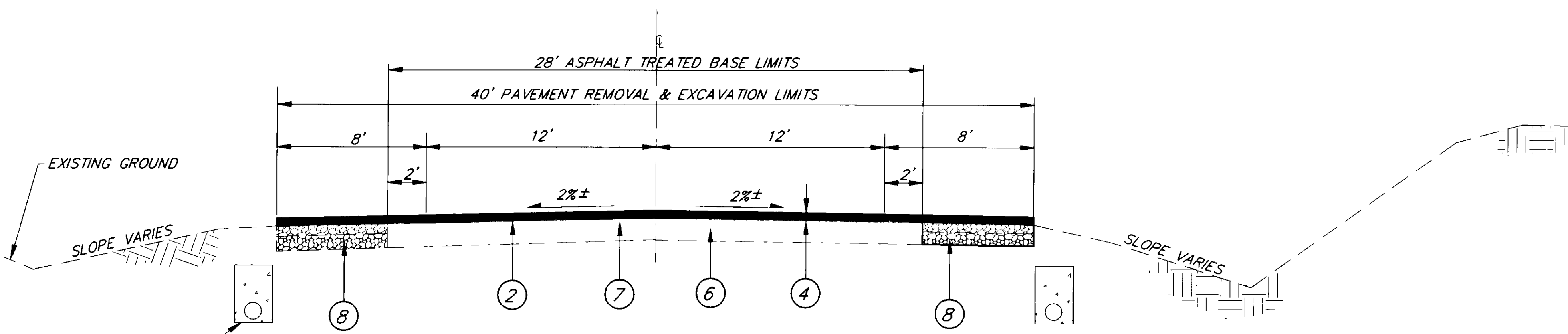
P:\NW\71808\DR-1\TSH-1\PLT-PCP(1) OR PLOT-PCP(2)

GENERAL NOTES

1. THE CONTRACTOR SHALL ADJUST ROADWAY CROSS SLOPES TO THE DESIGNED SUPERELEVATIONS. THE ENGINEER SHALL PROVIDE RATES & TRANSITION LENGTHS FROM AS-BUILTS.
2. ALL EXCESS EXCAVATION SHALL BE HAULED TO AN APPROVED WASTE SITE.
3. ALL EXISTING PAVED DRIVEWAY/APPROACHES SHALL BE TAPERED GROUND TO ALLOW A NEW VERTICAL BUTT JOINT AT THE EDGE OF THE OUTSIDE SHOULDER.
4. THE CONTRACTOR SHALL INSTALL THE UNDERDRAIN PRIOR TO PAVEMENT REMOVAL BETWEEN STATION 60+00 TO STATION 65+00.
5. THE CONTRACTOR SHALL FOLLOW THE CONSTRUCTION SEQUENCE AS OUTLINED IN SECTION 64.3.
6. PAVEMENT MATCH JOINTS PER DETAIL ON SHEET 5 ARE REQUIRED AT THE B.O.P., THE E.O.P., ENGINEERS CUT-OFF ROAD, FRITZ COVE ROAD AND THE COLLEGE ROAD. THESE PAVEMENT MATCH JOINTS WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED INCIDENTAL TO ITEM 202(2) REMOVAL OF PAVEMENT.
7. BORROW, TYPE "D" FOR EROSION CONTROL WILL BE PLACED AT THE INLET AND/OR OUTLET ENDS OF CULVERTS AT LOCATIONS DETERMINED BY AND AS DIRECTED BY THE ENGINEER.
8. PAVING AND RECONSTRUCTION LIMITS FOR THE APPROACHES SHOWN IN THE DRIVEWAY SUMMARY ON SHEET 3 SHALL BE AS DETERMINED BY THE ENGINEER.
9. COLD-PLANING WILL END AT APPROX. STA. 88+50, EXACT LOCATION WILL BE DETERMINED BY THE ENGINEER. FROM THE END OF THE COLD-PLANNED AREA A 2" THICK OVERLAY OF ASPHALT WILL BE APPLIED THROUGH THE FRITZ COVE ROAD/COLLEGE INTERSECTION. TAPERED PAVING WILL BE USED TO MATCH EXISTING CURB.
10. EXCAVATION LIMITS OF 40' BETWEEN STA. 60+00 AND 65+00 INCLUDE APPROX. 2 1/2" DEPTH ON SHOULDERS AND APPROX. 1" AT ASPHALT TREATED BASE LIMITS. ASBUILT PLANS INDICATE 4" OF ASPHALT BENEATH TRAVELED WAY, HOWEVER CORE SAMPLES INDICATE A THICKNESS OF 3". ASBUILT PLANS ALSO INDICATE 1 1/2" OF ASPHALT AT SHOULDERS WHICH CORE SAMPLES SHOW TO BE TRUE. THE INTENTION IS TO HAVE THE TOP OF ASPHALT TREATED BASE AND CRUSHED AGGREGATE AT THE EXISTING TOP OF PAVEMENT, THEN A 2" OVERLAY TO BE PLACED ATOP THE ASPHALT TREATED BASE AND CRUSHED AGGREGATE. THIS EXCAVATION WILL BE CONSIDERED INCIDENTAL TO OTHER ITEMS OF WORK AND NO SEPARATE PAYMENT WILL BE MADE.



"AS-BLT" 50+00 TO 60+00 AND 65+00 TO 90+50
(B.O.P. TO MATCH BRIDGE TO ENGINEER'S CUTOFF E.O.P.)



"AS-BLT" 60+00 TO 65+00

UNDERDRAIN (TYP)
SEE DETAIL ON SHEET 5

LABELING INDEX

- ① 1 1/4" COLD PLANING (SEE SECTION 408) - SEE NOTE 9 THIS SHEET.
- ② STE-1 ASPHALT FOR TACK COAT
- ③ 1 1/4" ASPHALT CONCRETE TYPE 1 CLASS "A"
- ④ 2" ASPHALT CONCRETE TYPE 1 CLASS "A"
- ⑤ RECYCLED ASPHALT PAVEMENT (4" DEPTH) ← 2" ASPHALT PAVEMENT
SEE SECTION 406 OF THE SPECIFICATIONS. ← 2" EXISTING BASE COURSE
- ⑥ 4" ASPHALT TREATED BASE, 2 LIFTS
- ⑦ PAVEMENT REMOVAL (ENTIRE WIDTH & DEPTH)
- ⑧ 4" CRUSHED AGGREGATE BASE COURSE

AS-BUILT

BY: B.A. DATE: 1-6-97

NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

PATH: P:\JNU\71808\DR\TYP	PLOT.F.PCP(4) OR PLOT.H.PCP(8)
BY:	DESCRIPTION OF CHANGE:
DATE:	
RECORD OF REVISIONS	

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
SOUTHEAST REGION DESIGN & CONSTRUCTION

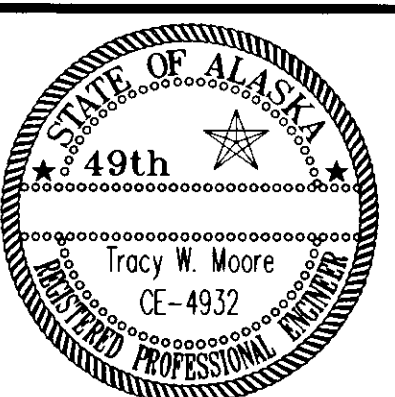
JUNEAU

ENGINEER'S CUT-OFF TO FRITZ COVE ROAD
GRINDING AND RESURFACING
NH-093-3(13) ~ 71808

ALASKA

TYPICAL SECTIONS

DESIGNED BY: F. MURPHY	PROJECT NO. 71808
DRAWN BY: B. ADAMS	DATE: APRIL 1995
CHECKED BY: T.W. MOORE	SHEET 2 OF 7



ESTIMATE OF QUANTITIES

ITEM NO.	ITEM	UNIT	QUANTITY
120(1)	DBE ADJUSTMENT	CONTINGENT SUM	ALL REQ'D.
202(2)	REMOVAL OF PAVEMENT	SQUARE YARD	2225
202(10)	SINGLE MAILBOX INSTALLATION	EACH	8
203(6D)	BORROW, TYPE "D"	TON	50
301(1)	CRUSHED AGGREGATE BASE COURSE	TON	200
401(1)	ASPHALT CONCRETE, TYPE 1, CLASS A	TON	4,700
401(2)	PBA-2 ASPHALT CEMENT	TON	230
402(2)	STE-1 ASPHALT FOR TACK COAT	TON	9
406(1)	RECYCLED ASPHALT PAVEMENT, 4" DEPTH	SQUARE YARD	7,200
406(2)	CSS-1 EMULSIFIED ASPHALT FOR RECYCLED PAVEMENT	TON	37
408(1)	PAVEMENT COLD PLANING	SQUARE YARD	8,800
605(7)	MODIFIED UNDERDRAIN	LINEAR FOOT	2,600
606(4)	REMOVAL AND RECONSTRUCTION OF GUARDRAIL	LINEAR FOOT	400
609(2)	CURB AND GUTTER, TYPE 1	LINEAR FOOT	200
614(4)	ADJUST EXISTING MONUMENT CASES	EACH	8
615(1)	STANDARD SIGNS	SQUARE FOOT	65
619(3)	STRAW BALES	EACH	26
633(1)	SILT FENCE	LINEAR FOOT	520
639(1)	APPROACHES	EACH	5
640(1)	MOBILIZATION AND DEMOBILIZATION	LUMP SUM	ALL REQUIRED
641(1)	EROSION AND POLLUTION CONTROL	CONTINGENT SUM	ALL REQUIRED
642(1)	CONSTRUCTION SURVEYING	LUMP SUM	ALL REQUIRED
642(2)	THREE PERSON SURVEY PARTY	hour	10
643(2)	TRAFFIC MAINTENANCE	LUMP SUM	ALL REQUIRED
643(3)	PERMANENT CONSTRUCTION SIGNING	LUMP SUM	ALL REQUIRED
643(4)	CONSTRUCTION SIGN	EACH PER DAY	1,000
643(7)	TRAFFIC CONE	EACH PER DAY	5,200
643(8)	DRUMS	EACH PER DAY	900
643(13)	TEMPORARY PAVEMENT MARKING	STATION	41
643(15)	FLAGGING	hour	2,000
670(1)	PAINTED TRAFFIC MARKINGS	LUMP SUM	ALL REQUIRED
670(6)	PERFORMED PAVEMENT MARKINGS	LUMP SUM	ALL REQUIRED
670(8)	RECESSED PAVEMENT MARKERS	EACH	75

BASIS OF ESTIMATE

ITEM NO.	ITEM	QUANTITY
401(3)	ASPHALT CONCRETE, TYPE 1	115 LBS./SQ.YD./INCH DEPTH
402(1)	PBA-2 ASPHALT CEMENT	6% OF ITEM 401(1) TYPE 1
402(2)	STE-1 ASPHALT FOR TACK COAT	0.07 GAL./SQ.YD.-253 GAL./TON @ 60°C
406(2)	CSS-1 EMULSIFIED ASPHALT	1.3 GAL./SQ.YD.-253 GAL./TON @ 60°C

DRIVEWAY SUMMARY

STATION	OFFSET	REMARKS
52+61	RT.	
61+20	RT.	GRADE AND PAVE PARKING AREA AS DIRECTED BY THE ENGINEER
73+78	LT.	
83+50	RT.	GRADE PARKING AREA AS DIRECTED, PAVE APPROACH AS DIRECTED
85+50	RT.	

STANDARD SIGNING SCHEDULE

SIGN NO.	STATION	OFFSET LT. RT.	CODE NO. (ASDS)	LEGEND	SIZE	AREA S.F.	NO. OF POSTS	FACING TRAFFIC	REMARKS
①	As-Built 50+75	30'	R1-1	STOP	30"X30"	6.25	1	S.B.	
②	" " 50+75	30'	D3-1	HAMILTON STREET	36"X8"	2.00	1	W.B.	MOUNT ABOVE #1
③	" " 56+50	26'	R2-5	SPEED ZONE AHEAD	24"X30"	5.00	1	W.B.	
④	" " 58+00	26'	M10-2	MILE 11	6"X12"	0.50	1	N.B.	
⑤	" " 58+00	26'	M10-2	MILE 11	6"X12"	0.50	1	S.B.	
⑥	" " 60+50	26'	R2-1	SPEED LIMIT 55	30"X36"	7.50	1	E.B.	
⑦	" " 60+50	26'	R2-1	SPEED LIMIT 40	30"X36"	7.50	1	W.B.	
⑧	" " 85+00	26'	D11-1	BIKE ROUTE	24"X18"	3.00	1	E.B.	MOUNT ABOVE #9
⑨	" " 85+00	26'	R7-1D	NO PARKING ANYTIME	12"X18"	1.50	1	S.B.	
⑩	" " 86+02	26'	D3-2L	← FRITZ COVE RD.	90"X12"	7.5	2	W.B.	7" SPACING BETWEEN POSTS 6" U.C. LETTERS, 4.5"L.C.
⑪	" " 88+00	26'	D11-1	BIKE ROUTE	24"X18"	3.00	1	W.B.	MOUNT ABOVE #12
⑫	" " 88+00	26'	R7-1D	NO PARKING ANYTIME	12"X18"	1.50	1	N.B.	
⑬	" " 89+40	30'	R1-1	STOP	30"X30"	6.25	1	N.B.	
⑭	" " 89+87	40'	R1-1	STOP	30"X30"	6.25	1	W.B.	
⑮	" " 89+87	40'	D3-1	AUKE LAKE WAY	48"X8"	2.67	1	W.B.	MOUNT ABOVE #14

MONUMENT SUMMARY

STATION	OFFSET	ADJUST CASE	REMARKS
56+66.27	℄	X	PI
63+66.11	℄	X	PS
67+16.11	℄	X	PSC
69+68.70	℄	X	PCS
73+18.70	℄	X	PT
83+94.72 BK.= 83+94.71 AHD.	℄	X	PS
86+94.71	℄	X	PSC
89+66.34	℄	X	POC

R.P.M. SUMMARY

STATION TO	STATION	40'	80'
50+00	56+00		X
56+00	70+00	X	
70+00	85+50		X
85+50	90+50	X	

STRIPING AND RPM NOTES:

- EXISTING NO-PASSING GROOVES IF PRESENT AT ENDS OF NO PASSING ZONES SHALL BE REFERENCED BEFORE CONSTRUCTION BEGINS BY THE CONTRACTOR FOR TEMPORARY AND PERMANENT MARKING PLACEMENT.
- LATERAL PLACEMENT OF RECESSED PAVEMENT MARKERS SHALL BE BETWEEN DOUBLE YELLOW STRIPES ON CENTERLINE.
- LOCATIONS OF MARKERS IN SITUATIONS WITH UNUSUAL GEOMETRICS WILL BE DETERMINED BY THE PROJECT ENGINEER.
- RPM QUANTITIES ARE APPROXIMATE ONLY AND MAY VARY.
- DO NOT REPLACE BIKE LANE SYMBOLS. PAINTED BIKE LANE SYMBOLS SHALL BE REPLACED WITH DETAILS SHOWN ON SHEET 7A.
- THE CONTRACTOR SHALL INVENTORY EXISTING STRIPING CONDITIONS PRIOR TO ANY PAVEMENT REMOVAL. FINAL STRIPING PLAN SHALL UTILIZE THE SAME CONFIGURATION. PERFORMED PAVEMENT MARKINGS SHALL BE USED FOR ALL CENTERLINE STRIPING, WHETHER SOLID OR DASHED. PAINTED TRAFFIC MARKING SHALL BE USED ELSEWHERE. 24" STOP BARS SHALL BE LOCATED @ ENGINEERS CUT-OFF RD. BOTH AUKE LAKE PARKING AREA APPROACHES, FRITZ COVE RD & COLLEGE RD. THE FOLLOWING IS AN APPROXIMATE STRIPING QUANTITY.

STRIPING QUANTITIES

STRIPE	PERFORMED PAVEMENT MARKINGS	PAINTED TRAFFIC MARKINGS
4" SOLID WHITE		8100 L.F.
4" SOLID YELLOW	6200 L.F.	
4" DASHED YELLOW	1900 L.F.	
24" WHITE		80 L.F.

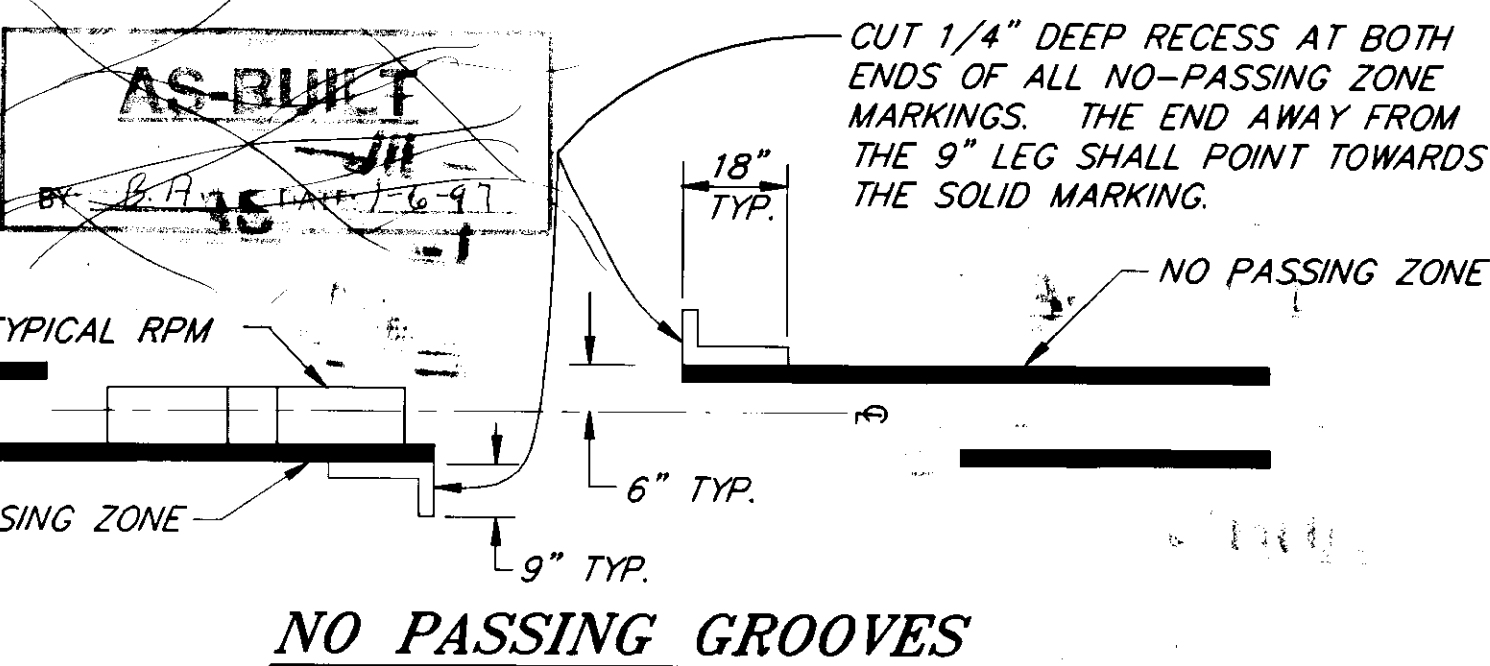
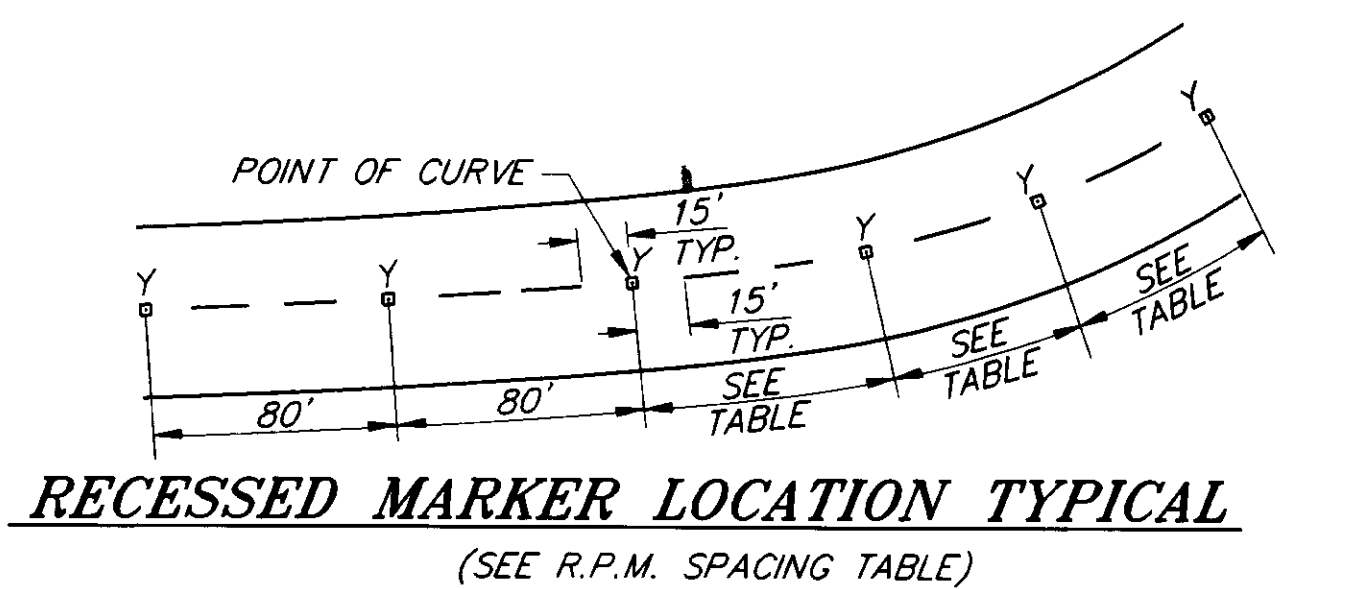
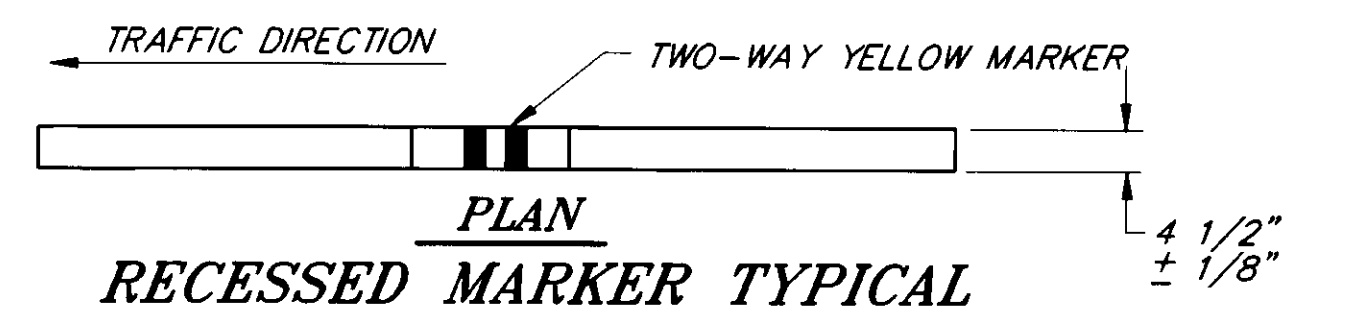
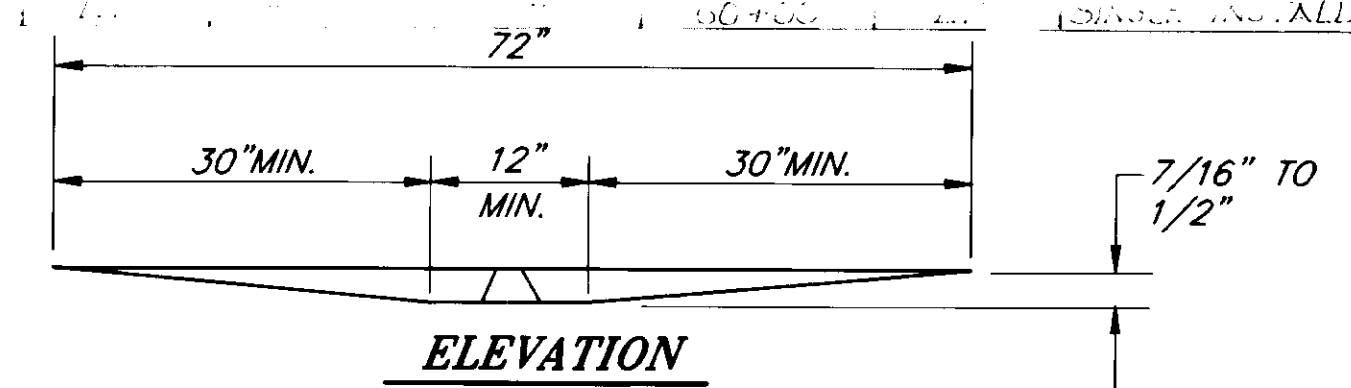
- SHOULDER STRIPES SHALL BE LOCATED AT 12' FROM CENTERLINE ON BOTH SIDES.
- NO PASSING ZONE AND GROOVES SHALL BE LOCATED BY THE ENGINEER.

GENERAL SIGNING NOTES

- SIGN LOCATIONS ARE APPROXIMATE ONLY AND ARE SUBJECT TO MINOR REVISIONS.
- ALL EXISTING SIGNS SHALL BE REMOVED AND SALVAGED AND DELIVERED TO THE DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES AT MILE 7 OLD GLACIER HIGHWAY.
- ALL SIGN POSTS SHALL BE TELESCOPING PERFORATED GALVANIZED STEEL; THE 2" SIZE SHALL BE USED ABOVE GROUND AND THE 2 1/4" SHALL BE USED BELOW GROUND.
- ALL D3-1 STREET NAME SIGNS SHALL HAVE THE LEGEND ON BOTH SIDES
- ALL PARKING SIGNS SHALL FACE 45 DEGREES TO TRAFFIC.

MAILBOX SUMMARY

STATION	OFFSET	REMARKS	STATION	OFFSET	REMARKS
49+50	LT	SINGLE INSTALLATION	61+50	RT	SINGLE INSTALLATION
52+50	LT	" "	73+40	LT	" "
54+20	LT	" "	76+90	LT	DOUBLE INSTALLATION
61+30	LT	" "	82+40	LT	SINGLE INSTALLATION



NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

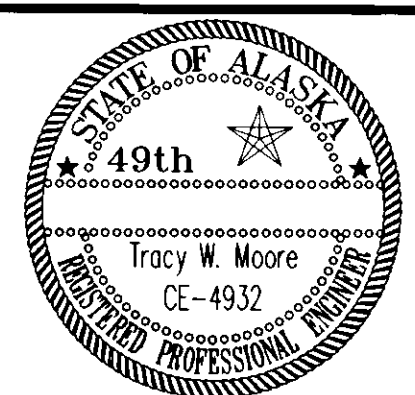
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BY:	DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
SOUTHEAST REGION DESIGN & CONSTRUCTION

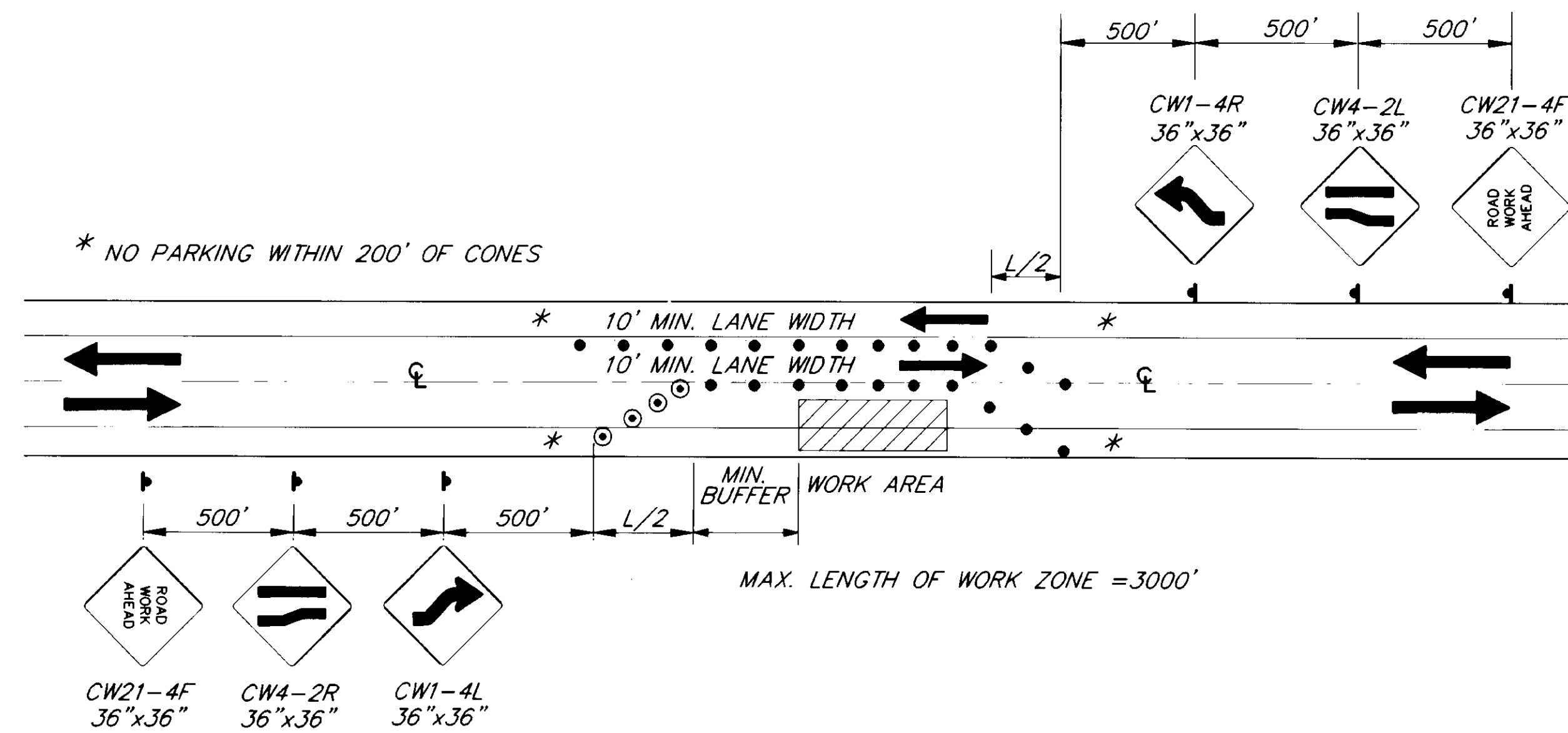
JUNEAU ENGINEER'S CUT-OFF TO FRITZ COVE ROAD
GRINDING AND RESURFACING ALASKA
NH-093-3(13) ~ 71808
ESTIMATE OF QUANTITIES, SUMMARIES

DESIGNED BY: F. MURPHY	PROJECT NO. 71808
DRAWN BY: K. SNYDER	DATE: APRIL 1995
CHECKED BY: T.W. MOORE	SHEET 3 OF 7



TRAFFIC CONTROL NOTES

1. TWO WAY LANE TRAFFIC SHALL BE MAINTAINED AT ALL TIMES, THROUGH ALL WORK AREAS.
2. TEMPORARY DRIVING LANES SHALL HAVE A MINIMUM WIDTH OF 10'-0".
3. CONSTRUCTION SIGNING SHALL BE IN PLACE ONLY WHEN THE CONDITIONS EXIST FOR WHICH THE SIGNS ARE INTENDED.
4. TEMPORARY PAVEMENT MARKINGS WILL BE REQUIRED AS DESCRIBED IN SECTION 643-3.04 OF THE SPECIFICATIONS.
5. CHANNELIZATION DEVICES IF USED AT NIGHT SHALL BE LIT IN ACCORDANCE WITH THE ALASKA TRAFFIC MANUAL.
6. DRIVEWAYS MAY BE CLOSED DURING ACTUAL WORK ON A GIVEN DRIVEWAY, PROVIDED THAT THE CLOSURE DOES NOT EXCEED 8 HOURS AND THE AFFECTED RESIDENTS HAVE BEEN GIVEN 24 HOURS NOTICE OF THE CLOSURE.
7. IT IS THE INTENT OF THIS TRAFFIC CONTROL PLAN (TCP) TO ILLUSTRATE SOME, NOT ALL, OF THE TRAFFIC CONTROL SETUPS WHICH WILL BE REQUIRED ON THIS PROJECT. PLANS FOR CONFIGURATIONS NOT COVERED BY THE TCP SHALL BE CREATED BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER FOR APPROVAL. WHERE APPROPRIATE, THEY SHALL INCORPORATE APPLICABLE PORTIONS OF DETAILS ON THESE SHEETS.
8. THIS PROJECT MAY BE UNDER CONSTRUCTION CONCURRENT WITH THE BROTHERHOOD BRIDGE TO ENGINEER'S CUTOFF PROJECT (70657). SHOULD THE BROTHERHOOD BRIDGE PROJECT BE ACTIVE AT THE START-UP OF THIS PROJECT, THE PERMANENT CONST. SIGNS G20-1 AND G20-2a SHALL BE MOVED UP TO THE ACTUAL BEGINNING OF THE PROJECT.



TWO-WAY TRAFFIC USING LEFT LANE

LEGEND

- SIGN
- CONE
- DRUM
- TYPE III BARRICADE
- FLAGGING STATION

FORMULAS FOR L (TAPER LENGTH)

40 MPH OR LESS $L = \frac{W \times S^2}{60}$

45 MPH OR GREATER $L = W \times S$

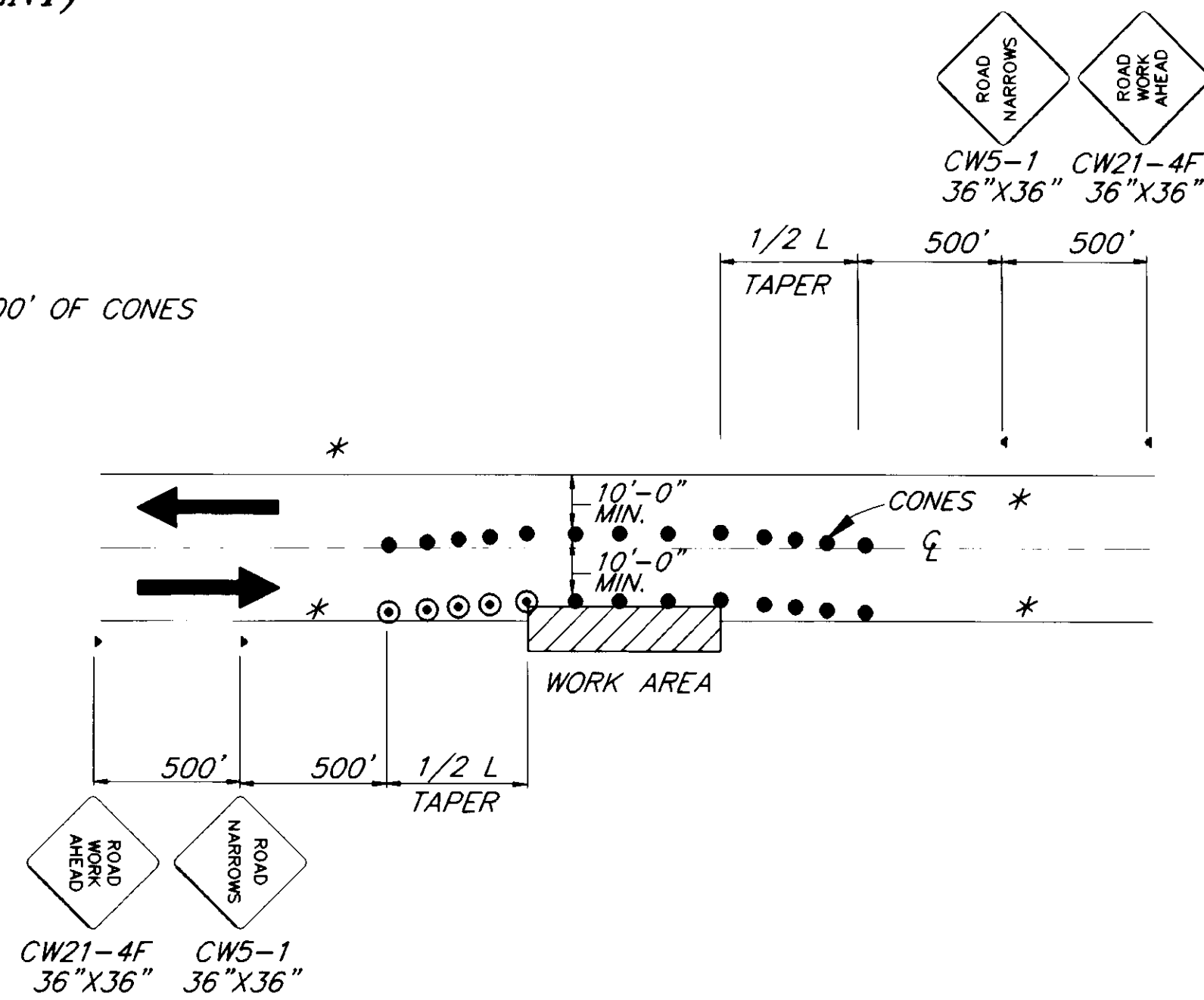
WHERE W = WIDTH OF OFFSET
S = POSTED SPEED LIMIT

DRUM OR CONE SPACING = S (IN FEET)

MIN. BUFFER S	LENGTH
20	35
25	55
30	85
35	120
40	170
45	220
50	280
55	335
60	415
65	485

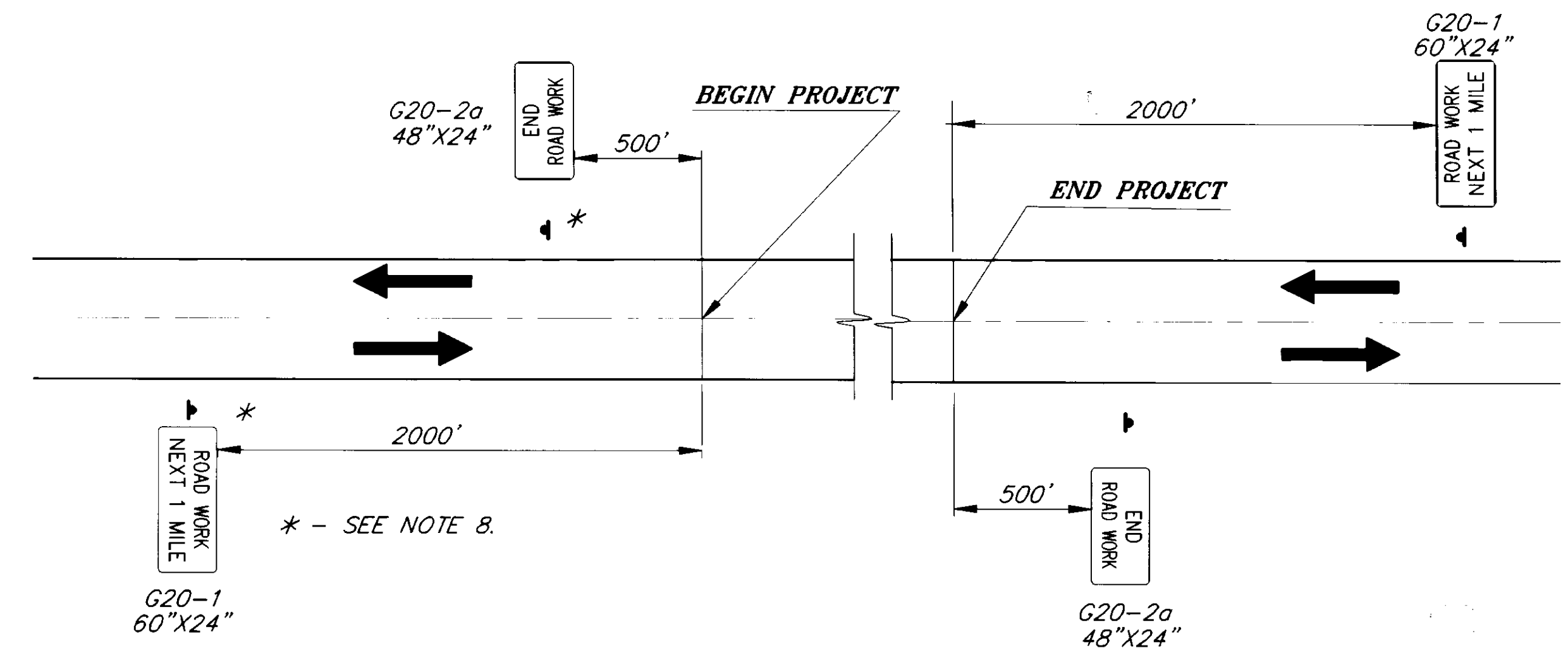
M-7 (ENCROACHMENT)

* NO PARKING WITHIN 200' OF CONES



ROADWAY ENCROACHMENT

NOTE: IF ONLY ONE LANE IS EFFECTED BY ROAD WORK (THAT IS, THE CONES ALONG THE WORK AREA ARE NO CLOSER THAN 10' TO CENTERLINE) THE CENTERLINE CONES FOR THE OPPOSING LANE MAY BE DELETED.



PERMANENT CONSTRUCTION SIGNING

AS-BUILT

BY: *B.G.* DATE: *1-16-97*

NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

RECORD OF REVISIONS		
NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
SOUTHEAST REGION DESIGN & CONSTRUCTION

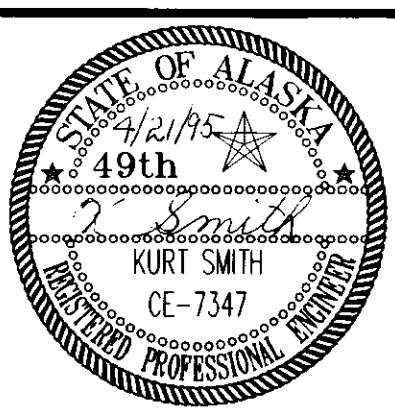
JUNEAU

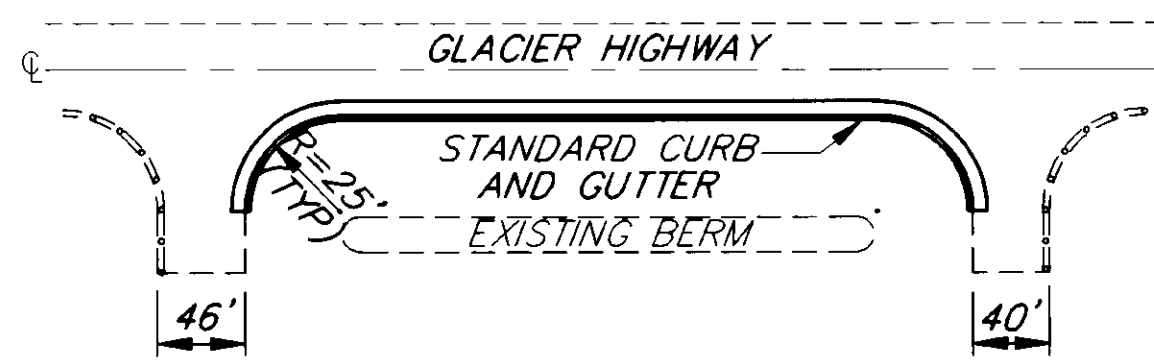
ENGINEER'S CUT-OFF TO FRITZ COVE ROAD
GRINDING AND RESURFACING
NH-093-3(13) ~ PROJECT NO. 71808

ALASKA

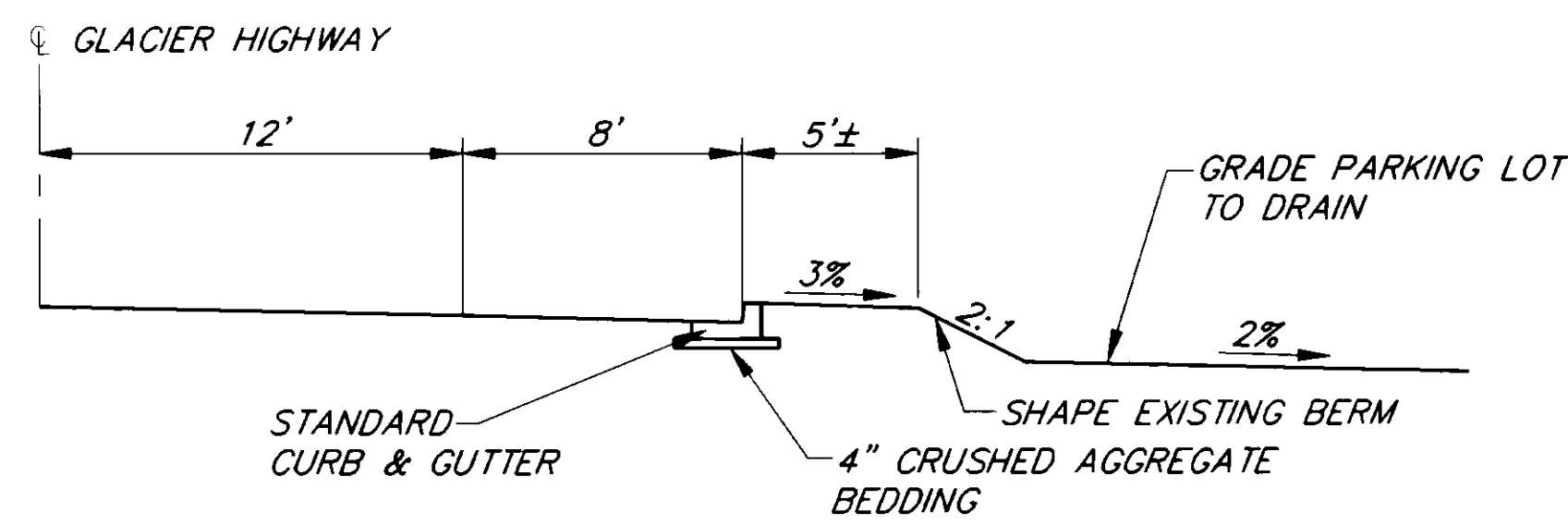
DESIGNED BY: KEN MATTSON
DRAWN BY: KEN MATTSON
CHECKED BY: KURT SMITH

PROJECT NO. 71808
DATE: APRIL 1995
SHEET 4 OF 7



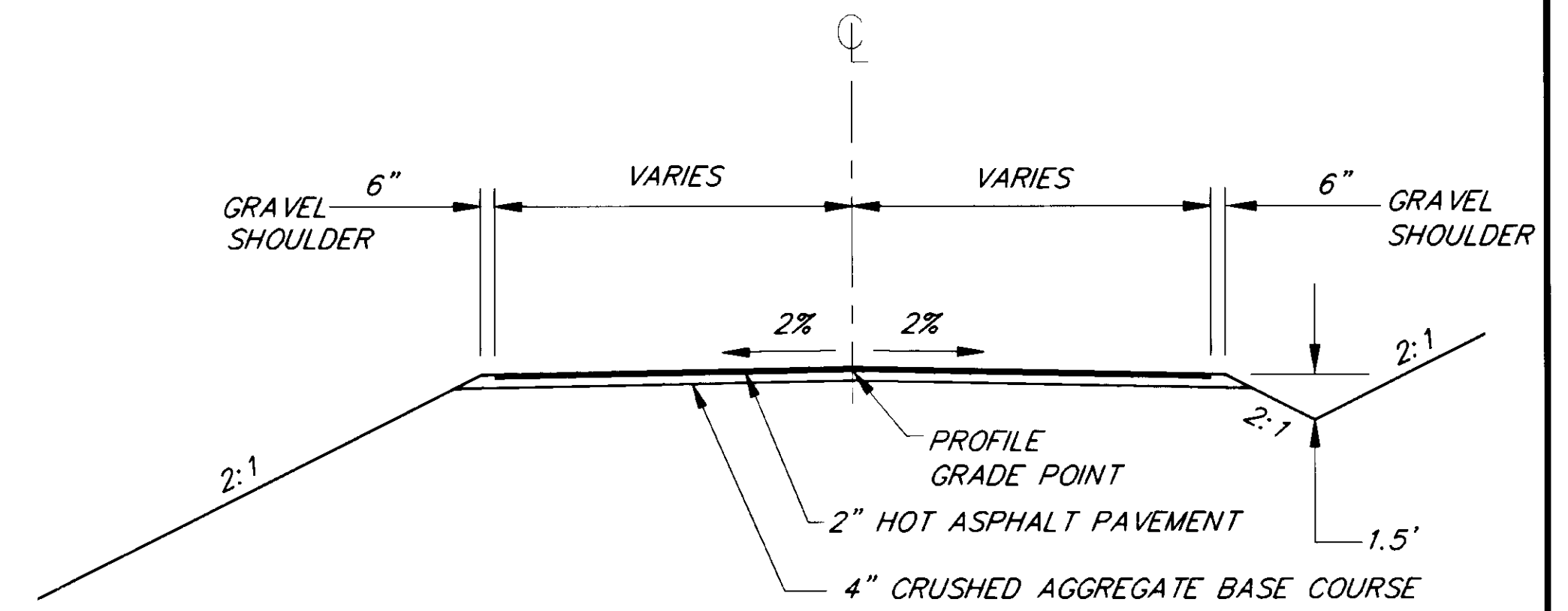


PLAN



TYPICAL SECTION

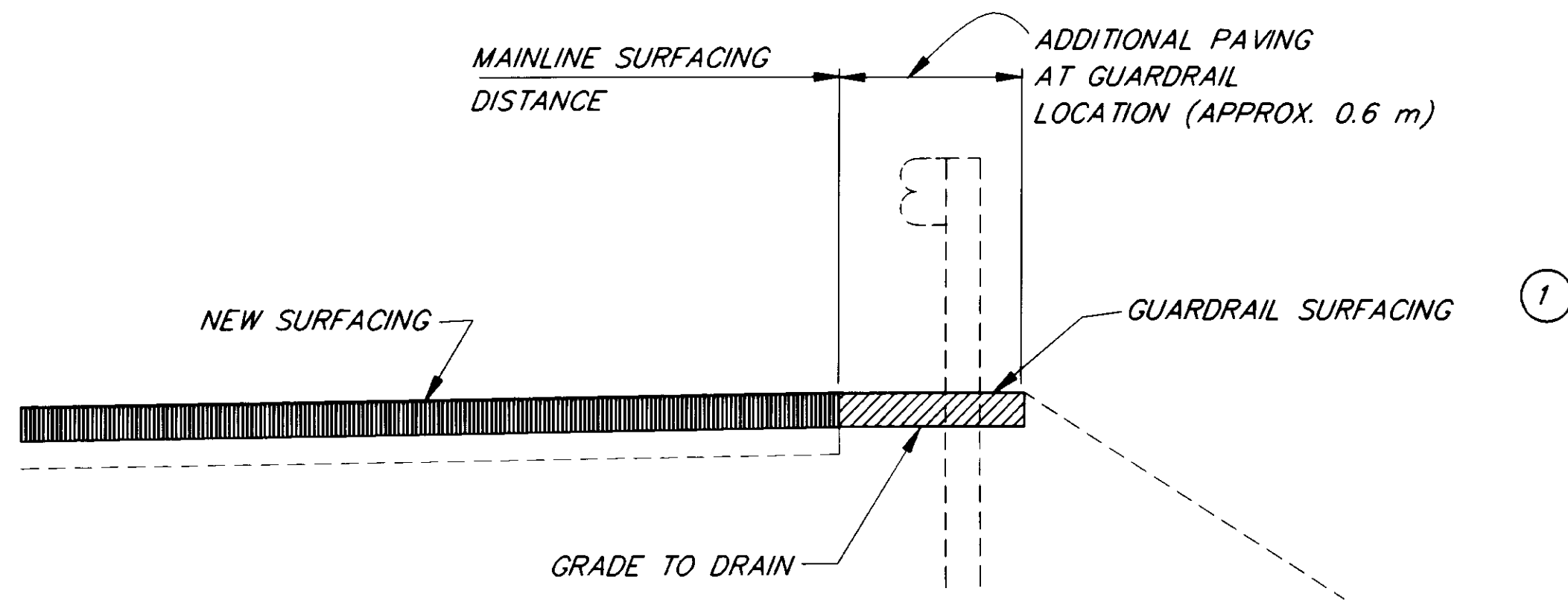
INTERSECTION DETAILS
AUKE LAKE PARKING LOT



TYPICAL SECTION
APPROACH/DRIVEWAY

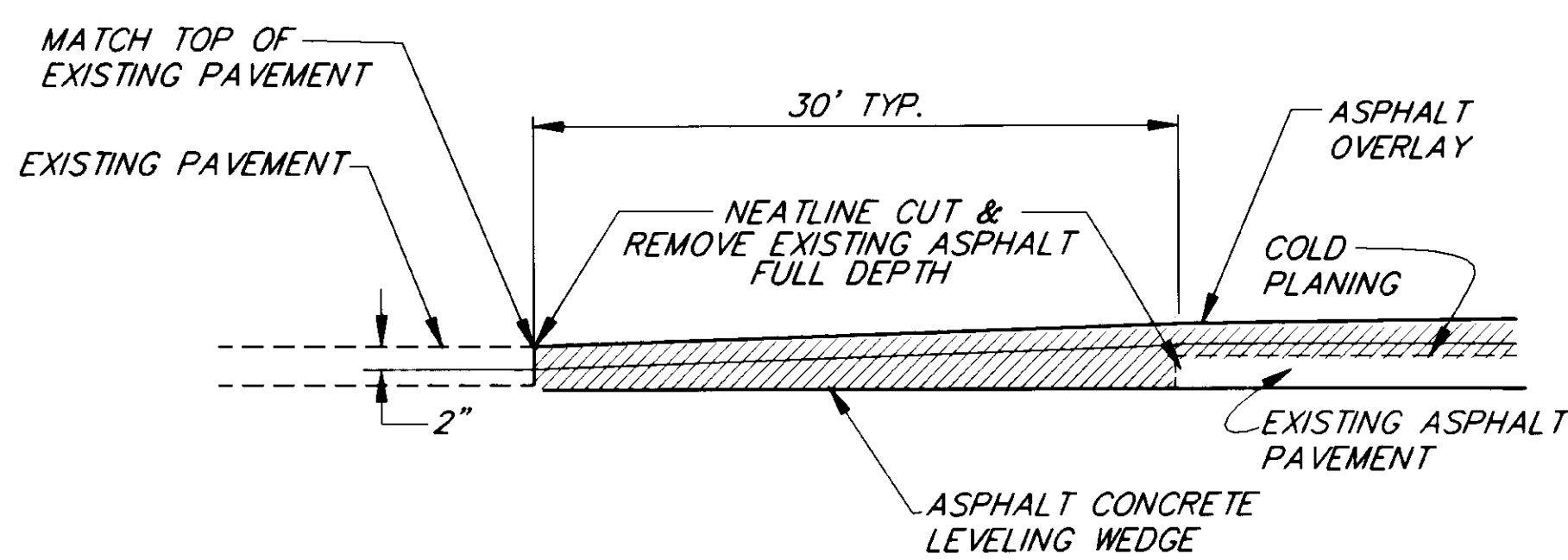
APPROACH NOTES

1. THIS DETAIL APPLIES TO THE APPROACHES SHOWN IN THE DRIVEWAY SUMMARY ON SHEET 3. ALL OTHER APPROACHES WITH EXISTING PAVEMENT SHALL UTILIZE A TAPERED GRINDING BUTT JOINT.
2. PAVING LIMIT SHALL BE 20' BEYOND THE EDGE OF PAVEMENT.



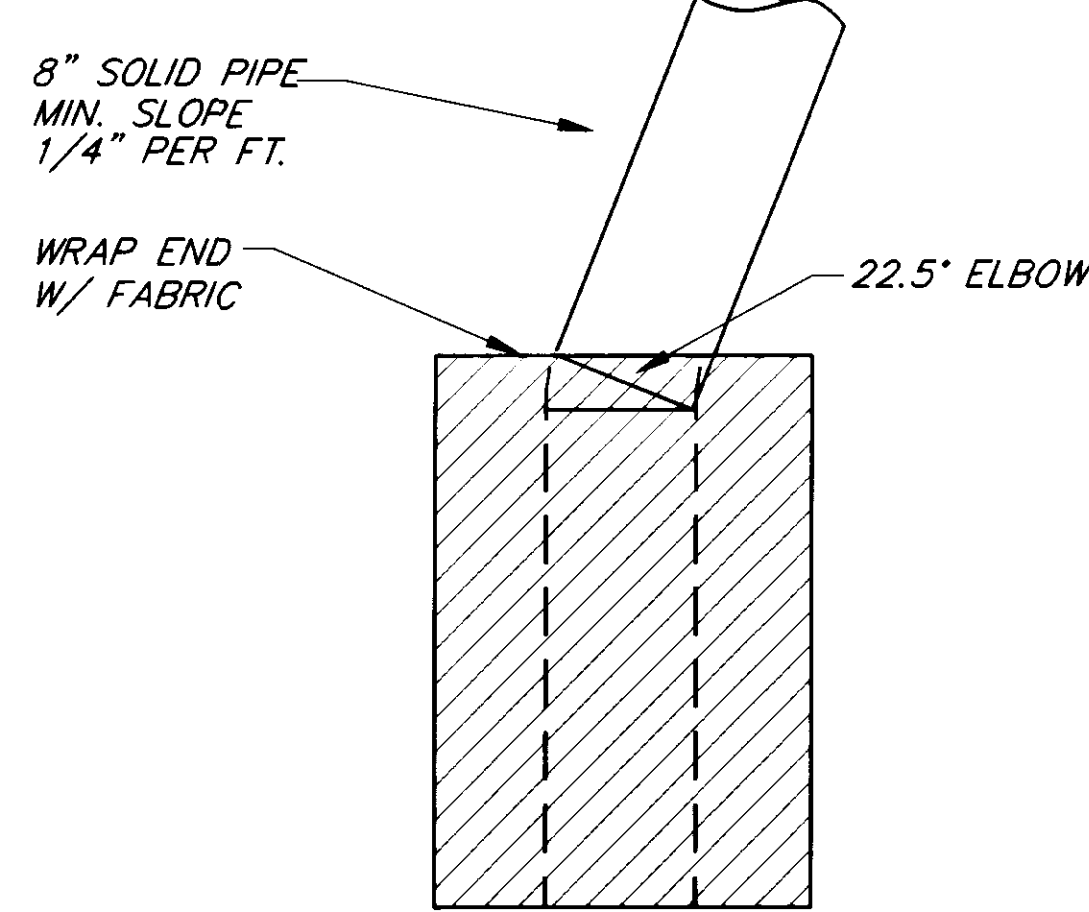
GUARDRAIL PAVING DETAIL

① GUARDRAIL SURFACING SHALL BE 2 INCHES OF HOT ASPHALT CONCRETE.

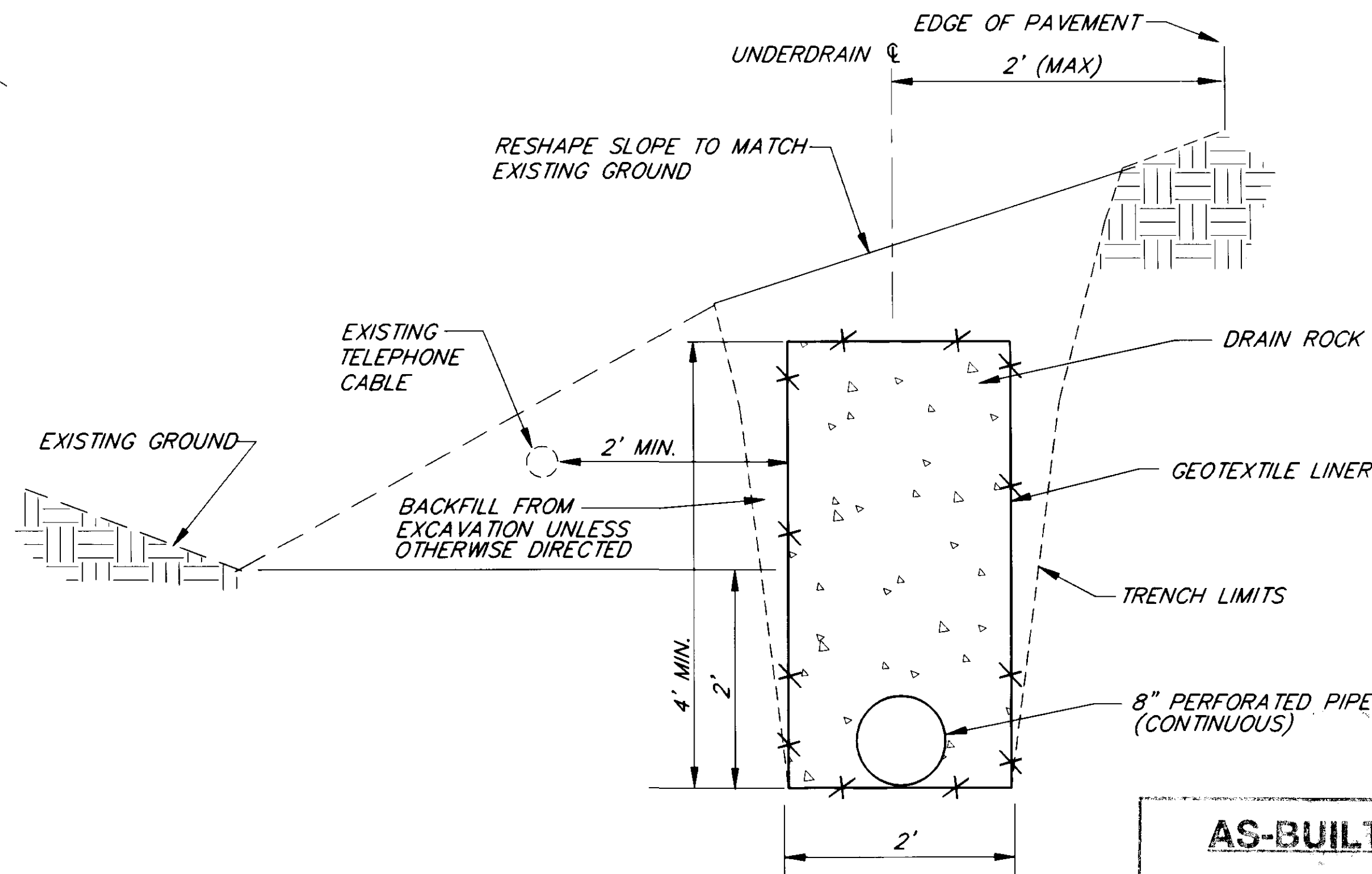


PAVEMENT MATCH JOINT

NOTE: A GROUND-IN TRANSITION MAY BE SUBSTITUTED, SUBJECT TO THE APPROVAL OF THE ENGINEER.



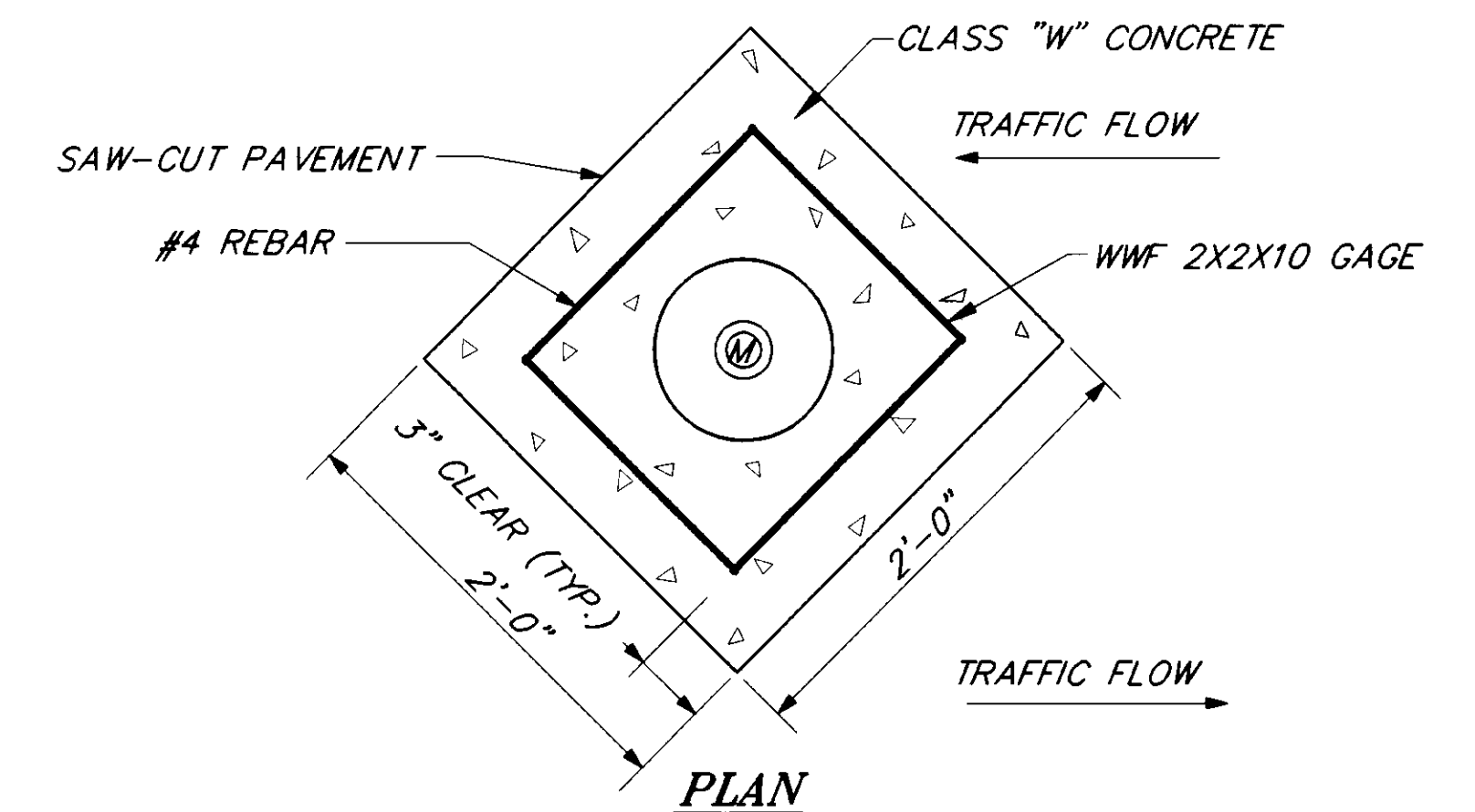
TOP VIEW



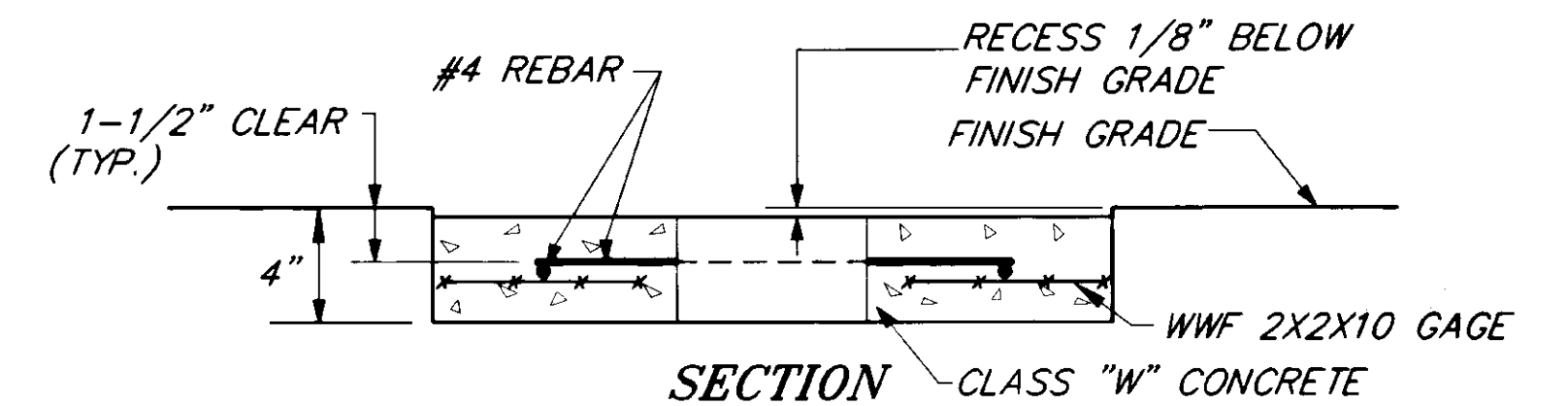
SECTION VIEW

MODIFIED UNDERDRAIN DETAIL

SEE SUMMARY TABLE, SHEET 3 FOR INSTALLATION LOCATIONS



PLAN



SECTION

MONUMENT ENCASEMENT DETAIL

ALTERNATE METHOD:
PAVE AROUND AND SEAL EDGE WITH RUBBERIZED ASPHALT FC MASTIC THAT HAS BEEN GROUND INTO ASPHALT CONCRETE 1" AROUND METAL FLANGE. CONTRACTOR TO SUBMIT DETAIL FOR APPROVAL PRIOR TO CONSTRUCTION.

AS-BUILT
BY: B.A. DATE: 1-6-97

NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

PATH: P:\JNU\71808\DR\—DET1 PLOT1.PCP(1) OR PLOT1.PCP(2)		
BY:	DATE:	DESCRIPTION OF CHANGE:
RECORD OF REVISIONS		

STATE OF ALASKA
DEPARTMENT OF TRANSPORTATION
AND PUBLIC FACILITIES
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU

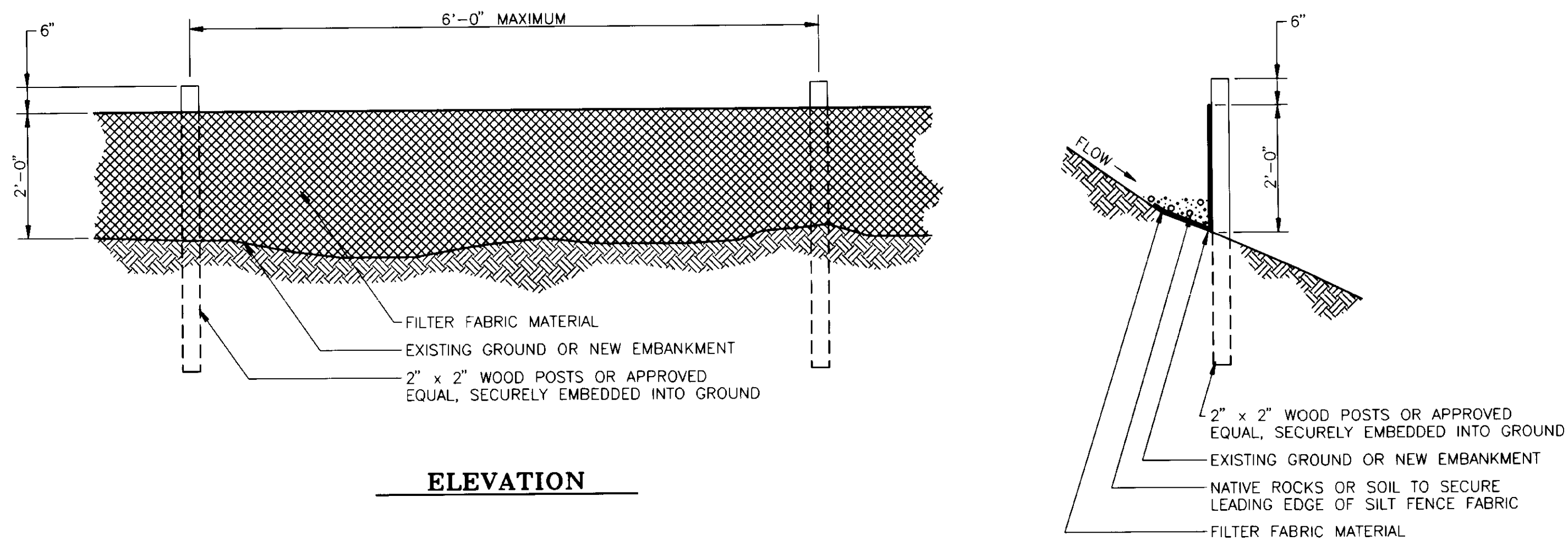
ENGINEER'S CUT-OFF TO FRITZ COVE ROAD
GRINDING AND RESURFACING
NH-093-3(13) ~ 71808
MISCELLANEOUS DETAILS

ALASKA

DESIGNED BY:
F. MURPHY
DRAWN BY:
K. SNYDER
CHECKED BY:
T.W. MOORE

PROJECT NO.
71808
DATE:
APRIL 1995
SHEET 5 OF 7





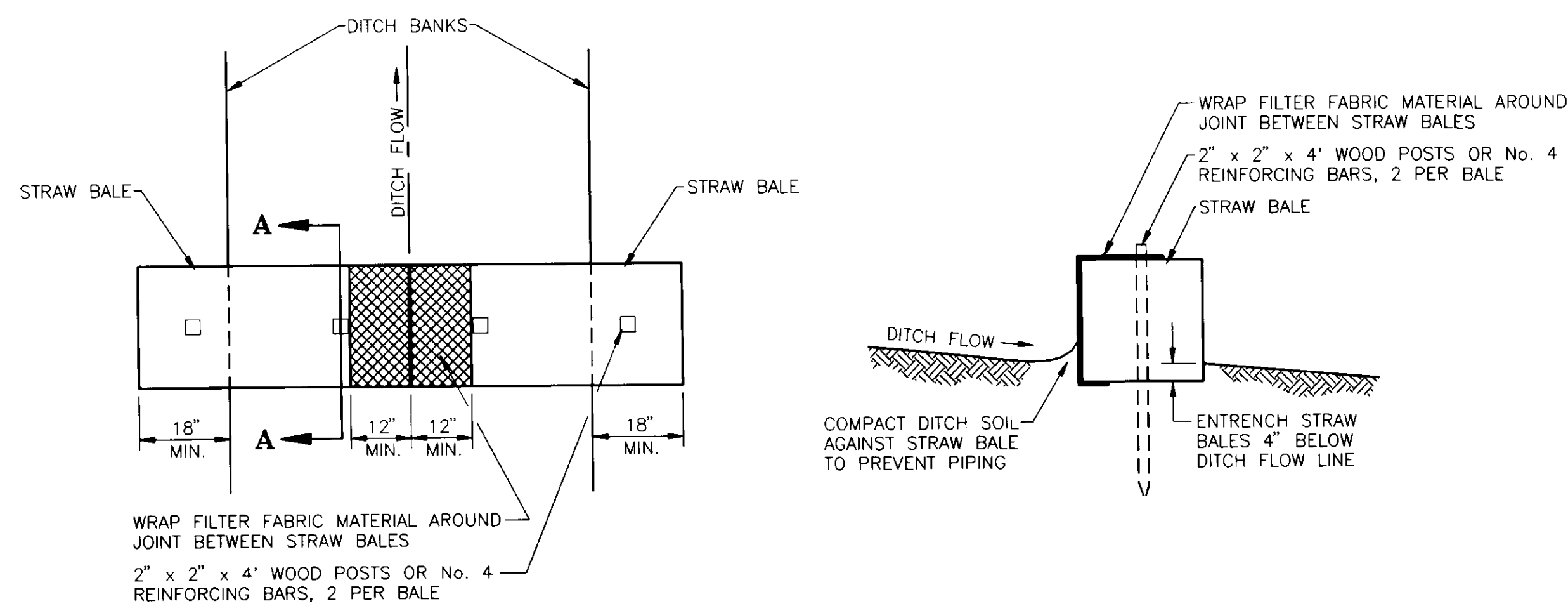
ELEVATION

SECTION

SILT FENCE DETAILS

SILT FENCE SUMMARY				
ASBUILT STATION TO	ASBUILT STATION	OFFSET	L.F. REQ'D.	REMARKS
65+50	66+50	RT.	100	FACE OF VEGETATION
72+30	76+50	RT.	420	FACE OF VEGETATION

NOTE:
 PETERSON HILL CREEK, AUKE CREEK AND AUKE LAKE ARE CONSIDERED TO BE SENSITIVE ENVIRONMENTALLY AND CAUTION SHALL BE USED TO ENSURE THAT NO CONTAMINATED RUN-OFF ENTERS THESE WATERWAYS.



PLAN

SECTION A-A

TYPICAL STRAW BALE SEDIMENT CONTROL FILTER INSTALLATION DETAILS

STRAW BALE SUMMARY			
ASBUILT STATION	OFFSET	No. BALES REQUIRED	REMARKS
50+00	RT.	2	IN DITCH
50+00	LT.	2	INLET OF CMP
54+50	LT.	2	IN DITCH
54+50	RT.	2	IN DITCH
57+60	RT.	2	INLET OF CMP
69+50	RT.	2	END OF DITCH
70+00	LT.	2	IN DITCH
70+63	LT.	2	INLET OF CMP
75+40	LT.	2	INLET OF CMP
75+40	RT.	2	IN CREEK THAT EXITS CMP
79+10	LT.	2	INLET OF CMP
83+70	LT.	2	IN DITCH
86+30	LT.	2	IN DITCH

AS-BUILT
 BY: B.A. DATE: 1-6-97

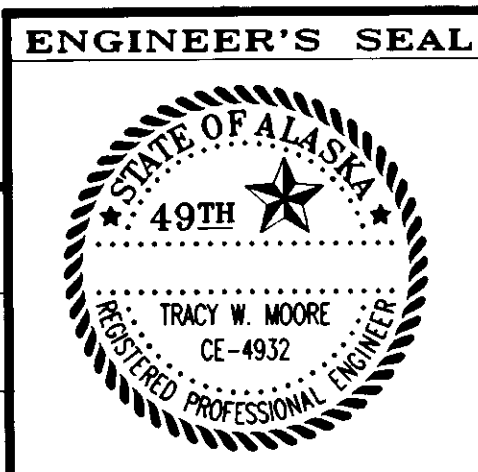
RECORD OF REVISIONS		
BY:	DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA
 DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES
 SOUTHEAST REGION DESIGN & CONSTRUCTION

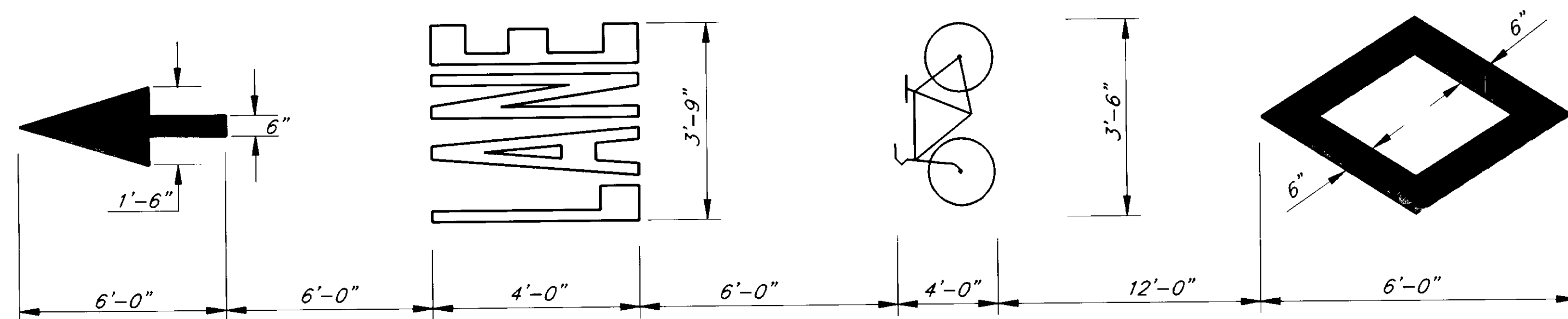
JUNEAU ALASKA
 ENGINEER'S CUT-OFF TO FRITZ COVE ROAD
 PROJECT NO. NH-093-3(13) / 71808
POLLUTION CONTROL DETAILS

NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

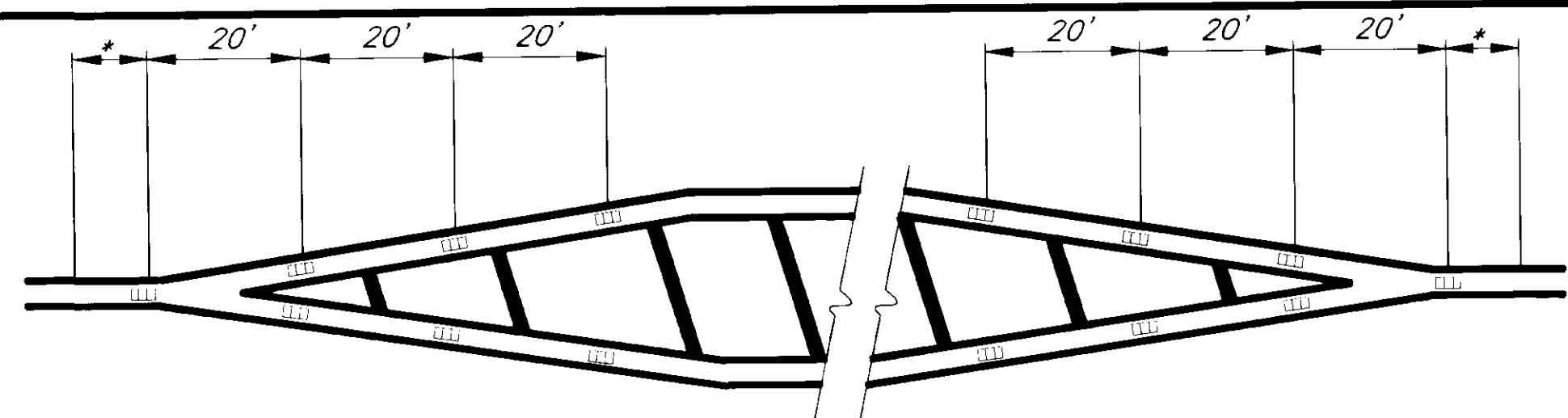
DESIGNED BY: <u>D. BLACKBURN</u>	PROJECT NO. <u>71808</u>
DRAWN BY: <u>SIERRA CLUBBERS</u>	DATE: <u>APRIL, 1995</u>
CHECKED BY: <u>T. MOORE</u>	SHEET <u>7 OF 7</u>



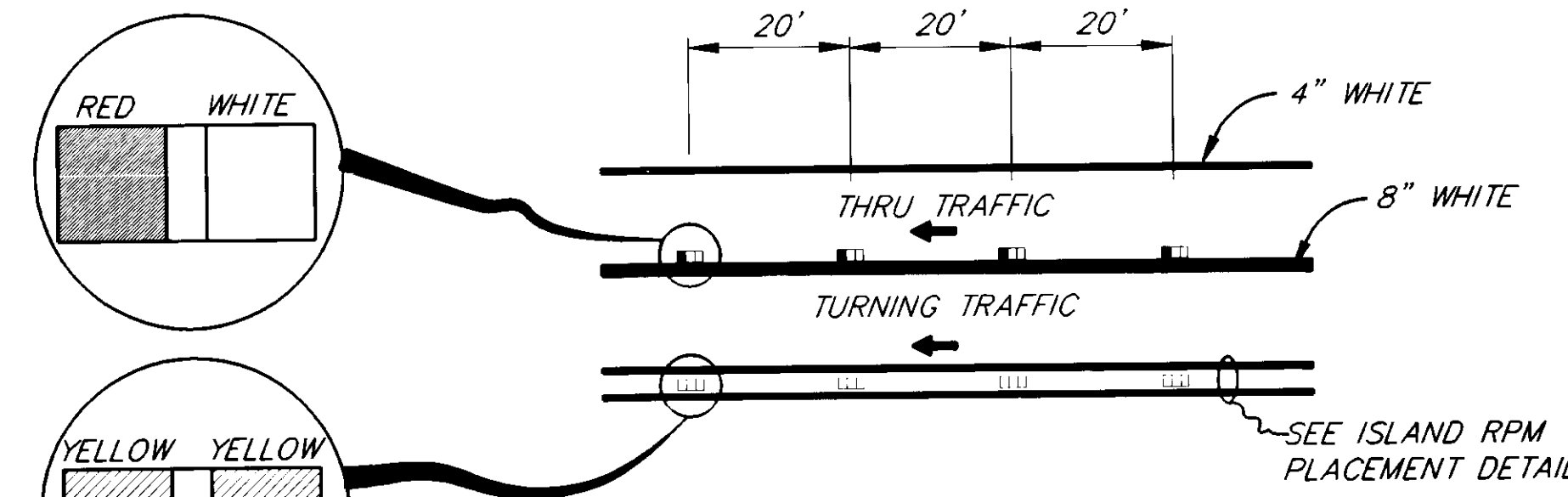
* CONTINUE 20' SPACING IN ALL DOUBLE YELLOW LINES.



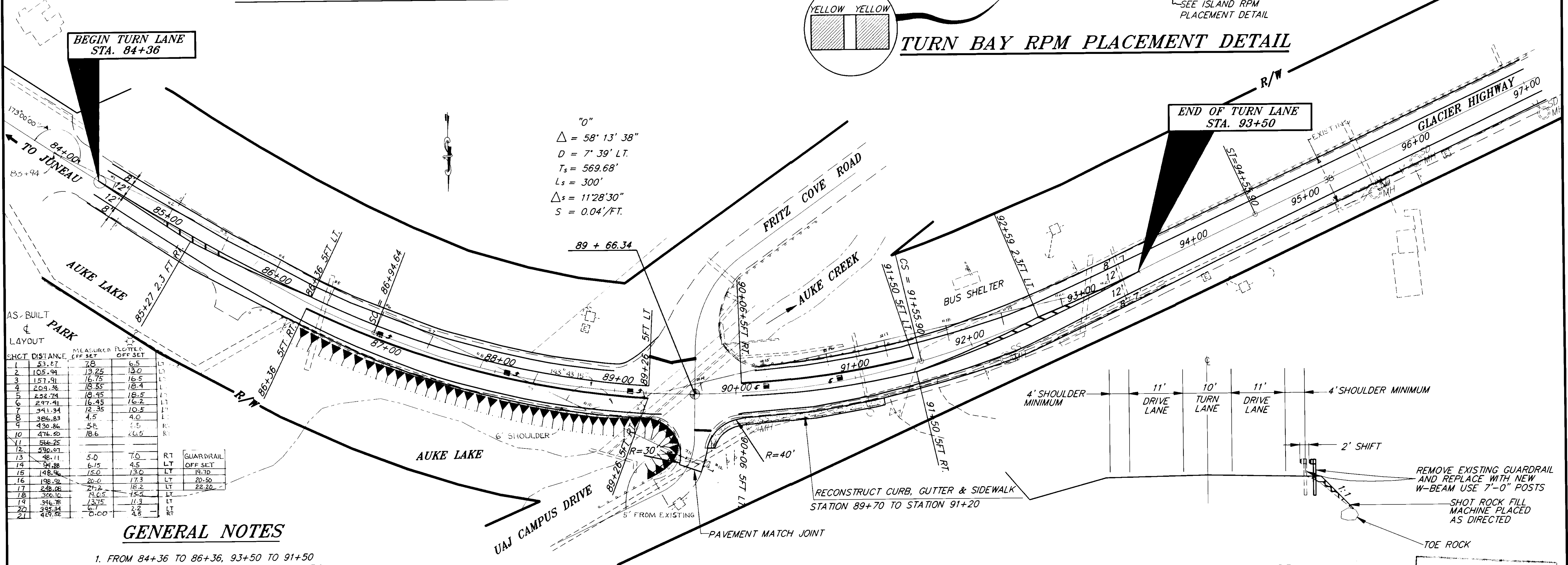
DETAIL PAINTED BIKE LANE



ISLAND RPM PLACEMENT DETAIL



TURN BAY RPM PLACEMENT DETAIL



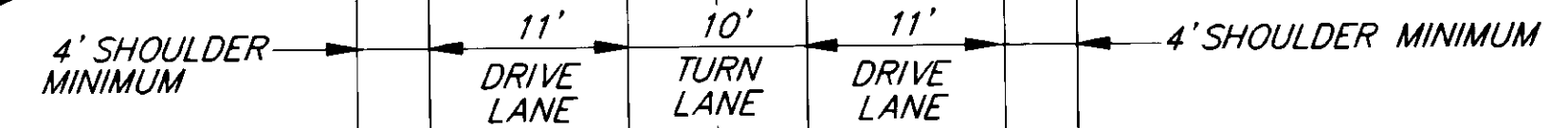
"0"
 $\Delta = 58^\circ 13' 38''$
 $D = 7' 39''$ LT.
 $T_s = 569.68'$
 $L_s = 300'$
 $\Delta s = 11' 28' 30''$
 $S = 0.04'/FT.$

AS-BUILT LAYOUT

SHCT	DISTANCE	MEASURED OFF SET	PLOTTED OFF SET	TYPE
1	53.87	7.8	6.5	LT
2	105.99	19.25	13.0	LT
3	157.91	16.75	16.5	LT
4	209.36	15.55	18.4	LT
5	252.74	19.45	18.5	LT
6	297.41	16.45	16.2	LT
7	341.34	12.35	10.5	LT
8	386.83	4.5	4.0	LT
9	430.86	5.8	4.5	LT
10	476.50	18.6	14.5	LT
11	526.25			
12	570.07			
13	611	5.0	7.0	RT
14	648.88	6.15	4.5	LT
15	688.96	15.0	13.0	LT
16	729.02	20.0	17.3	LT
17	770.08	27.2	18.2	LT
18	810.10	14.05	15.2	LT
19	846.38	13.75	11.2	LT
20	885.34	5.7	2.2	LT
21	925.30	0.00	4.5	RT

GENERAL NOTES

- FROM 84+36 TO 86+36, 93+50 TO 91+50 THE LANE WIDTH SHALL SMOOTHLY TRANSITION FROM 12' TO 11' AND THE SHOULDER SHALL SMOOTHLY TRANSITION FROM 8' TO 4'.
- SIDEWALK SHALL BE 1 1/2" ASPHALT.



TYPICAL SECTION

REMOVE EXISTING GUARDRAIL AND REPLACE WITH NEW W-BEAM USE 7'-0" POSTS
 SHOT ROCK FILL MACHINE PLACED AS DIRECTED
 TOE ROCK

AS-BUILT
 BY: B.D. DATE: 1-6-97

NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

PATH: P:\JNU\71808\DR\PROP7A < PLOT.PCP(40) OR PLOT.PCP(80) >

BY:	DATE:	DESCRIPTION OF CHANGE:

RECORD OF REVISIONS

STATE OF ALASKA
 DEPARTMENT OF TRANSPORTATION
 AND PUBLIC FACILITIES
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU ENGINEER'S CUTOFF TO FRITZ COVE ROAD
 GRINDING & RESURFACING
 NH-093-3(13) ~ PROJECT NO. 71808
LEFT TURN LANES-FRITZ COVE RD. INTER.

DESIGNED BY: D. BLACKBURN	PROJECT NO. 71808
DRAWN BY: K. KLEMMETSON	DATE: MAY, 1995
CHECKED BY: T.W. MOORE	SHEET 7A OF 7

