

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION  
 DESIGN AND CONSTRUCTION DIVISION  
*Juneau, Alaska*

**NORTH DOUGLAS HIGHWAY  
 HELIPORT TO EAGLECREST  
 ROAD IMPROVEMENTS**

PROJECT NO. STP-0959(16) 71108  
 DRAINAGE, GRADING, & PAVING

INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	ESTIMATE OF QUANTITIES, AND TYPICAL SECTION
3	SUMMARY TABLES
4	MISCELLANEOUS DETAILS
5	TRAFFIC CONTROL PLAN
6-8	PLAN SHEETS
9	DRIVEWAY SUMMARY

The following Standard Drawings apply to this project :

A-1	C-01.03	C-02.01	C-03.01	D-04.10
D-05.10	G-04.04W	G-14.04W	G-18.00W	M-20.10
M-23.10	S-00.00	S-05.00	S-20.00	S-30.01
T-21.01				

**"AS - BUILT" PLANS**

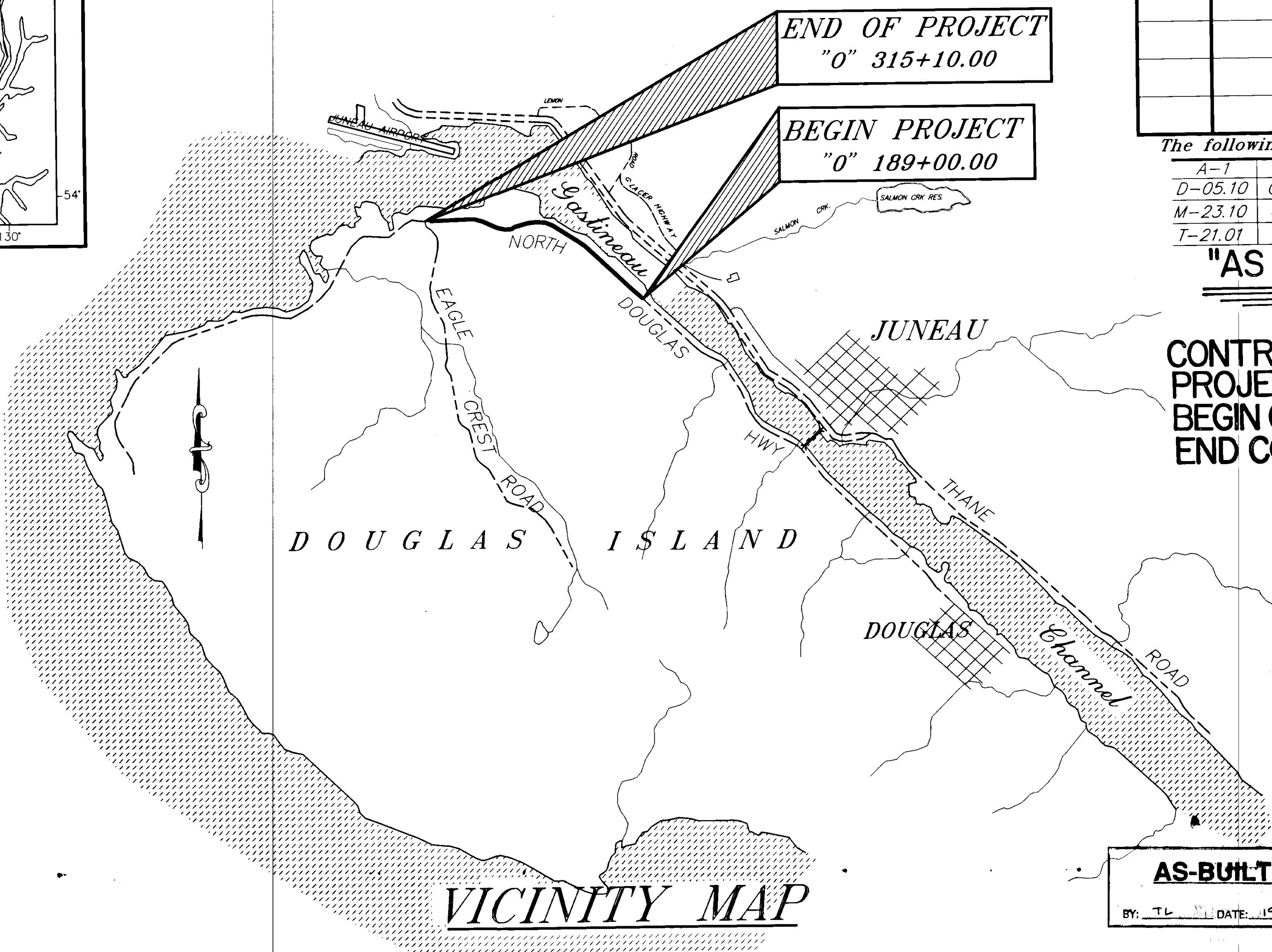
CONTRACTOR: Red Samm Const. Inc.,  
 PROJECT ENGINEER: Larry Geise  
 BEGIN CONSTRUCTION: 8-1-93  
 END CONSTRUCTION: 10-30-93

**DESIGN DESIGNATION**

ADT 1994 .....	1,160
ADT 2004 .....	1,410
DHV 14% .....	197
% T .....	5 %
V .....	55 M.P.H.
EAL (10 YR.) .....	69,130

**DESIGN DATA**

WIDTH OF PAVEMENT .....	34.0'
LENGTH OF PAVEMENT .....	12,610' (2.388 MI.)
LENGTH OF PROJECT .....	12,610' (2.388 MI.)



**VICINITY MAP**

**AS-BUILT**  
 BY: JL DATE: 1996

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN SECTION

APPROVED: [Signature] Date: 6/3/93  
 Regional Preconstruction Engineer

APPROVED: [Signature] Date: 6/3/93  
 Director, S.E. Region Design & Construction

PROJECT NUMBER: <b>71108</b>	ENGINEER'S SEAL
DATE: 5/28/93	
SHEET 1 OF 9	

P:\UNU\71108\93\PLANSET\1 SHEET  
 AS-BUILT, TL, 1996

# ESTIMATE OF QUANTITIES

ITEM NO.	ITEM	UNIT	TOTAL
120(1)	DBE ADJUSTMENT	CS	ALL REQ'D
201(1B)	CLEARING (APPROX. 7.8 ACRES)	L.S.	ALL REQ'D
202(4)	REMOVAL OF CULVERT PIPE	LF	2,470
202(10)	SINGLE MAIL BOX INSTALLATION	EACH	75
202(12)	MULTIPLE MAIL BOX INSTALLATION	EACH	6
203(11)	LINEAR GRADING	STATION	126
203(12)	DITCH RECONDITIONING	STATION	84.22
<b>Subtotal</b>			
301(1)	CRUSHED AGGREGATE BASE COURSE	TON	5804.78
304(1)	SUBBASE, GRADING "E"	TON	2403
401(1)	ASPHALT CONCRETE, TYPE II, CLASS "B"	TON	5364.87
401(2)	PBA-2 ASPHALT CEMENT	TON	610.01
401(6)	ASPHALT PRICE ADJUSTMENT	CS	ALL REQ'D
402(2)	STE-1 TACK COAT	TON	17.57
407(1)	ASPHALT LEVELING COURSE	TON	4016.49
603(9-24)	24-INCH CORRUGATED ALUMINUM PIPE	LF	16.08
603(9-36)	36-INCH CORRUGATED ALUMINUM PIPE	LF	314
603(17-18)	18-INCH PIPE	LF	985
606(1)	W-BEAM GUARDRAIL	LF	1087.5
606(5)	REMOVAL & DISPOSAL OF GUARDRAIL	LF	475
606(6)	END ANCHORAGES	EACH	6
611(3)	RIPRAP	LS	ALL REQ'D
614(4)	ADJUST EXISTING MONUMENT CASES	EACH	12
615(1)	STANDARD SIGNS	SF	68.9
618(1)	SEEDING	ACRE	6.0
<b>Subtotal</b>			
632(1)	GEOTEXTILE, PAVING	SY	3304
639(3)	APPROACHES	EACH	135
640(1)	MOBILIZATION AND DEMOBILIZATION	LS	ALL REQ'D
641(1)	EROSION AND POLLUTION CONTROL-ADMINISTRATION	LS	ALL REQ'D
641(2)	EROSION AND POLLUTION CONTROL-MANPOWER	M.H.	8.5
641(3)	EROSION AND POLLUTION CONTROL	CS	ALL REQ'D
642(1)	CONSTRUCTION SURVEYING	LS	ALL REQ'D
643(2)	TRAFFIC MAINTENANCE	LS	ALL REQ'D
643(3)	PERMANENT CONSTRUCTION SIGNING	LS	ALL REQ'D
643(4)	CONSTRUCTION SIGN	EACH/DAY	378
643(7)	TRAFFIC CONE	EACH/DAY	1993
643(8)	DRUM	EACH/DAY	642
643(13)	TEMPORARY PAVEMENT MARKING	STATION	293
643(15)	FLAGGING	STATION	11969.5
670(1)	PAINTED TRAFFIC MARKINGS	LS	ALL REQ'D
670(8)	RECESSED PAVEMENT MARKERS	EACH	175

## GENERAL NOTES:

- HORIZONTAL STATIONING BASED ON THE CENTERLINE P.C. MONUMENT APPROXIMATELY 900' BEYOND THE HELIPORT ENTRANCE BEING STATION "0" 191+55.94.
- APPROACH, MAILBOX, FIRE HYDRANT, GUARDRAIL, AND SIGN LOCATIONS ARE APPROXIMATE ONLY, AND ARE SUBJECT TO MINOR FIELD REVISIONS.
- GEOTEXTILE LOCATIONS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS WILL BE DETERMINED BY THE PROJECT ENGINEER. GEOTEXTILE SHALL BE PLACED JUST PRIOR TO THE FINAL COURSE.
- THE CONTRACTOR SHALL LIMIT THE LINEAR GRADING TO A LENGTH THAT CAN BE BACKFILLED AND COMPACTED BY THE END OF THE SHIFT. NO DROP-OFF AT THE EDGE OF PAVEMENT SHALL EXCEED 2 INCHES DURING NON-WORKING HOURS.
- TACK COAT SHALL BE APPLIED TO EXISTING PAVEMENT PRIOR TO LEVELING COURSE. TACK COAT APPLICATION BETWEEN THE LEVELING COURSE AND FINAL LIFT SHALL BE AT THE DISCRETION OF THE ENGINEER.
- THE ADJUSTMENT OF A VALVE BOX LEFT OF STATION 191+10 AND 3 VALVE BOXES LEFT OF STATION 256+09 SHALL BE CONSIDERED INCIDENTAL TO 203(11), LINEAR GRADING.

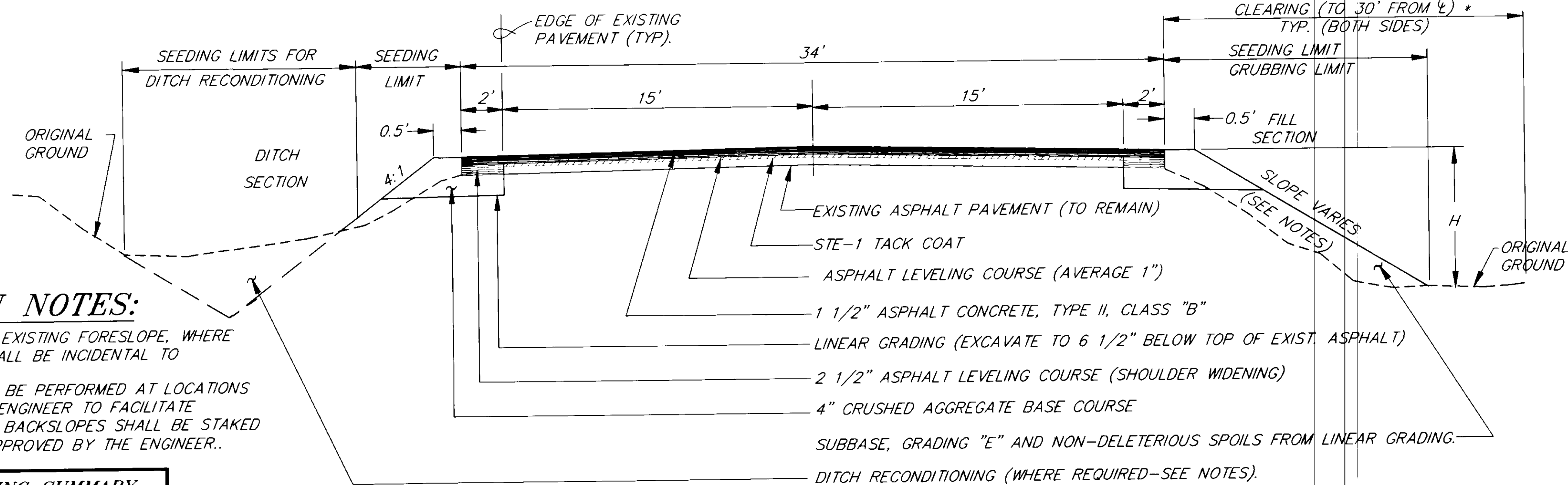
p:\jnu\7108#93\PLANS\EST\EST 1=4

BY:	DATE:	DESCRIPTION OF CHANGE:
TL	1996	AS-BUILT WITH CORRECTIONS

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
NORTH DOUGLAS HIGHWAY  
HELIPORT TO EAGLECREST ROAD IMPROVEMENTS  
PROJECT NO. STP-0959(16) 71108

## TYPICAL SECTION AND ESTIMATE OF QUANTITIES



## DITCH SECTION NOTES:

- REMOVAL OF VEGETATION ON EXISTING FORESLOPE, WHERE REQUIRED TO PLACE FILL, SHALL BE INCIDENTAL TO LINEAR GRADING.
- DITCH RECONDITIONING SHALL BE PERFORMED AT LOCATIONS DEEMED NECESSARY BY THE ENGINEER TO FACILITATE DRAINAGE. DITCH CUTS AND BACKSLOPES SHALL BE STAKED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER.

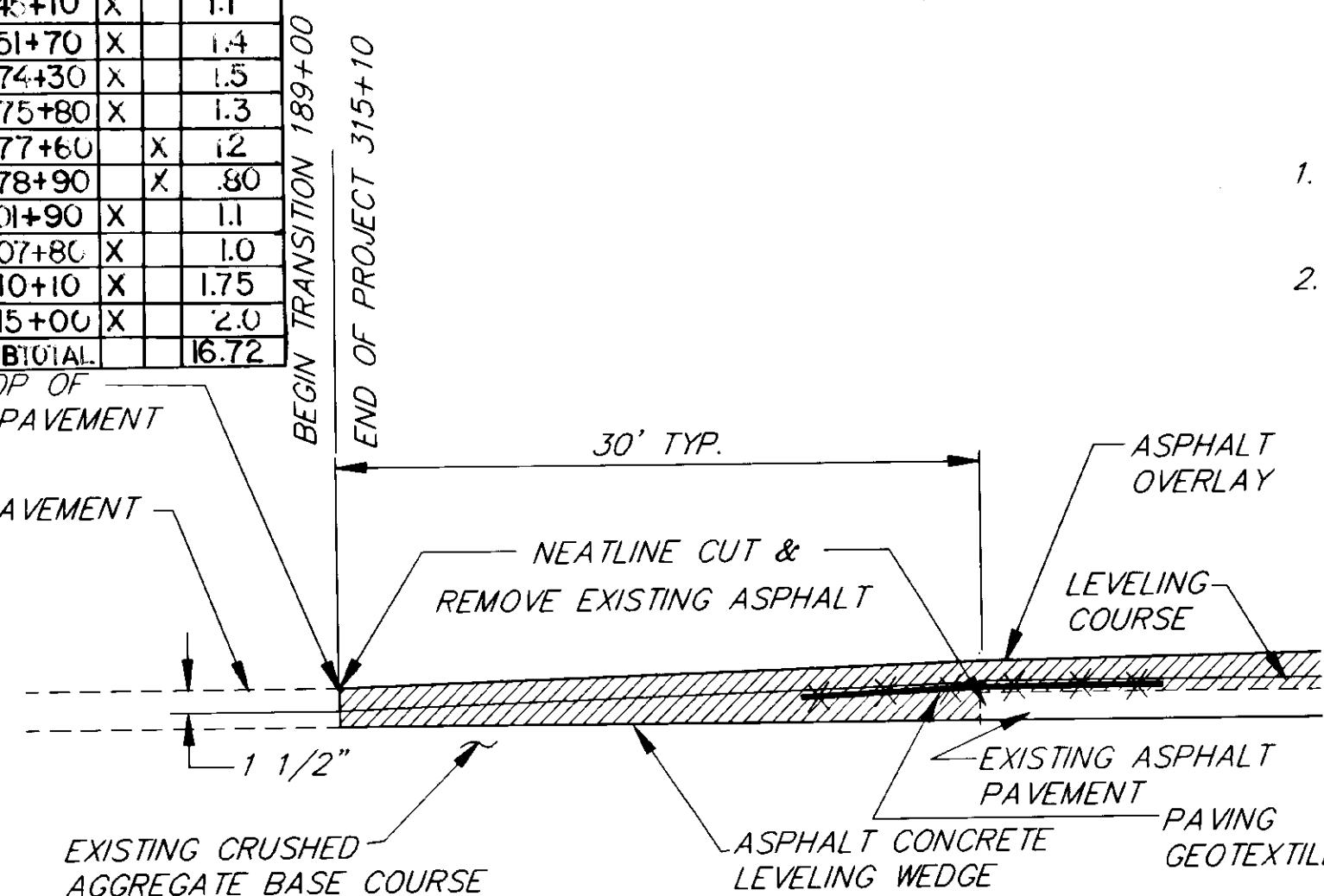
## DITCH RECONDITIONING SUMMARY

FROM STA.	TO STA.	LT.	RT.	STA.	FROM STA.	TO STA.	LT.	RT.	STA.	
190+00	191+90	X		1.9	214+50	215+50	X		1.0	
192+80	193+50	X		0.7	232+37	232+54	X		4.7	
194+50	195+00	X		0.5	233+20	233+50	X		4.0	
197+00	198+80	X		1.8	238+30	238+50	X		2.0	
200+00	202+10	X		2.1	242+00	243+50	X		1.5	
202+40	204+90	X		2.5	244+00	245+10	X		1.1	
207+00	208+00	X		1.0	250+30	251+70	X		1.4	
211+00	211+90	X		0.9	272+80	274+30	X		1.5	
212+40	213+00	X		0.6	274+50	275+80	X		1.3	
216+20	217+00	X		0.8	276+40	277+60	X		1.2	
217+50	218+00	X		0.5	278+10	278+90	X		0.80	
218+30	219+30	X		1.0	300+80	301+90	X		1.1	
221+00	221+70	X		0.7	306+80	307+80	X		1.0	
223+80	224+50	X		0.7	308+35	310+10	X		1.75	
225+20	228+60	X		3.4	313+00	315+00	X		2.0	
(A) 225+40	227+00		X	1.6	SUBTOTAL					16.72
227+50	228+50		X	1.0	MATCH TOP OF EXISTING PAVEMENT					
229+30	229+60	X		0.3	BEGINNING AND END OF PROJECT					
233+50	235+00	X		1.5	NOTE: A GROUND-IN TRANSITION MAY BE SUBSTITUTED, SUBJECT TO THE APPROVAL OF THE ENGINEER.					
235+80	236+20	X		0.4						
237+70	238+30	X		0.6						
237+70	239+70	X	X	2.0						
239+10	239+50	X		0.4						
242+10	243+50	X	X	1.4						
243+80	245+00	X	X	1.2						
246+00	246+20	X		0.2						
246+60	248+50	X	X	1.9						
247+00	249+00	X		2.0						
249+50	249+90	X		0.4						
251+60	252+80	X		1.2						
251+70	255+00	X	X	3.3						
253+50	255+10	X		1.6						
256+20	257+50	X		1.3						
258+10	260+50	X		2.4						
(B) 261+90	263+60	X		1.7						
263+90	264+50	X		0.6						
265+30	265+90	X		0.6						
266+50	272+50	X		6.0						
275+90	277+60	X		1.7						
277+90	278+40	X		0.5						
278+80	279+40	X		0.6						
(C) 285+50	287+20		X	1.7						
285+90	289+20	X		3.3						
291+60	292+30	X		0.7						
294+50	295+40	X		0.9						
296+00	296+30	X		0.3						
296+70	297+50	X		0.8						
297+90	298+90	X		1.0						
299+50	300+20	X		0.7						
302+90	303+50	X		0.6						
304+30	305+20	X		0.9						
305+70	306+10	X		0.4						
310+00	310+70	X		0.7						
SUBTOTAL									67.5	

- (A) DAYLIGHT DITCH OUT TO THE NORTH.  
(B) EXCAVATE SPECIAL DITCH FROM 261+90 TO 263+10.  
(C) DAYLIGHT DITCH OUT TO THE NORTH.

## TYPICAL SECTION

STATION "L" 189+00 TO STATION "L" 315+10  
(EXCEPT THAT PAVEMENT WIDTH SHALL TRANSITION SMOOTHLY FROM EXISTING WIDTH AT 189+00 TO 34' AT 190+00).



## TRANSITION DETAIL

NOTE: A GROUND-IN TRANSITION MAY BE SUBSTITUTED, SUBJECT TO THE APPROVAL OF THE ENGINEER.

## BASIS OF ESTIMATE

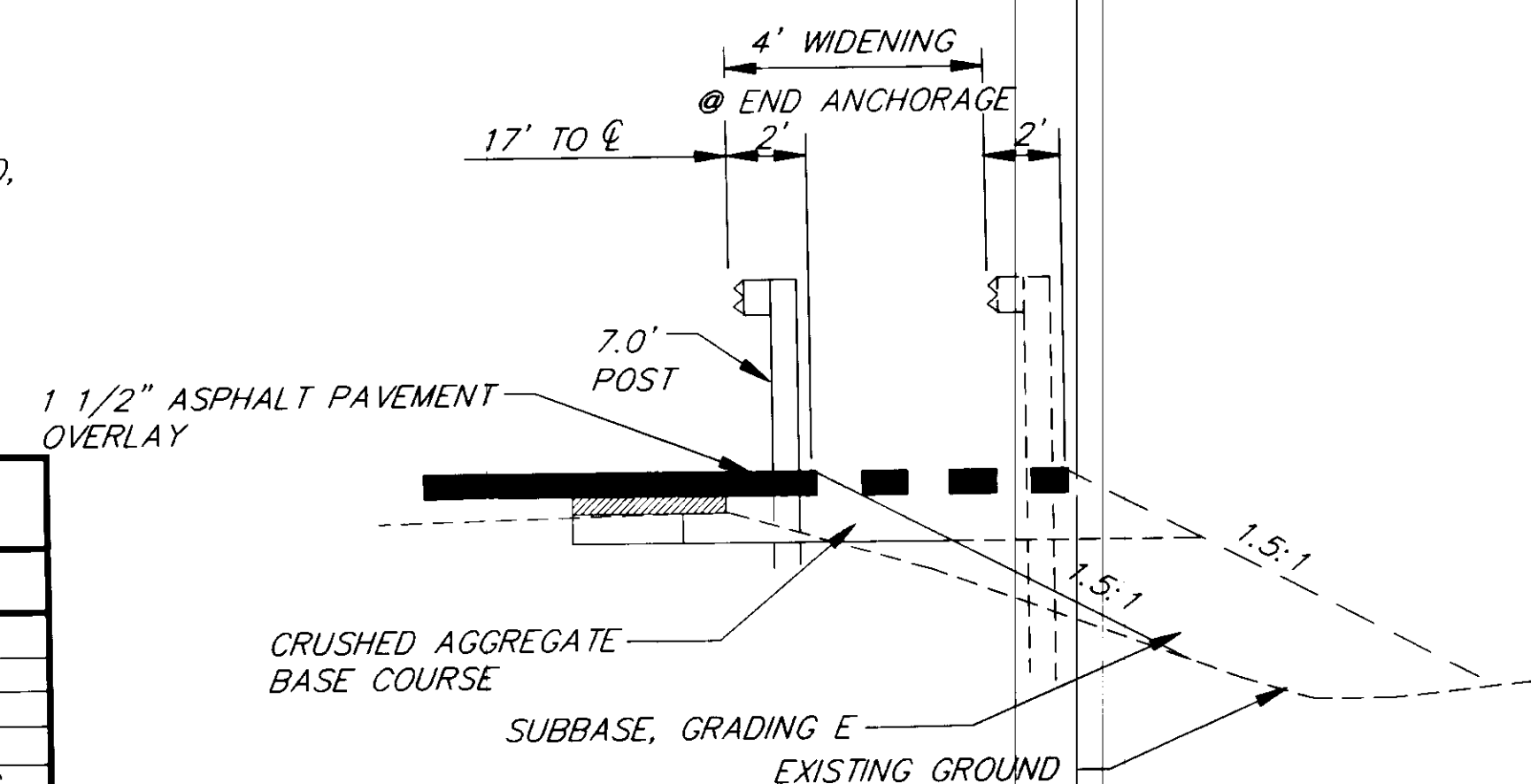
ITEM NO.	ESTIMATING FACTOR
301(1)	1.96 TON/CUBIC YARD
304(1)	1.85 TONS/CUBIC YDS
401(1)	114 LBS/SQ. YD./INCH DEPTH
401(2)	6% OF ITEM 401(1) TYPE II, CLASS "B"
402(2)	0.10 GAL/SQ. YD. - 253 GAL/TON @ 60" C
407(1)	114 LBS/SQ. YD. - 1 INCH DEPTH

## FILL SECTION NOTES:

- REMOVAL OF VEGETATION ON EXISTING SLOPES IN FILL AREAS SHALL BE INCIDENTAL TO LINEAR GRADING.
- TYPICAL FILL SLOPE IS 4:1, HOWEVER EXCEPTIONS ARE AS FOLLOWS:  
IF "H" EXCEEDS 3', SLOPE MAY BE STEEPENED TO CATCH AT TOE OF EXISTING FILL SLOPE.  
IF "H" EXCEEDS 5', SLOPE MAY BE STEEPENED TO CATCH AT 5' BELOW FINISH SHOULDER.  
IN NO CASE SHALL THE FINISHED FILL SLOPE BE STEEPER THAN 1 1/2:1. THE ENGINEER MAY REQUIRE MINOR SLOPE ADJUSTMENTS FOR AESTHETICS.

## AS-BUILT

BY: TL DATE: 1996

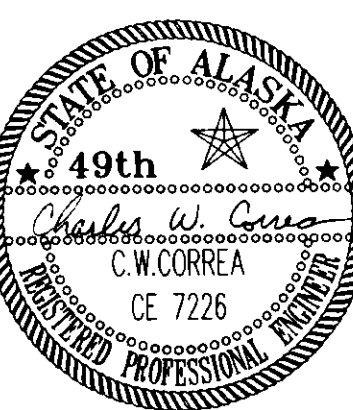


## GUARDRAIL INSTALLATION DETAIL

NOTE: ALL POSTS SHALL BE WOOD 7'-0" LONG, SEE STANDARD DRAWINGS FOR OTHER DETAILS.

NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

DESIGNED BY:	PROJECT NO.
C. CORREA	71108
DRAWN BY:	DATE:
C. Anderson	5/28/93
CHECKED BY:	SHEET
P. JONES	2 OF 9



# MAILBOX SUMMARY

STATION	LT	RT	SINGULAR.	MULTIPLE NO.	BOX NO.
192+28	X			2	7099,7101
194+07	X		1		7125
196+28	X		1		7303
196+38		X	1		7174
211+90	X		1		7595
199+59		X	1		7348
202+52		X	1		7330
205+28		X	1		7460
206+91		X	1		7480
260+26		X	1		8480
208+89	X		1		7509
209+23	X		1		7513
210+47	X		1		7545
213+72		X	1		7608
213+92	X		1		7625
215+42	X		1		7635
215+67		X	1		7640
217+07	X		1		7677
217+08		X	1		7670
218+77		X	1		7700
220+45	X		1		7737
220+73		X	1		7730
222+25	X		1		7745
272+76		X	1		8752
224+98	X		1		7833
227+50		X	REMOVE STAND (NO BOX)		
229+18	X		1		7887
231+42	X		1		7925
232+35		X	1		7940
233+19		X	1		7950
234+97		X	1		8000
235+61	X		1		7995
236+92		X	1		8030
237+22	X		1		8063
240+25		X	1		8100
305+50	X		1		9361
243+83	X		1	2	8163
246+22	X		1		8221
246+78	X		1		8223
248+87		X	1		8250
222+55	X		1	2	7753,7745

# MAILBOX SUMMARY

STATION	LT	RT	SINGULAR.	MULTIPLE NO.	BOX NO.
249+10	X		1		8265
249+46		X	1		8270
249+94	X		1		8267
255+75	X		1		8393
255+89		X	1		
257+36		X	1		8438
257+47	X		1		8437
259+55		X	1		8470
260+62	X		1		8463
261+55	X		1		8473
263+57	X		1		8525
265+26	X		1		
266+02		X	1		8530
268+33		X	1		8544
270+60		X	1		8746
272+21		X	1		8750
278+05		X	1		8866
280+05		X	1		8870
285+51		X	1		8982
306+90		X	1		9374
287+61		X	1		8992
290+34		X	1		9070
291+19	X		1		9087
291+64		X	1		9096
293+15	X		1		9123
294+02		X	1		9128
295+50	X		1		9161
296+16		X	1	2	9156,9160
297+72		X	1		9180
297+89	X		1		9223
301+95		X	1	2	9240,9240A
304+94		X	1		9360
305+57	X		1		9363
306+43	X		1		9367
306+46		X	1		9370
308+88		X	1		9396
309+25		X	1		9430
309+37	X		1		9375
311+18		X	1	3	9436,9440,9500
312+37	X		1		9445
TOTAL					75

# GUARDRAIL SUMMARY

BEGIN STATION	END STATION	LT/RT	REMOVE & DISPOSE LENGTH	INSTALL LENGTH	NO. OF END ANCHORAGE INSTALLS	REMARKS
192+18.5	196+06	RT		387.5	2 EACH	7' POSIS METAL
281+10	283+60	LT	250'			
281+35	283+60	RT	225'			
280+22	283+85	LT		362.5'	2 EACH	7' POSTS
280+47	283+85	RT		337.5'	2 EACH	7' POSTS
TOTALS			475'	1087.5'	6 EACH	

# GEOTEXTILE PAVING SUMMARY

STATION TO STATION	FABRIC DIMENSIONS	QUANTITY (S.Y.)	STATION TO STATION	FABRIC DIMENSIONS	QUANTITY (S.Y.)
ALL CROSS-CULVERT TRENCH SAWCUTS	60 EA. x 6' X 30'	1,200	255+98, TRANSVERSE	6' X 30'	20
189+30, BOP TRANSITION	6' X 30'	20	260+90 TO 261+30, RT.	12' X 40'	53
190+30, TO 191+10, LT. (EXIST. SAWCUT)	6' X 80'	53	281+00 TO 283+70, CL., SAWCUT	6' X 270'	180
195+00 TO 195+75, LT.	12' X 75'	100	293+03, TRANSVERSE	6' X 30'	20
197+50 TO 198+00, RT.	12' X 50'	67	297+33, TRANSVERSE	6' X 30'	20
200+50 TO 202+00, LT.	12' X 150'	200	302+50 TO 304+30, RT.	12' X 180'	240
203+25 TO 205+00, LT.	12' X 175'	233	303+25 TO 304+00, LT.	12' X 75'	100
206+00 TO 206+50, LT.	12' X 50'	67	304+30, TRANSVERSE	6' X 30'	20
206+00 TO 206+50, RT.	12' X 50'	67	314+80, EOP TRANSITION	6' X 30'	20
208+00 TO 208+50, LT.	12' X 50'	67	280+85 TO 281 CL.	6' X 15'	10
211+25 TO 213+00, LT.	12' X 175'	233	280+85 TRANSVERSE, LT.	6' X 15'	10
233+80, TRANSVERSE	6' X 30'	20	283+70 TRANSVERSE, LT.	6' X 15'	10
237+40, TRANSVERSE	6' X 30'	20	314+30 TRANSVERSE	6' X 30'	20
251+53, TRANSVERSE	6' X 30'	20			
202+70 TO 203+80, RT.	12' X 110'	147	GEOTEXTILE TOTAL = 3304 S.Y.		
220+80 TO 221+30, RT.	12' X 50'	67			

# ADJUST EXISTING MONUMENT CASE SUMMARY

STATION	PT.	OFFSET
191+55.94	PC	0
206+08.58	PT	0
223+10.06	PC	0
239+20.09	PT	0
245+39.12	PC	0
250+87.82	PT	0
253+03.12	PC	0
258+82.83	PT	0
273+03.24	PC	0
279+82.68	PT	0
282+52.11	PC	0
289+97.53	PT	0
TOTAL		12

# CROSS CULVERT SUMMARY

STATION	REMOVE EXISTING	INSTALL 24" 36"	REMARKS
191+20	54	54	
193+52	67	68	
195+85	74	74	
202+43	62	66	LOWER ENTIRE CMP 0.5'
205+92	60	60	
207+41	67	68	
209+92	86	86	
213+28	64	68	
214+25	58	60	
216+20	55	60	LOWER ENTIRE CMP 0.5'
218+30	64	64	
220+10	73	74	
221+79	62+40	72	SKEW NEW CULVERT
228+56	55	60	
231+02	61	64	
233+74	66	66	
245+76	66	66	66 SKEW NEW CULVERT
250+28	66	66	
256+25	70	70	
259+45	56	56	
263+16	79	80	
265+46	68	68	LOWER INLET 0.5'
269+49	72+70	72	LOWER INLET 1.0'
275+87	56		DO NOT REPLACE
288+12	71	74	LOWER INLET 0.7'
290+98	69	74	
294+06	92	92	
299+53	64	68	
307+40	66		DO NOT REPLACE
310+02	60	66	LOWER INLET 0.5'
TOTALS		2093 LF	1602LF 314LF

# REMOVAL OF STRUCTURES AND OBSTRUCTIONS SUMMARY

STATION	LT.	RT.	DESCRIPTION
227+50		X	MAILBOX STAND
234+90		25'	SINGLE PILING
245+80	X		COLLAPSED TIMBER RETAINING WALL
253+50	X	X	4-VERTICAL 4" PVC PIPES IN PAVEMENT-REMOVE TO 8" BELOW ASPHALT, BACKFILL WITH D-1 AND ASPHALT CONCRETE.

### CROSS CULVERT NOTES:

- UNLESS OTHERWISE NOTED ABOVE, NEW CULVERT INVERT ELEVATIONS SHALL MATCH EXISTING CULVERT INVERT ELEVATIONS.
- THE CBJ HAS A 14" D.I.P. WATERLINE THAT GENERALLY LIES 5' BELOW THE EXISTING LEFT DITCH LINE. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO THE WATER LINE, SERVICES, AND INSULATION THAT HIS OPERATION CAUSES, AT HIS OWN EXPENSE. WATERLINE AS-BUILTS WILL BE AVAILABLE AT THE PROJECT OFFICE.

**AS-BUILT**  
BY: TL DATE: 1996

ENGINEER'S SEAL



# SIGNING SUMMARY

NO.	STATION	OFFSET*		CODE NO.	LEGEND	SIZE	AREA S.F.	NO. OF POSTS	POST SIZE	FACING TRAFFIC	REMARKS
		LT	RT								
1	191+00		29'	R2-1	SPEED LIMIT 45	30 X 36	7.5	1	2.5	WB	
2	191+00	29'		R2-1	SPEED LIMIT 45	30 X 36	7.5	1	2.5	EB	
3	200+00	29'		R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	EB	
4	200+00		29'	R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	WB	
5	216+50		29'	M10-2	5	6 X 8	.33	1	2.0	WB	
6	216+50		29'	M10-2	5	6 X 8	.33	1	2.0	EB	INSTALL ON SAME POST AS 5
7	226+00	29'		R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	WB	
8	226+00		29'	R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	WB	
9	254+00		29'	R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	WB	
10	254+00	29'		R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	EB	
11	271+00		29'	M10-2	6	6 X 8	.33	1	2.0	WB	
12	271+00		29'	M10-2	6	6 X 8	.33	1	2.0	EB	INSTALL ON SAME POST AS 11
13	280+00		29'	R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	WB	
14	280+00	29'		R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	EB	
15	295+00		29'	R2-1	SPEED LIMIT 45	30 X 36	7.5	1	2.5	WB	
16	295+00	29'		R2-1	SPEED LIMIT 45	30 X 36	7.5	1	2.5	EB	
17	310+00		29'	R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	EB	
18	310+00		29'	R7-SPECIAL	NO PARKING ON PAVEMENT OR WITHIN 8 FEET	12 X 18	1.5	1	2.0	WB	
19	312+85		30'	D1-2	EAGLE CREST SKI AREA	84 X 24	14	2	2.5	WB	6" U.C. 4.5" L.C.
20	315+06		29'	D3-1	FISH CREEK ROAD	42 X 8	2.33	1	2.5	E+WB	INSTALL ABOVE STOP SIGN
21	315+06		29'	R1-1	STOP	30 X 30	6.25	1	2.5	WB	
TOTAL						68.9					

\* EXCEPT IN DITCH SECTIONS, OFFSET SHALL BE 2' BEYOND DITCH BOTTOM.

- SIGNING NOTES:
- SIGN LOCATIONS ARE APPROXIMATE AND MAY BE SUBJECT TO MINOR FIELD ADJUSTMENT BY THE ENGINEER.
  - SIGN PANELS HEIGHTS SHALL MEET THE "EXPRESSWAY" DIMENSION LISTED ON STANDARD DRAWING S-05.00.
  - SIGN PANELS SHALL BE UNFRAMED 0.080" THICKNESS.
  - SIGN POSTS SHALL BE NEW, GALVANIZED, PERFORATED STEEL TUBE WITH TELESCOPING SLEEVE SOIL EMBEDMENT BASES AS SHOWN ON STANDARD DRAWING S-30.01.
  - ALL EXISTING HIGHWAY SIGNS SHALL BE REMOVED AND STOCKPILED AT THE DOT/PF MAINTENANCE STATION AT 6860 OLD GLACIER HIGHWAY.

DATE	DESCRIPTION OF CHANGE
T.L. 1996	AS-BUILT WITH CORRECTIONS

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

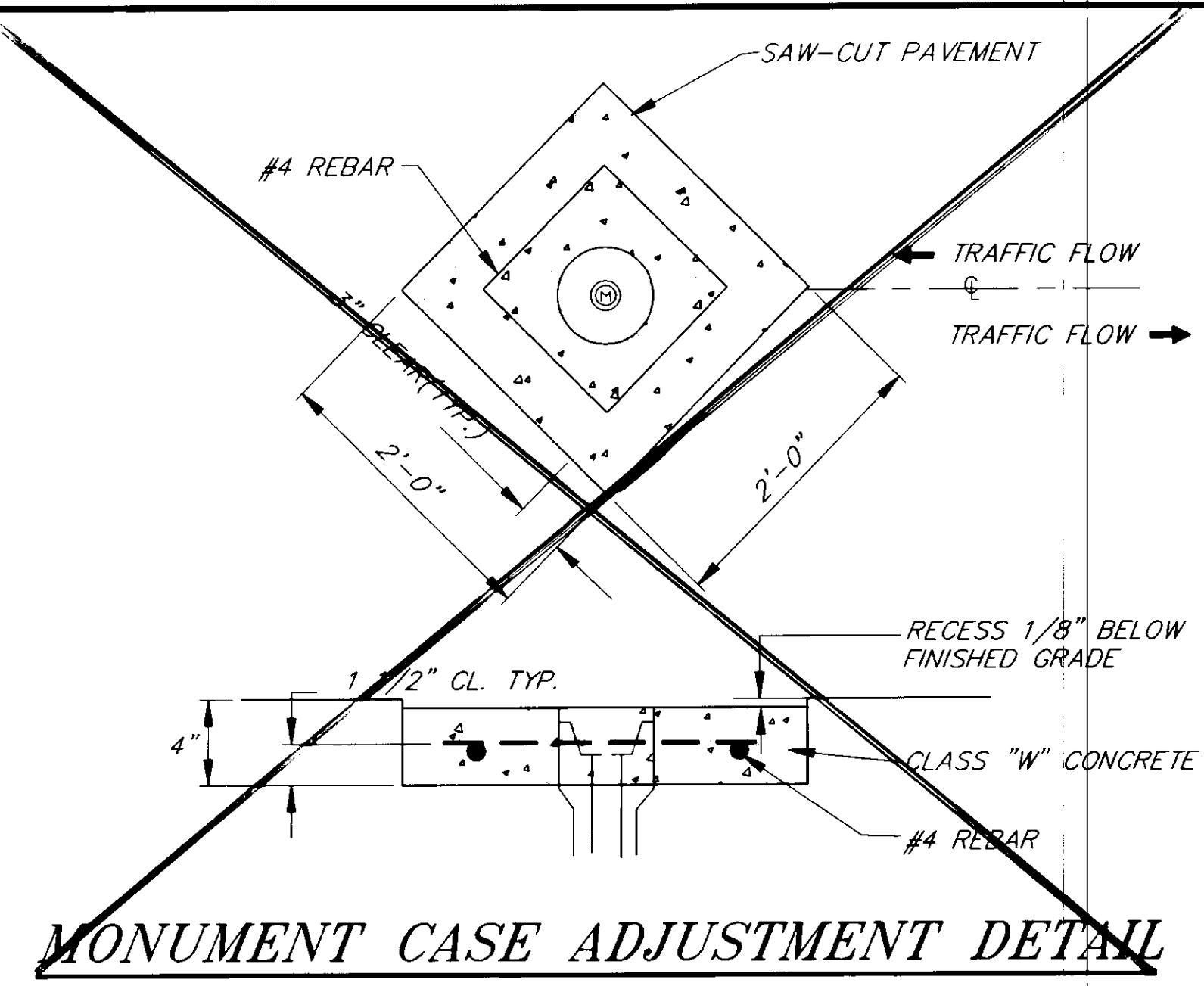
JUNEAU

NORTH DOUGLAS HIGHWAY  
HELIPORT TO EAGLECREST ROAD IMPROVEMENTS  
PROJECT NO. STP-0959(16) 71108  
SUMMARY TABLES

ALASKA

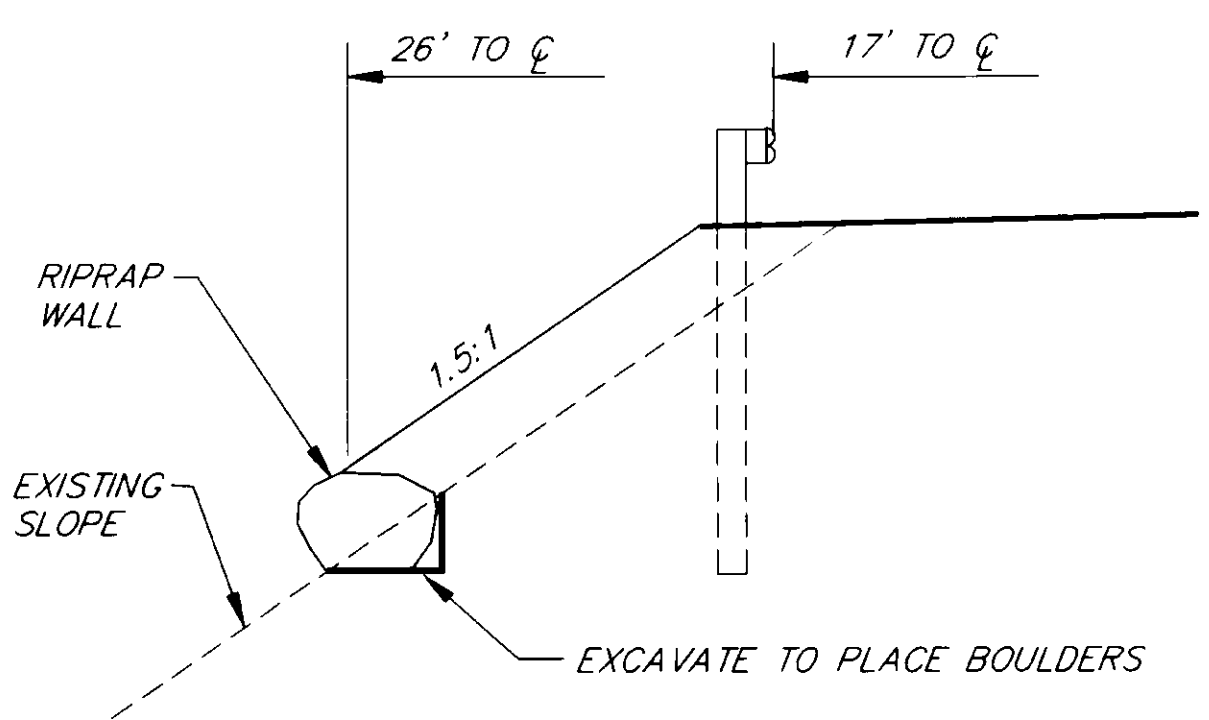
DESIGNED BY:  
C.W. CORREA  
DRAWN BY:  
AUTOCAD/BILL ADAMS  
CHECKED BY:  
P. JONES

PROJECT NO.  
71108  
DATE:  
5/28/93  
SHEET 3 OF 9

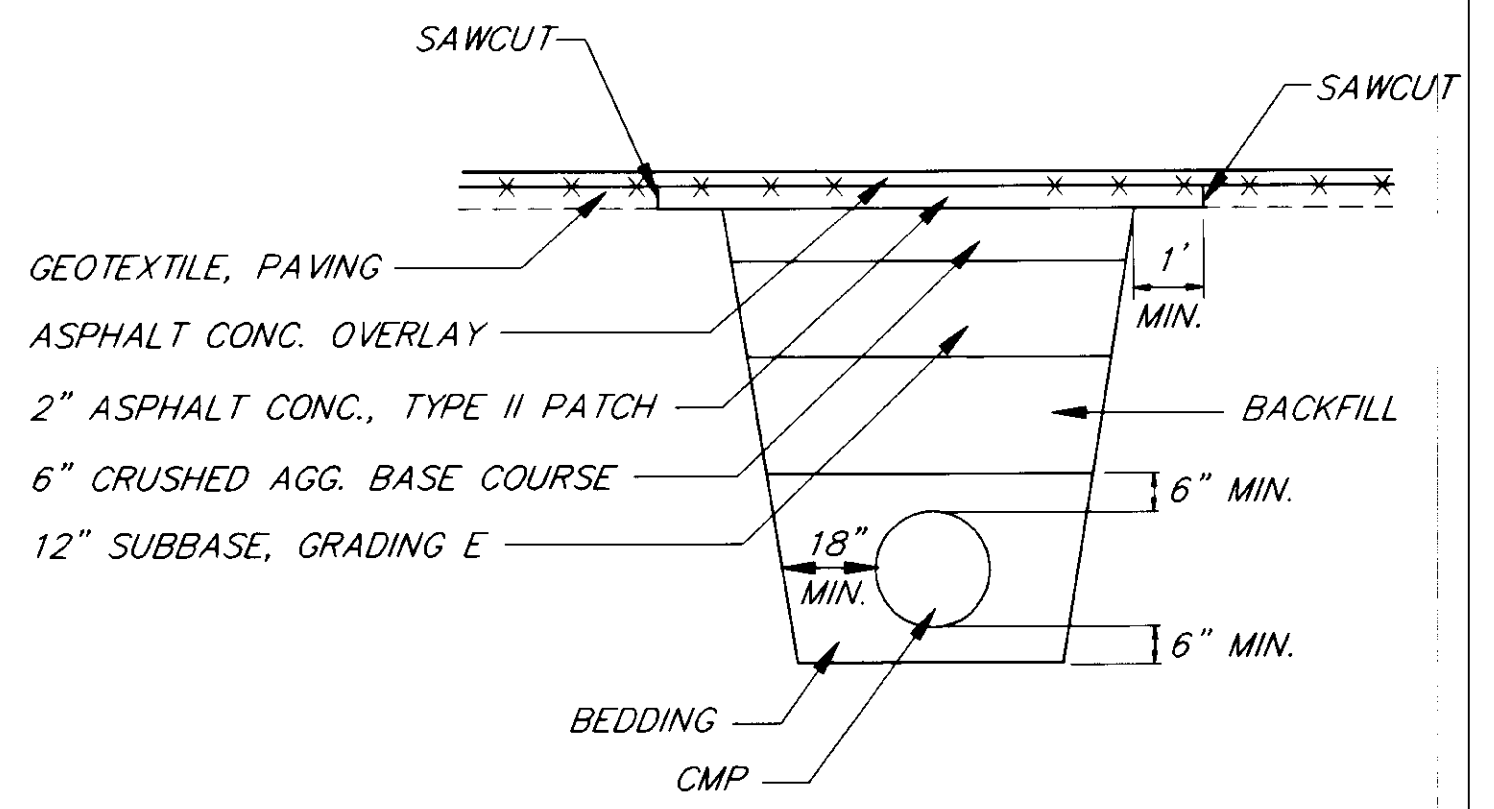


**MONUMENT CASE ADJUSTMENT DETAIL**

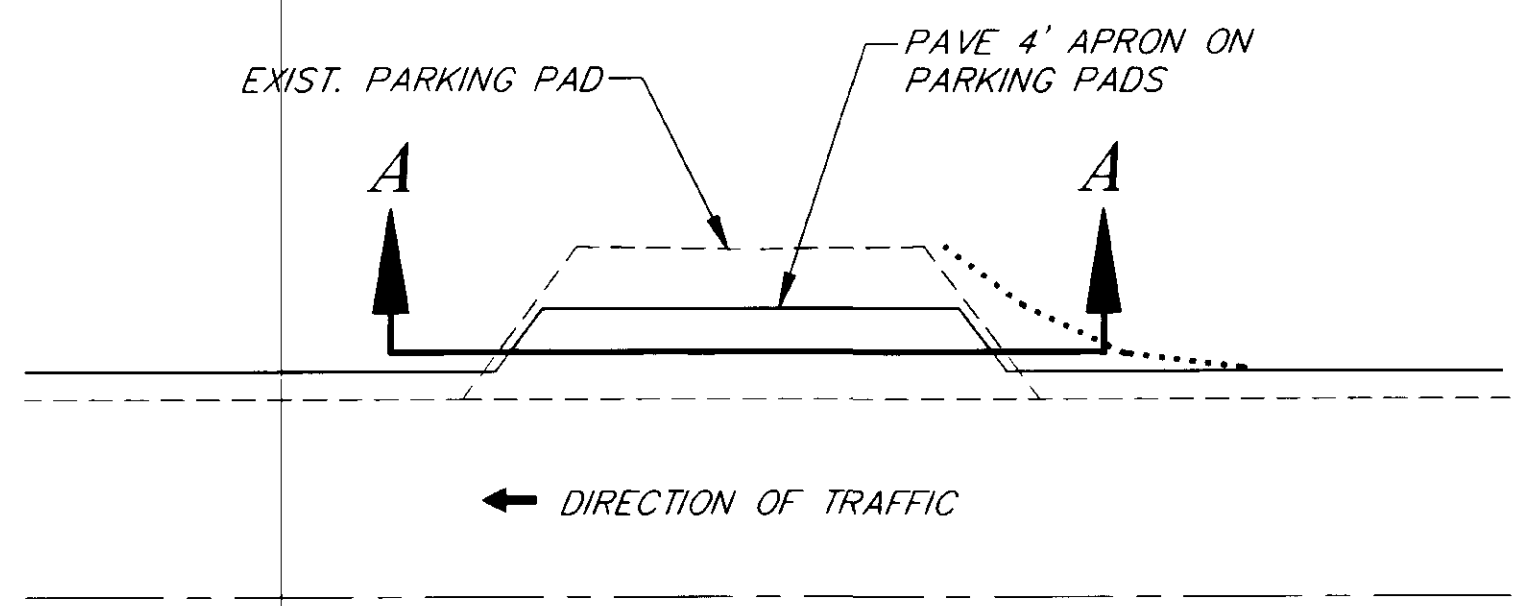
RISERS WERE PLACED ON MONUMENT CASES PRIOR TO PAVING.



**RIPRAP DETAIL**  
282+46 TO 282+56, LT.

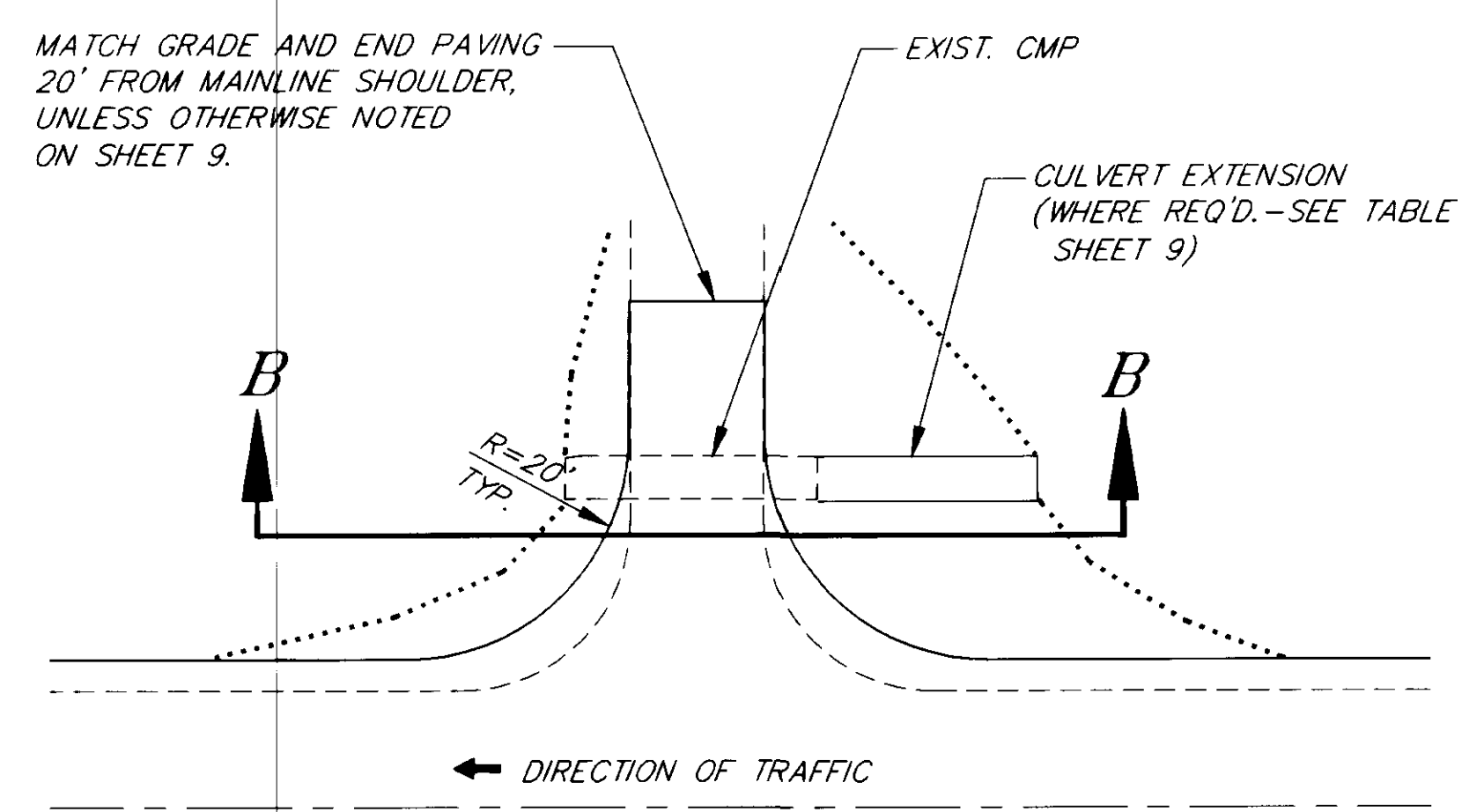


**CROSS CULVERT TRENCH DETAIL**

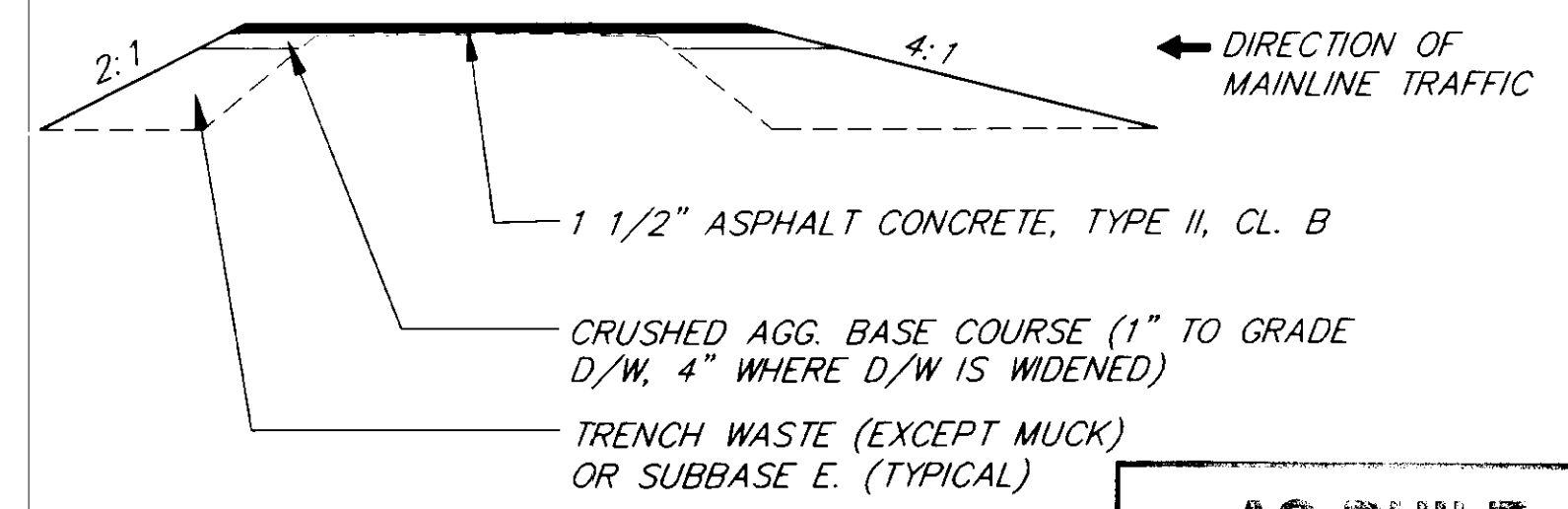


**PARKING PAD DETAIL**

NOTE: SEE SHEET 8 FOR SECTION A-A



**DRIVEWAY DETAIL**



**SECTION B-B**

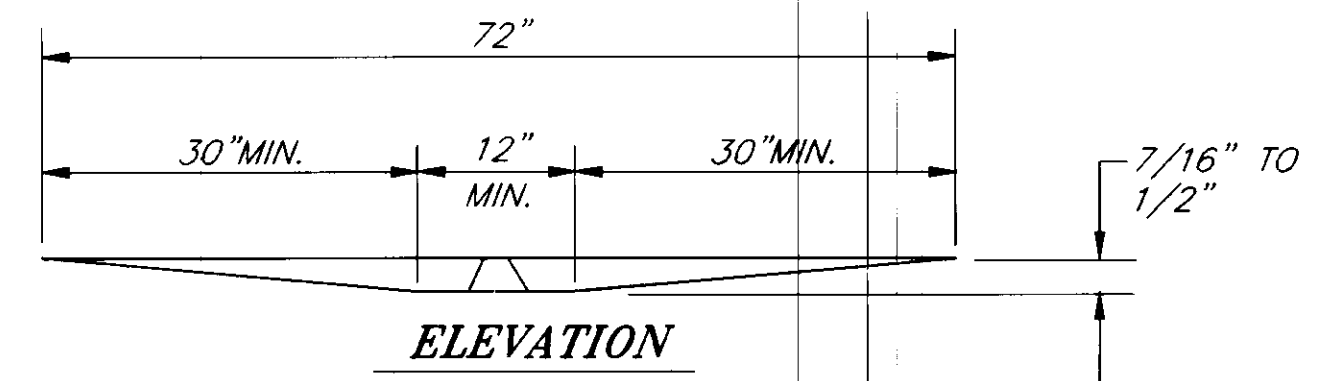
**AS-BUILT**  
BY: T.L. DATE: 1996

**DRIVEWAY NOTES:**

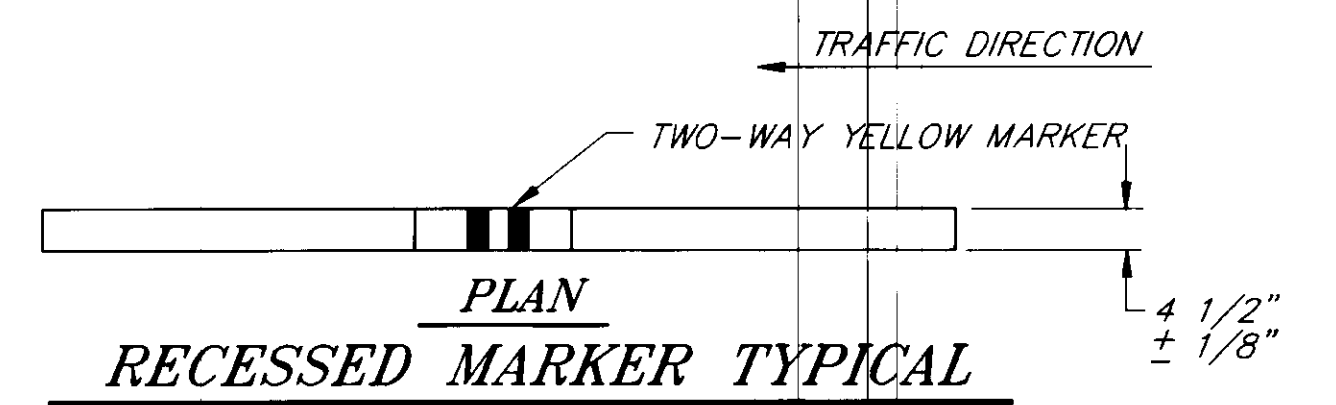
1. THE DRIVEWAY FORESLOPES FACING ONCOMING TRAFFIC SHALL BE 4:1. THOSE FACING AWAY FROM ONCOMING TRAFFIC SHALL BE 2:1.
2. 4:1 FORESLOPE WILL CARRY AROUND TO BACK OF DITCH IN DITCH AREAS, OR TO 20' FROM MAINLINE SHOULDER IN FILL AREAS. EXISTING CULVERTS SHALL BE EXTENDED AS REQUIRED TO ACCOMMODATE 4:1 FORESLOPE.
3. THE EXISTING PAVEMENT ON 5 TO 10 DRIVEWAYS MAY BE DIRECTED BY THE ENGINEER TO BE REMOVED IN ORDER TO REGRADE THE DRIVEWAY WITH BASE COURSE. THIS REMOVAL IS INCIDENTAL TO 639 (3).

**R.P.M. SPACING TABLE**

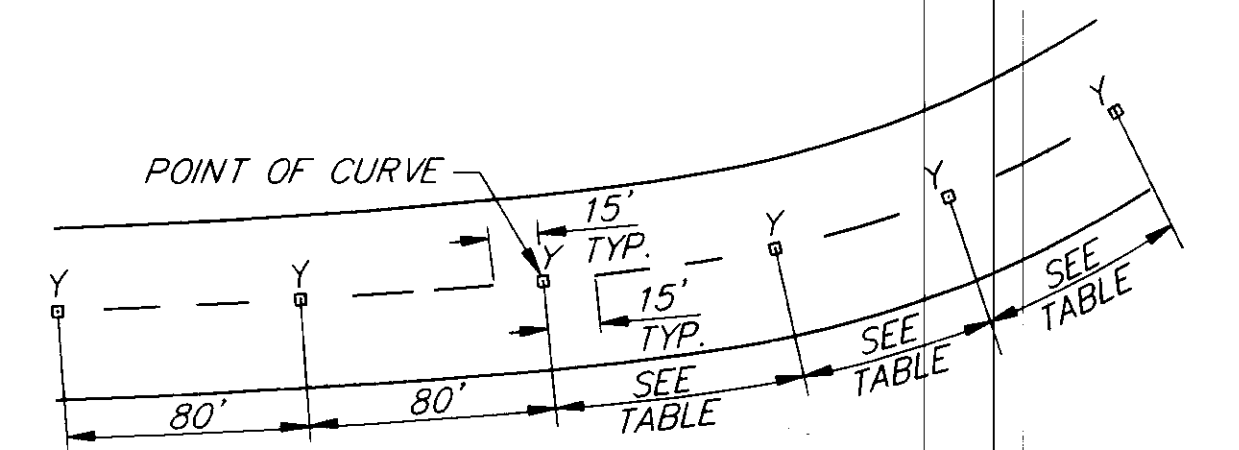
STATION TO	STATION	80'	40'
BOP 189+00	PC 245+39.12	72	
PC 245+39.12	PT 250+87.82		14
PT 250+87.82	PC 253+03.12	3	
PC 253+03.02	PT 258+82.83		15
PT 258+82.83	EOP 315+00	71	
TOTAL		175	EA



**ELEVATION**

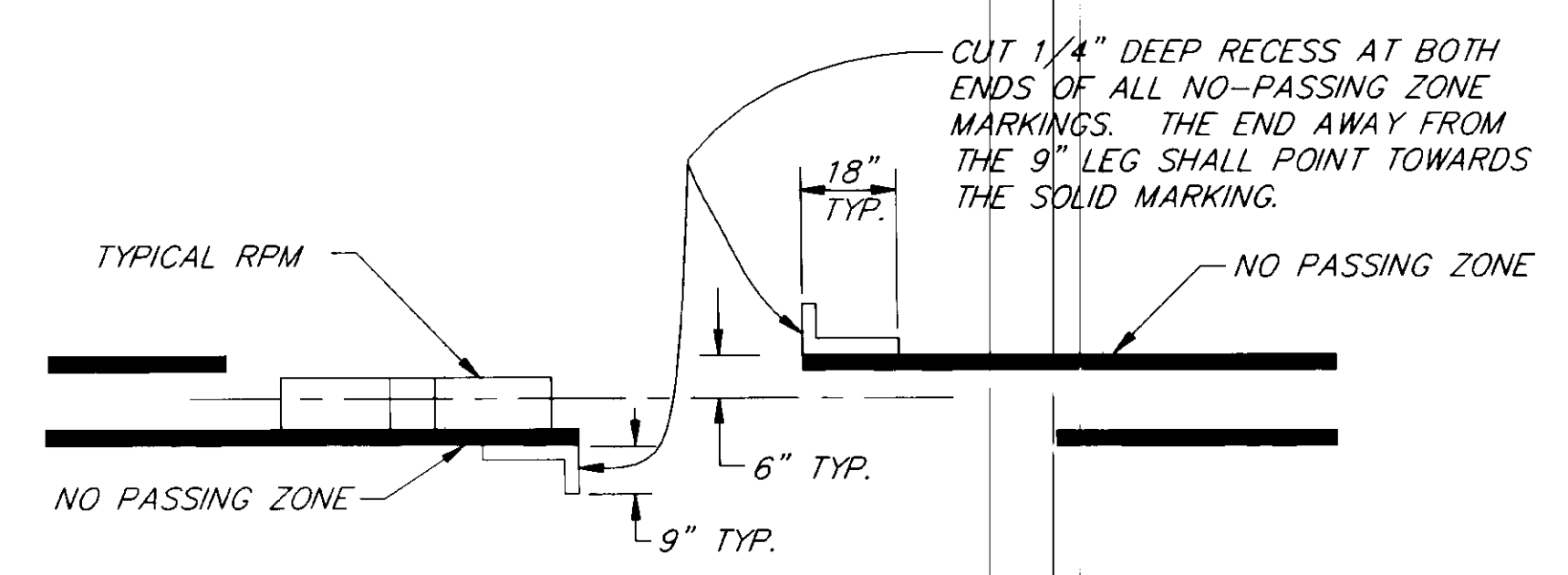


**RECESSED MARKER TYPICAL**



**RECESSED MARKER LOCATION TYPICAL**

(SEE R.P.M. SPACING TABLE)



**NO PASSING GROOVES**

**STRIPING AND RPM NOTES:**

1. STRIPED LANE WIDTHS SHALL BE 11'. WIDTH SHALL TAPER IN 100' TO MATCH EXISTING AT B.O.P. AND E.O.P.
2. EXISTING NO-PASSING GROOVES AT ENDS OF NO PASSING ZONES SHALL BE REFERENCED BEFORE CONSTRUCTION BEGINS BY THE CONTRACTOR FOR TEMPORARY AND PERMANENT MARKING PLACEMENT.
3. LATERAL PLACEMENT OF RECESSED PAVEMENT MARKERS SHALL BE BETWEEN DOUBLE YELLOW STRIPES OR IN LINE WITH SKIP STRIPES.
4. LOCATIONS OF MARKERS IN SITUATIONS WITH UNUSUAL GEOMETRICS WILL BE DETERMINED BY THE PROJECT ENGINEER.
5. RPM QUANTITIES ARE APPROXIMATE ONLY AND MAY VARY.

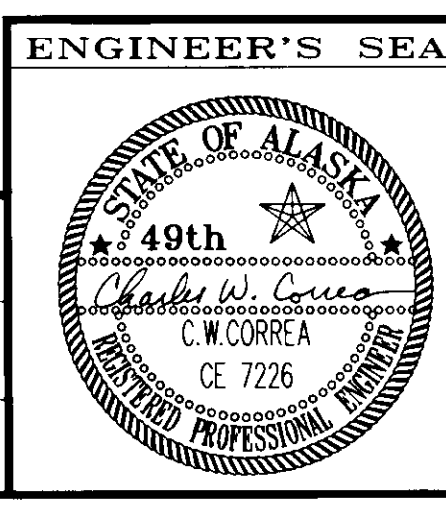
NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

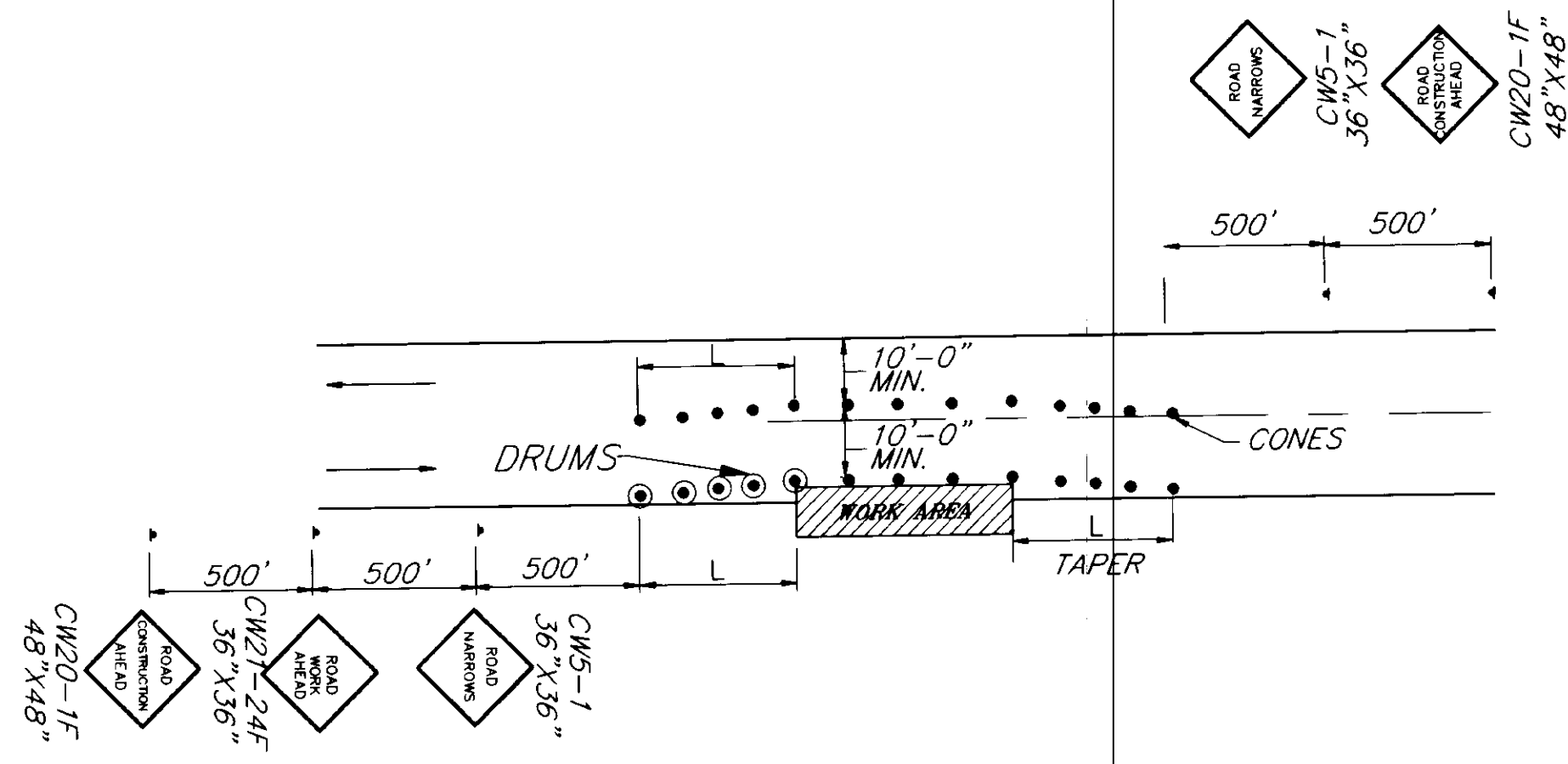
BY:	DATE:	DESCRIPTION OF CHANGE:
TL	1996	AS-BUILT WITH CORRECTIONS

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
NORTH DOUGLAS HIGHWAY  
HELIPORT TO EAGLECREST ROAD IMPROVEMENTS  
PROJECT NO. STP-0959(16) 71108  
MISCELLANEOUS DETAILS

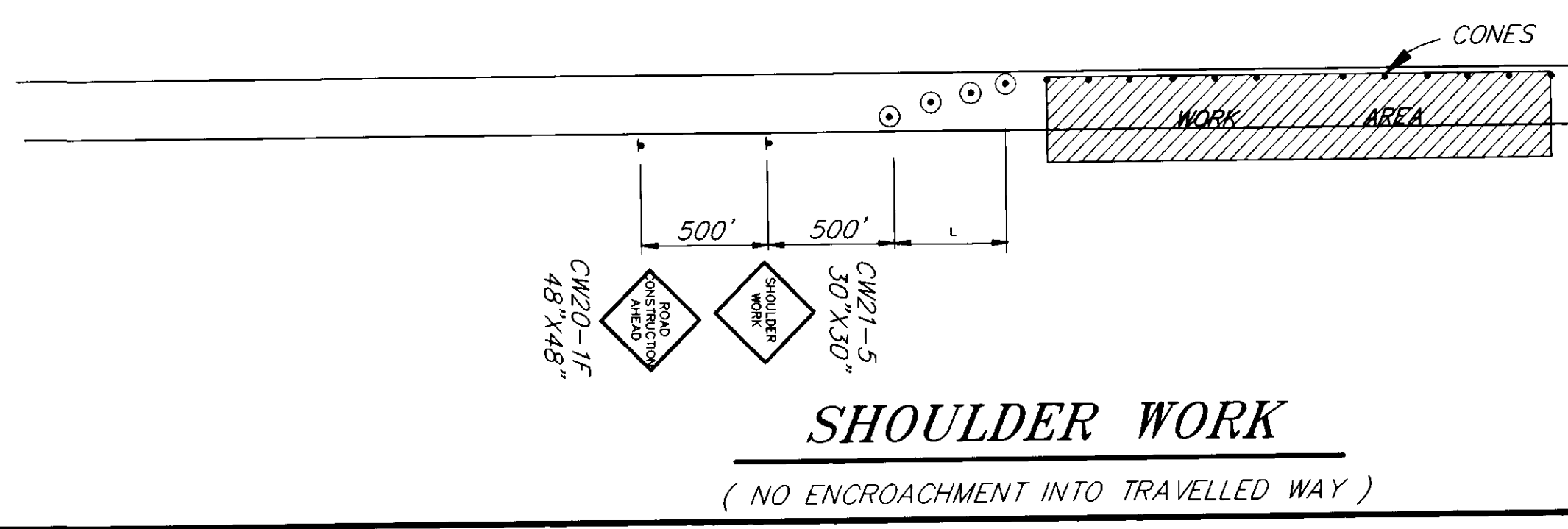
ALASKA	DESIGNED BY: C.W. CORREA	PROJECT NO. 71108
	DRAWN BY: AUTOCAD/BILL ADAMS	DATE: 5/28/93
	CHECKED BY: P. JONES	SHEET 4 OF 9





**ROADWAY ENCROACHMENT**

NOTE: IF ONLY ONE LANE IS EFFECTED BY ROAD WORK (THAT IS, THE CHANNELIZING DEVICES ALONG THE WORK AREA ARE NO CLOSER THAN 10' TO CENTERLINE) THE CENTERLINE CONES FOR THE OPPOSING LANE MAY BE DELETED.



**SHOULDER WORK**

(NO ENCROACHMENT INTO TRAVELLED WAY)

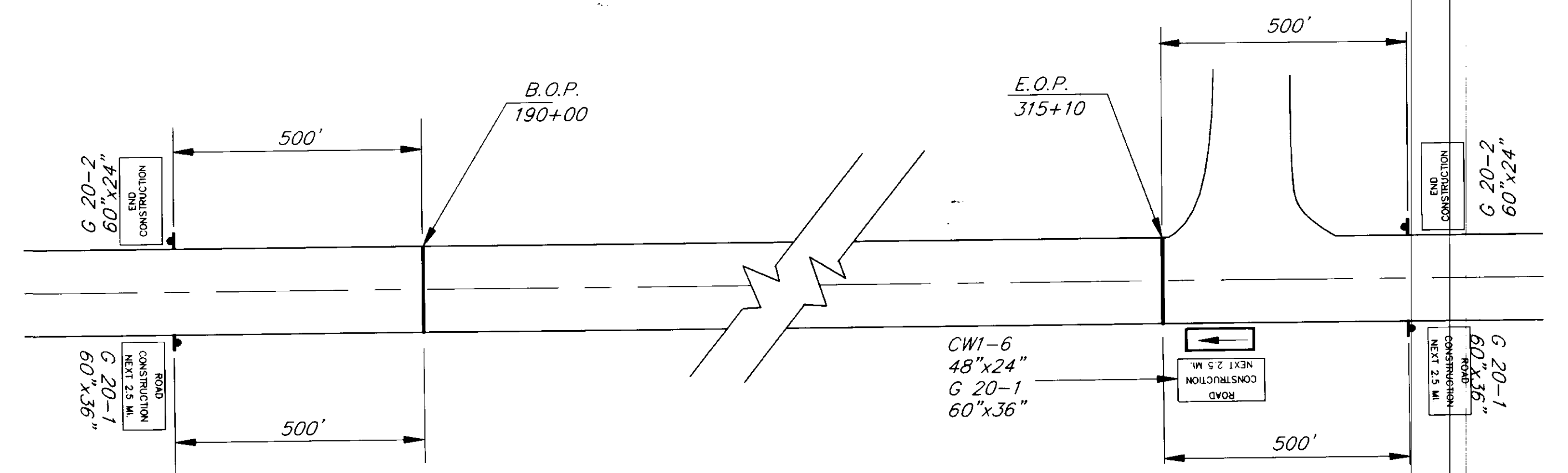
**TRAFFIC CONTROL NOTES**

1. A MINIMUM OF ONE LANE SHALL BE MAINTAINED OPEN AT ALL TIMES AT WORK AREAS.
2. TWO LANES SHALL BE MAINTAINED OPEN AT ALL TIMES AT WORK SITES DURING NON-WORKING HOURS.
3. MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES SHALL BE EQUAL TO THE SPEED LIMIT IN FEET. CONES SHALL BE A MINIMUM OF 28" TALL. DRUMS FOR DELINEATION AT NIGHT SHALL HAVE TYPE C STEADY BURN LIGHTS. DRUMS TO MARK OBSTRUCTIONS AT NIGHT SHALL HAVE TYPE C FLASHING LIGHTS.
4. FLOOD LIGHTS SHALL BE PROVIDED FOR FLAGGER STATIONS DURING NIGHT OPERATIONS, POSITIONED SO THAT ONCOMING TRAFFIC IS NOT BLINDED.
5. A SINGLE FLAGGER MAY BE APPROVED BY THE ENGINEER IF THE ENTIRE WORK AREA IS LESS THAN 50' LONG AND IS VISIBLE FROM BOTH DIRECTIONS.
6. THE MAXIMUM LENGTH OF ONE LANE, TWO-WAY WORK AREA SHALL BE 2,500 FEET. THE MINIMUM DISTANCE BETWEEN ONE LANE WORK AREAS SHALL BE 1,000 FEET.
7. TEMPORARY PAVEMENT MARKINGS ARE REQUIRED ON THE LEVELING COURSE AND THE FINAL LIFT IN ACCORDANCE WITH SECTION 643 OF THE SPECIAL PROVISIONS.
8. CONSTRUCTION SIGNS SHALL ONLY BE IN PLACE WHEN THE CONDITIONS EXIST FOR WHICH THEY ARE INTENDED.
9. CHANNELIZING DEVICES IN THE LEADING TAPER SHALL BE DRUMS. THEREAFTER, DRUMS SHALL BE PLACED EVERY 250' ALONG WORK AREAS, WITH CONES BEING APPROPRIATELY SPACED IN BETWEEN THE DRUMS.

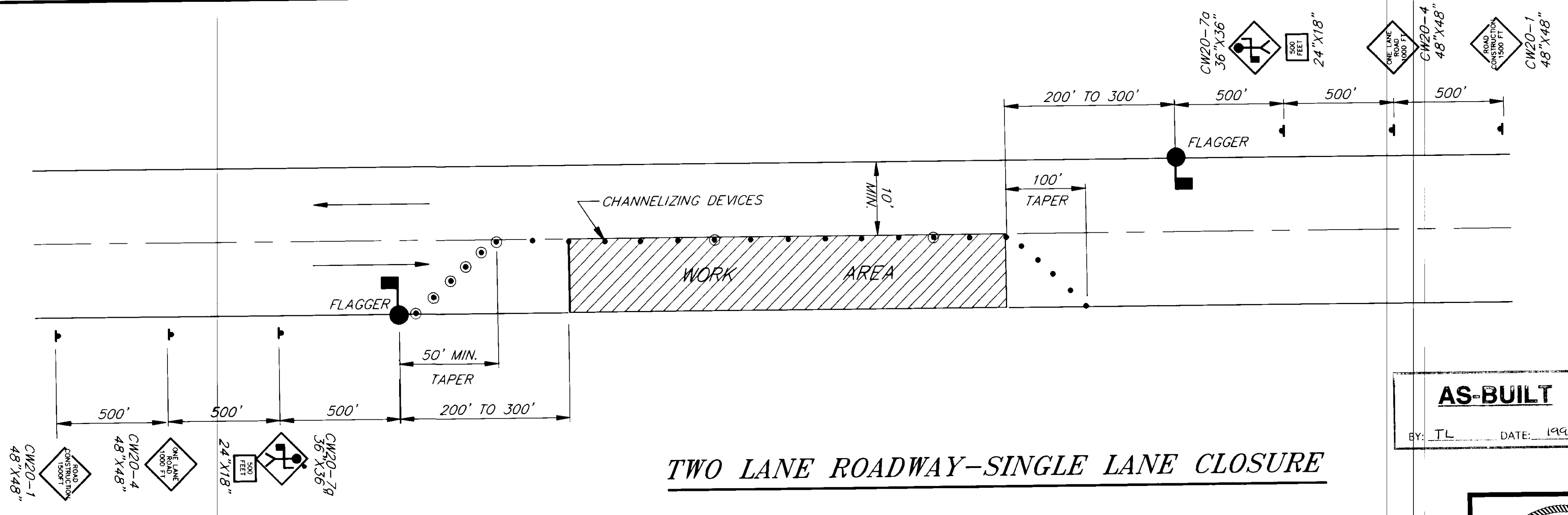
L = SXW L = MIN. LENGTH OF TAPER IN FEET  
 S = POSTED SPEED LIMIT  
 W = WIDTH OF OFFSET

**LEGEND**

- ..... SIGN
- ..... CONE
- ..... DRUM
- ..... FLAGGING STATION



**PERMANENT CONSTRUCTION SIGNING**



**TWO LANE ROADWAY-SINGLE LANE CLOSURE**

**AS-BUILT**  
 BY: TL DATE: 1996

NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

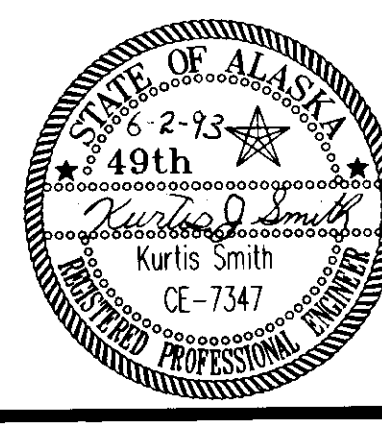
PATH	DATE	DESCRIPTION OF CHANGE
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STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU

NORTH DOUGLAS HIGHWAY  
 HELIPORT TO EAGLECREST ROAD IMPROVEMENTS  
 PROJECT NO. STP-0959(16) 71108  
**TRAFFIC CONTROL PLAN**

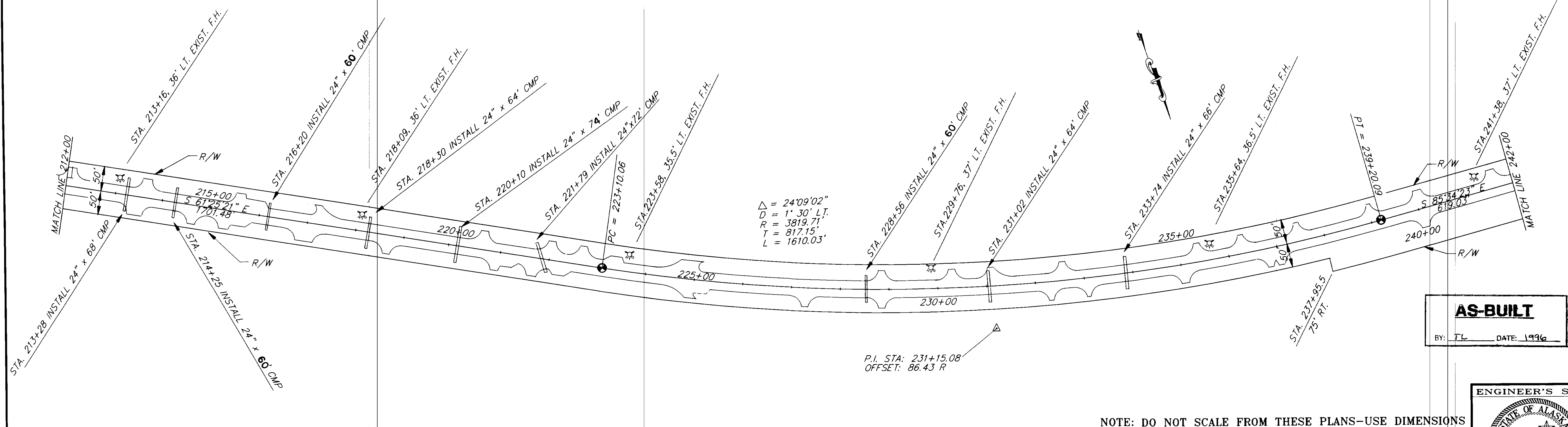
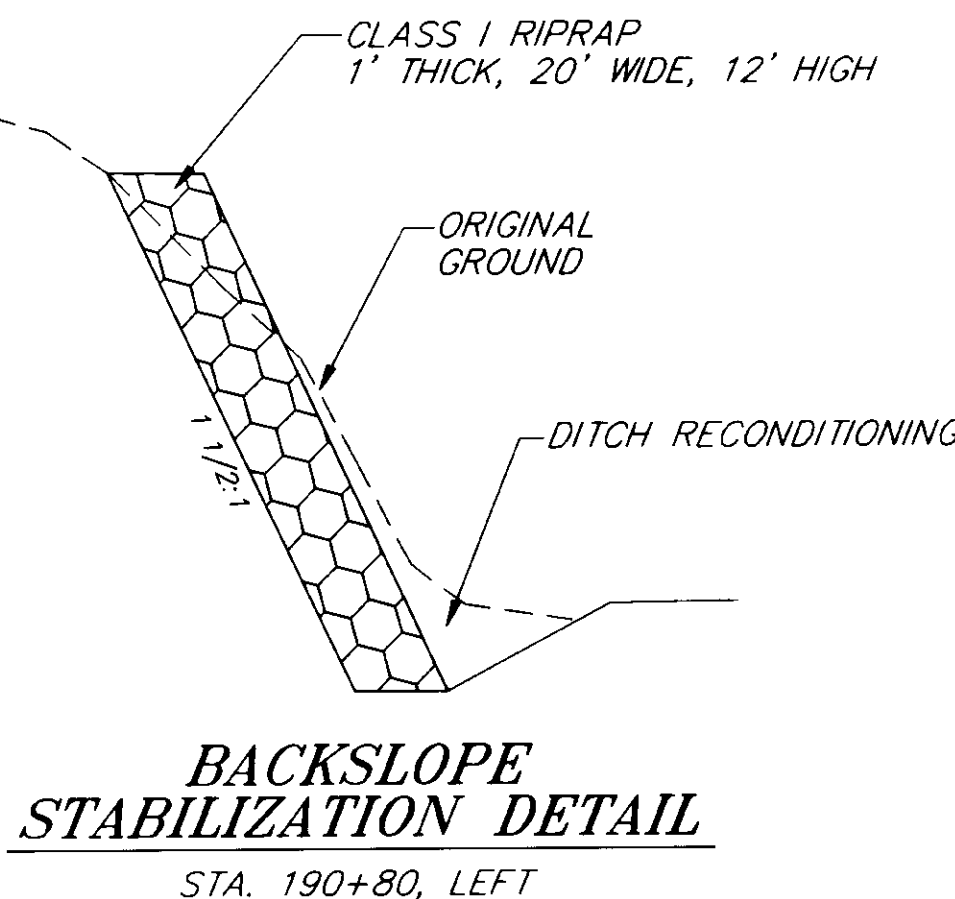
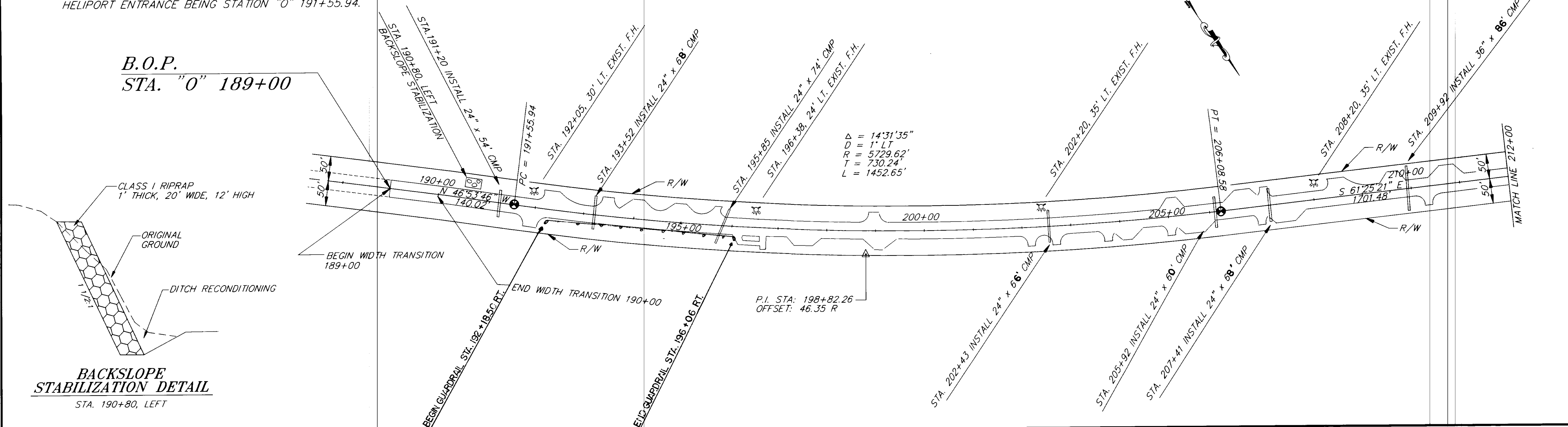
DESIGNED BY: C. CORREA	PROJECT NO. 71108
DRAWN BY: BILL ADAMS	DATE: 5/28/93
CHECKED BY: K. SMITH	SHEET 5 OF 9



**BASIS OF HORIZONTAL CONTROL:**

HORIZONTAL STATIONING IS BASED ON THE CENTERLINE P.C. MONUMENT APPROXIMATELY 900' BEYOND THE HELIPORT ENTRANCE BEING STATION "0" 191+55.94.

**B.O.P.**  
STA. "0" 189+00



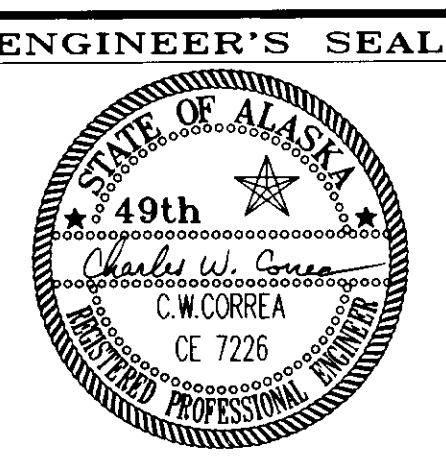
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BY: TL DATE: 1996

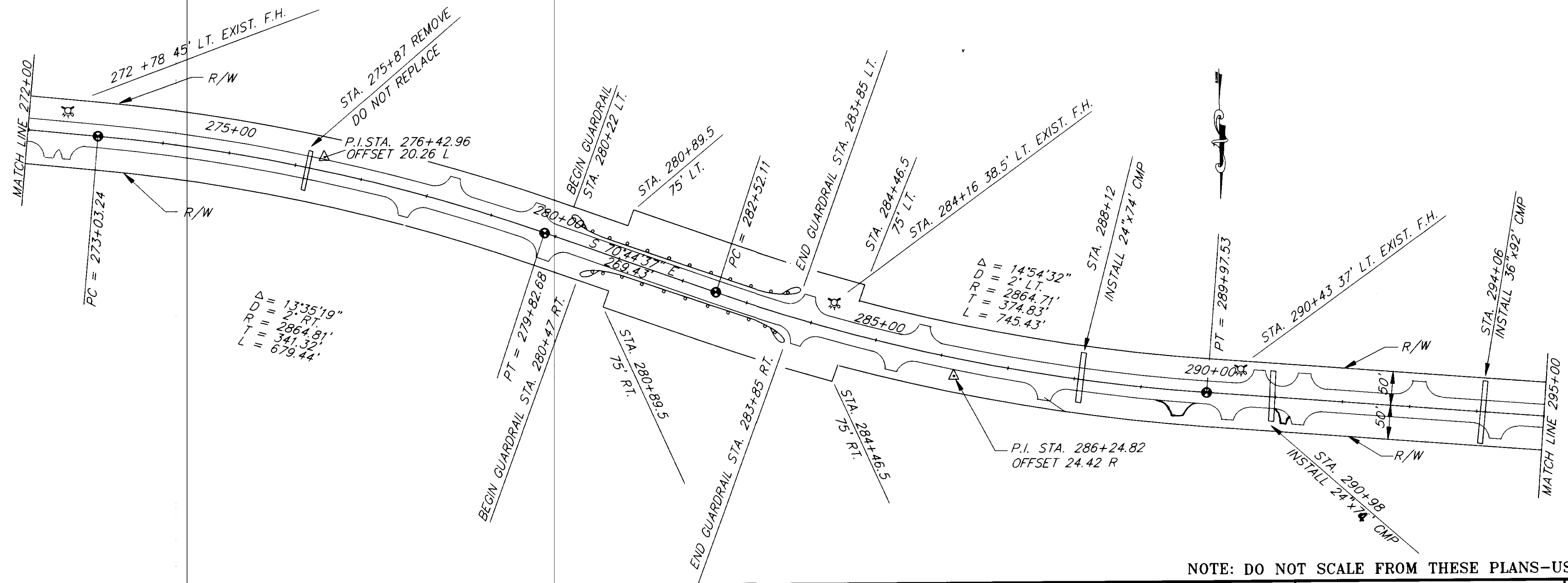
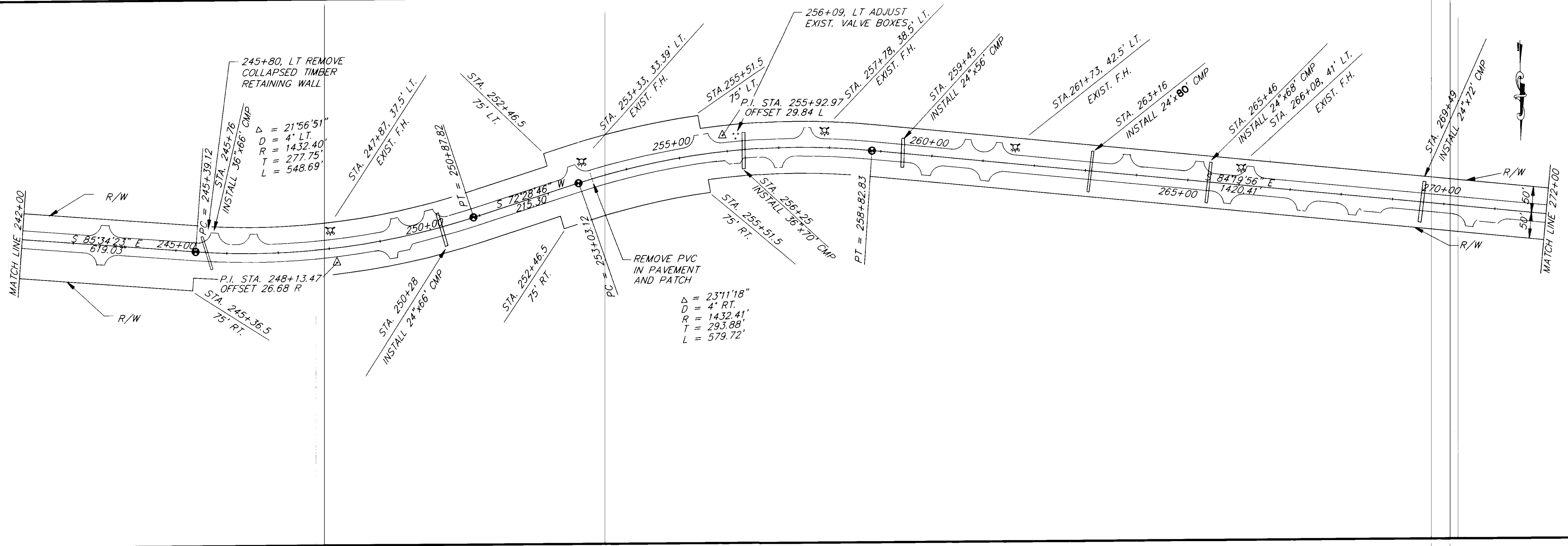
RECORD OF REVISIONS		
NO.	DATE	DESCRIPTION OF CHANGE
1	1996	AS BUILT WITH CORRECTIONS

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

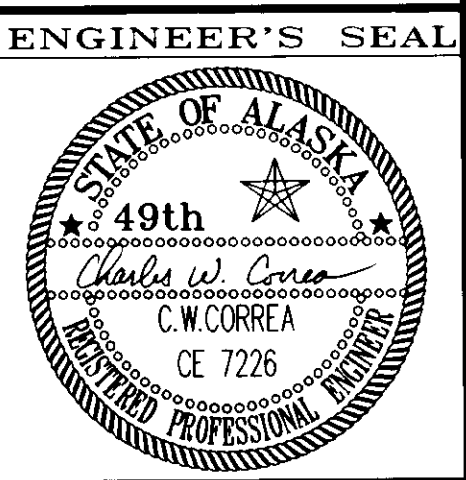
JUNEAU  
NORTH DOUGLAS HIGHWAY  
HELIPORT TO EAGLECREST  
ROAD IMPROVEMENTS  
PROJECT NO. STP-0959(16) 71108  
**PLAN**

NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS  
ALASKA  
DESIGNED BY: C.W. CORREA  
DRAWN BY: BILL ADAMS  
CHECKED BY: P. JONES  
PROJECT NO. 71108  
DATE: 5/28/93  
SHEET 6 OF 9





**AS-BUILT**  
 BY: J.L. DATE: 1996



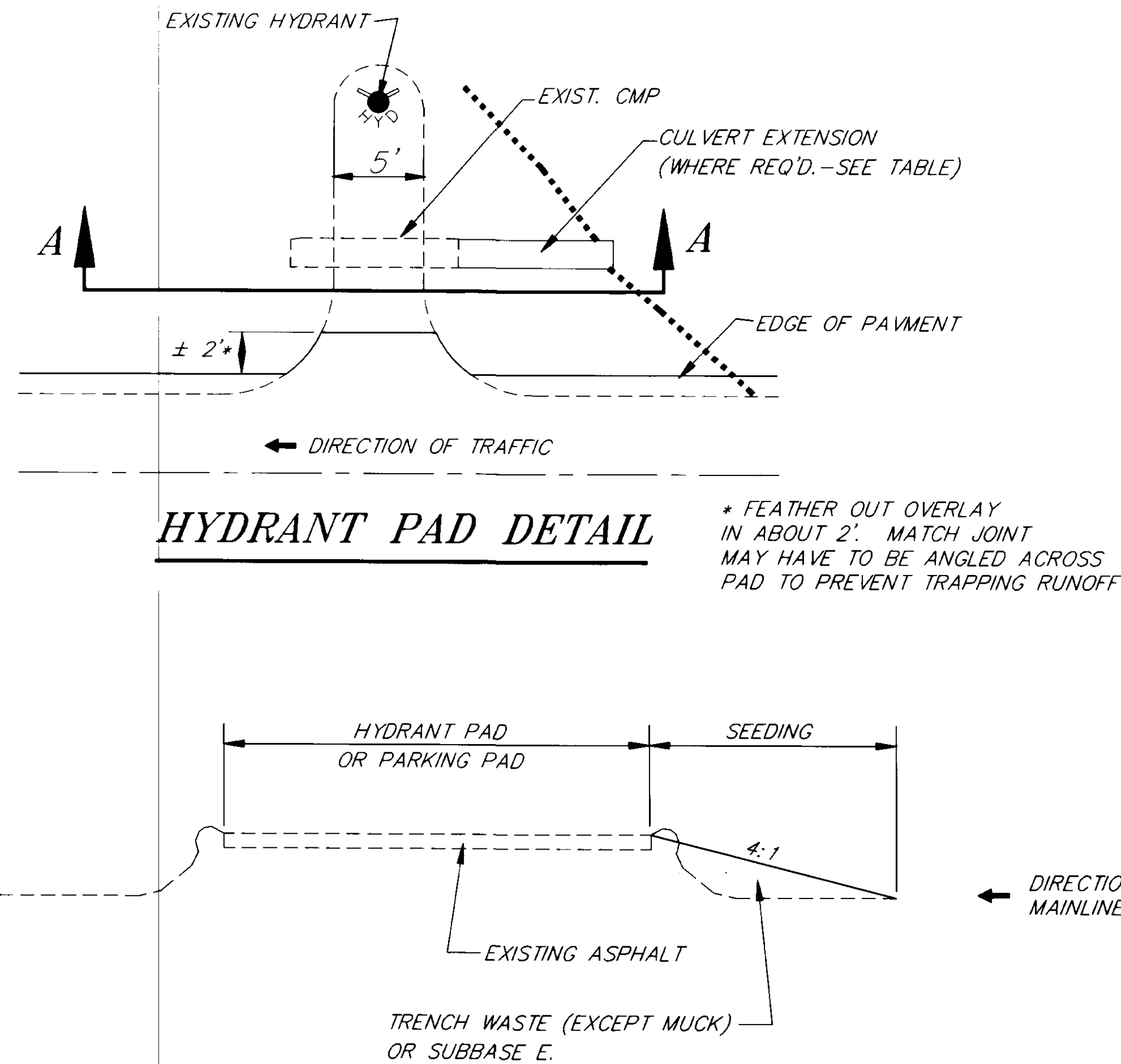
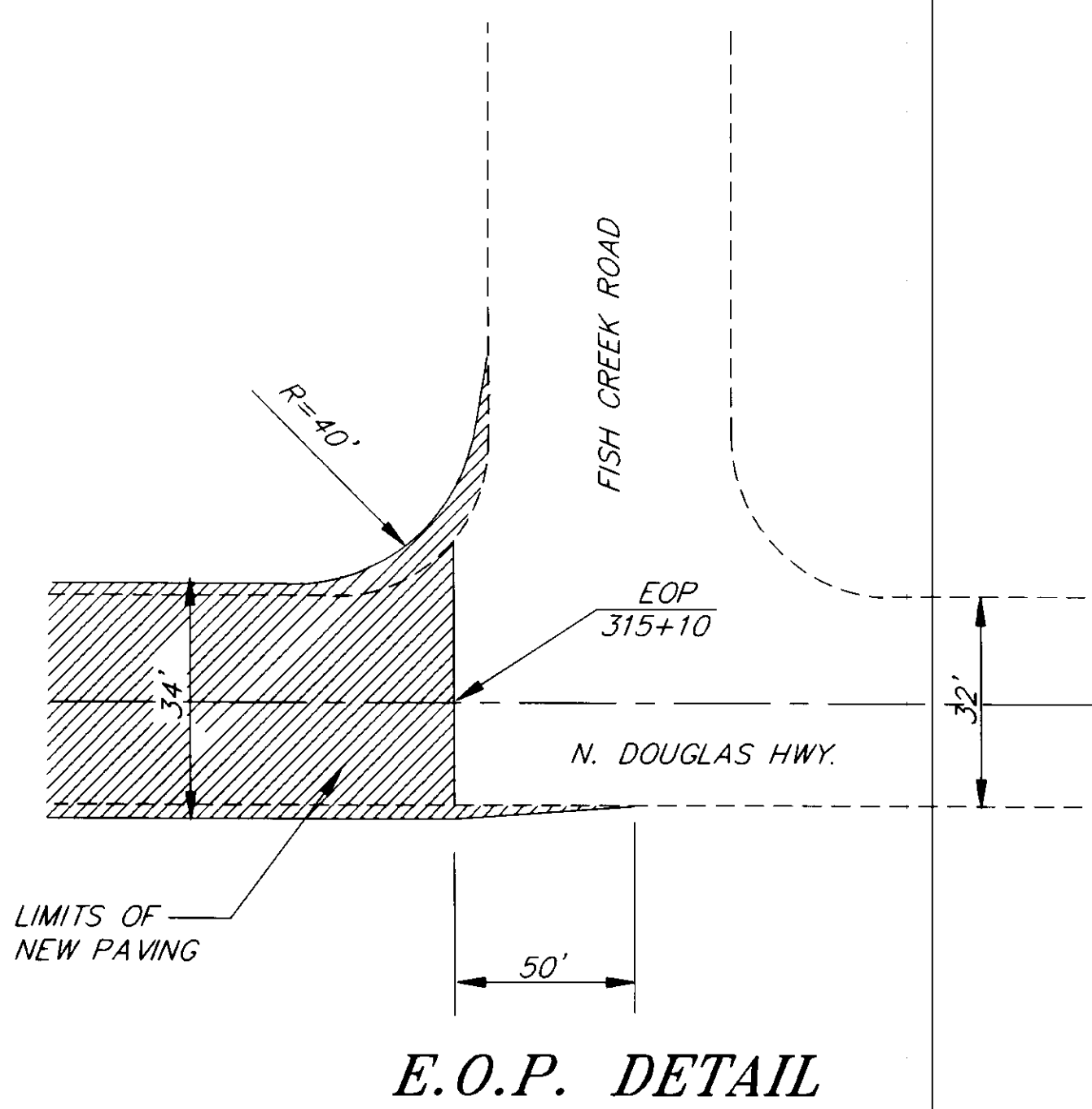
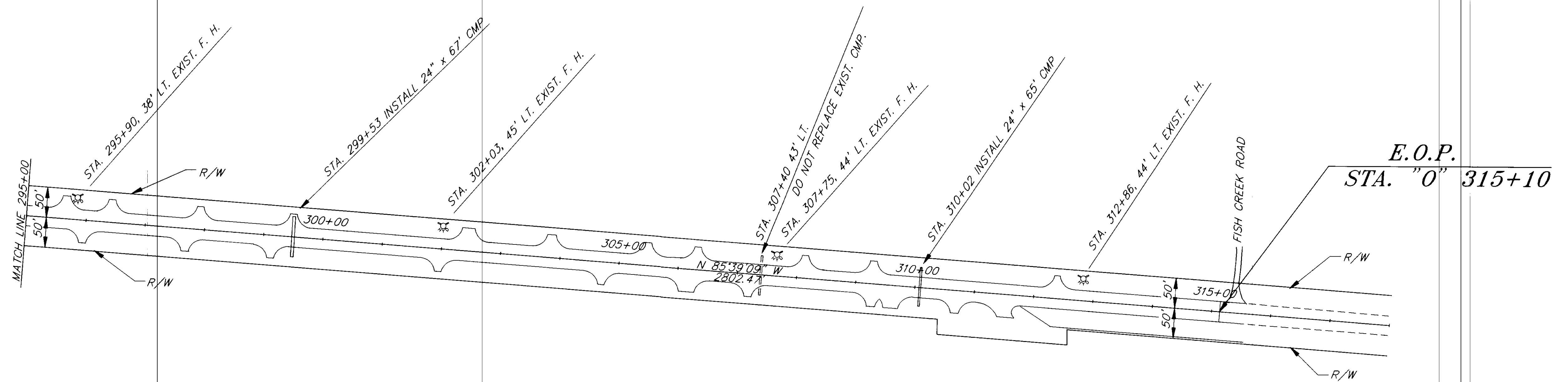
NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS

RECORD OF REVISIONS		
NO.	DATE	DESCRIPTION OF CHANGE
1	T.L. 1996	AS BUILT WITH CORRECTIONS

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 NORTH DOUGLAS HIGHWAY  
 HELIPORT TO EAGLECREST  
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 PROJECT NO. STP-0959(16) 71108  
**PLAN**

ALASKA	DESIGNED BY: C.W. CORREA	PROJECT NO. 71108
	DRAWN BY: BILL ADAMS	DATE: 5/28/93
	CHECKED BY: P. JONES	SHEET 7 OF 9



STATION	LT.	APPROX. WIDTH	CULVERT			REMARKS
			SIZE	MATERIAL	EXTEND BACK	
192+05	X	5'	18"	STEEL		
196+38	X	5'	18"	STEEL		COMBINE WITH DRIVEWAY
202+20	X	5'	18"	STEEL		①
208+12	X	5'	18"	STEEL		
213+16	X	5'	18"	STEEL		①
218+09	X	5'	18"	STEEL		
223+58	X	5'	18"	STEEL		COMBINE WITH PARKING PAD
229+76	X	5'	18"	STEEL		
235+64	X	5'	18"	STEEL		COMBINE WITH DRIVEWAY
241+38	X	5'	18"	STEEL		
247+87	X	5'	18"	STEEL		COMBINE WITH DRIVEWAY
253+33	X	5'	18"	STEEL		COMBINE WITH DRIVEWAY
257+78	X	5'	18"	STEEL		
261+73	X	5'	18"	STEEL		①
272+78	X	5'	18"	STEEL		
274+44	X	5'	18"	STEEL		WATER VAULT
277+69	X	5'	18"	STEEL		
284+16	X	5'	18"	STEEL		COMBINE WITH DRIVEWAY
290+43	X	5'	18"	STEEL		
295+90	X	5'	18"	STEEL		
302+03	X	5'	18"	STEEL		
307+75	X	5'	18"	STEEL		
312+86	X	5'	18"	STEEL		

NOTE:  
 ① REMOVE EXISTING 18" X 16' CMP AND INSTALL NEW 18" X 20' CMP 0.5' LOWER THAN THE EXISTING ONE TO FACILITATE DRAINAGE.

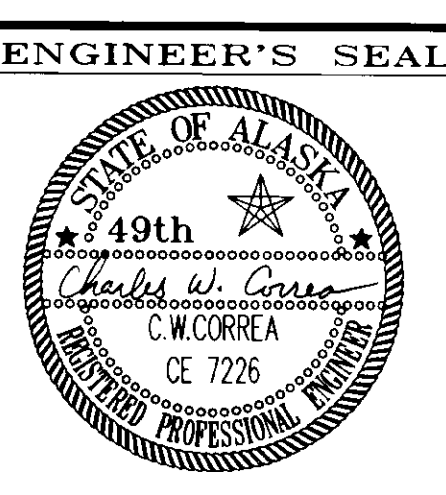
**AS-BUILT**  
 BY: T.L. DATE: 1996

PATH:	DATE:	DESCRIPTION OF CHANGE:
P: JUNU ANDOUGHEL DR/P3	1996	AS-BUILT

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 NORTH DOUGLAS HIGHWAY  
 HELIPORT TO EAGLECREST  
 ROAD IMPROVEMENTS  
 PROJECT NO. STP-0959(16) 71108  
**PLAN**

ALASKA  
 DESIGNED BY: C.W. CORREA  
 DRAWN BY: C.S.A./WDA  
 CHECKED BY: P. JONES  
 PROJECT NO. 71108  
 DATE: 5/28/93  
 SHEET 8 OF 9



NOTE: DO NOT SCALE FROM THESE PLANS-USE DIMENSIONS

# DRIVEWAY SUMMARY

TYPE CODES: D/W-DRIVEWAY  
P/P-PARKING PAD  
T/O-TURN OUT

NOTE: SEE SHEET 4 FOR CONSTRUCTION DETAILS & NOTES.  
Ⓐ REMOVE EXISTING 30' CMP, INSTALL 18"X50' CMP 0.5' LOWER.  
Ⓑ REMOVE EXISTING 20' CMP, INSTALL 18"X32' CMP 0.6' LOWER.

# DRIVEWAY SUMMARY

STA.	OFFSET		TYPE	WIDTH	EXIST. SURFACE		DRIVEWAY CULVERT				REMARKS
	LT	RT			PAVED	UNPAVED	SIZE	MATERIAL	EXTEND BACK	EXTEND AHD.	
191+93		X	D/W	27'		X					
192+50	X		D/W	16'		X					
193+82	X		D/W	18'	X		18"	STEEL		10'	
194+40	X		P/P	16'		X	18"	STEEL			REMOVE 18x40' & INSTALL 18x40'
195+30	X		P/P	13'	X						
196+20		X	P/P	16'	X						PAVE AROUND TO TIE INTO D/W
196+60		X	D/W	20'	X						MATCH EXISTING GRADE 50' RT. OF C
196+60	X		D/W	11'	X		18"	STEEL		10'	
197+48		X	P/P	60'	X						
199+09	X		D/W	12'	X						
199+20		X	P/P	17'	X						
202+20		X	D/W	15'	X						
202+66		X	D/W	16'	X						
203+56		X	D/W	14'	X						
203+79		X	P/P	18'		X					
205+09		X	D/W	12'		X					MATCH EXISTING GRADE 50' RT. OF C
205+09	X		P/P	23'		X	24"	STEEL		6'	
205+57		X	P/P	35'		X					
206+32	X		P/P	15'		X					
206+72		X	D/W	14'	X						MATCH EXISTING GRADE 50' RT. OF C
206+75	X		D/W	14'	X			STEEL	18"	6'	
207+11		X	P/P	37'		X					
208+72	X		D/W	18'	X		18"	STEEL		9'	
209+45	X		D/W	19'		X	18"	STEEL		7'	
210+39		X	P/P	24'	X						
210+60	X		D/W	17'	X						
212+09	X		D/W	19'		X	18"	STEEL		9'	
213+47		X	D/W	30'	X						
213+64	X		D/W	17'	X						
214+50		X	D/W	17'	X						
215+14		X	D/W	16'		X					
215+56		X	P/P	12'		X					
215+60	X		D/W	15'	X						
215+88	X		P/P	30'		X					
216+92		X	D/W	40'		X					PAVE TO TIE INTO CONCRETE APRON
217+22	X		D/W	16'	X		18"	STEEL		7'	
218+63		X	D/W	16'	X						
220+62	X		D/W	16'	X		18"	STEEL		9'	
220+92		X	D/W	17'	X						
221+07		X	P/P	20'	X						
221+53		X	P/P	16'		X					
221+85		X	D/W	15'		X	18"	STEEL		4'	
222+05	X		P/P	25'		X					
222+41	X		P/P	17'	X		18"	STEEL		8'	
223+34	X		P/P	27'		X	18"	STEEL		8'	
224+69	X		D/W	15'	X		18"	STEEL		12'	
224+98		X	D/W	17'	X						MATCH EXISTING GRADE 50' RT. OF C
225+25		X	P/P	16'		X					
227+29		X	D/W	18'	X						
228+98	X		D/W	17'	X		18"	STEEL		12'	
229+00		X	D/W	18'	X						
230+45	X		D/W	17'		X	18"	STEEL		10'	
231+23	X		D/W	21'	X						
232+21		X	D/W	15'	X						
232+48	X		D/W	16'	X		18"	STEEL		10'	
233+02		X	D/W	14'	X						INSTALL 18"x40'
235+09		X	D/W	15'		X					
235+75	X		D/W	15'	X		18"	STEEL		8'	
236+36	X		D/W	15'		X	18"	STEEL		7'	
236+78		X	D/W	15'	X						
237+04		X	P/P	18'		X					
237+45	X		D/W	19'	X		18"	STEEL		40'	REMOVE 18"x30' & INSTALL 18"x40'
238+98	X		D/W	14'		X				6'	
239+98	X		P/P	20'	X		18"	STEEL		9'	
240+09		X	D/W	21'	X		18"	STEEL		9'	
241+65	X		D/W	13'		X	18"	ALUM.		10'	
289+50		X	D/W	15'	NEW						
291+20		X	D/W	15'	NEW						

STA.	OFFSET		TYPE	WIDTH	EXIST. SURFACE		DRIVEWAY CULVERT				REMARKS
	LT	RT			PAVED	UNPAVED	SIZE	MATERIAL	EXTEND BACK	EXTEND AHD.	
243+62		X	D/W	15'	X						
243+66	X		D/W	17'	X		18"	STEEL			REMOVE EXIST. 18" X 30' & INSTALL 18" X 44'
245+76	X		D/W	17'		X	24"	STEEL			REMOVE 27'x24' & INSTALL 36'x18"
246+58		X	D/W	18'	X		18"	STEEL			INSTALL 18" X 36' CMP
249+04		X	D/W	15'		X					
249+25	X		D/W	18'	X		18"	STEEL			
249+25		X	P/P	25'	X						
250+10	X		D/W	12'	X						
252+97	X		D/W	14'	X		18"	STEEL		5'	
255+50	X		D/W	35'	X						
256+06		X	P/P	28'		X					
257+16		X	D/W	15'		X					
257+54	X		D/W	12'	X		18"	STEEL			
259+97		X	D/W	19'	X						
260+74	X		D/W	20'	X		18"	STEEL			
261+08		X	D/W	15'		X					
261+40	X		D/W	12'		X	18"	STEEL			Ⓑ
263+79	X		D/W	11'		X	18"	ALUMINUM			
265+10	X		D/W	14'	X		18"	STEEL			
265+85		X	D/W	16'		X					
266+21	X		D/W	13'	X		18"	STEEL			
267+21		X	D/W	24'		X					
268+20		X	D/W	22'		X					
268+54		X	P/P	33'		X					
270+51		X	D/W	15'		X					
270+79		X	P/P	33'		X					
272+33		X	D/W	12'		X					
272+60		X	D/W	12'		X					
277+86		X	D/W	16'		X					
278+57	X		D/W	17'		X	18"	STEEL			INSTALL 37'x18"
279+60	X		D/W	16'		X	18"	STEEL		7'	
279+92		X	D/W	16'		X					
283+92		X	D/W	12'	X						
283+95	X		D/W	16'	X						ABANDON EXISTING CULVERT
285+38		X	D/W	16'	X		18"	STEEL	4'		
285+63	X		D/W	11'	X		18"	STEEL			REMOVE 30'x18' & INSTALL 40'x18"
287+72		X	D/W	14'		X					
288+98		X	D/W	17'		X					
290+18		X	D/W	15'	X						
290+83	X		D/W	13'	X						
291+36	X		D/W	14'	X		18"	STEEL		6'	
291+38		X	D/W	24'		X					
292+98	X		D/W	15'	X						
294+28		X	D/W	24'	X						
295+68	X		D/W	15'	X		18"	STEEL		0	
295+90		X	D/W	16'	X						
296+49	X		D/W	17'	X		18"	STEEL			REMOVE 32'x18' & INSTALL 35'x18"
297+55		X	D/W	14'	X						
297+77	X		D/W	14'	X		18"	STEEL			REMOVE 30'x18' & INSTALL 37'x18"
300+14		X	D/W	14'		X					
300+50	X		D/W	17'	X		18"	STEEL		4'	
302+12		X	D/W	13'	X						
302+52	X		D/W	24'		X	18"	STEEL			INSTALL 56'x18"
303+84	X		D/W	17'	X		18"	STEEL			INSTALL 40'x18"
304+72		X	D/W	15'		X					
305+44	X		D/W	14'		X	18"	ALUMINUM			REMOVE 30'x18' & INSTALL 40'x18"
306+25		X	D/W	25'	X						
306+32	X		D/W	16'	X		18"	STEEL			REMOVE 32'x18' & INSTALL 40'x18"
307+12		X	D/W	16'		X					
308+25	X		D/W	14'		X	18"	STEEL		4'	
309+06		X	D/W	18'	X						
309+25	X		D/W	12'		X	18"	STEEL		4'	
309+50		X	P/P	30'		X					
310+70		X	D/W	16'	X						
311+40		X	D/W	15'	X						
312+10		X	T/O	126'	X						
312+25	X		D/W	11'		X	18"	ALUMINUM		10'	

**AS-BUILT**  
BY: T.L. DATE: 1996

NO.	DATE	DESCRIPTION OF CHANGE
1	1996	AS BUILT WITH CORRECTIONS

RECORD OF REVISIONS

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
NORTH DOUGLAS HIGHWAY  
HELIPORT TO EAGLECREST  
ROAD IMPROVEMENTS  
PROJECT NO. STP-0959(16) 71108  
ALASKA  
DRIVEWAY SUMMARY

DESIGNED BY: C.W. CORREA  
DRAWN BY: BILL ADAMS  
CHECKED BY: P. JONES  
PROJECT NO. 71108  
DATE: 5/28/93  
SHEET 9 OF 9

