

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEASTERN REGION  
DESIGN AND CONSTRUCTION DIVISION

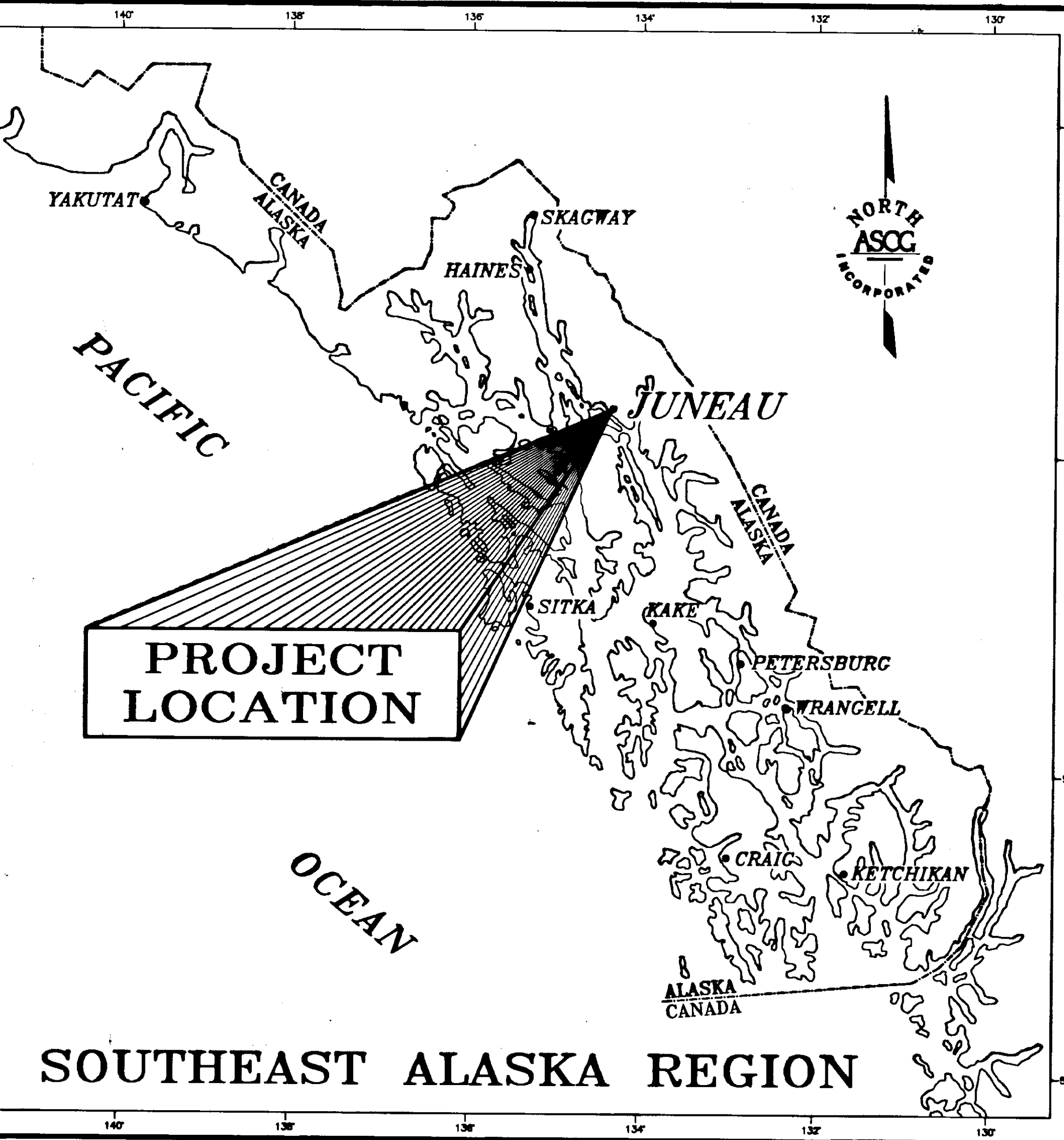
— JUNEAU —  
**GLACIER HIGHWAY**  
Highland Dr. to Egan Dr.

Grading, Paving, Drainage & Utilities  
F-M-0976 (1) (69062)

INDEX OF SHEETS

SHEET NUMBER	DESCRIPTION
1	TITLE SHEET
2-3	TYPICAL SECTIONS
4-5	ESTIMATE OF QUANTITIES
6	TRAFFIC CONTROL PLAN
7	DRIVEWAY SUMMARY
8	DRIVEWAY DETAILS
9-10	DRAINAGE SUMMARIES
11	SIGNING SUMMARY
12	UTILITY CONDUIT SUMMARY
13	RETAINING WALL SUMMARY
14-15	MISCELLANEOUS SUMMARIES
16-28	PLAN AND PROFILE SHEETS
29-31	BIKEPATH PLAN AND PROFILE SHEETS
32	INTERSECTION DETAILS
33-34	BUS PULL-OUT DETAILS
35-42	CONSTRUCTION DETAILS
43	MT. JUNEAU STORM DRAIN CLEANOUT
44	LANDSCAPE PLAN
45-46	PARKING STRUCTURE PLANS
47	ILLUMINATION PLAN
47-A	ILLUMINATION DETAILS
48-54	SIGNING STRIPING AND UTILITY PLANS

THE FOLLOWING STANDARD DRAWINGS APPLY TO THIS PROJECT:  
A-1, C-01.03, C-02.01, C-03.01, D-01.02, D-04.10, D-05.10, D-20.02, D-23.00, D-24.00, D-26.01, D-27.01, F-01.01, G-02.00, G-04.05W, G-14.04W, I-20.11, I-40.00, L-03.02, L-10.02, L-14.00, L-20.01, L-23.01, L-30.02, M-01.00, M-05.00, M-16.01, M-23.11, S-00.00, S-05.00, S-20.00, S-30.01, S-34.00, T-20.00, T-31.00 AND U-03.00



PROJECT SUMMARY

WIDTH OF PAVEMENT

"H" 0+28.46 TO "H" 2+38.26	=	24'-0"
STA. 10+00.00 (B.O.P.) TO STA. 14+50.00	=	35'-0"
STA. 14+50.00 TO STA. 19+50.00	=	39'-0"
STA. 19+50.00 TO STA. 81+00.00 (E.O.P.)	=	30'-0"
"A" 0+54.47 TO "A" 6+38.73	=	8'-0"
"B" 10+50.00 TO "B" 28+59.36	=	8'-0"

LENGTH OF PAVEMENT = 9,703.42 L.F., 1.838 MILES

LENGTH OF PROJECT = 9,703.42 L.F., 1.838 MILES

DESIGN DESIGNATION

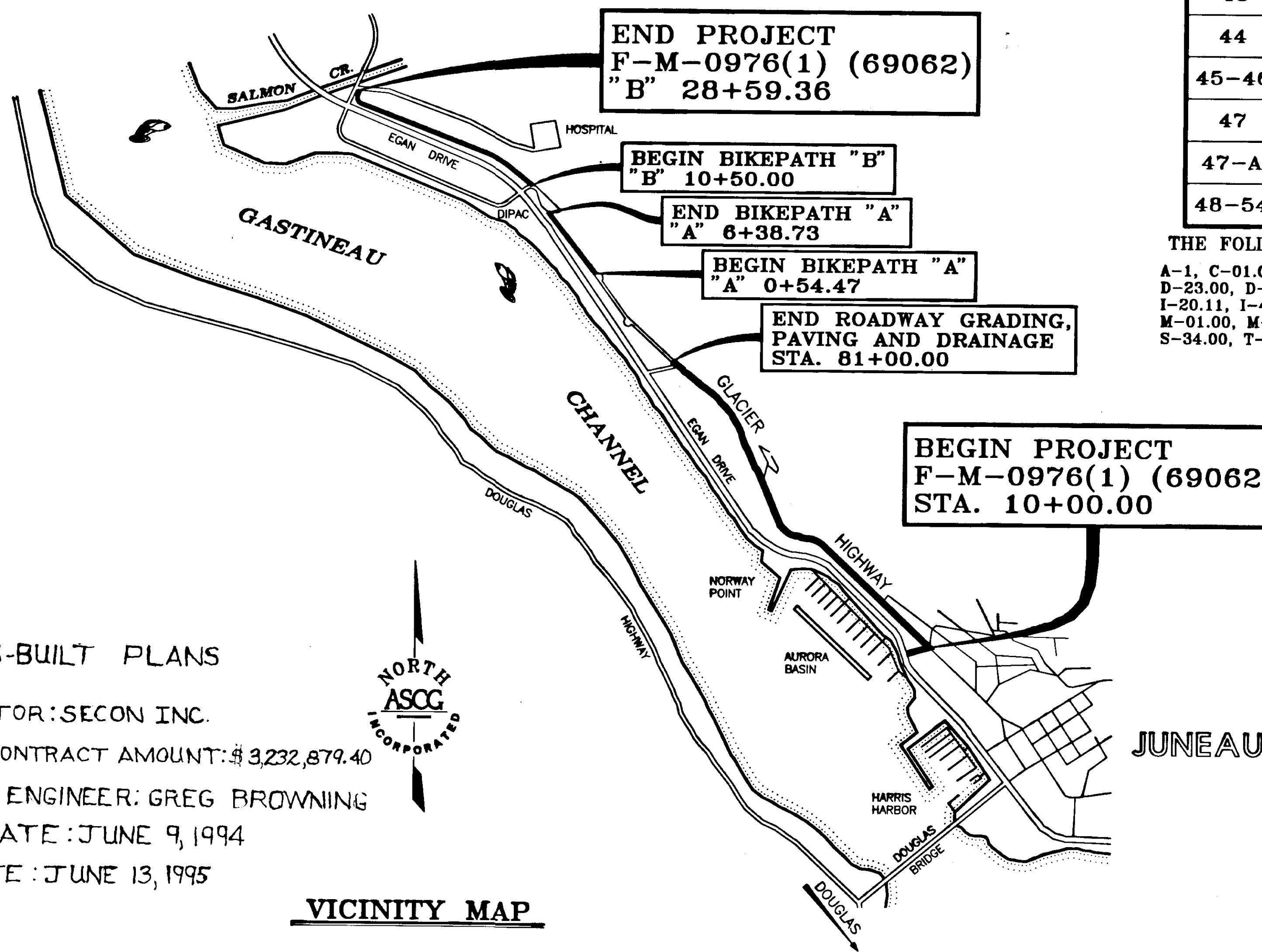
A.D.T. 1994	=	890
A.D.T. 2014	=	1,320
D.H.V.	=	190
% T	=	4%
V	=	40 M.P.H.
E.A.L.	=	151,040

AS-BUILT PLANS

CONTRACTOR: SECON INC.  
ORIGINAL CONTRACT AMOUNT: \$ 3,232,879.40  
PROJECT ENGINEER: GREG BROWNING  
START DATE: JUNE 9, 1994  
END DATE: JUNE 13, 1995



VICINITY MAP



STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN SECTION

APPROVED *[Signature]* 5/18/94  
SOUTHEAST REGION PRE-CONSTRUCTION ENGINEER DATE

APPROVED *[Signature]* 5/16/94  
DIRECTOR, SOUTHEAST REGION DESIGN AND CONSTRUCTION DATE

DRAWN IN AUTOCAD RELEASE 11 - DRAWING FILE NAME 69062-01.DWG

**ASCG**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

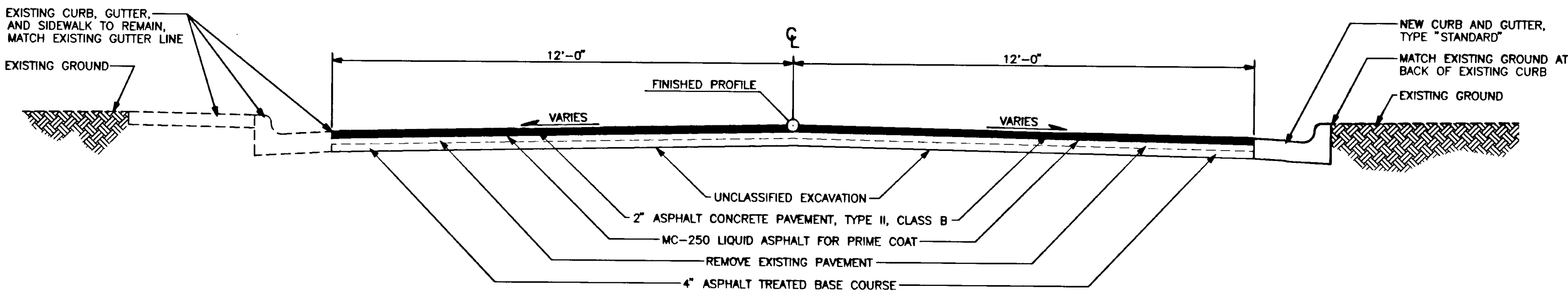
PROJECT NUMBER:  
F-M-0976(1) (69062)

DATE:  
MARCH 1994

SHEET 1 OF 55

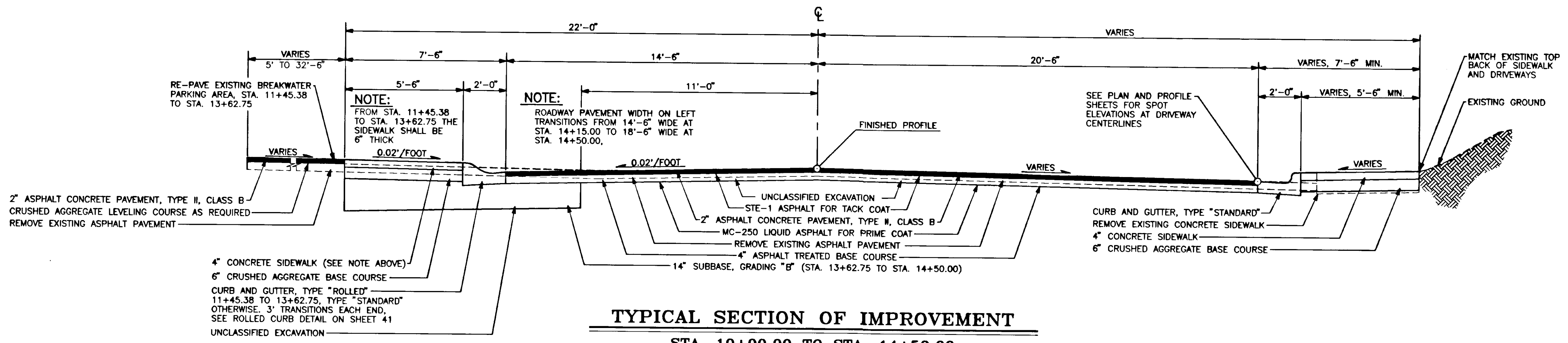
ENGINEER'S SEAL



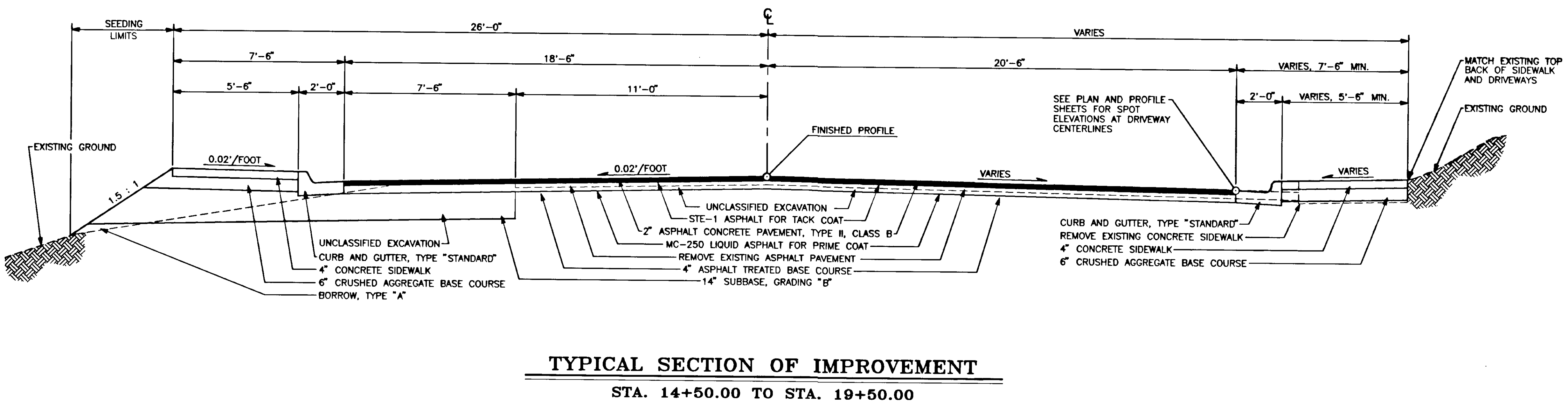


**TYPICAL SECTION OF IMPROVEMENT**  
**"H" 0+28.46 TO "H" 2+38.26**

BASIS OF ESTIMATE		
ITEM NUMBER	ITEM	ESTIMATING FACTOR
203 (6)	BORROW, TYPE "A"	1.90 TONS PER CUBIC YARD
301 (1)	CRUSHED AGGREGATE BASE COURSE	1.96 TONS PER CUBIC YARD
304 (1)	SUBBASE, GRADING "B"	1.90 TONS PER CUBIC YARD
306 (1)	ASPHALT TREATED BASE COURSE	108 POUNDS PER SQUARE YARD PER INCH DEPTH
306 (2)	ASPHALT MATERIAL, TYPE PBA-2	4 1/2% OF ITEM 306 (1)
401 (1)	ASPHALT CONCRETE PAVEMENT, TYPE II, CLASS B	113 POUNDS PER SQUARE YARD PER INCH DEPTH
401 (2)	PBA-2 ASPHALT CEMENT	6% OF ITEM 401 (1)
402 (2)	STE-1 ASPHALT FOR TACK COAT	0.10 GAL. PER SQUARE YARD - 253 GAL. PER TON
403 (1)	MC-250 LIQUID ASPHALT FOR PRIME COAT	0.25 GAL. PER SQUARE YARD - 256 GAL. PER TON



**TYPICAL SECTION OF IMPROVEMENT**  
**STA. 10+00.00 TO STA. 14+50.00**



**TYPICAL SECTION OF IMPROVEMENT**  
**STA. 14+50.00 TO STA. 19+50.00**

NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0978(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
**TYPICAL SECTIONS**



DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

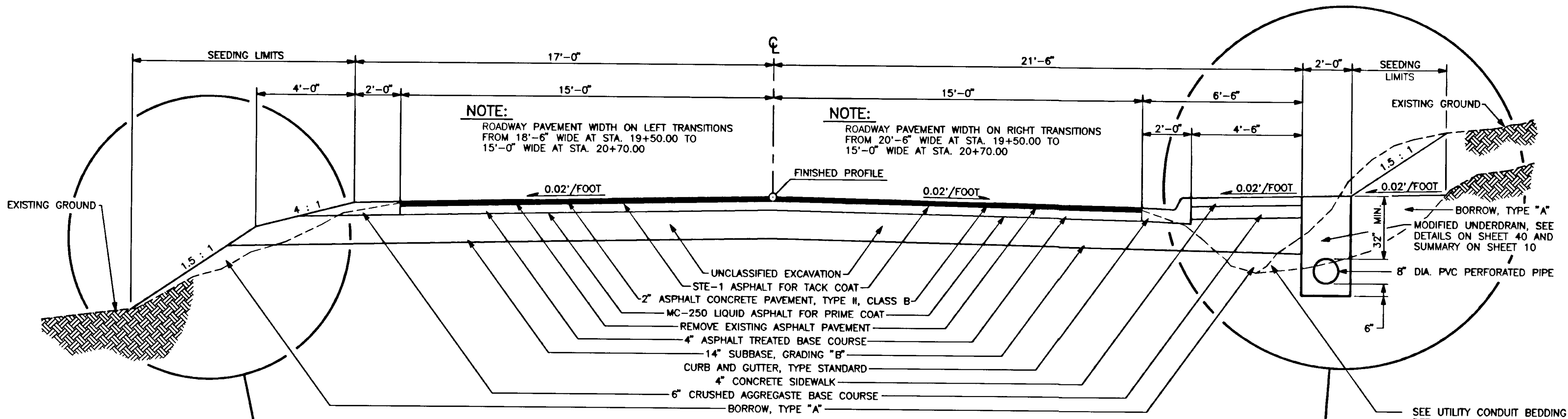
DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

PATH = E:\ACAD\NORWAY\T  
DRAWN IN AUTOCAD RELEASE 11  
DRAWING PLOT SCALE: 1" = 1'  
DRAWING LAST UPDATED: 3/17/1994

PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 2 OF 55

**GENERAL NOTES**

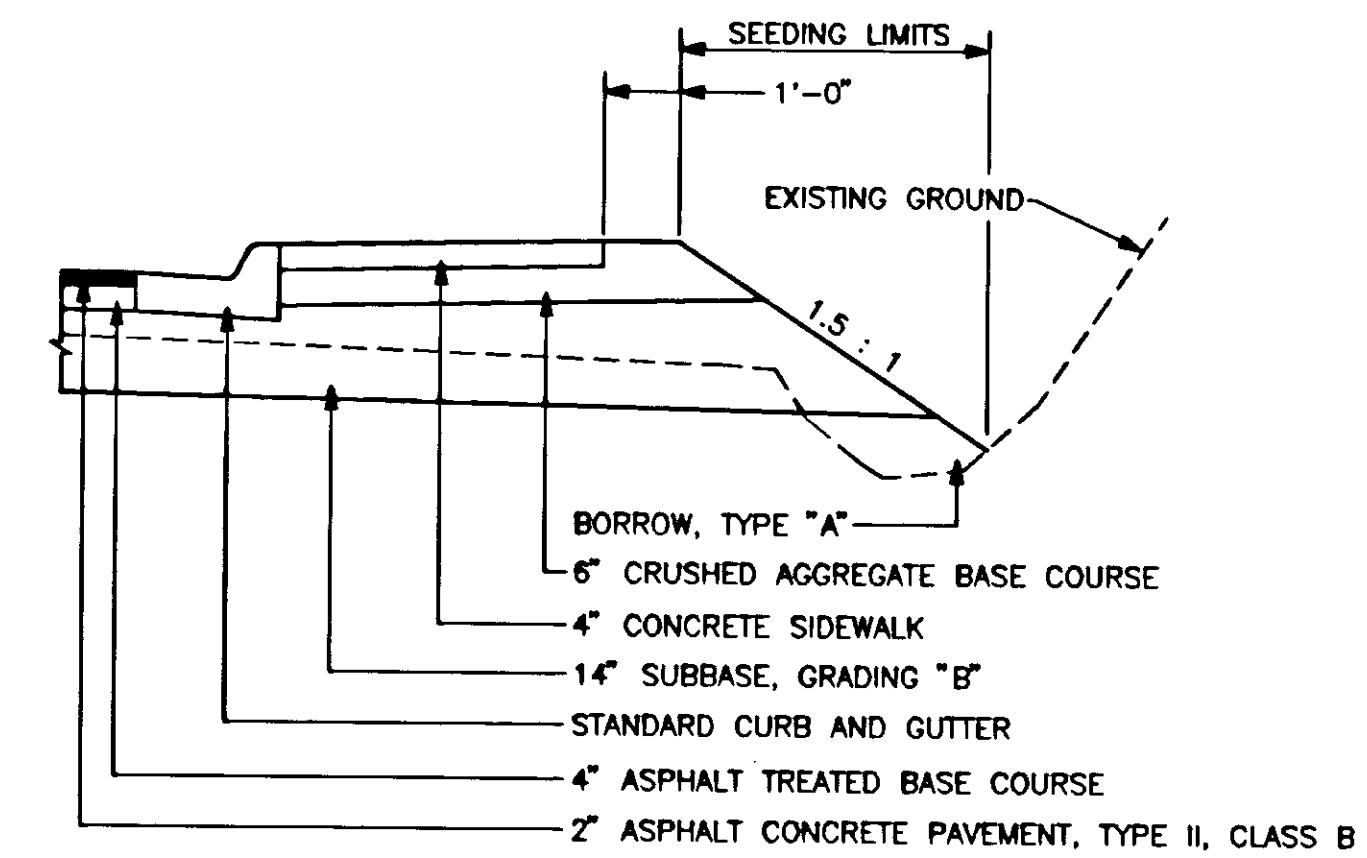
1. THE LOCATIONS OF EXISTING TOPOGRAPHY, UTILITIES, BUILDINGS, ETC. AS SHOWN ON THESE PLANS ARE APPROXIMATE ONLY AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR.
2. ALIGNMENT AND GRADE AS SHOWN ON THESE PLANS ARE SUBJECT TO MINOR FIELD REVISIONS.
3. THE LOCATIONS OF PROPOSED IMPROVEMENTS (I.E. CULVERTS, INLETS, MAILBOXES, ETC) ARE SUBJECT TO MINOR FIELD REVISIONS.
4. ONGOING CONSTRUCTION UNDER CITY AND BOROUGH OF JUNEAU CONTRACT NO. E93-123, CHANNEL DRIVE SEWER EXTENSION WILL CHANGE CONDITIONS UPON WHICH THE DESIGN OF THIS PROJECT IS BASED. MINOR FIELD ADJUSTMENTS MAY BE NECESSARY IN THE LOCATION AND QUANTITY OF CERTAIN ITEMS OF WORK.



**TYPICAL SECTION OF IMPROVEMENT**

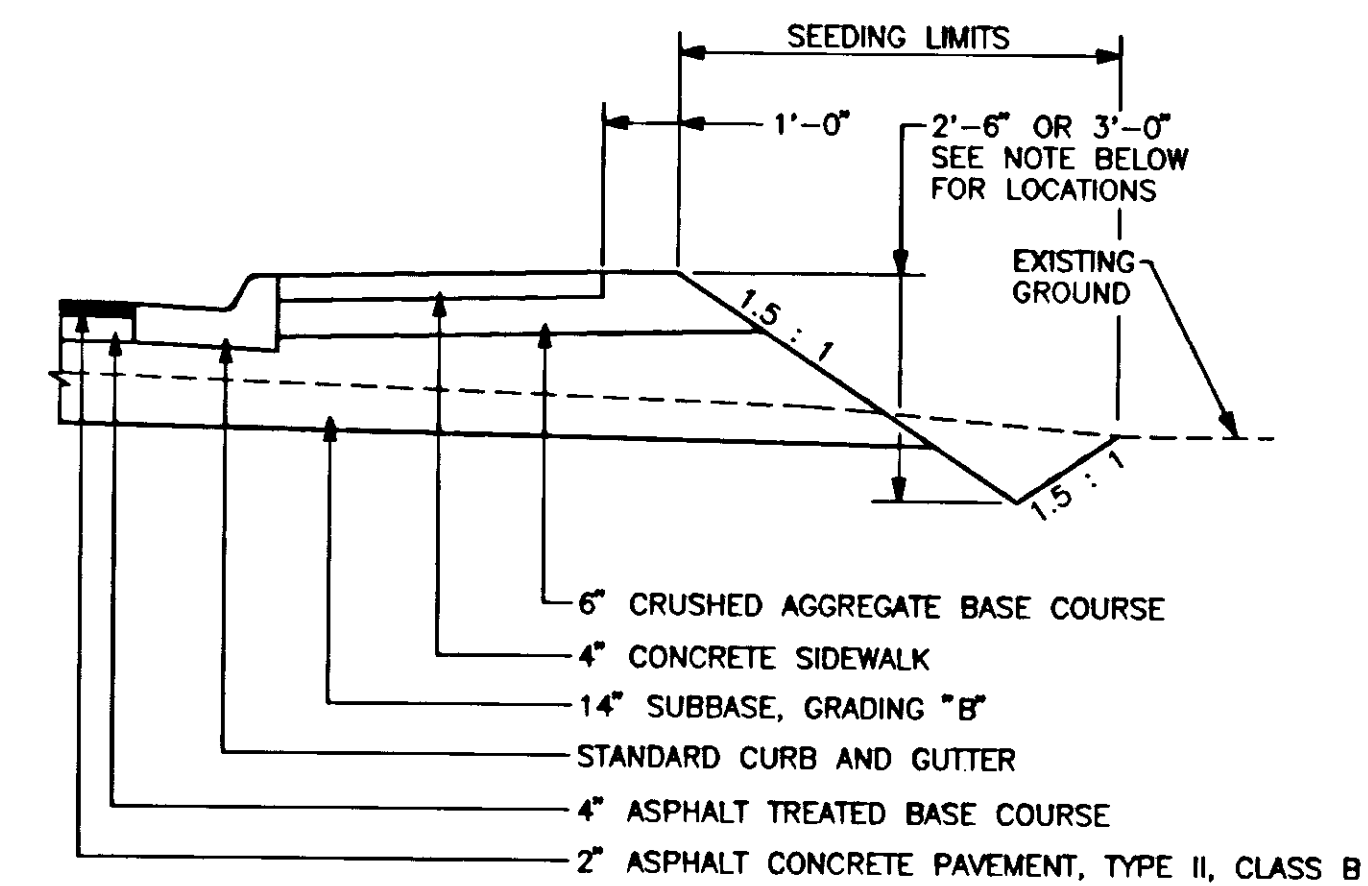
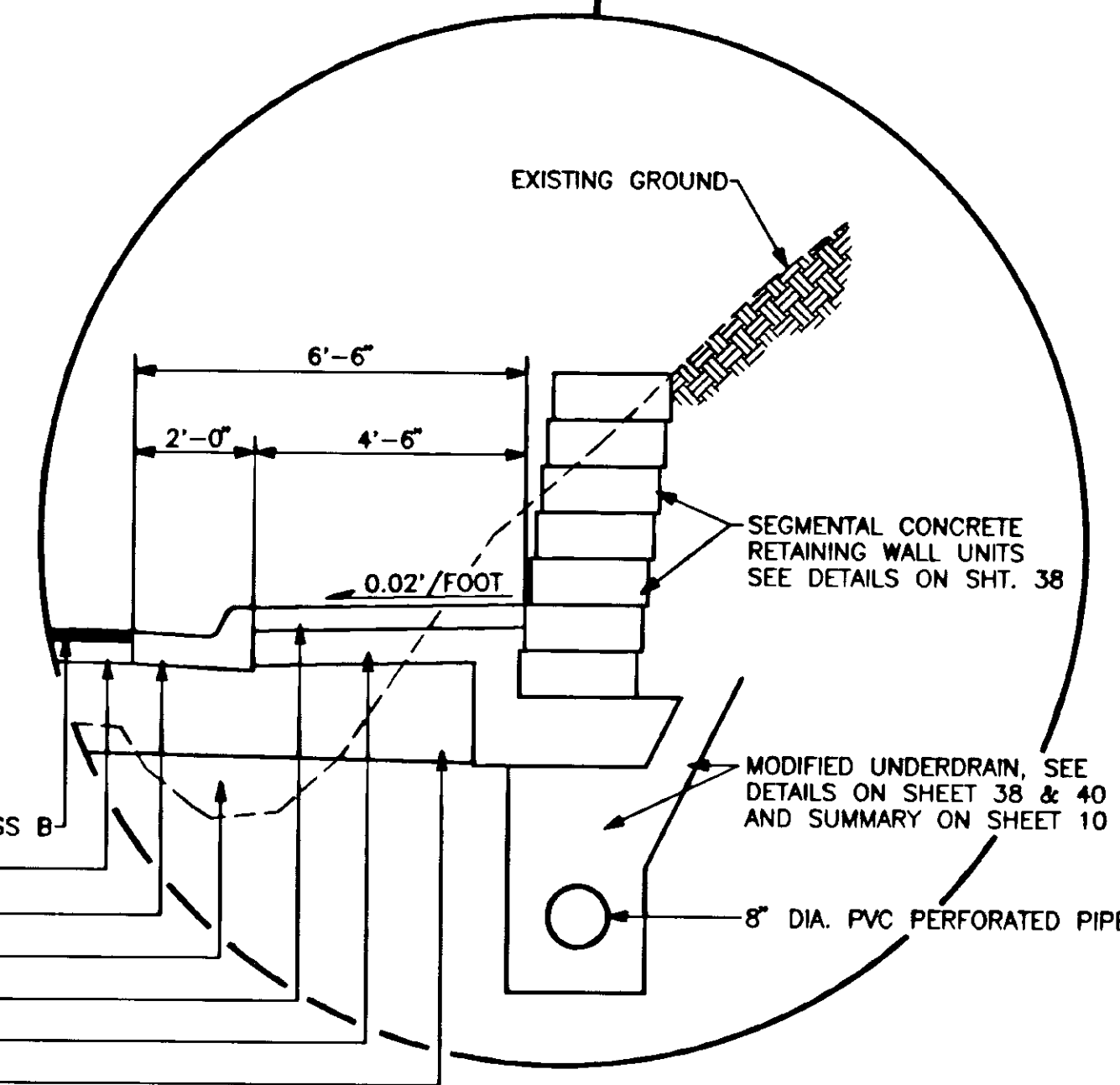
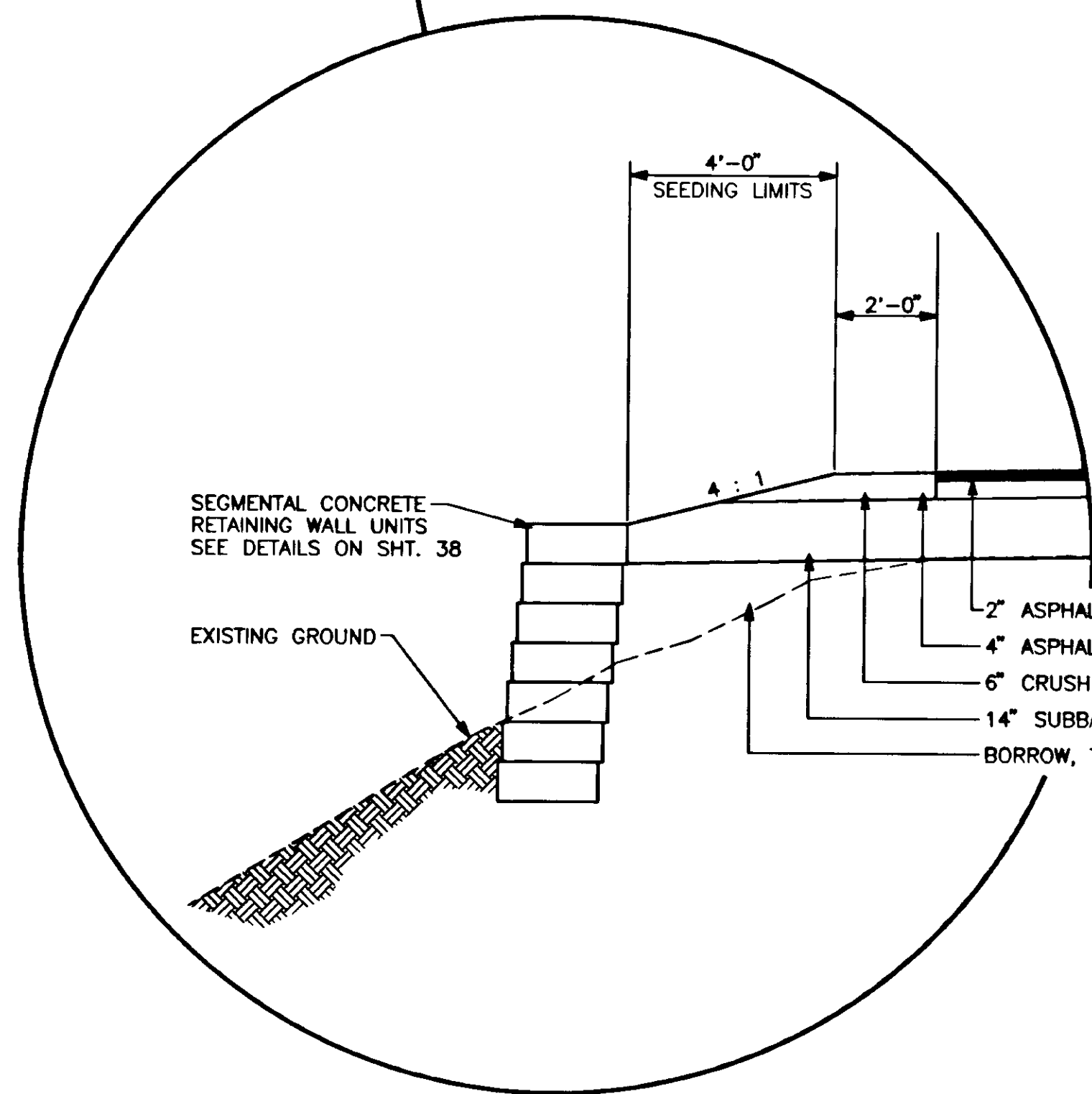
STA. 19+50.00 TO STA. 81+00.00

NOTE:  
PAVEMENT WILL BE ROTATED ABOUT THE CENTERLINE FOR SUPERELEVATION TRANSITIONS.



**SLOPE EXCEPTION DETAIL**

THIS DETAIL APPLYS FROM 30+75 TO 31+75, RT., FROM 49+50 TO 50+36, RT. AND FROM 68+50 TO 69+09, RT.



**SPECIAL DITCH DETAIL**

NOTE:  
3'-0\"/>

NO.	DATE:	DESCRIPTION OF CHANGE:

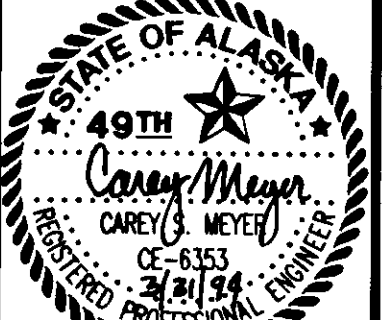
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
TYPICAL SECTIONS



DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 3 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

ITEM NUMBER	ITEM	PAY UNIT	SHEET TOTALS																PROJECT TOTAL	ITEM NUMBER						
			16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31			44					
120 (1)	DBE ADJUSTMENT	CONTINGENT SUM																	ALL REQUIRED	120 (1)						
201 (1B)	CLEARING	LUMP SUM																	ALL REQUIRED	201 (1B)						
201 (6)	SELECTIVE TREE REMOVAL	EACH				1	1	9	APPROXIMATE PROJECT QUANTITY EQUALS 1.6 ACRES																32 12	201 (6)
202 (1)	REMOVAL OF STRUCTURES AND OBSTRUCTIONS	LUMP SUM		(1)		(5)	(1)	(3)											ALL REQUIRED	202 (1)						
202 (2)	REMOVAL OF PAVEMENT	SQUARE YARD	1383	2367	2043	1519	1548	1507	1529	1544	1512	1479	1454	1467	1881	137		634	21425 22,004	202 (2)						
202 (3)	REMOVAL OF SIDEWALK	SQUARE YARD	36	258	144														1078 1,378	202 (3)						
202 (4)	REMOVAL OF CULVERT PIPE	LINEAR FOOT		290	386	568	162	164	106	244	356	242	278	140	186	298	583	57	2388 3,122	202 (4)						
202 (6)	REMOVAL OF MANHOLES	EACH		1		2												2 3	202 (6)							
202 (8)	REMOVAL OF INLETS	EACH		3	3	3												8 10	202 (8)							
202 (10)	SINGLE MAILBOX INSTALLATION	EACH				1	1		1	1			1	1				7 6	202 (10)							
202 (11)	MULTIPLE MAILBOX INSTALLATION	EACH				1	1				1								3	202 (11)						
202 (12)	DOUBLE MAILBOX INSTALLATION	EACH				1	1				1									202 (12)						
203 (3)	UNCLASSIFIED EXCAVATION	CUBIC YARD	52	925	1254	708	1273	1366	1365	512	202	573	179	235	739	312	160	170	11 10	203 (3)						
203 (6)	BORROW, TYPE "A"	TON	4	190	158	580	249	632	458	1215	1014	1076	1595	1197	300	255	76	38	10,579 10,025	203 (6)						
301 (1)	CRUSHED AGGREGATE BASE COURSE	TON	245	245	220	265	235	290	280	235	235	285	382	330	185	315	475	350	5,087 4,572	301 (1)						
304 (1)	SUBBASE, GRADING "B"	TON		575	1570	2060	1990	2020	1975	2100	2210	2104	2071	2060	1965				2,797 2,700	304 (1)						
306 (1)	ASPHALT TREATED BASE COURSE	TON	121	583	544	433	434	436	438	460	479	456	442	435	460				5833.9 5,721	306 (1)						
306 (2)	ASPHALT MATERIAL, TYPE PBA-2	TON	5.4	27.9	24.5	19.5	19.5	19.6	19.7	20.7	21.6	20.6	19.8	19.6	20.7				262.53 259.1	306 (2)						
401 (1)	ASPHALT CONCRETE PAVEMENT, TYPE II, CLASS B	TON	95	350	291	236	239	241	253	255	252	259	287	256	264				3870 3,278	401 (1)						
401 (2)	PBA-2 ASPHALT CEMENT	TON	5.7	21.0	17.5	14.2	14.3	14.5	15.2	15.3	15.1	15.9	15.0	15.4	15.8				242.81 194.9	401 (2)						
402 (2)	STE-1 ASPHALT FOR TACK COAT	TON	0.2	1.0	1.0	0.8	0.8	0.8	0.8	0.9	0.9	0.8	0.9	0.8	0.9				7.46 10.6	402 (2)						
403 (3)	MC-250 LIQUID ASPHALT FOR PRIME COAT	TON	0.8	2.8	2.5	2.1	2.1	2.1	2.2	2.2	2.2	2.3	2.2	2.3	2.3				0 28.1	403 (3)						
506 (5)	PARKING STRUCTURES	EACH							1										2	506 (5)						
506 (6)	STAIRWAY ADJUSTMENT	LUMP SUM																		506 (6)						
506 (7)	LANDSCAPE TIMBERS	LINEAR FOOT												61	7					506 (7)						
509 (1)	SEGMENTAL CONCRETE RETAINING WALL	SQUARE FOOT			680	1010	70	2580	1060	1755	360	1959	5179	1298	1110					509 (1)						
601 (1)	METAL FLUME DOWNDRAIN	LINEAR FOOT							6	6		5						19,180.7 17,061	601 (1)							
603 (17-12)	12 INCH PIPE	LINEAR FOOT		88										80				492 168	603 (17-12)							
603 (17-18)	18 INCH PIPE	LINEAR FOOT		366	278	92		94								32		854.5 1,078	603 (17-18)							
603 (17-24)	24 INCH PIPE	LINEAR FOOT			82	156	228	118	174	240	214	56	60	98	176	6			1,646.5 1,608	603 (17-24)						
603 (17-48)	48 INCH PIPE	LINEAR FOOT											68						68	603 (17-48)						
603 (23)	COLLECTION BASINS	EACH				1	1	1	1	4	2		1	1	3			21 15	603 (23)							
604 (3)	RECONSTRUCT EXISTING MANHOLE	EACH																1 2	604 (3)							
604 (4)	ADJUST EXISTING MANHOLE	EACH		2	2	2	3	2	2	3	2	1	3	2	1	1	1	23 28	604 (4)							
604 (5A)	INLET, TYPE "A"	EACH		2	3	1	1	1	1			1						4 12	604 (5A)							
604 (5B)	INLET, TYPE "B"	EACH		2	1	2		2			2		1	2				21 12	604 (5B)							
604 (5C)	FIELD INLET	EACH			1	2	1		1	2	1	1						11 13	604 (5C)							
604 (8)	MT. JUNEAU STORM DRAIN CLEANOUT	LUMP SUM														3				604 (8) (8A)						
605 (7)	MODIFIED UNDERDRAIN SYSTEM	LINEAR FOOT			216	222	229	596	595	506	595	599	599	597	430				4715 5,184	605 (7)						
606 (1)	W-BEAM GUARDRAIL	LINEAR FOOT																175 175.5	606 (1)							
606 (5)	REMOVAL AND DISPOSAL OF GUARD RAIL	LINEAR FOOT		40	600	600	175	80		91			112.5	597	38	25		1,648	606 (5)							
606 (6)	END ANCHORAGES	EACH																		606 (6)						
607 (3)	6 FOOT CHAIN LINK FENCE	LINEAR FOOT				425	21								1				1	606 (6)						
607 (4)	RECONSTRUCTED FENCE	LINEAR FOOT														560				607 (3)						
607 (7)	REMOVAL OF FENCE	LINEAR FOOT				425	21													607 (4)						
608 (3)	ASPHALT SIDEWALK	SQUARE YARD													560	1037	606		2,865 1,008	607 (7)						
608 (6)	4-INCH CONCRETE SIDEWALK	SQUARE YARD	53	352	468	142	213	246	263	254	205	245	162	207	204				2,809 2,304	608 (3)						
608 (7)	6-INCH CONCRETE SIDEWALK	SQUARE YARD		330	95	165	135	48	33	51	75	55	135	95	46				2868 3,014	608 (6)						
609 (2A)	CURB AND GUTTER, TYPE STANDARD	LINEAR FOOT	285	828	973	599	608	589	595	653	572	601	595	605	501				1662.5 1,263	608 (7)						
609 (2B)	CURB AND GUTTER, TYPE ROLLED	LINEAR FOOT		218																609 (2A)						
615 (1)	STANDARD SIGNS	SQUARE FOOT		12.41	40.58		16	3		16	11.25	3	3	25	16.75	5.25	4	8.5	212 218	609 (2B)						
618 (1)	SEEDING	ACRE		0.04	0.07	0.19	0.12	0.09	0.10	0.09	0.14	0.11	0.09	0.08	0.08	0.07	0.03	0.03	167.7 164.74	615 (1)						
618 (5)	SEEDING, WILDFLOWER MIX	ACRE																		618 (1)						
621 (1)	TREES	EACH																4.501 1.33		618 (5)						
625 (1)	PIPE HAND RAIL	LINEAR FOOT								102		112						0.4	12	621 (1)						

NO.	DATE	DESCRIPTION OF CHANGE

**RECORD OF REVISIONS**

STATE OF ALASKA  
**DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES**  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
**ESTIMATE OF QUANTITIES**

**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 4 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

ITEM NUMBER	ITEM	PAY UNIT	SHEET TOTALS																	PROJECT TOTAL	ITEM NUMBER	
			16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	44			
627 (4)	FIRE HYDRANT ADJUSTMENT	EACH																			5 6	627 (4)
627 (6)	FIRE HYDRANT RELOCATION	EACH																			7	627 (6)
627 (10)	ADJUSTMENT OF VALVE BOX	EACH																			41 34	627 (10)
627 (11)	RELOCATION OF VALVE AND VALVE BOX	EACH																			14 5	627 (11)
635 (1)	INSULATION BOARD	M.B.M.																			63 7.54	635 (1)
639 (1)	DRIVEWAYS	EACH																			68 58	639 (1)
640 (1)	MOBILIZATION AND DEMOBILIZATION	LUMP SUM																			ALL REQUIRED	640 (1)
641 (1)	EROSION AND POLLUTION CONTROL ADMINISTRATION	LUMP SUM																			ALL REQUIRED	641 (1)
641 (2)	EROSION AND POLLUTION CONTROL	CONTINGENT SUM																			ALL REQUIRED	641 (2)
642 (1)	CONSTRUCTION SURVEYING	LUMP SUM																			ALL REQUIRED	642 (1)
642 (2)	3 PERSON SURVEY PARTY	LABOR HOUR																			5.26 10	642 (2)
643 (2)	TRAFFIC MAINTENANCE	LUMP SUM																			ALL REQUIRED	643 (2)
643 (3)	PERMANENT CONSTRUCTION SIGNING	LUMP SUM																			ALL REQUIRED	643 (3)
643 (4)	CONSTRUCTION SIGN	EACH PER DAY																			1,511 2,500	643 (4)
643 (5)	TYPE II BARRICADE	EACH PER DAY																			4,470 1,000	643 (5)
643 (7)	TRAFFIC CONE	EACH PER DAY																			18,110 10,000	643 (7)
643 (13)	TEMPORARY PAVEMENT MARKING	STATION	3	12	9	9	12	12	12	12	7	6	12	12	12						208.6 130	643 (13)
643 (15)	FLAGGING	HOUR																			4050.5 7,000	643 (15)
643 (18)	WATERING	M-GAL.																			452 1,000	643 (18)
644 (1)	FIELD OFFICE	LUMP SUM																			ALL REQUIRED	644 (1)
645 (1)	TRAINING PROGRAM 1 TRAINEE/APPRENTICE	LABOR HOUR																			773 500	645 (1)
660 (3)	HIGHWAY LIGHTING SYSTEM COMPLETE	LUMP SUM																			ALL REQUIRED	660 (3)
661 (2)	LOAD CENTER, TYPE 2	EACH																			3	661 (2)
670 (1)	PAINTED TRAFFIC MARKINGS	LUMP SUM																			ALL REQUIRED	670 (1)
670 (8)	RECESSED PAVEMENT MARKERS	EACH	5	8	11	8	12	13	13	11	10	8	11	11	6						140 127	670 (8)
680 (1)	RELOCATION OF AEL & P FACILITIES	LUMP SUM																			ALL REQUIRED	680 (1)
681 (1)	CONDUIT BANK, COMPLETE	LUMP SUM																			ALL REQUIRED	681 (1)
681 (2)	VAULTS	EACH																			5	681 (2)
681 (2)	EXTRA CONDUIT BANK WORK	LUMP SUM																			ALL REQUIRED	681 (2)

BY:	DATE:	DESCRIPTION OF CHANGE:

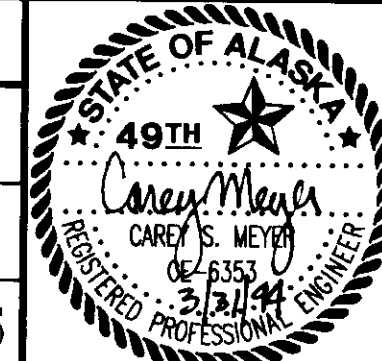
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
ESTIMATE OF QUANTITIES

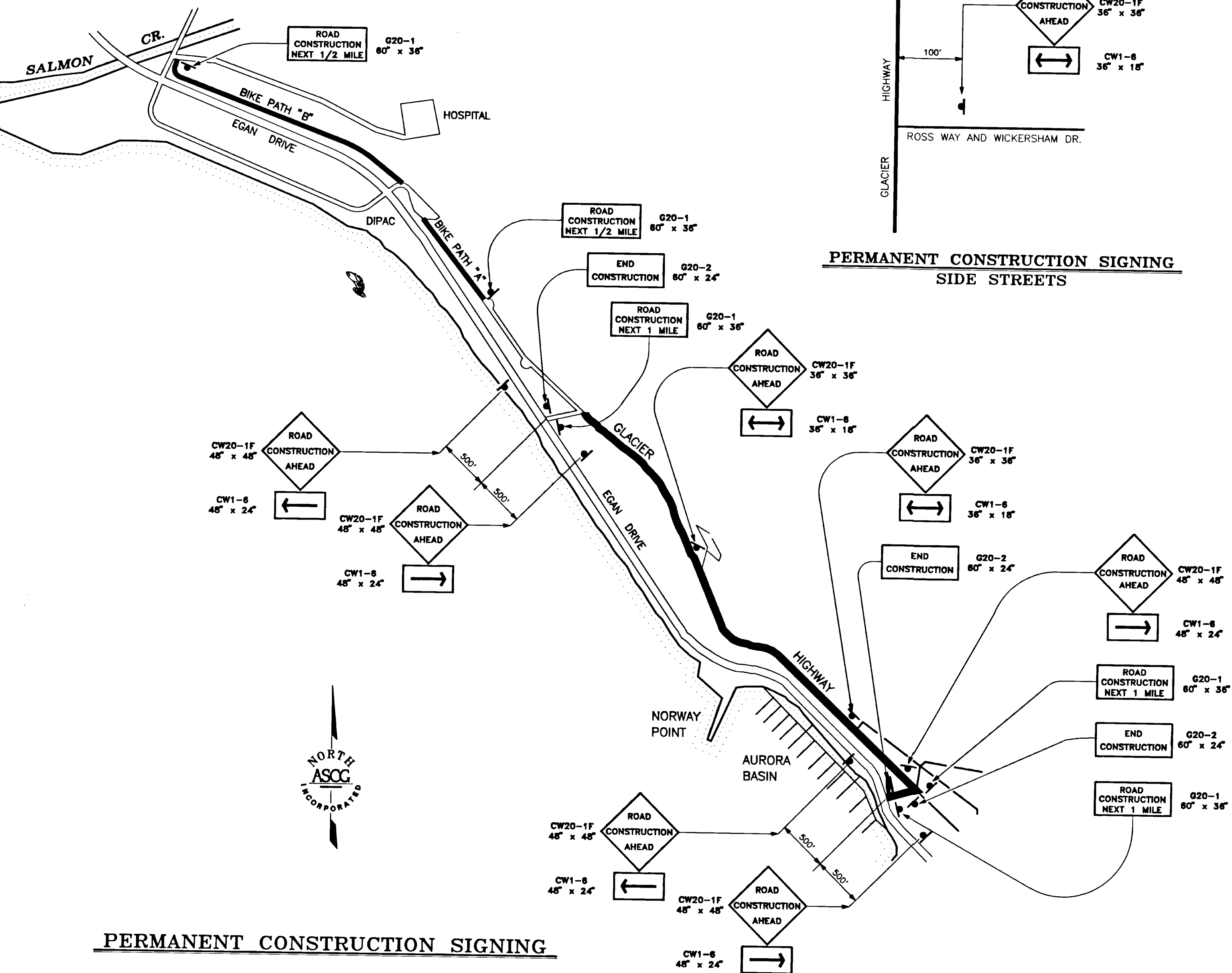


DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

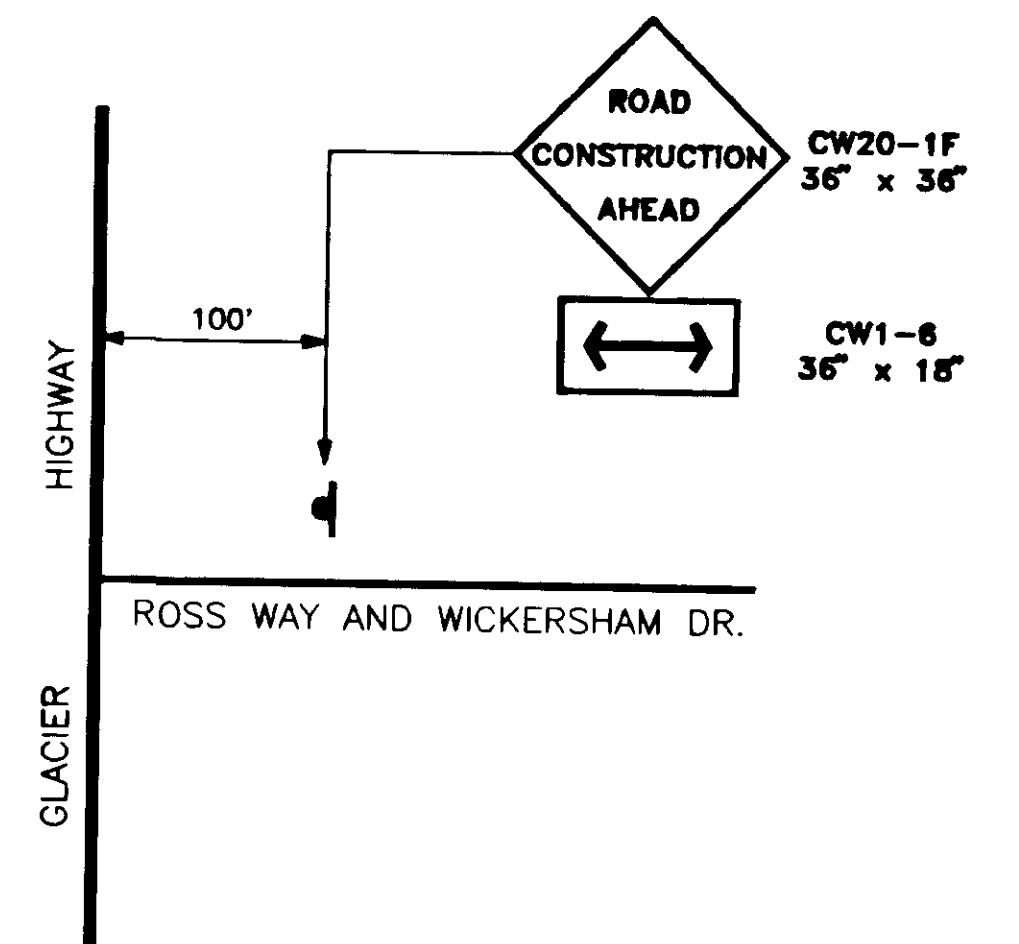
PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 5 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

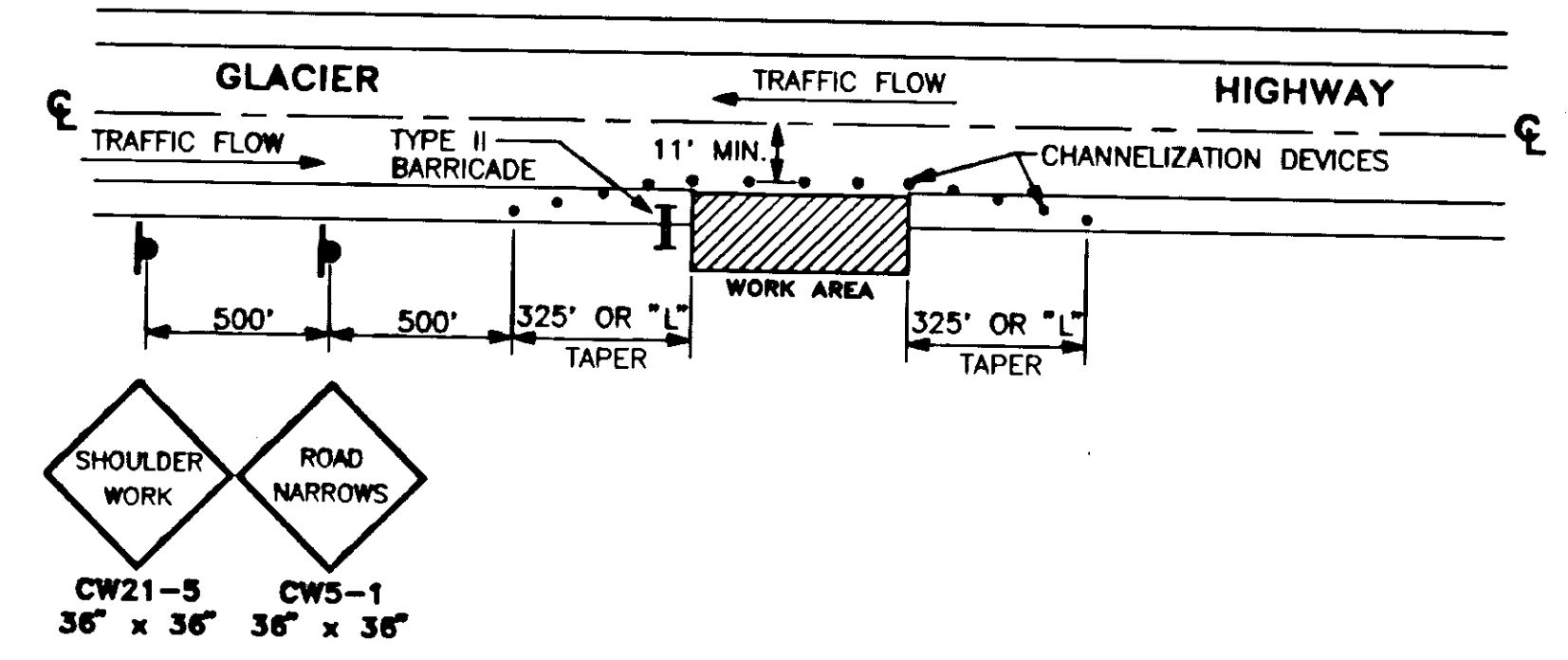


**PERMANENT CONSTRUCTION SIGNING  
SIDE STREETS**



**TRAFFIC CONTROL NOTES**

- ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. REFER TO STANDARD DRAWING C-03.01 FOR ONE LANE CLOSURE TRAFFIC CONTROL DETAILS.
- TWO-WAY COMMUNICATIONS SHALL BE MAINTAINED BETWEEN FLAGGERS AT ALL TIMES, EITHER THROUGH VISUAL CONTACT OR THROUGH RADIO CONTACT.
- TWO TRAFFIC LANES SHALL BE MAINTAINED AT ALL TIMES DURING NON-WORKING HOURS AND IN NON-WORK AREAS WITHIN THE PROJECT LIMITS.
- DRIVING LANES SHALL BE A MINIMUM OF 11' WIDE.
- INTERSECTING SIDE STREETS SHALL REMAIN ACCESSIBLE AT ALL TIMES.
- DRIVEWAYS MAY BE CLOSED ONLY DURING PERIODS WHEN ACTUAL WORK IS BEING PERFORMED ON GIVEN DRIVEWAY. THE PERIOD OF CLOSURE MAY NOT EXCEED EIGHT HOURS. AFFECTED RESIDENTS SHALL BE GIVEN NOTICE 24 HOURS PRIOR TO THE CLOSURE OF THE DRIVEWAY ACCESS.
- CONES SHALL BE USED FOR CHANNELING TRAFFIC THROUGH THE CONSTRUCTION AREA, CONES SHALL HAVE A MINIMUM HEIGHT OF 28 INCHES. MAXIMUM SPACING BETWEEN THE CONES SHALL BE EQUAL TO THE SPEED LIMIT IN FEET.
- THE MAXIMUM LENGTH OF A TWO-WAY ONE-LANE WORK AREA SHALL BE 1000 FT. THE MINIMUM SEPARATION BETWEEN ONE-LANE WORK AREAS SHALL BE 1000 FT.
- TEMPORARY PAVEMENT MARKING SHALL BE REQUIRED AS DESCRIBED IN SECTION 643-3.04 OF THE SPECIFICATIONS.
- FLOOD LIGHTING SHALL BE PROVIDED FOR FLAGGER STATIONS DURING ALL NIGHT TIME OPERATIONS.
- A SINGLE FLAGGER MAY BE APPROVED BY THE ENGINEER IF THE ENTIRE WORK AREA IS LESS THAN 50 FT. LONG AND IS VISIBLE FROM BOTH DIRECTIONS.



**TYPICAL SHOULDER WORK ZONE  
TRAFFIC CONTROL**

**PERMANENT CONSTRUCTION SIGNING**

DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

STATE OF ALASKA  
**DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES**  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
**TRAFFIC CONTROL PLAN**

**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: **D.L.M.**  
 DRAWN BY: **J.E.M.**  
 CHECKED BY: **D.L.M.**

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS  
PATH - E:\AD\NORWAYPT  
 DRAWN IN AUTOCAD RELEASE 11  
 DRAWING PLOT SCALE: 1" = 1'  
 DRAWING LAST UPDATED: 3/17/1994  
 PROJECT No.:  
**69062**  
 DATE:  
**MARCH 1994**  
 SHEET **6** OF **55**



**DRIVEWAY SUMMARY**

STATION	OFFSET		RADIUS		DRIVEWAY WIDTH	DEPRESSED CURB WIDTH		LANDING		PAVING LIMITS	RECONSTRUCTION LIMITS	CROSS SLOPE	PAVING MATERIAL	REMARKS
	LEFT	RIGHT	LEFT	RIGHT		LEFT	RIGHT	% GRADE	LENGTH					
21+97.51		X	20'		14'	10'	10'	2%	5.00'	21+97.76, 30.22' RT.	21+95.13, 52.67' RT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMITS
22+38.26		X	20'		14'	10'	10'	2%	5.00'	22+39.01, 26.23' RT.	22+42.73, 46.18' RT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMITS
23+47.42		X	63.5'		26'	31.75'	31.75'	2%	5.00'	23+47.49, 25.82' RT.	23+48.42, 40.56' RT.	LT. TO RT.	ASPHALT	DEPRESSED CURB AND SIDEWALK CONTINUOUS THROUGH BOTH DRIVEWAYS
23+88.10		X			20'			2%	5.00'	23+88.00, 30.05' RT.	23+88.02, 30.82' RT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMITS, RECONSTRUCTION WIDTH EQUAL TO DRIVEWAY WIDTH
25+89.01		X	49'		16'	11.5'	37.5'	2%	0.09'	25+88.97, 30.00' RT.	25+88.97, 30.00' RT.	LT. TO RT.	ASPHALT	GRADE PARKING AREA TO MATCH CROSS SLOPE AT PAVING LIMITS
26+58.90		X	36'		20'	10'	26'	2%	7.76'	26+58.90, 26.50' RT.	26+58.90, 29.26' RT.	LT. TO RT.	ASPHALT	MATCH EXISTING ASPHALT DRIVEWAY
27+11.19		X	20'		20'	10'	10'	2%	10.32'	26+58.90, 26.50' RT.	26+58.90, 29.26' RT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMITS, RECONSTRUCTION WIDTH EQUAL TO DRIVEWAY WIDTH
27+60.38		X	30'		30'	15'	15'	2%	5.89'	27+60.76, 27.39' RT.	27+60.76, 27.39' RT.	LT. TO RT.	CONCRETE	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMITS
28+09.63		X	40'		NONE	20'	20'	N/A	N/A	N/A	N/A	N/A	N/A	MATCH EXISTING CONCRETE DRIVEWAY, RECONSTRUCTION WIDTH EQUAL TO DRIVEWAY WIDTH
28+52.37		X	42.5'		12'	21.25'	21.25'	2%	5.46'	28+52.37, 26.96' RT.	28+52.37, 26.96' RT.	LT. TO RT.	CONCRETE	DEPRESSED CURB AND SIDEWALK FOR MAILBOX ACCESS ONLY
28+78.40		X			12'			2%	5.00'	28+78.40, 40.20' RT.	28+78.40, 40.20' RT.	LT. TO RT.	CONCRETE	DEPRESSED CURB & SIDEWALK CONTINUOUS THROUGH BOTH DRIVEWAYS.
29+92.96		X	34.5'		13'	24.5'	10'	2%	16.39'	29+92.26, 26.15' RT.	29+90.49, 37.68' RT.	LT. TO RT.	ASPHALT	MATCH EXISTING CONCRETE DRIVEWAYS
31+86.57		X	36.5'		18'	26.5'	10'	2%	10.65'	31+87.94, 26.50' RT.	31+89.36, 31.75' RT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
33+82.84	X		20'	20'	20'			3.65%	5.9'	33+82.84, 31.65' LT.	33+82.84, 31.65' LT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
34+15.99	X		20'	20'	14'			6.00%	1.64'	34+15.99, 30.41' LT.	34+15.99, 30.41' LT.	LT. TO RT.	ASPHALT	GRADE PARKING AREA TO MATCH CROSS SLOPE AT PAVING LIMIT, CONTINUE SUPERELEVATION TO DAYLIGHT
34+47.67		X	28'		10'	14'	14'	-	-	34+47.67, 31.50' RT.	34+73.73, 69.57' RT.	RT. TO LT.	ASPHALT	MATCH EXISTING CONCRETE SLAB, CONTINUE SUPERELEVATION TO DAYLIGHT
35+17.92	X		20'	20'	20'			-	-	35+17.92, 23.95' LT.	35+17.92, 23.95' LT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT. RETAINING WALLS REQUIRED
35+69.00		X	30'		NONE	15'	15'	N/A	N/A	N/A	N/A	N/A	N/A	DAYLIGHT FROM 17' LT. TO PARKING STRUCTURE. MATCH EXISTING STRUCTURE
35+69.46	X		20'	20'	12'			-	-	35+69.46, 25.11' LT.	35+69.46, 25.11' LT.	LT. TO RT.	ASPHALT	DEPRESSED CURB AND SIDEWALK FOR MAILBOX ACCESS ONLY
36+33.34	X		20'	20'	26'			-	-	36+33.34, 20.15' LT.	36+33.34, 20.15' LT.	LT. TO RT.	ASPHALT	DAYLIGHT FROM 17' LT. TO PARKING STRUCTURE. MATCH EXISTING STRUCTURE
39+09.43	X		9'	9'	24'			-2%	20'	39+09.43, 24.16' LT.	39+09.43, 35.00' LT.	LT. TO RT.	ASPHALT	FILL FROM 17' LT. TO PARKING STRUCTURE. MATCH EXISTING STRUCTURE
40+07.00		X	30'		NONE	15'	15'	N/A	N/A	N/A	N/A	N/A	N/A	RETAINING WALL REQUIRED. SEE SHEETS 13, 21, & 38
40+40.93	X		20'	20'	23'			-	-	40+40.93, 28.26' LT.	40+40.93, 28.26' LT.	LT. TO RT.	ASPHALT	DEPRESSED CURB AND SIDEWALK FOR MAILBOX ACCESS ONLY
42+32.01	X		20'	20'	27.50'			-2%	10'	42+32.01, 29.94' LT.	42+26.91, 39.34' LT.	RT. TO LT.	ASPHALT	DAYLIGHT FROM 17' LT. TO PARKING STRUCTURE. MATCH EXISTING STRUCTURE
42+33.66		X	28'		28'	14'	14'	2%	17.94'	42+33.66, 30.06' RT.	42+33.66, 39.44' RT.	LT. TO RT.	ASPHALT	PAVING LIMIT WIDTH = 27.5'. RECONSTRUCTION LIMIT WIDTH = 21'. GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT. SEE SHEET 22
43+61.52	X		20'	20'	27.81'			-	-	43+61.52, 29.83' LT.	43+86.63, 115.43' LT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
44+41.55	X		20'	20'	22'			-2%	9.86'	44+41.55, 26.86' LT.	44+41.55, 26.86' LT.	LT. TO RT.	ASPHALT	PAVING LIMIT WIDTH = 24.4'. RECONSTRUCTION LIMIT WIDTH = 11'. GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT. SEE SHEET 22
45+84.42	X		20'	20'	24'			-	-	45+84.42, 19.35' LT.	45+84.42, 19.35' LT.	LT. TO RT.	ASPHALT	CONSTRUCT PARKING STRUCTURE. SEE DETAILS ON SHEET 45
46+19.83		X	34.5'		14'	24.5'	10'	2%	5'	46+17.15, 30.02' RT.	46+11.24, 47.96' RT.	RT. TO LT.	ASPHALT	MATCH EXISTING CONCRETE SLAB
46+25.18	X		20'	20'	16'			-	-	46+25.18, 29.99' LT.	46+25.18, 29.99' LT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
48+37.73	X		20'	10'	15'			-	-	48+31.01, 45.52' LT.	48+31.01, 45.52' LT.	RT. TO LT.	ASPHALT	MATCH EXISTING ASPHALT APPROACH
51+84.65		X	37'		20'	27'	10'	2%	6.42'	51+81.19, 26.91' RT.	51+81.19, 26.91' RT.	RT. TO LT.	ASPHALT	MATCH EXISTING ASPHALT DRIVEWAY, STANDARD CURB AND GUTTER ON RIGHT, SEE DETAILS ON SHEET 33
52+47.09		X	20'		14'	10'	10'	2%	3.52'	52+46.88, 26.50' RT.	52+46.88, 26.50' RT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
54+35.79		X	20'		18'	10'	10'	1.6%	12.86'	54+35.56, 26.50' RT.	54+35.20, 34.35' RT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
55+86.57		X	60'		20'	50'	10'	2%	10.04'	55+86.13, 26.50' RT.	55+85.69, 31.50' RT.	LT. TO RT.	ASPHALT	MATCH GRADE AT GARAGE ENTRANCE
58+14.21	X		20'	20'	20'			-2%	10'	58+14.21, 37.59' RT.	58+14.21, 37.59' RT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
59+08.16	X		20'	10'	20'			-2%	6.79'	59+08.16, 32.81' LT.	59+21.85, 46.07' LT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
60+52.22	X		20'	20'	16'			-	-	60+52.22, 33.19' LT.	60+48.69, 69.31' LT.	RT. TO LT.	ASPHALT	PAVING LIMIT WIDTH = 20.2'. RECONSTRUCTION LIMIT WIDTH = 15'. GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT. SEE SHEET 25
61+20.16		X	36.5'		20'	26.5'	10'	2%	1.19'	61+19.94, 26.50' RT.	61+19.94, 26.50' RT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT. SEE SHEET 25
62+58.98	X		20'		19'	10'	10'	2%	10.84'	62+59.07, 28.25' RT.	62+59.13, 32.34' RT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT. SEE SHEET 25
63+21.16	X		20'		17'	10'	10'	2%	5.92'	63+20.91, 28.08' RT.	63+20.91, 28.08' RT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT. SEE SHEET 25
64+88.52	X		20'		20'	10'	10'	2%	24'	64+88.52, 27.63' RT.	64+88.52, 45.50' RT.	RT. TO LT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT. SEE SHEET 25
65+64.28	X		44'		24'	32'	12'	2%	18'	65+64.28, 27.42' RT.	65+64.28, 35.50' RT.	RT. TO LT.	ASPHALT	RETAINING WALL REQUIRED. SEE SHEETS 13, 26, AND 38
66+92.97	X		34.5'		14'	24.5'	10'	-	-	66+96.08, 27.11' RT.	66+98.66, 33.84' RT.	RT. TO LT.	ASPHALT	RETAINING WALL REQUIRED. SEE SHEETS 13, 26, AND 38
67+45.63	X		N/A	N/A	60'			-2%	4'	67+45.63, 21.00' LT.	67+45.63, 21.00' LT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PAVING LIMIT
67+65.01	X		20'		20'	10'	10'	2%	2.16'	67+65.01, 27.48' RT.	67+65.01, 27.48' RT.	RT. TO LT.	ASPHALT	CONSTRUCT PARKING STRUCTURE. SEE DETAILS ON SHEET 46
69+15.60	X		20'	5.66'	15'			VERTICAL CURVE		70+52.84, 46.60' RT.	70+52.84, 46.60' RT.	LT. TO RT.	ASPHALT	PAVING LIMIT WIDTH = 50.9'. SEE SHEET 26. THREE DRIVEWAYS, ONE APPROACH
69+44.99		X	57'		27.15'	37'	20'	2%	2.38'	69+48.07, 23.88' RT.	69+48.07, 23.88' RT.	RT. TO LT.	CONCRETE	SEE DETAIL ON SHEET 8
70+80.67		X	37'		20'	27'	10'	2%	5'	69+61.57, 26.50' RT.	69+61.57, 44.83' RT.	RT. TO LT.	ASPHALT	MATCH EXISTING CONCRETE DRIVEWAY, CONSTRUCT ADJACENT PARKING AREA, SEE LINE BELOW
71+20.53	X		20'	20'	30'			2%	10'	70+80.67, 26.50' RT.	70+89.41, 31.32' RT.	RT. TO LT.	ASPHALT	GRADE 15' AT 15% FROM LANDING, CATCH AT RECONSTRUCTION LIMIT
73+74.24		X	49'		25.8'	36'	13'	2%	19.94'	71+20.53, 36.00' RT.	71+20.53, 36.00' RT.	LT. TO RT.	ASPHALT	TWO DRIVEWAYS SHARE ONE APPROACH. GRADE DRIVEWAYS TO MATCH CROSS SLOPES AT PAVING LIMITS
73+86.48	X		20'	20'	25.8'			2%	20.70'	73+74.24, 26.50' RT.	73+59.74, 34.64' RT.	RT. TO LT.	ASPHALT	RETAINING WALL REQUIRED. SEE SHEETS 13, 27, AND 38
74+90.07	X		41'		20'	31'	10'	-2%	10'	73+86.48, 29.88' LT.	73+86.48, 37.81' LT.	LT. TO RT.	ASPHALT	TWO DRIVEWAYS SHARE ONE APPROACH. GRADE DRIVEWAYS TO MATCH AT PAVING LIMITS. SEE SHEET 27
75+57.75	X		20'		20'	10'	10'	2%	15.33'	74+90.07, 30.35' RT.	74+90.07, 45.55' RT.	RT. TO LT.	ASPHALT	PAVING LIMIT WIDTH = 21.5'. RECONSTRUCTION LIMIT WIDTH = 20'
75+82.00	X		20'	20'	20'			-2%	10'	75+57.75, 30.42' RT.	75+57.75, 36.83' RT.	RT. TO LT.	ASPHALT	RETAINING WALL REQUIRED. SEE SHEETS 13, 27, AND 38
76+21.51	X		20'		20'	10'	10'	2%	10'	75+82.00, 29.52' LT.	75+82.00, 35.29' LT.	LT. TO RT.	ASPHALT	NEW DRIVEWAY. GRADE TO MATCH CROSS SLOPE AT PROPERTY LINE
77+25.16	X		20'	20'	20'			-2%	10'	76+21.51, 30.25' RT.	76+21.51, 53.53' RT.	RT. TO LT.	ASPHALT	NEW DRIVEWAY, PAVING LIMIT WIDTH = 21.7', RECONSTRUCTION LIMIT WIDTH = 20'
77+46.91	X		50'		16'	33.7'	16.3'	-	-	77+25.16, 30.37' LT.	77+25.16, 39.03' LT.	LT. TO RT.	ASPHALT	GRADE DRIVEWAY TO MATCH CROSS SLOPE AT PROPERTY LINE. CONSTRUCT PARKING AREA BESIDE DRIVEWAY. SEE SHEET 28.
77+65.99	X		CENTERED AT 77+64.06		12'			-	-	77+46.91, 29.47' RT.	77+46.91, 34.85' RT.	LT. TO RT.	ASPHALT	PAVING LIMIT WIDTH = 21'. RECONSTRUCTION LIMIT WIDTH = 20'
								-	-	77+65.99, 29.31' RT.	77+74.69, 34.70' RT.	LT. TO RT.	ASPHALT	DEPRESSED CURB AT SIDEWALK CONTINUES THROUGH BOTH DRIVEWAYS. GRADE DRIVEWAYS TO MATCH CROSS SLOPE AT PAVING LIMIT. SEE SHEET 28

NOTE: SEE SHEETS 33 AND 34 FOR DEPRESSED CURB AND SIDEWALK LOCATIONS REQUIRED AT BUS STOP LOCATIONS.

NOTE: SEE SHEET 9 FOR NEW DRIVEWAYS

NO.	DATE	DESCRIPTION OF CHANGE

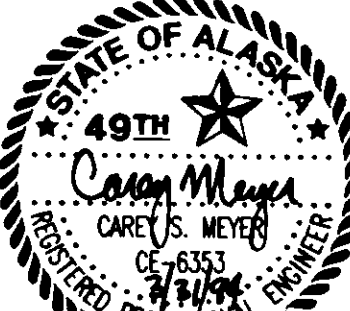
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
DRIVEWAY SUMMARY

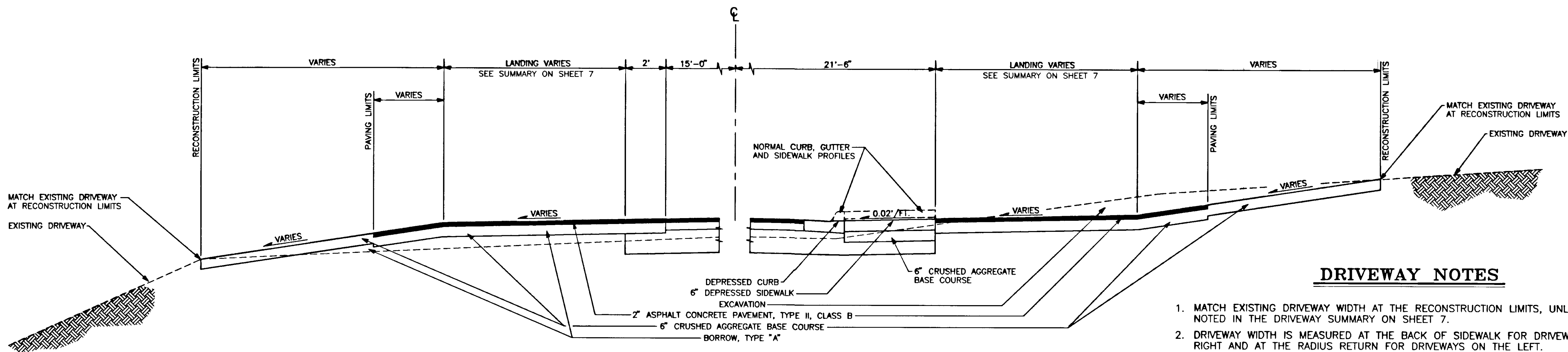


DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

PROJECT No: 69062  
DATE: MARCH 1994  
SHEET 7 OF 55



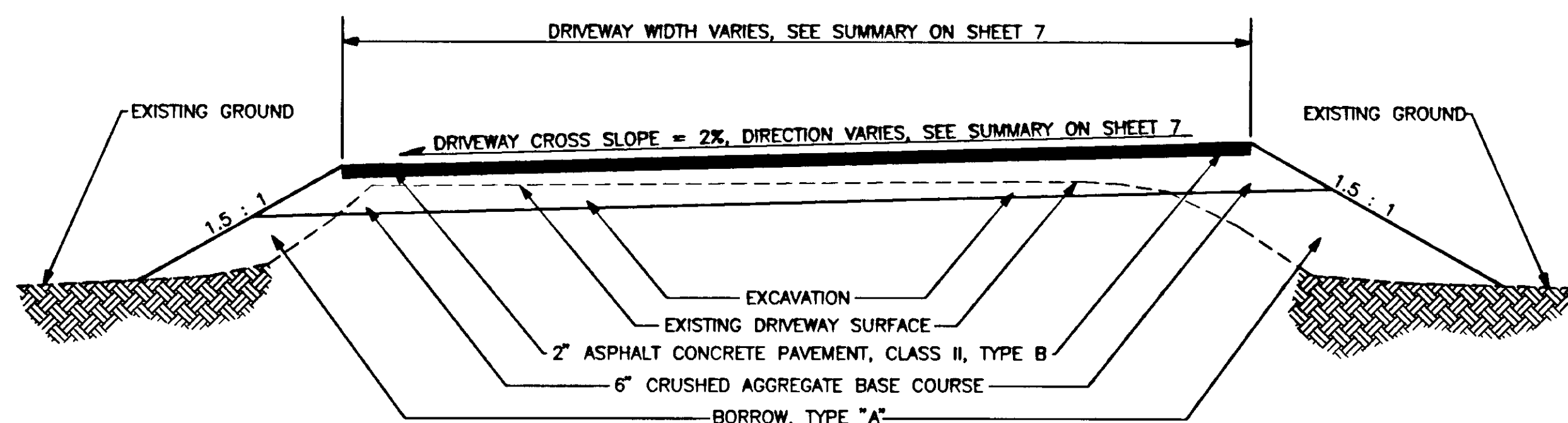
DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



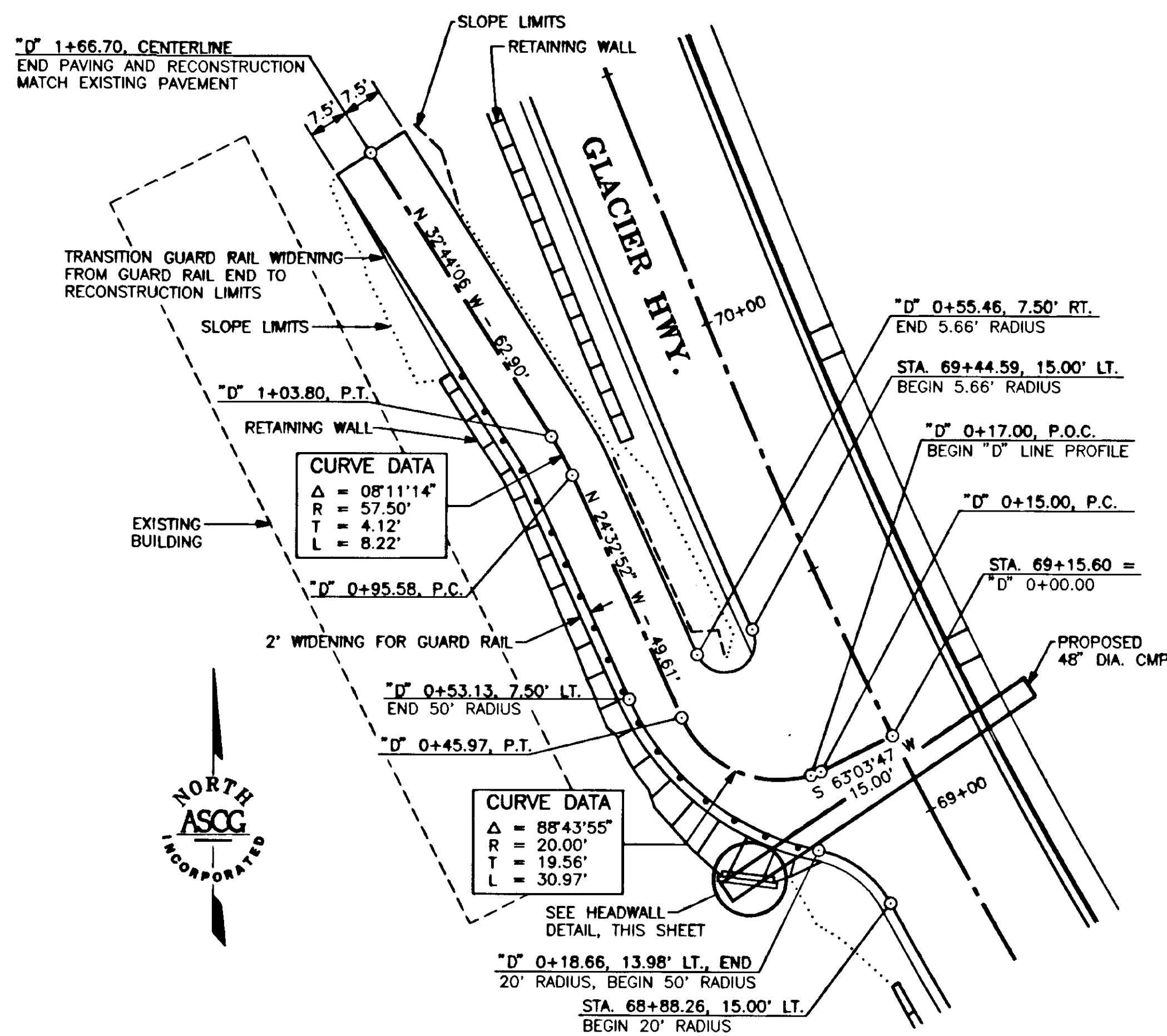
**DRIVEWAY NOTES**

1. MATCH EXISTING DRIVEWAY WIDTH AT THE RECONSTRUCTION LIMITS, UNLESS OTHERWISE NOTED IN THE DRIVEWAY SUMMARY ON SHEET 7.
2. DRIVEWAY WIDTH IS MEASURED AT THE BACK OF SIDEWALK FOR DRIVEWAYS ON THE RIGHT AND AT THE RADIUS RETURN FOR DRIVEWAYS ON THE LEFT.
3. DRIVEWAYS ON THE RIGHT BETWEEN HIGHLAND DRIVE AND ROSS WAY REQUIRE CURB CUTS. SEE DETAILS ON SHEET 41 AND SUMMARY ON SHEET 14.
4. DRIVEWAYS ON THE RIGHT BETWEEN ROSS WAY AND THE E.O.P. REQUIRE DEPRESSED CURB AND SIDEWALK. SEE DETAILS ON SHEET 41 AND SUMMARY ON SHEET 7.

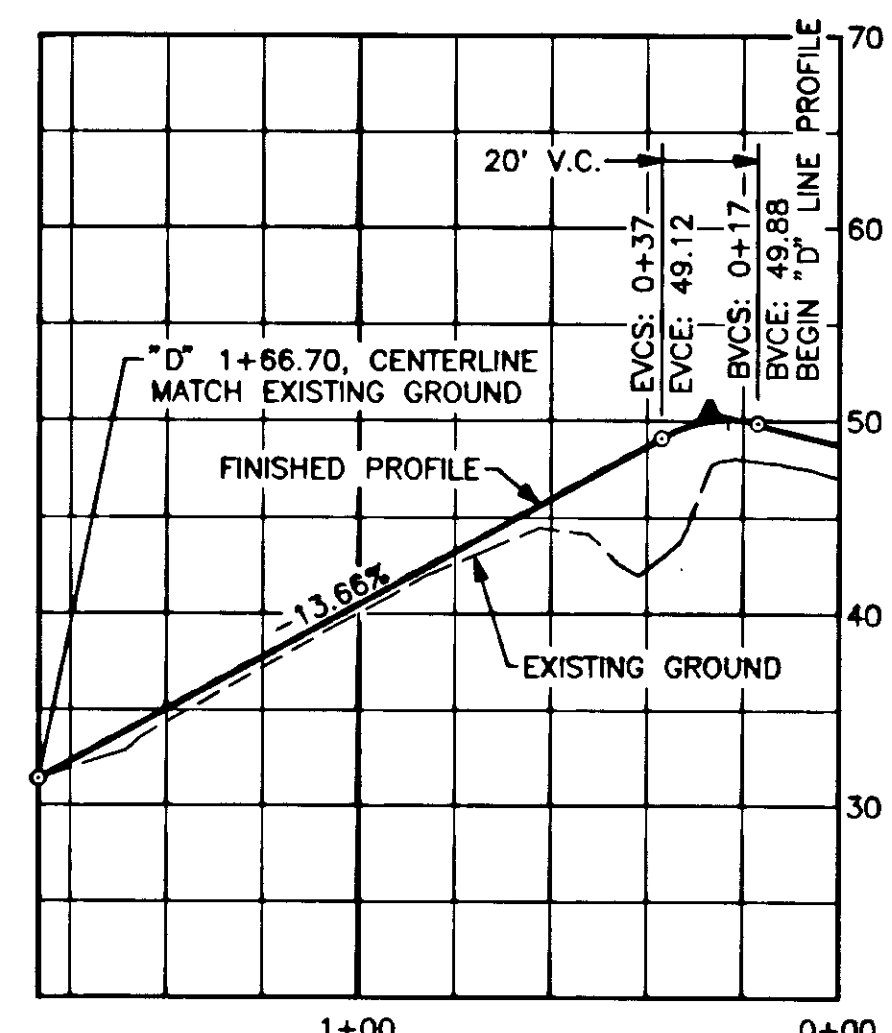
**TYPICAL DRIVEWAY PROFILES**



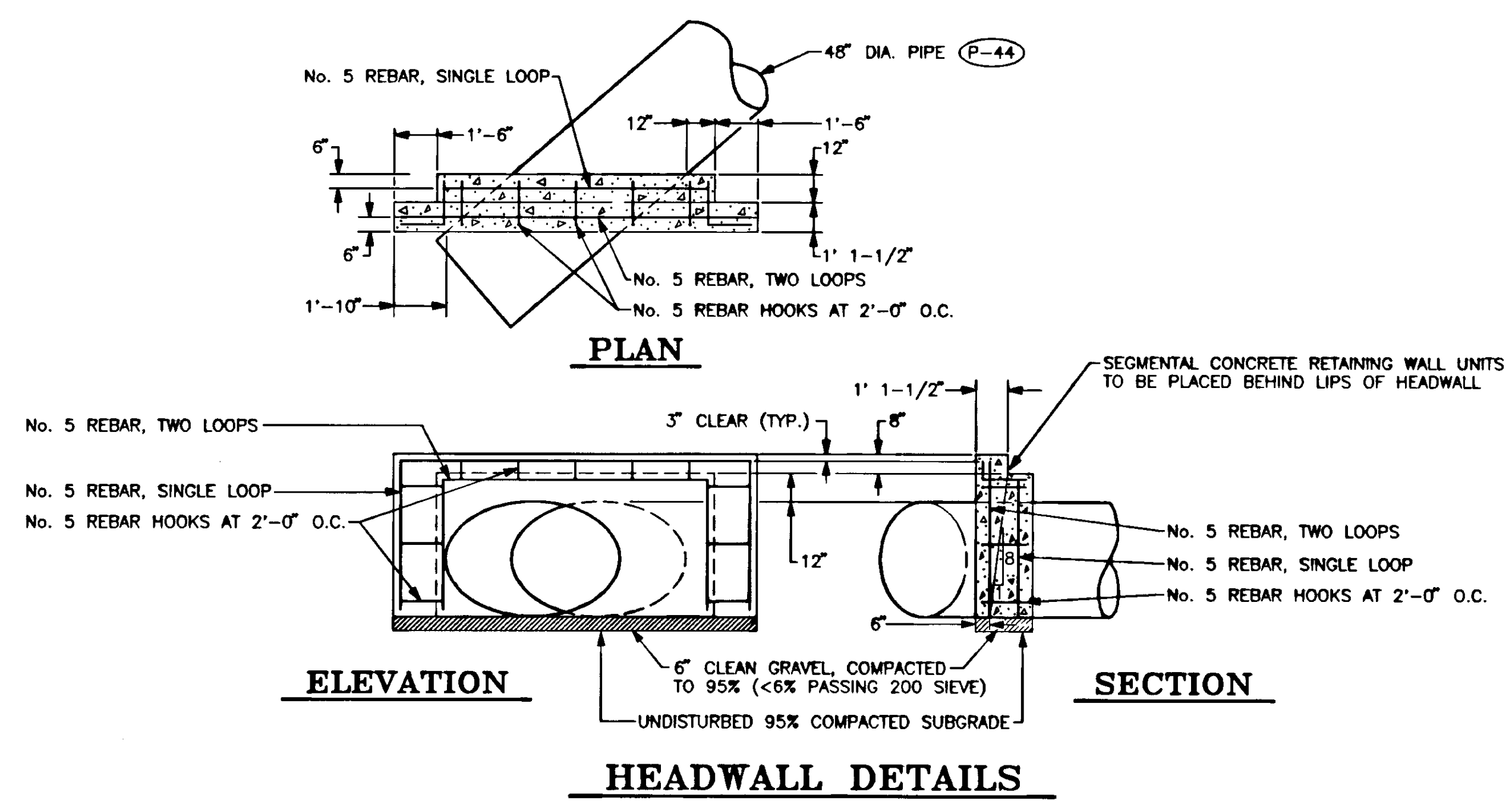
**TYPICAL DRIVEWAY SECTION**



HIGH POINT ELEV = 50.07  
 HIGH POINT STA = 0+23.10  
 VPI STA = 0+27  
 VPI ELEV = 50.48  
 A.D. = 19.66  
 K = 1.02



**PROFILE**



**HEADWALL DETAILS**

NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0978(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 DRIVEWAY DETAILS

**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 8 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

**PIPE INSTALLATION SUMMARY**

CULVERT NUMBER	DIA.	LENGTH	INLET			OUTLET			COLLECTION BASIN REQUIRED
			STATION	OFFSET	INVERT	STATION	OFFSET	INVERT	
P-1	12"	88' 75	(S-1)		35.3	EXST STR. @10+19	19' RT.	EXST INV.	
P-2	18"	38'	(S-2)		35.8	(S-3)		34.9	
P-3	18"	152'	(S-3)		34.3	EXST STR. @10+60	46' LT.	23.6	
P-4	18"	178'	(S-4)		40.0	(S-5)		38.0	
P-5	18"	70'	(S-5)		37.5	EXST STR. @17+20	43' LT.	32.9	
P-6	18"	42'	(S-6)		40.5	(S-7)		39.5	
P-7	18"	22'	(S-7)		39.0	EXST STR. @18+76	37' LT.	32.0	
P-8	18"	78'	EXST STR. @18+82	40' RT.	37.0	EXST STR. @18+76	37' LT.	30.3	
P-9	18"	66'	(S-8)		40.5	EXST STR. @18+82	40' RT.	38.0	
P-10	24"	82'	(S-9)		40.0	EXST STR. @20+52	52' LT.	25.2	
P-11	24"	8'		22+19	40.7	(S-10)		40.2	YES
P-12	24"	40'	(S-10)		39.7	EXST STR. @22+15	24' LT.	36.6	
P-13	24"	48'	(S-11)		40.0	EXST STR. @25+75	32' LT.	EXST INV.	
P-14	18"	20'	(S-12)		43.2	(S-11)		41.4	
P-15	24"	60'	(S-13)		40.5	EXST STR. @26+94	32' LT.	32.6	
P-16	18" 12	10'	EXST STR. @26+74	34' RT.	EXST INV.	(S-13)		42.0	
P-17	18"	62'	(S-14)		42.1	(S-13)		41.0	
P-18	24"	38'	(S-15)		46.8	EXST STR. @29+80	30' LT.	44.0	YES
P-19	24"	22'		30+02	50.3	(S-15)		47.3	
P-20	24"	64'		31+31	47.5	EXST STR. @31+10	31' LT.	42.1	
P-21	24"	88'	(S-16)		52.7	32+80	68' LT.	49.5	
P-22	24"	46'		35+02	59.9	(S-17)		54.6	
P-23	24"	84'	(S-17)		48.5	35+08	49' LT.	45.9	
P-24	18"	94'	(S-18)		55.1	(S-19)		54.6	
P-25	24"	8'		37+51	54.4	(S-19)		54.1	YES
P-26	24"	84'	(S-19)		48.7	37+47	30' LT.	48.2	
P-27	24"	18'	(S-20)		56.4	(S-21)		55.9	
P-28	24"	52'	(S-21)		55.4	41+95	35' LT.	52.5	
P-29	24"	58'		45+24	57.0	45+08	29' LT.	54.0	YES
P-30	24"	84'		47+08	59.2	EXST STR. @47+05	38' LT.	46.0	YES
P-31	18"	54'	(S-22)		60.3	47+08	27' RT.	59.5	
P-32	24"	62'		48+10	59.3	(S-23)		50.0	YES
P-33	24"	60'		48+81	59.0	48+86	30' LT.	49.0	YES
P-34	24"	54'		50+36	57.5	50+36	29' LT.	45.0	YES
P-35	24"	8'		54+09	55.6	(S-24)		55.3	YES
P-36	24"	46'	(S-24)		52.8	53+99	32' LT.	52.3	
P-37	24"	168' 16"	(S-25)		55.1	(S-24)		53.3	
P-38	18"	10'		55+69	55.4	(S-25)		55.2	YES
P-39	18"	80'	(S-26)		56.5	(S-25)		55.7	
P-40	18"	12'	(S-27)		47.0	(S-28)		46.6	
P-41	24"	56'	(S-28)		46.1	62+98	37' LT.	39.0	
P-42	24"	68'	(S-29)		48.6	64+65	28' LT.	34.3	
P-43	18"	8'	(S-30)		45.0	(P-44)		44.2	
P-44	48"	68'		69+12	43.0	69+04	41' LT.	31.4	YES
P-45	12"	80'	(S-31)		43.3	(S-32)		39.7	
P-46	24"	8'		73+14	40.0	(S-32)		39.7	YES
P-47	24"	42'	(S-32)		39.2	73+13	25' LT.	35.5	
P-48	24"	48'	(S-33)		36.1	75+12	25' LT.	28.4	
P-49	24"	64'		76+40	32.3	76+38 7/10	24' LT.	27.0	YES
P-50	24"	58'		77+86	26.6	77+87	32' LT.	21.3	YES
P-51	24"	8'		80+32	25.4	(S-34)		25.1	YES
P-52	24"	46'	(S-34)		24.6	80+32	30' LT.	23.5	
P-53	24"	6'	EX PIPE @3+30	6' RT.	25.1	(S-35)		25.4	
P-54	18"	12'	(S-37)		25.8	(S-36)		25.4	
P-55	18"	18'	(S-36)		24.9	EX STR @ A 5+08	20.5' RT.	24.5	
P-56	18"	16'		74+54.6	39.8	74+70.2	26.25' RT.	39.4	
P-57	18"	36'		74+76.7	39.2	(S-33)		36.6	
	12"	15'		80+00		80+00	RT.		
	18"	16'		80+40		80+66	RT.		

NOTE: EXST STR = EXISTING STRUCTURE

**PIPE REMOVAL SUMMARY**

LENGTH	FROM		TO	
	STATION	OFFSET	STATION	OFFSET
80'	10+19	19' RT.	10+98	25' RT.
160'	10+60	45' LT.	12+18	25' LT.
50'	12+13	24' RT.	12+18	25' LT.
38'	16+94	23' RT.	17+17	6' LT.
36'	17+17	6' LT.	17+20	43' LT.
12'	18+71	28' RT.	18+81	23' RT.
6'	19+43	24' RT.	19+41	18' RT.
48' 77	18+82	40' RT.	18+76	37' LT.
60'	19+41	18' RT.	18+81	19' RT.
88'	20+20	29' RT.	20+52	52' LT.
22' 32	20+52	52' LT.	20+69	18' RT.
26'	21+90	21' RT.	22+16	21' RT.
44'	22+15	23' LT.	22+18	20' RT.
28'	22+21	21' RT.	22+49	18' RT.
240'	22+16	17' RT.	24+59	16' RT.
30'	24+59	16' RT.	24+89	16' RT.
22'	25+57	15' RT.	25+79	17' RT.
20'	25+79	17' RT.	25+99	16' RT.
40'	25+99	16' RT.	26+39	16' RT.
14'	26+79	26' RT.	26+88	15' RT.
48'	26+88	15' RT.	26+94	32' LT.
54'	26+88	15' RT.	27+41	14' RT.
28'	27+41	14' RT.	27+69	14' RT.
54'	29+79	24' RT.	29+80	30' LT.
24'	29+80	26' RT.	30+02	28' RT.
64'	31+10	31' LT.	31+31	30' RT.
20'	31+79	30' RT.	31+98	29' RT.
112'	35+02	62' RT.	35+08	49' LT.
52'	37+47	30' LT.	37+51	22' RT.
54'	41+88	18' RT.	41+95	35' LT.
52'	45+08	29' LT.	45+14	22' RT.
46'	47+07	27' LT.	47+08	18' RT.
52'	48+13	18' RT.	48+15	34' LT.
58'	49+08	24' RT.	49+14	32' LT.
54'	51+12	21' RT.	51+13	32' LT.
34'	51+71	20' RT.	52+05	20' RT.
54'	53+99	32' LT.	54+08	21' RT.
190' 30	55+70	17' RT.	55+88	170' LT.
30'	55+71	20' RT.	56+00	20' RT.
52'	56+84	22' RT.	56+84	30' LT.
30'	57+61	11' RT.	57+61	18' LT.
58'	60+96	21' RT.	61+03	36' LT.
62'	61+93	22' RT.	62+20	32' LT.
62'	62+84	23' RT.	62+98	37' LT.
60'	63+78	24' RT.	64+38	24' RT.
30'	64+82	12' RT.	64+83	18' LT.
40'	64+82	22' RT.	65+21	20' RT.
30'	65+39	17' RT.	65+68	17' RT.
36'	66+72	17' RT.	67+08	18' RT.
28'	67+52	18' RT.	67+80	17' RT.
64'	69+05	41' LT.	69+13	22' RT.
50'	69+18	24' RT.	69+69	20' RT.
44'	73+13	21' RT.	73+13	22' LT.
10'	74+57	22' RT.	74+67	22' RT.
40'	74+72	22' RT.	75+12	23' RT.
46'	75+13	21' RT.	75+12	25' LT.
44'	76+38	20' RT.	76+38	24' LT.
42'	77+36	19' RT.	77+77	19' RT.
52'	77+86	20' RT.	77+87	32' LT.
48'	80+32	18' RT.	80+32	30' LT.
16'	74+65	RT.		

**NEW DRIVEWAY SUMMARY**

STATION	LEFT	RIGHT	WIDTH
14+50	X		
35+40	X		40' (COMBINED W/35+40)
38+40	X		24'
40+70	X		50' (COMBINED W/40+40)
53+00	X		14'
55+00	X		36'
64+10		X	17'
74+50		X	23'
76+40	X		
80+40		X	
46+00	X		34'

BY:	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

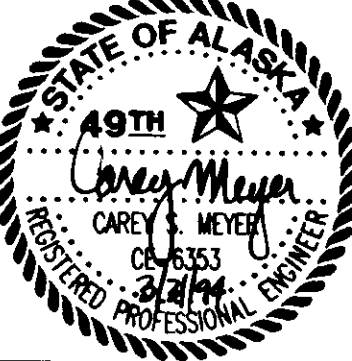
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
PIPE SUMMARIES



DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

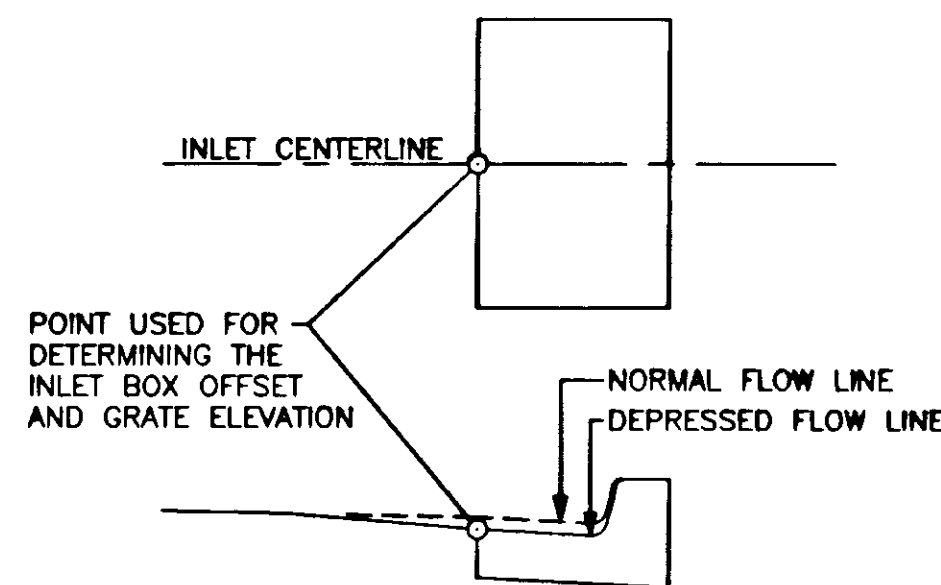
PROJECT NO. 69062  
DATE: MARCH 1994  
SHEET 9 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

**DRAINAGE STRUCTURE SUMMARY**

STRUCT. No.	STATION	OFFSET		TYPE	GRATE ELEVATION	STRUCT. No.	STATION	OFFSET		TYPE	GRATE ELEVATION
		LEFT	RIGHT					LEFT	RIGHT		
S-1	11+08.01		20.50	"A"	37.43	S-20	41+90.00		30.00'	FIELD	67.36
S-2	<del>12+11.00</del> 12+87		20.50'	"A"	38.86	S-21	41+90.00		15.00'	"A" (2'x4') 8"	60.89
S-3	<del>12+11.00</del> 12+00	14.50'		"B"	38.98	S-22	47+62.00		26.00'	FIELD	62.1
S-4	15+15.00		20.50'	"B"	42.80	S-23	48+16.00	37.0'		FIELD	52.5
S-5	16+95.00		20.50'	"A" "B"	43.94	S-24	54+07.00		15.00'	"B"	59.44
S-6	18+63.00		20.50'	"A"	43.38	S-25	55+69.99		15.00'	"B"	59.28
S-7	18+63.00	18.50		"A" "B"	43.61	S-26	56+49.00		27.00'	FIELD	59.0
S-8	19+44.00		20.50'	"B"	43.63	S-27	62+84.00		27.00'	FIELD	54.5
S-9	20+70.00		28.50'	FIELD	42.27	S-28	62+84.00		15.00'	"A" "B"	54.36
S-10	22+18.00		15.00'	"A" "B"	44.21	S-29	64+65.00		15.00'	"A" "B"	52.45
S-11	25+79.00		15.00'	"B"	45.01	S-30	69+04.00		15.00'	"B"	47.97
S-12	25+99.00		15.00'	"B"	45.14	S-31	72+33.00		15.00'	"B"	44.87
S-13	26+80.00		26.00'	FIELD	46.1	S-32	73+13.00		15.00'	"B"	44.53
S-14	27+42.00		26.00'	FIELD "A"	47.0	S-33	75+13.00		23.00'	FIELD	39.96
S-15	29+80.00		26.00'	FIELD	52.3	S-34	80+32.00		12.22'	"A" "B"	29.36
S-16	32+80.00		15.00'	"A" "B"	56.72	S-35	"A" 3+30		11.90'	FIELD	29.2
S-17	<del>35+05.00</del> 34+88		15.00'	"A" "B"	58.72	S-36	"A" <del>4+95</del> 5+06		7.50'	FIELD	27.6
S-18	36+58.00		15.00'	"B"	58.50	S-37	"A" <del>4+94</del> 5+06	6.5'		FIELD	27.6
S-19	37+51.00		15.00'	"B"	58.79						



SEE STANDARD DRAWING D-24.00 FOR FURTHER INFORMATION

**OFFSET AND ELEVATION DETAIL**

**8" MODIFIED UNDERDRAIN SUMMARY**

BEGIN STATION	INVERT ELEV	DESCRIPTION	END STATION	INVERT ELEV	DESCRIPTION	REMARKS
19+60	-	INSTALL CLEANOUT	19+62	42.0	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
19+62	42.0	ANGLE POINT	20+66	40.5	ANGLE POINT	
20+66	40.5	ANGLE POINT	20+68	39.8	CONNECT TO 24" CULVERT	
20+70	39.8	CONNECT TO 24" CULVERT	20+72	40.5	ANGLE POINT	
20+72	40.5	ANGLE POINT	21+76	42.0	ANGLE POINT	
21+76	42.0	ANGLE POINT	21+78	-	INSTALL CLEANOUT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
24+20	-	INSTALL CLEANOUT	24+22	42.9	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
24+22	42.9	ANGLE POINT	25+83	42.6	ANGLE POINT	
25+83	42.6	ANGLE POINT	25+83	42.5	CONNECT TO 18" CULVERT	
25+95	43.7	CONNECT TO 18" CULVERT	25+95	43.8	ANGLE POINT	
25+95	43.8	ANGLE POINT	26+52	44.1	ANGLE POINT	
26+52	44.1	ANGLE POINT	26+54	-	INSTALL CLEANOUT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
31+70	52.0	OUTFALL IN EXISTING DITCH	32+76	53.6	ANGLE POINT	
32+76	53.6	ANGLE POINT	32+79	-	INSTALL CLEANOUT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
32+80	53.2	CONNECT TO 24" CULVERT	32+84	53.7	ANGLE POINT	
32+84	53.7	ANGLE POINT	34+96	55.7	ANGLE POINT	
34+96	55.7	ANGLE POINT	35+00	-	INSTALL CLEANOUT	INSTALL CLEANOUT IN SIDEWALK @ 20.5' RT
35+04	-	INSTALL CLEANOUT	35+07	56.7	ANGLE POINT	INSTALL CLEANOUT IN SIDEWALK @ 20.5' RT
35+07	56.7	ANGLE POINT	37+51	55.0	CONNECT TO 24" CULVERT	
37+51	55.0	CONNECT TO 24" CULVERT	40+27	58.4	ANGLE POINT	
40+27	58.4	ANGLE POINT	40+29	-	INSTALL CLEANOUT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
40+33	-	INSTALL CLEANOUT	40+35	58.2	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
40+35	58.2	ANGLE POINT	41+90	56.9	CONNECT TO 24" CULVERT	
41+90	56.9	CONNECT TO 24" CULVERT	45+20	60.2	ANGLE POINT	
45+20	60.2	ANGLE POINT	45+23	-	INSTALL CLEANOUT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
45+24	57.7	CONNECT TO 24" CULVERT	47+04	60.1	ANGLE POINT	
47+04	60.1	ANGLE POINT	47+06	-	INSTALL CLEANOUT	
47+08	59.3	CONNECT TO 24" CULVERT	47+08	60.2	ANGLE POINT	
48+06	60.2	ANGLE POINT	48+08	-	INSTALL CLEANOUT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
48+10	59.7	CONNECT TO 24" CULVERT	48+10	59.98	ANGLE POINT	INSTALL CLEANOUT
49+45	59.98	ANGLE POINT	49+47	-	INSTALL CLEANOUT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
50+37	-	INSTALL CLEANOUT	50+40	58.8	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
50+40	58.8	ANGLE POINT	54+09	56.2	CONNECT TO 24" CULVERT	
54+10	-	INSTALL CLEANOUT	54+13	56.4	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
54+13	56.4	ANGLE POINT	55+69	55.7	CONNECT TO 18" CULVERT	
55+71	55.7	CONNECT TO 18" CULVERT	56+46	57.4	ANGLE POINT	
56+46	57.4	ANGLE POINT	56+48	-	INSTALL CLEANOUT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
57+52	-	INSTALL CLEANOUT	57+55	55.9	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
57+55	55.9	ANGLE POINT	62+80	48.6	ANGLE POINT	
62+80	48.6	ANGLE POINT	62+84	47.7	CONNECT TO 24" CULVERT	
62+85	-	INSTALL CLEANOUT	62+88	51.3	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
62+88	51.3	ANGLE POINT	64+65	49.3	CONNECT TO 24" CULVERT	
64+66	-	INSTALL CLEANOUT	64+69	49.4	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
64+69	49.4	ANGLE POINT	69+12	44.2	CONNECT TO 24" CULVERT	
69+12	-	INSTALL CLEANOUT	69+16	44.9	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
69+16	44.9	ANGLE POINT	73+13	40.6	CONNECT TO 24" CULVERT	
73+13	-	INSTALL CLEANOUT	73+13	41.5	ANGLE POINT	
73+13	41.5	ANGLE POINT	75+13	36.8	CONNECT TO 24" CULVERT	
75+13	-	INSTALL CLEANOUT	75+13	36.9	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
75+13	36.9	ANGLE POINT	76+38	32.7	CONNECT TO 24" CULVERT	
77+38	32.7	CONNECT TO 24" CULVERT	77+38	32.6	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
77+38	32.6	ANGLE POINT	77+82	28.9	ANGLE POINT	
77+82	28.9	ANGLE POINT	77+82	27.3	CONNECT TO 24" CULVERT	
77+82	27.3	CONNECT TO 24" CULVERT	77+90	28.8	ANGLE POINT	INSTALL CLEANOUT FLUSH WITH SIDEWALK
77+90	28.8	ANGLE POINT	78+32	26.1	CONNECT TO 24" CULVERT	

BY:	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 DRAINAGE SUMMARIES

**ASCCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.






PROJECT No. 69062  
 DATE: MARCH 1994  
 SHEET 10 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

PATH = E:\ALASKA\NORWAY  
 DRAWN IN AUTOCAD RELEASE 11  
 DRAWING PLOT SCALE: 1" = 1'  
 DRAWING LAST UPDATED: 3/17/1994

**SIGNING SUMMARY**

SIGN No.	STATION	OFFSET		CODE NUMBER	LEGEND	SIGN PANEL		FACING TRAFFIC	POST MATERIAL	REMARKS
		LEFT	RIGHT			SIZE	AREA S.F.			
1	10+95		35'	D3-1	HIGHLAND DR.	8 x 42	2.33	NB & SB	2.5 x 2.5 PT	
2	10+95		35'	D3-1	GLACIER AVE.	8 x 42	2.33	EB & WB		MOUNT BELOW SIGN No. 1
3	11+40	22'		R1-1	STOP	30 x 30	6.25	SB	2.5 x 2.5 PT	
4	14+50	28'		R7-102	NO PARKING HERE TO CORNER	12 x 18	1.5	SB	2 x 2 PT	
5	16+25	28'		S1-1	CHILDREN WALKING SYMBOL	36 x 36	9.00	SB	2.5 P	
6	16+25	28'		W7-3A	NEXT 1/2 MILE	18 x 24	3.00	SB		MOUNT BELOW SIGN No. 5
7	19+36		33'	D3-1	ROSS WAY	8 x 36	2.00	NB & SB		
8	19+36		33'	D3-1	GLACIER AVE.	8 x 42	2.33	EB & WB	2.5 x 2.5 PT	MOUNT BELOW SIGN No. 7
9	19+36		33'	R1-1	STOP	30 x 30	6.25	WB		MOUNT BELOW SIGN No. 8
10	19+50	28'		R7-103	NO PARKING WITHIN 6' OF PAVEMENT	12 x 18	1.50	SB	2 x 2 PT	
11	19+50		30'	R7-101	NO PARKING ANY TIME	12 x 18	1.50	NB	2 x 2 PT	
12	21+50		21.5'	R2-1	SPEED LIMIT 40	30 x 36	7.50	NB	2.5 x 2.5 PT	
13	21+50	21'		R2-1	SPEED LIMIT 20	30 x 36	7.50	SB	2.5 x 2.5 PT	
14	29+50	21'		R7-103	NO PARKING WITHIN 6' OF PAVEMENT	12 x 18	1.50	SB	2 x 2 PT	
15	29+50		24.5'	R7-101	NO PARKING ANY TIME	12 x 18	1.50	NB	2 x 2 PT	
16	31+40		24.5'	W1-5R		36 x 36	9.00	NB	2.5 P	
17	31+40		24.5'	W13-1	35 M.P.H.	24 x 24	4.00	NB		MOUNT BELOW SIGN No. 16
18	39+50		24.5'	R7-101	NO PARKING ANY TIME	12 x 18	1.50	NB	2 x 2 PT	
19	39+50	21'		R7-103	NO PARKING WITHIN 6' OF PAVEMENT	12 x 18	1.50	SB	2 x 2 PT	
20	46+05	21'		W1-5L		36 x 36	9.00	SB	2.5 P	
21	46+05	21'		W13-1	35 M.P.H.	24 x 24	4.00	SB		MOUNT BELOW SIGN No. 20
22	50+00	21'		R7-103	NO PARKING WITHIN 6' OF PAVEMENT	12 x 18	1.50	SB	2 x 2 PT	
23	50+00		24.5'	R7-101	NO PARKING ANY TIME	12 x 18	1.50	NB	2 x 2 PT	
24	57+32		28'	D3-1	WICKERSHAM DR.	8 x 48	2.67	NB & SB		
25	57+32		28'	D3-1	GLACIER HWY.	8 x 42	2.33	EB & WB	2.5 x 2.5 PT	MOUNT BELOW SIGN No. 24
26	57+32		28'	R1-1	STOP	30 x 30	6.25	WB		MOUNT BELOW SIGN No. 25
27	60+00	21'		R7-103	NO PARKING WITHIN 6' OF PAVEMENT	12 x 18	1.50	SB	2 x 2 PT	
28	60+00		21.5'	R7-101	NO PARKING ANY TIME	12 x 18	1.50	NB	2 x 2 PT	
29	68+80		24.5'	R7-101	NO PARKING ANY TIME	12 x 18	1.50	NB	2 x 2 PT	
30	68+80	21'		R7-103	NO PARKING WITHIN 6' OF PAVEMENT	12 x 18	1.50	SB	2 x 2 PT	
31	70+50		24.5'	W3-1	STOP AHEAD SYMBOL STOP AHEAD	36 x 36 18 x 24	9.00 3.00	NB NB	2.5 P	MOUNT BELOW STOP AHEAD SYMBOL
32	74+50		25.5'	W1-1L		36 x 36	9.00	NB	2.5 P	
33	74+50		25.5'	W13-1	15 MPH	24 x 24	4.00	NB		MOUNT BELOW SIGN No. 32
34	77+66	21'		R7-103	NO PARKING WITHIN 6' OF PAVEMENT	12 x 18	1.50	SB	2 x 2 PT	
35	78+00	21'		R2-1	SPEED LIMIT 40	30 x 36	7.50	SB	2.5 x 2.5 PT	
36	78+00		21.5'	R7-101	NO PARKING ANY TIME	12 x 18	1.50	NB	2 x 2 PT	
37	80+17	42.5'		R1-1	STOP	30 x 30	6.25	SB	2.5 x 2.5 PT	
38	"A" 1+08		16.5'	OM-4		18 x 18	2.25	NB	2 x 2 PT	
39	"A" 1+12		8'	R5-3	NO MOTORIZED VEHICLES	18 x 24	3.00	NB	2 x 2 PT	
40	"B" 10+69	9.5'		R1-2	YIELD	24 x 24	4.00	NB	2 x 2 PT	
41	"B" 25+50		8'	W1-1R		18 x 18	2.25	NB	2 x 2 PT	
42	"B" 28+00		7.5'	W1-1L		18 x 18	2.25	SB	2 x 2 PT	
43	"B" 28+60		9.5'	R1-2	YIELD	24 x 24	4.00	NB	2 x 2 PT	
	25+95		X	R7-101	NO PARKING ANY TIME	12 x 18	1.50			

**SIGNING NOTES**

- ALL SIGNS SHALL BE INSTALLED SUCH THAT THE BOTTOM EDGE OF THE LOWEST SIGN PANEL IS 7 FEET ABOVE THE ROADWAY, SIDEWALK OR CURB.
- ALL SIGN PANELS SHALL BE 0.080" ALUMINUM.
- ALL NEW SIGN PANELS SHALL BE UNFRAMED.
- SIGN POST MATERIAL SHALL BE AS INDICATED:  
 2 X 2 PT - 2" X 2" TELESCOPING PERFORATED GALVANIZED STEEL POSTS WITH 2 1/4" X 2 1/4" PERFORATED GALVANIZED STEEL SLEEVES.  
 2.5 X 2.5 PT - 2 1/2" X 2 1/2" PERFORATED GALVANIZED STEEL POSTS WITH 2 3/4" X 2 3/4" TELESCOPING PERFORATED GALVANIZED STEEL SLEEVES.  
 2.5 P - 2 1/2" GALVANIZED STEEL PIPE WITH SLIP BASE.
- SEE STANDARD DRAWING S-30.01 FOR SLEEVE TYPE SOIL EMBEDMENT.
- SEE STANDARD DRAWING S-34.00 FOR SLIP BASE DETAILS.
- ALL EXISTING SIGNS SHALL BE SALVAGED AND DELIVERED TO THE ADOT&P MAINTENANCE YARD AT 7-MILE GLACIER HIGHWAY.

BY:	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

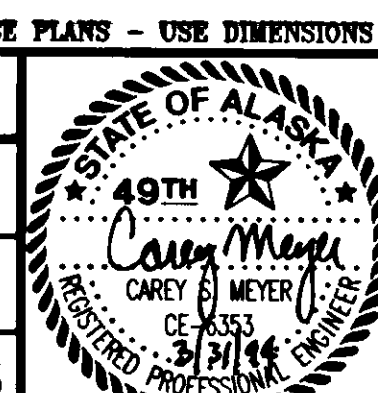
STATE OF ALASKA  
**DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES**  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
**SIGNING SUMMARY**

**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: **D.L.M.**  
 DRAWN BY: **J.E.M.**  
 CHECKED BY: **D.L.M.**

PROJECT No.: **69062**  
 DATE: **MARCH 1994**  
 SHEET **11** OF **55**



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

TELEPHONE & CATV. CONDUIT											
FROM			TO			CONDUIT UTILITY	CONDUIT NUMBER	CONDUIT SIZE (IN.)	PVC 45' SWEEP	PVC 90' SWEEP	CONDUIT LENGTH
STATION	OFFSET	STRUCTURE	STATION	OFFSET	STRUCTURE						
33+18	25' RT	POLE RISER	33+35	SIDEWALK	VAULT #1	T	1-4	4	4	108	
33+18	25' RT	POLE RISER	33+35	SIDEWALK	VAULT #1	T	6	2	0	27	
33+18	25' RT	POLE RISER	34+15	26.50' RT	PEDESTAL	C	2	2	2	123	
33+18	25' RT	POLE RISER	40+00	23' RT	POLE RISER	C	1	3	2	712	
33+18	25' RT	POLE RISER	40+00	23' RT	POLE RISER	C	3-5	2	6	2136	
33+35	SIDEWALK	VAULT #1	34+15	26.50' RT	PEDESTAL	T	6	2	1	90	
33+35	SIDEWALK	VAULT #1	39+80	SIDEWALK	VAULT #2	T	1-4	4	0	2580	
34+15	26.50' RT	PEDESTAL	35+23 1/3	34' RT	PEDESTAL	C	2	2	2	136	
34+15	26.50' RT	PEDESTAL	35+23 1/3	34' RT	PEDESTAL	T	6	2	2	136	
35+23 1/8	34' RT	PEDESTAL	34+95	50' LT	RES 1321	C	6	2	0	98	
35+23 1/8	34' RT	PEDESTAL	34+95	50' LT	RES 1321	T	7	2	0	98	
35+23 1/8	34' RT	PEDESTAL	35+22	32' RT	POLE RISER	C	6	2	0	20	
35+23 1/8	34' RT	PEDESTAL	35+22	32' RT	POLE RISER	T	7	2	0	20	
35+23 1/8	34' RT	PEDESTAL	35+30	36' LT	POLE RISER	C	6	2	0	80	
35+23 1/8	34' RT	PEDESTAL	35+30	36' LT	POLE RISER	T	7	2	0	80	
35+23 1/8	34' RT	PEDESTAL	36+25	78' RT	RES 1360	C	6	2	0	125	
35+23 1/8	34' RT	PEDESTAL	36+25	78' RT	RES 1360	T	7	2	0	125	
35+23 1/8	34' RT	PEDESTAL	36+45	30' LT	PEDESTAL	T	7	2	1	180	
35+23 1/8	34' RT	PEDESTAL	36+45	30' LT	PEDESTAL	C	6	2	1	180	
35+23 1/8	34' RT	PEDESTAL	39+80	SIDEWALK	VAULT #2	T	6	2	1	471	
35+23 1/8	34' RT	PEDESTAL	40+00	23' RT	POLE RISER	C	2	2	2	503	
39+80	SIDEWALK	VAULT #2	40+00	23' RT	POLE RISER	T	1-4	4	4	120	
39+80	SIDEWALK	VAULT #2	40+00	23' RT	POLE RISER	T	6	2	0	30	
68+22	30' RT	POLE RISER	68+45	SIDEWALK	VAULT #3	T	6	2	0	33	
68+22	30' RT	POLE RISER	68+45	SIDEWALK	VAULT #3	T	1-4	4	4	132	
68+22	30' RT	POLE RISER	69+01	29.38' RT	PEDESTAL	C	2	2	2	99	
68+22	30' RT	POLE RISER	75+20 5	30.42' RT	PEDESTAL	C	3-5	2	6	2184	
68+22	30' RT	POLE RISER	75+20 5	30.42' RT	PEDESTAL	C	1	3	2	728	
68+45	SIDEWALK	VAULT #3	69+01	29.38' RT	PEDESTAL	T	6	2	1	69	
68+45	SIDEWALK	VAULT #3	73 74+50 80	SIDEWALK	VAULT #4	T	1-4	4	0	2420	
69+01	29.38' RT	PEDESTAL	68+68	60' LT	RES 2011	C	6	2	1	115	
69+01	29.38' RT	PEDESTAL	68+68	60' LT	RES 2011	T	7	2	1	115	
69+01	29.38' RT	PEDESTAL	70+56	30.25' RT	PEDESTAL	T	6	2	2	180	
69+01	29.38' RT	PEDESTAL	71+90	30.04' RT	PEDESTAL	C	2	2	2	309	
70+56	30.25' RT	PEDESTAL	71+90	30.04' RT	PEDESTAL	T	6	2	2	160	
71+90	30.04' RT	PEDESTAL	71+85 95	25' RT	POLE RISER	C	6	2	0	20	
71+90	30.04' RT	PEDESTAL	71+85 95	25' RT	POLE RISER	T	7	2	0	20	
71+90	30.04' RT	PEDESTAL	73+45	29.93' RT	PEDESTAL	C	2	2	2	185	
71+90	30.04' RT	PEDESTAL	73+45	29.93' RT	PEDESTAL	T	6	2	2	180	
73+45	29.93' RT	PEDESTAL	73+30 34	30.04' LT	PEDESTAL	C	6	2	0	68	
73+45	29.93' RT	PEDESTAL	73+30 34	30.04' LT	PEDESTAL	T	7	2	0	68	
73+45	29.93' RT	PEDESTAL	73+40	22' RT	POLE RISER	C	6	2	0	26	
73+45	29.93' RT	PEDESTAL	73+40	22' RT	POLE RISER	T	7	2	0	26	
73+45	29.93' RT	PEDESTAL	73 74+50 80	SIDEWALK	VAULT #4	T	6	2	1	68	
73+45	29.93' RT	PEDESTAL	75+20 25	30.42' RT	PEDESTAL	C	2	2	2	205	
73 74+50 80	SIDEWALK	VAULT #4	75+20 25	30.42' RT	PEDESTAL	T	6	2	1	83	
73 74+50 80	SIDEWALK	VAULT #4	80+42 75	SIDEWALK	VAULT #5	T	1-4	4	0	2368	
75+20 5	30.42' RT	PEDESTAL	75+75	90' RT	RES 2100	C	6	2	0	85	
75+20 5	30.42' RT	PEDESTAL	75+75	90' RT	RES 2100	T	7	2	0	85	
75+20 5	30.42' RT	PEDESTAL	77 76+97 03	29.86' RT	PEDESTAL	C	2	2	2	207	
75+20 5	30.42' RT	PEDESTAL	77 76+97 03	29.86' RT	PEDESTAL	T	6	2	2	203	
75+20 5	30.42' RT	PEDESTAL	81 80+42 57	24' RT	POLE RISER	C	3-5	2	6	1656	
75+20 5	30.42' RT	PEDESTAL	81 80+42 57	24' RT	POLE RISER	C	1	3	2	552	
77 76+97 03	29.86' RT	PEDESTAL	76+95 91	30.03' LT	PEDESTAL	C	6	2	0	65	
77 76+97 03	29.86' RT	PEDESTAL	76+95 91	30.03' LT	PEDESTAL	T	7	2	0	65	
77 76+97 03	29.86' RT	PEDESTAL	77+97 87	29.07' RT	PEDESTAL	C	2	2	2	130	
77 76+97 03	29.86' RT	PEDESTAL	77+97 87	29.07' RT	PEDESTAL	T	6	2	2	126	
77+97 87	29.07' RT	PEDESTAL	77+91	23.5' RT	POLE RISER	C	6	2	0	22	
77+97 87	29.07' RT	PEDESTAL	77+91	23.5' RT	POLE RISER	T	7	2	0	22	
77+97 87	29.07' RT	PEDESTAL	79+15 63	25' RT	PEDESTAL	T	6	2	2	139	
77+97 87	29.07' RT	PEDESTAL	81 80+42 57	24' RT	POLE RISER	C	2	2	2	270	
79+15 63	25' RT	PEDESTAL	79+13 70	23.5' RT	POLE RISER	T	7	2	0	20	
79+15 63	25' RT	PEDESTAL	80+42 75	SIDEWALK	VAULT #5	T	6	2	1	135	
80+42 75	SIDEWALK	VAULT #5	81 80+42 57	24' RT	POLE RISER	T	6	2	0	16	
80+42 75	SIDEWALK	VAULT #5	81 80+42 57	24' RT	POLE RISER	T	1-4	4	0	72	

NO.	DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
UTILITY CONDUIT SUMMARY



DESIGNED BY: D.L.M.  
DRAWN BY: C.M.B.  
CHECKED BY: D.L.M.

PROJECT NO.  
69062  
DATE:  
MARCH 1994  
SHEET 12 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

PATH - E:\ALASKA\NORTHWEST  
DRAWN BY AUTOCAD RELEASE 11  
DRAWING PLOT SCALE: 1" = 1'  
DRAWING LAST UPDATED: 3/17/1994

**RETAINING WALL SUMMARY**

LEFT SIDE					RIGHT SIDE									
STATION	OFFSET	TOP WALL ELEV	HEIGHT "H"	REMARKS	STATION	OFFSET	TOP WALL ELEV	HEIGHT	REMARKS	STATION	OFFSET	TOP WALL ELEV	HEIGHT	REMARKS
					69+01.49	23.41' LT	49.7	1'	BEGIN WALL AT BEGIN 52' R.	19+63.45	26.41' RT	45.4	1'	BEGIN WALL
					69+04.05	26.39' LT	49.7	7'	ON 52' RADIUS	20+49	22.46' RT	47.6	3'	
					69+11.02	32.75' LT	49.8	16'	ON 52' RADIUS	20+64.63	21.78' RT	45.6	1'	END WALL
					69+19.50	38.03' LT	49.3	17'	ON 52' RADIUS					
36+78.76	21' LT	59.6	1'	BEGIN WALL	69+24.95	40.36' LT	48.8	17'	ON 52' RADIUS	20+74.70	21.5' RT	45.6	1'	BEGIN WALL
37+71	21' LT	59.5	6'		69+42.58	43.36' LT	46.7	15'	END 52' RADIUS	20+83	21.5' RT	49.6	5'	
38+21	21' LT	59.3	5'		69+54.92	43.31' LT	45.0	11'		21+61	21.5' RT	49.8	5'	
38+90.78	21' LT	59.0	4'	END WALL	69+83.51	43.72' LT	40.9	5'	BEGIN 48' RADIUS	21+75.83	21.5' RT	46.8	2'	END WALL
					69+87.09	43.96' LT	40.3	4'	ON 48' RADIUS					
38+95.78	22.5' LT	59.4	1'	BEGIN WALL	69+90.14	44.40' LT	39.8	4'	END 48' RADIUS	24+23.38	21.5' RT	46.2	1'	BEGIN WALL
38+94	38' LT	58.5	8'		69+95.99	45.45' LT	39.0	5'		24+31	21.5' RT	51.2	6'	
39+24	38' LT	58.7	8'		70+09.57	47.96' LT	37.0	4'	END WALL	25+40	21.5' RT	51.5	6'	
39+24.10	30' LT	59.6	6'	END WALL						25+75.80	21.5' RT	46.7	1'	END WALL
					69+85.86	21' LT	47.4	1'	BEGIN WALL					
44+30.55	23' LT	62.0	5'	BEGIN WALL	70+20	21' LT	46.7	1'		26+05.02	21.5' RT	46.9	1'	BEGIN WALL
44+52.55	27' LT	62.2	5'	END WALL	70+47	21' LT	46.2	3'		26+10	21.5' RT	49.9	4'	
					70+63	21' LT	46.0	4'		26+41	21.5' RT	51.2	5'	
48+66	23' LT	61.3	2'	BEGIN WALL	70+80.81	21' LT	45.7	4'	END WALL	26+45.48	21.5' RT	50.3	4'	END WALL
48+70.72	31' LT	61.0	8'											
48+75.92	31' LT	61.1	8'		71+05.53	29.8' LT	46.0	1'	BEGIN WALL	28+97.1	21.5' RT	52.3	1'	BEGIN WALL
48+80.91	21' LT	61.6	2'	END WALL	71+05.53	36' LT	45.9	3'		29+02	21.5' RT	53.4	2'	
49+06	21' LT	61.6	2'		71+35.53	36' LT	45.7	3'		29+30.21	21.5' RT	52.9	1'	END WALL
49+23	21' LT	61.3	8'		71+41.32	21' LT	45.0	7'						
49+68	21' LT	60.8	4'		71+60	21' LT	44.8	7'		33+83	21.5' RT	58.5	1'	BEGIN WALL
50+11	21' LT	60.5	2'		71+78	21' LT	44.6	1'		34+33.04	21.5' RT	59.0	1'	
50+70	21' LT	60.2	3'		72+22	21' LT	44.0	1'		34+39.76	33.05' RT	62.0	2'	
51+38	21' LT	59.6	7'		72+25	21' LT	44.0	2'		34+44.12	41.23' RT	66.7	1'	END WALL
51+58	21' LT	59.5	6'		72+37.36	21' LT	43.9	2'	END WALL					
52+00	21' LT	59.2	6'	END WALL						34+56.92	34.82' RT	66.9	1'	BEGIN WALL
					72+55	21' LT	43.6	2'	BEGIN WALL	34+53.30	28.86' RT	62.8	2'	
53+90	21' LT	58.5	1'	BEGIN WALL	72+74	21' LT	43.3	3'		34+62.32	21.5' RT	63.0	3'	
54+20	21' LT	58.5	1'	END WALL	72+84	21' LT	43.2	1'		35+13	21.5' RT	63.3	4'	
					73+08	21' LT	42.7	1'	END WALL	35+78	21.5' RT	63.4	4'	
60+75.73	21' LT	55.2	1'	BEGIN WALL						37+15	21.5' RT	68.3	8'	
60+97	21' LT	55.1	1'		75+04	21' LT	37.6	1'	BEGIN WALL	37+48.40	21.5' RT	67.4	7'	END WALL
61+26	21' LT	55.0	3'		75+12	21' LT	37.3	3'						
61+54	21' LT	54.9	2'		75+20	21' LT	37.2	1'	END WALL	40+35.17	21.5' RT	63.8	1'	BEGIN WALL
63+18	21' LT	53.9	3'							40+58	21.5' RT	63.7	2'	
63+22	21' LT	53.9	3'		21+54	LT			BEGIN WALL	41+46	21.5' RT	68.5	6'	
63+26	31' LT	53.6	3'		24+50	LT			END WALL	41+72	21.5' RT	68.4	6'	
63+31	31' LT	53.6	3'							42+15	21.5' RT	67.1	5'	
63+36	21' LT	53.7	3'		36+20	LT			BEGIN WALL	42+49.15	21.5' RT	63.6	1'	END WALL
63+75	21' LT	52.4	3'		36+39	LT			END WALL					
64+53	21' LT	51.6	3'							42+47	21.5' RT	63.7	1'	BEGIN WALL
65+46	21' LT	51.8	4'							42+51	21.5' RT	65.8	3'	
65+65	21' LT	51.4	3'							42+55	21.5' RT	65.7	3'	END WALL
65+98	21' LT	50.4	6'											
66+19	21' LT	50.2	1'							47+40.33	21.5' RT	64.8	1'	BEGIN WALL
66+69	21' LT	49.9	2'							47+59	21.5' RT	66.8	3'	END WALL
67+55	21' LT	49.8	5'											
68+32	21' LT	49.7	4'							47+64	21.5' RT	65.8	2'	BEGIN WALL
68+58	21' LT	49.5	5'							47+68	21.5' RT	66.8	3'	
68+75	21' LT	49.3	4'	END WALL						47+83	21.5' RT	64.8	1'	END WALL

NOTE:  
HEIGHT ("H") GIVEN IS EXPOSED WALL FACE HEIGHT,  
SEE DETAIL ON SHEET 38.

DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

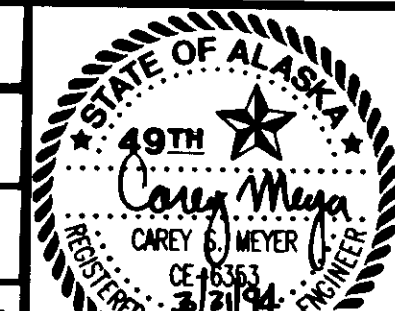
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
**RETAINING WALL SUMMARY**



DESIGNED BY: D.L.M.  
DRAWN BY: D.L.M.  
CHECKED BY: D.L.M.

PROJECT NO.  
69062  
DATE:  
MARCH 1994  
SHEET 13 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

VALVE AND VALVE BOX SUMMARY			
STATION	OFFSET		REMARKS
	LEFT	RIGHT	
10+15.12	18.60'		ADJUST VALVE BOX
10+22.50		19.81'	ADJUST VALVE BOX
10+32.51		11.53'	ADJUST VALVE BOX
10+37.86		11.55'	ADJUST VALVE BOX
10+38.36		16.16'	ADJUST VALVE BOX
10+42.22	15.91'		ADJUST VALVE BOX
10+56.82		34.84'	ADJUST VALVE BOX
13+93.54	19.90'		ADJUST VALVE BOX
14+48.63		28.59'	ADJUST VALVE BOX
19+09.25		10.41'	ADJUST VALVE BOX
19+11.97		12.34'	ADJUST VALVE BOX
19+16.80		10.44'	ADJUST VALVE BOX
20+81.53		16.41'	RELOCATE OUT OF C&G
22+46.81		23.06'	ADJUST VALVE BOX
24+25.66		17.58'	ADJUST VALVE BOX
26+49.07		20.84'	ADJUST VALVE BOX
27+44.32		17.94'	ADJUST VALVE BOX
28+68.22		16.92'	RELOCATE OUT OF C&G
29+39.23		16.22'	RELOCATE OUT OF C&G
32+23.05		16.53'	RELOCATE OUT OF C&G
32+84.28		13.52'	ADJUST VALVE BOX
33+98.25		13.79'	ADJUST VALVE BOX
34+64.62		13.75'	ADJUST VALVE BOX
41+26.22		16.63'	RELOCATE OUT OF C&G
45+65.77	18.96'		ADJUST VALVE BOX
45+91.60		14.03'	ADJUST VALVE BOX
46+30.33		18.44'	ADJUST VALVE BOX
50+90.83		20.05'	ADJUST VALVE BOX
52+34.89		19.02'	ADJUST VALVE BOX
54+52.83		19.29'	ADJUST VALVE BOX
56+43.42		18.04'	ADJUST VALVE BOX
56+76.75		8.44'	ADJUST VALVE BOX
56+80.68	22.59'		ADJUST VALVE BOX
65+86.24		22.80'	ADJUST VALVE BOX
66+30.04		21.37'	ADJUST VALVE BOX
68+79.15		21.50'	ADJUST VALVE BOX
70+84.91		29.18'	ADJUST VALVE BOX
71+81.03		25.40'	ADJUST VALVE BOX
75+38.11		19.22'	ADJUST VALVE BOX

NOTE: C&G = CURB AND GUTTER

MANHOLE SUMMARY							
STATION	OFFSET		REMARKS	STATION	OFFSET		REMARKS
	LEFT	RIGHT			LEFT	RIGHT	
10+84		1'	ADJUST RIM	48+62		18'	ADJUST RIM
12+18		25'	REMOVE ADJUST	51+79		16'	ADJUST RIM
14+75		1'	ADJUST RIM	54+24		16'	ADJUST RIM
18+70		1'	ADJUST RIM	56+94		17'	ADJUST RIM
21+18		25'	ADJUST RIM	60+86		17'	ADJUST RIM
22+67		11'	ADJUST RIM	64+78		18'	ADJUST RIM
24+60			REMOVE	68+17		15'	ADJUST RIM
26+21		11'	ADJUST RIM	69+56		17'	ADJUST RIM
26+88			REMOVE	72+70		19'	ADJUST RIM
29+56		4'	ADJUST RIM	74+93		16'	ADJUST RIM
31+52		19'	ADJUST RIM	78+55		17'	ADJUST RIM
33+60		20'	ADJUST RIM				
35+43		22'	ADJUST RIM	"A" 1+04		6'	RECONSTRUCT
36+55		17'	ADJUST RIM	"A" 3+78	2'		ADJUST RIM
40+34		17'	ADJUST RIM	"B" 13+39		5'	RECONSTRUCT
43+75		17'	ADJUST RIM				
46+39		17'	ADJUST RIM				

FIRE HYDRANT SUMMARY						
FROM STATION	OFFSET		TO STATION	OFFSET		REMARKS
	LEFT	RIGHT		LEFT	RIGHT	
19+21.22	25.34'		19+21.22	28.50'		RELOCATE
24+39.46		19.78'	24+17.34	26.00'		RELOCATE
29+21.30		19.73'	29+46.03	26.00'		RELOCATE
34+06.31		15.06'	33+50.00	26.00'		RELOCATE
39+97.22		26.62'				ADJUST
45+96.51		23.57'				ADJUST
52+27.43		25.57'				ADJUST
57+84.84		18.79'	57+88.00	26.00'		RELOCATE
63+84.88		30.25'				ADJUST
68+22.15		26.33'				ADJUST
74+63.15		32.52'	73+46	26 RT		ADJUST RELOCATE
79+82.98		20.65'	80+50.00	21.55'		RELOCATE
"A" 6+28.41		1.90'	"A" 7+28.58	0.66'	9 RT	RELOCATE
			"B" 9+08			
24+17		X	23+65		X	RELOCATE

DRIVEWAY CURB CUT SUMMARY				
STATION	OFFSET		WIDTH	REMARKS
	LEFT	RIGHT		
"H" 2+03.24		X	40'	ACCESS TO LOWER BREAKWATER PARKING AREA, NO SIDEWALK BEHIND CURB, 3' CURB TRANSITIONS AT EACH END OF DEPRESSED CURB
11+39.33		X	12'	
11+74.79		X	12'	
12+24.23		X	12.5'	
12+76.80		X	11'	
13+27.44		X	17'	
13+92.27		X	10'	
14+32.58	X		20'	
14+46.87		X	17.5'	
14+80.25		X	11'	
15+28.75		X	13.5'	
15+78.04		X	12'	
16+29.35		X	13'	
16+81.67		X	10'	
17+53.64		X	9.5'	
18+00.76		X	8'	
18+62.40		X	27'	

MISC. PIPE CONNECTION SUMMARY			
FROM STATION	OFFSET	TO	REMARKS
22+17	23' RT	COLLECTION BASIN	8" METAL PIPE
22+20	23' RT	COLLECTION BASIN	3" PVC PIPE
24+53	27' RT	UNDERDRAIN	4" METAL PIPE
24+58	16' RT	UNDERDRAIN	2" METAL PIPE
25+51	17' RT	UNDERDRAIN	4" PVC PIPE
29+76	26' RT	FIELD INLET S-15	10" PVC PIPE
29+79	27' RT	FIELD INLET S-15	4" PVC PIPE
29+80	27' RT	FIELD INLET S-15	2" PVC PIPE
50+38	31' RT	COLLECTION BASIN	18" CMP
52+23	25' RT	UNDERDRAIN	6" PVC PIPE
54+12	21' RT	COLLECTION BASIN	12" CMP
55+76	32' RT	UNDERDRAIN	2-4" PVC PIPES
56+01	21' RT	UNDERDRAIN	4" PVC PIPE
56+46	18' RT	CURB INLET S-26	4" PERF. PIPE
62+85	30' RT	FIELD INLET S-27	6" PIPE
62+88	31' RT	FIELD INLET S-27	6" PIPE
67+07	20' RT	UNDERDRAIN	6" PVC PIPE
69+68	23' RT	UNDERDRAIN	4" PVC PIPE
74+16	24' RT	UNDERDRAIN	4" METAL PIPE
74+25	24' RT	UNDERDRAIN	4" PVC PIPE
75+12	27' RT	CURB INLET S-33	12" PIPE
77+35	20' RT	UNDERDRAIN	4" PERF. PIPE
77+87	28' RT	COLLECTION BASIN	6" PIPE

INLET REMOVAL SUMMARY			
STATION	OFFSET		REMARKS
	LEFT	RIGHT	
10+98.2		24.8'	
12+13.3		24.0'	STILL IN USE
15+19.7		24.9'	
16+93.7		23.5'	
18+71.2		27.4'	
19+42.9		24.2'	
25+78.6		16.6'	
25+98.9		16.5'	
27+41.1		14.0'	
75+12.7		23.0'	

METAL FLUME DOWNDRAIN SUMMARY			
STATION	OFFSET	DIAMETER	LENGTH
35+08	49' LT.	24"	6'
41+95	35' LT.	24"	6'
53+99	32' LT.	24"	5'

WHEEL CHAIR RAMP SUMMARY			
STATION	OFFSET		REMARKS
	LEFT	RIGHT	
10+17		X	SEE STANDARD DRAWING I-20.11
10+25	X		SEE STANDARD DRAWING I-20.11
"H" 0+30	X		SEE STANDARD DRAWING I-20.11
"H" 0+40		X	DEPRESSED SIDEWALK IN ISLAND
10+89		X	SEE STANDARD DRAWING I-20.11
11+21	X		DEPRESSED SIDEWALK, SEE DETAILS ON SHEET 41
18+88		X	SEE STANDARD DRAWING I-20.11
19+31		X	SEE STANDARD DRAWING I-20.11
19+47	X		DEPRESSED SIDEWALK, SEE DETAILS ON SHEET 41
56+59		X	DEPRESSED SIDEWALK, SEE DETAILS ON SHEET 41
57+35		X	SEE STANDARD DRAWING I-20.11
80+97		X	DEPRESSED SIDEWALK, SEE DETAILS ON SHEET 41

RECORD OF REVISIONS		
BY:	DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
MISCELLANEOUS SUMMARIES

**ASCCG**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 14 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

**GUARDRAIL SUMMARY**

FROM		TO		LENGTH		REMARKS
STATION	OFFSET	STATION	OFFSET	REMOVE	INSTALL	
15+60	22' LT	29+75	13' LT	1415 L.F.		
34+25	20' LT	35+05	20' LT	80 L.F.		
48+40	45' LT	49+20	19' LT	91 L.F.		INSTALL TERMINAL END ON LAST POST
69+05.4	25' LT	70+09.4	46' LT		125.0 112.5 L.F.	INSTALL TERMINAL ENDS ON END POSTS GUARD RAIL TYPE G2S, SEE STD. DWG. G-02.00
79+43	64' LT	79+50	91' LT	34 L.F.		
79+36	56' LT	79+50	91' LT		38 L.F.	INSTALL BREAKAWAY CABLE TERMINAL
"A" 1+00	3' LT	"A" 1+08	25' RT	28 L.F.		
"A" 1+10	32' RT	"A" 1+11	9' RT		25 L.F.	INSTALL TERMINAL ENDS ON END POSTS

**FENCE SUMMARY**

FROM		TO		LENGTH			REMARKS
STATION	OFFSET	STATION	OFFSET	REMOVE	INSTALL	RECONSTRUCT	
23+75	19.39' LT	28+20.67	32.18' LT	446 L.F.			
<del>24+50</del> 23+75	24.31' LT	<del>33+00</del> 28+20.67	32.18' LT		780 446 L.F.		CONNECT NEW 6' FENCE TO EXISTING AT 28+20.67
"A" 1+00	7.6' LT	"A" 2+30	7.6' LT	130 L.F.			
"A" 1+00	7.6' LT	"A" 2+30	7.6' LT		130 L.F.		CONNECT NEW 6' FENCE TO EXISTING AT "A" 1+00.00
"A" 2+30	7.6' LT	"A" 6+60	5.6' LT	430 L.F.			
"A" 2+30	7.6' LT	<del>"B" 9+90</del> "A" 6+60	5.6' LT		183 430 L.F.		CONNECT NEW 6' FENCE TO EXISTING AT "A" 6+60
"B" 10+63	9.6' LT	"B" 27+06	9.8' LT		1,643 L.F.	1,643 L.F.	MOVE FENCE FABRIC TO INSIDE OF POSTS REPLACED

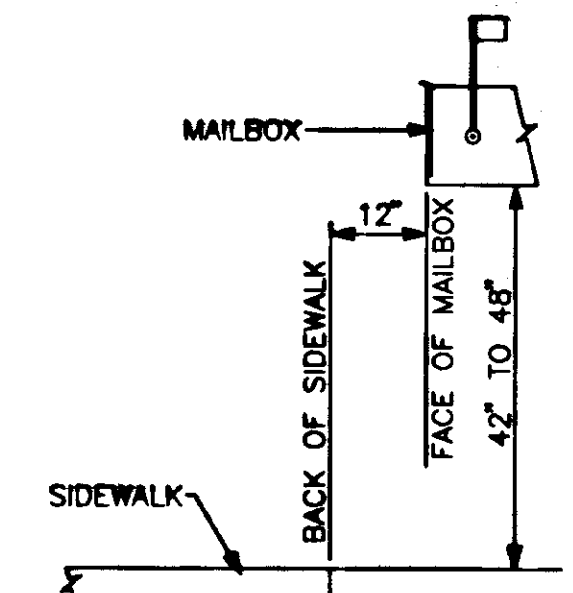
**MAIL BOX REMOVAL SUMMARY**

STATION	OFFSET		REMARKS
	LEFT	RIGHT	
23+65		17'	1850
25+59		15'	MULTIPLE AT AURORA ARMS, START
25+71		15'	MULTIPLE AT AURORA ARMS, END
26+43		15'	1910, 1916
27+98		17'	MULTIPLE START, 1930, 1936,
28+09		17'	1950, 1960, MULTIPLE, END
30+08		23'	1970
32+07		18'	MULTIPLE, 2000, 2030
35+27	24'		1323
36+00	22'		1341
40+13	16'		1441
46+69		12'	1540
48+54	23'		1547
52+11		13'	1670
56+41		12'	MULTIPLE AT WICKERSHAM, START
56+57		13'	MULTIPLE AT WICKERSHAM, END
60+70	18'		ONE BOX, 1800, 1801
61+09		14'	1841
65+67		12'	1921 WICKERSHAM
67+06		12'	1931
67+76		13'	1941
69+12	24'		2011
69+19		19'	2020
70+65		18'	2030
71+52	18'		2031
73+50		19'	2080
74+74		17'	2090
76+18		19'	2100
77+76		12'	MULTIPLE, 2150, 2170

**MAIL BOX INSTALLATION SUMMARY**

STATION	OFFSET		REMARKS
	LEFT	RIGHT	
23+63.5		X	1850
25+65.7		X	BEGIN MULTIPLE AT AURORA ARMS PLACED IN AURORA ARMS PARKING LOT
25+77.7		X	END MULTIPLE AT AURORA ARMS -
26+46.9		X	DOUBLE - 1910, 1916
28+03.6		X	BEGIN MULTIPLE - 1930, 1936
28+15.6		X	END MULTIPLE - 1950, 1960
30+03.1		X	1970
31+98.2		X	DOUBLE - 2000, 2030
35+69.25		X	DOUBLE - 1323, 1341
40+07.60		X	1441
46+29.760		X	DOUBLE - 1540, 1547
51+92.60		X	1670
56+06.0		X	BEGIN MULTIPLE AT WICKERSHAM
56+22.0		X	END MULTIPLE AT WICKERSHAM
61+32.2		X	DOUBLE - 1 FOR 1800 & 1801, 1841
65+84.95		X	1921 WICKERSHAM
67+03.00		X	DOUBLE - 1931, 1941
69+74.285		X	DOUBLE - 2011, 2020
70+93.715		X	DOUBLE - 2030, 2031
73+95.0		X	2080
75+06.6		X	DOUBLE - 2090, 2100
77+74.6		X	DOUBLE - 2150, 2170
20+60		X	1800

NOTE: STATION GIVEN IS CENTERLINE OF MAILBOX



**MAILBOX DETAIL**

**REMOVAL OF STRUCTURES AND OBSTRUCTIONS**

STATION	OFFSET		DESCRIPTION OF ITEM TO BE REMOVED
	LEFT	RIGHT	
11+35	37'		5' x 5' CONCRETE SLAB
22+30		19'	3' X 5' TRASH ENCLOSURE - RECONSTRUCT AT 24' RIGHT
24+75		18'	80' ROCK WALL
26+00		18'	40' ROCK WALL
26+45		17'	10' CONCRETE RETAINING WALL
27+60		18'	18' ROCK WALL
28+85		17'	43' PLANTER BOX
35+28		14'	216' STEEL BIN WALL
39+44	19'		21' WOOD BENCH WITH 3 POSTS
39+72	20'		26' WOOD BOLLARDS WITH ROPE
67+05		22'	45' WOOD BOLLARDS WITH ROPE
70+53		17'	12' ROCK WALL GARDEN
71+95		19'	115' ROCK WALL
74+81		28'	26' WOOD CRIB WALL
74+90		30'	RELOCATE ROCK PATHWAY AS REQUIRED TO ACCOMODATE RETAINING WALL RELOCATE YARD LIGHT TO 75+06, 31.5' RT.
75+12		23'	STEEL GRATE & BOX
75+17		25'	DISCONNECT THAW WIRE AT METER

**WOOD STAIRWAY ADJUSTMENT SUMMARY**

STATION	OFFSET		DESCRIPTION OF WORK
	LEFT	RIGHT	
20+67		21'	CONSTRUCT NEW STAIRWAY FROM BACK OF NEW SIDEWALK TO MATCH EXISTING STAIRWAY AT 20+65
40+30		21'	CONSTRUCT NEW STAIRWAY/WALKWAY FROM BACK OF NEW SIDEWALK TO MATCH EXISTING STAIRWAY AT 41+05
58+56		21'	CONSTRUCT NEW STAIRWAY FROM BACK OF NEW SIDEWALK TO MATCH EXISTING STAIRWAY AT 58+68
65+78		21'	CONSTRUCT NEW STAIRWAY FROM BACK OF NEW SIDEWALK TO MATCH EXISTING STAIRWAY AT 65+58
69+25		27'	CONSTRUCT NEW STAIRWAY FROM BACK OF NEW SIDEWALK TO MATCH EXISTING STAIRWAY AT 69+25
71+35	32'		CONSTRUCT NEW STAIRWAY TO MATCH EXISTING STAIRWAY AT 71+43
14+00	X		CONSTRUCT NEW STAIRWAY TO MATCH EXISTING STAIRWAY

**MONUMENT SUMMARY**

STATION	OFFSET	DESCRIPTION OF POINT
21+50.41	31.21' RT.	PROPERTY CORNER
26+33.17	20.58' RT.	C4 USS 1462/C1 USS 1452
28+05.54	19.06' RT.	PROPERTY CORNER
28+65.54	19.33' RT.	PROPERTY CORNER

NOTE: THESE MONUMENTS MAY BE DISTURBED, ANY OTHER MONUMENTS ENCOUNTERED MUST BE REFERENCED AND REPLACED.

**HAND RAIL INSTALLATION SUMMARY**

FROM		TO		LENGTH	REMARKS
STATION	OFFSET	STATION	OFFSET		
48+55	21.0'	48+82	21.0'	102 L.F.	21 L.F.
62+96	21.0'	63+70	21.0'	112 L.F.	97 L.F.
71+05	LT	71+40	LT	32 L.F.	
39+00	LT	39+25	LT	41 L.F.	

**SELECTIVE TREE REMOVAL SUMMARY**

STATION	OFFSET		REMARKS
	LEFT	RIGHT	
27+68		23'	
29+35		22'	
35+94		15'	
36+09		15'	
36+24		16'	
36+33		16'	
36+48		17'	
36+57		18'	
36+72		20'	
36+81		20'	
36+96		20'	
73+00		20'	

BY:	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0978(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
MISCELLANEOUS SUMMARIES

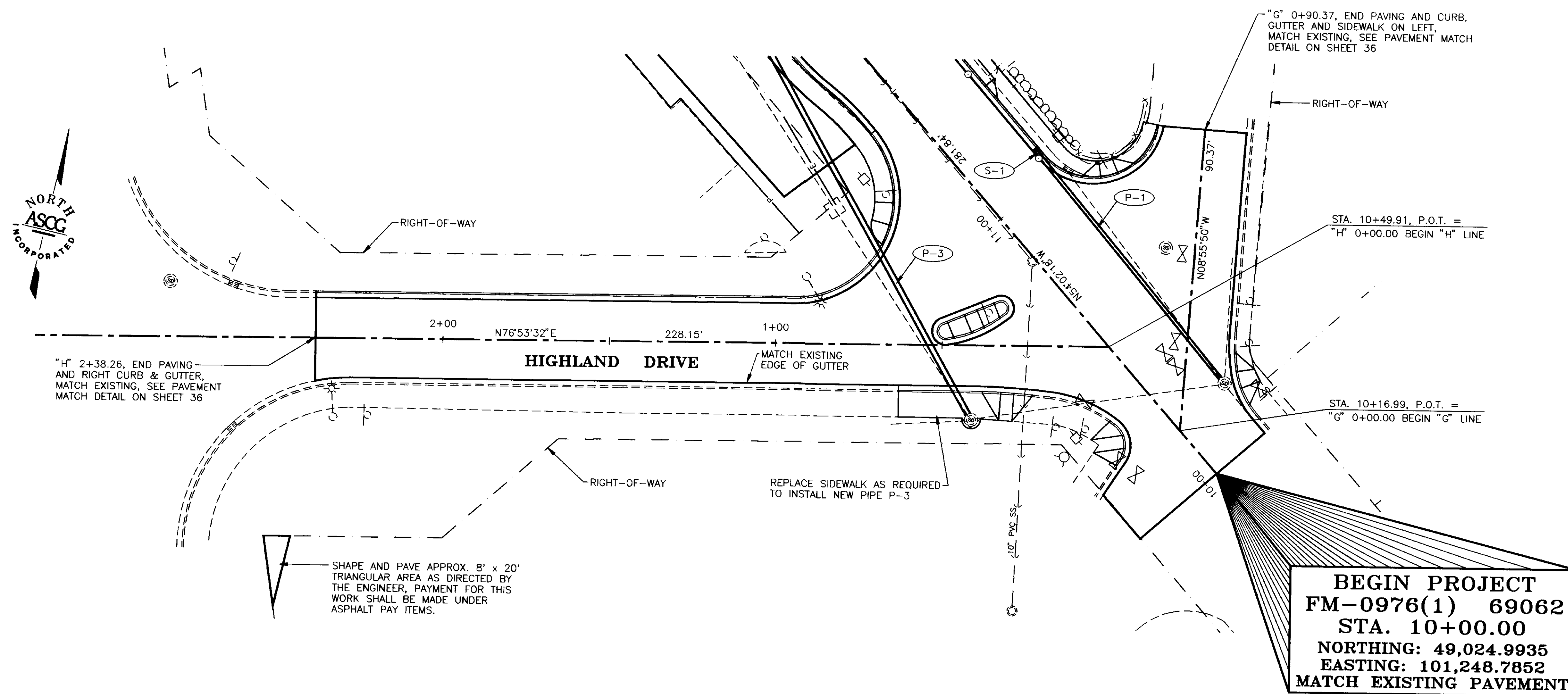


DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

PATH - EVACUATION ROUTE  
DRAWN IN AUTOCAD RELEASE 11  
DRAWING PLOT SCALE: 1" = 1'  
DRAWING LAST UPDATED: 3/17/1994

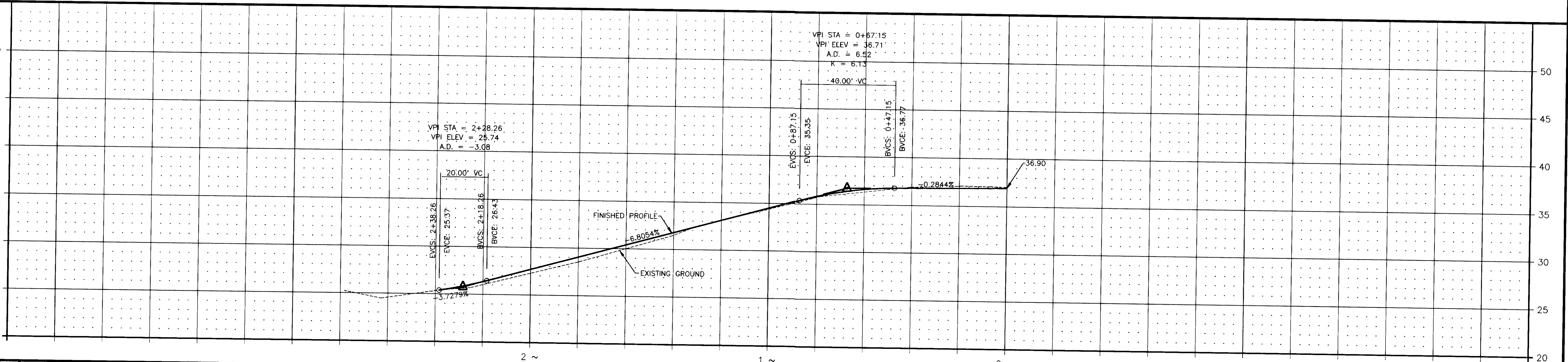
PROJECT NO. 69062  
DATE: MARCH 1994  
SHEET 15 OF 55



**BEGIN PROJECT**  
**FM-0976(1) 69062**  
**STA. 10+00.00**  
**NORTHING: 49,024.9935**  
**EASTING: 101,248.7852**  
**MATCH EXISTING PAVEMENT**

**HORIZONTAL CONTROL**  
 THE PROJECT BASIS OF BEARING IS N45°56'51"W  
 BETWEEN EGAN DRIVE CENTERLINE MONUMENTS AT  
 STATION P.C. "O" 12+49.53 AND P.O.T. "L" 12+63.69

**VERTICAL CONTROL**  
 THE BASIS OF VERTICAL CONTROL IS THE EGAN DRIVE  
 CENTERLINE MONUMENT AT STATION P.O.T. "L" 12+63.69  
 WITH AN ELEVATION OF 32.02' ABOVE MLLW.



DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 "H" 0+00.00 TO "H" 2+28.15



DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

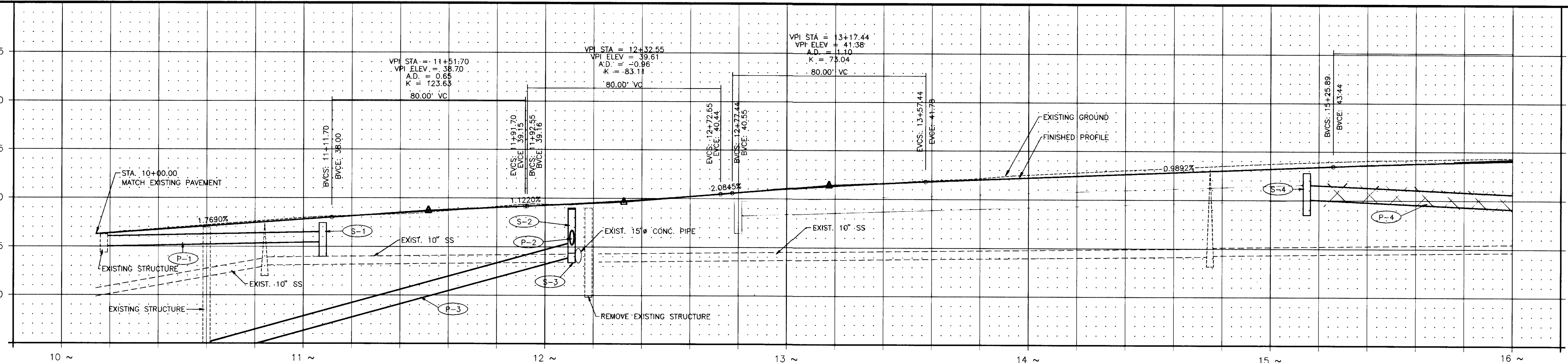
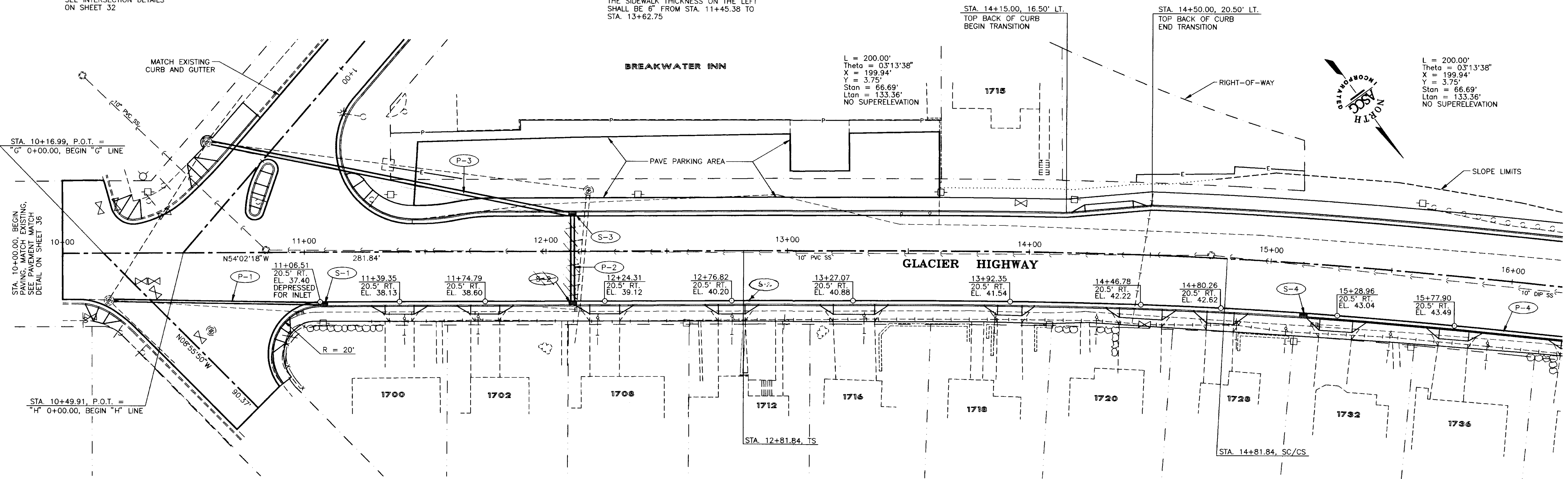
PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 16 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

NOTE:  
SEE INTERSECTION DETAILS  
ON SHEET 32

NOTE:  
THE SIDEWALK THICKNESS ON THE LEFT  
SHALL BE 6" FROM STA. 11+45.38 TO  
STA. 13+62.75



BY:	DATE:	DESCRIPTION OF CHANGE:
<b>RECORD OF REVISIONS</b>		

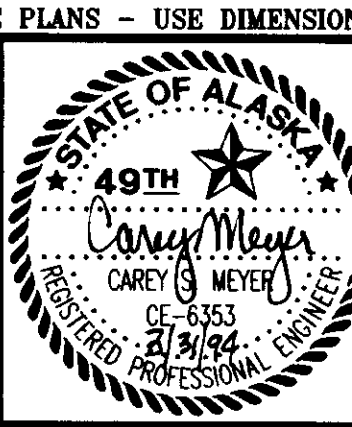
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

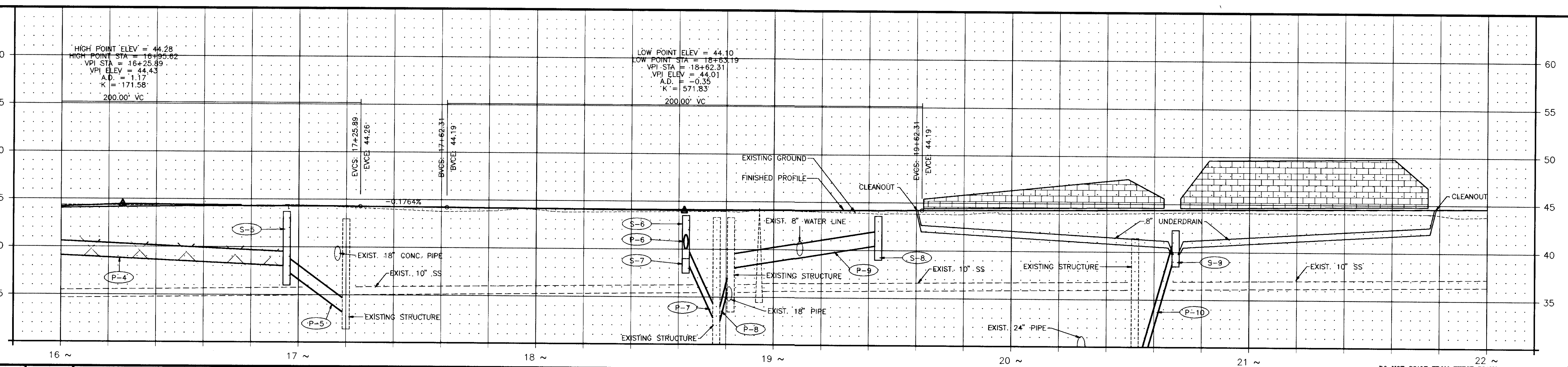
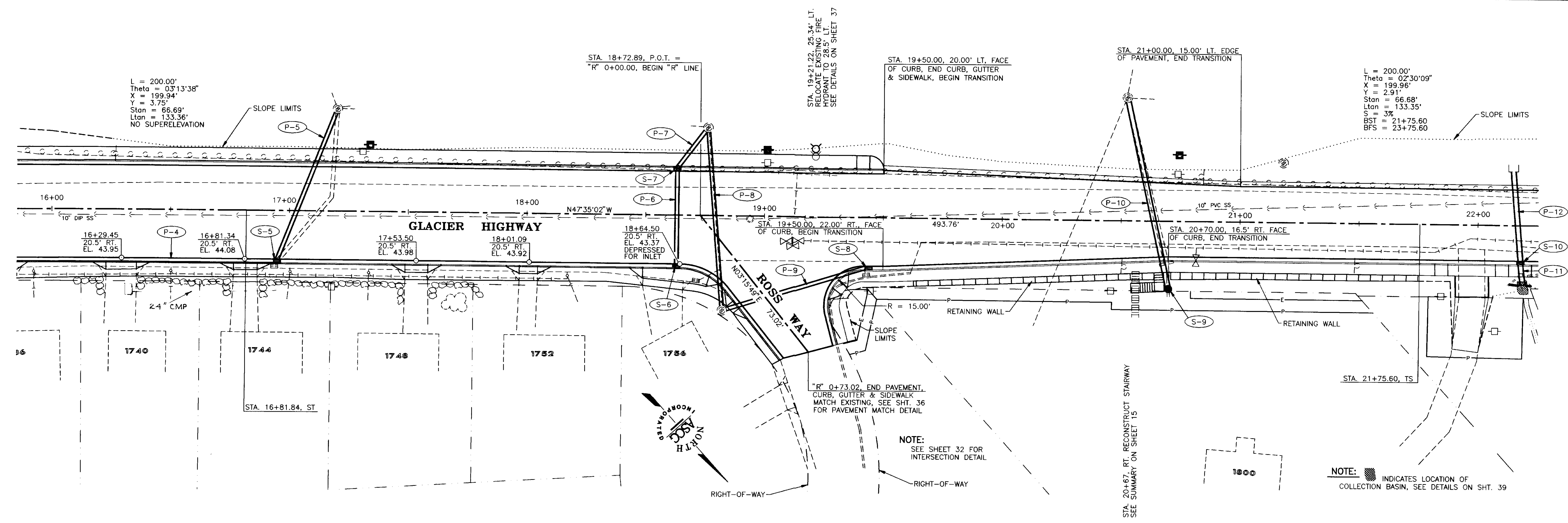
JUNEAU  
FM-0978(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
PLAN AND PROFILE  
STA. 10+00.00 TO STA. 16+00.00



DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 17 OF 55





NO.	DATE	DESCRIPTION OF CHANGE

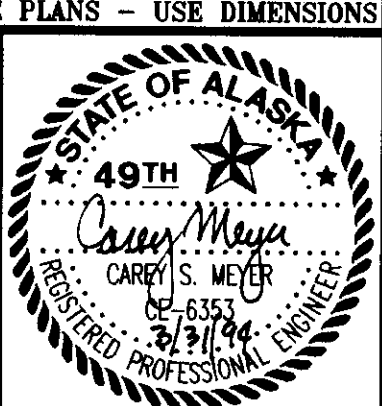
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 16+00.00 TO STA. 22+00.00

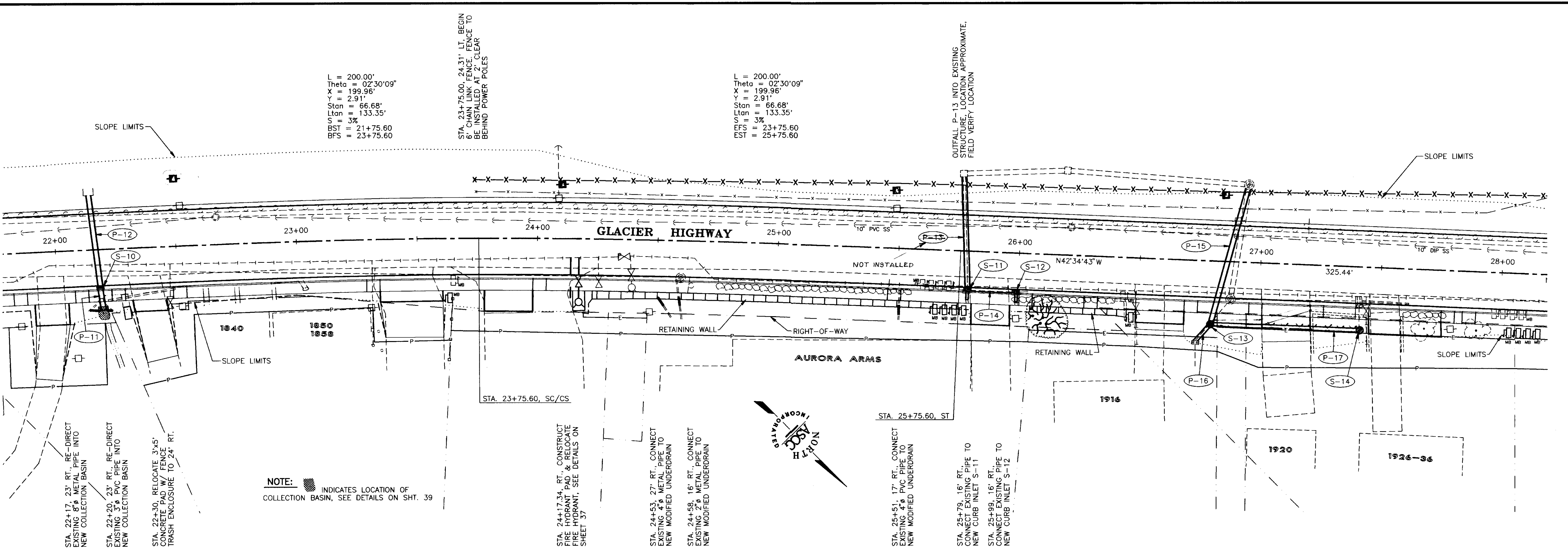


DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 18 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



L = 200.00'  
 Theta = 02°30'09"  
 X = 199.96'  
 Y = 2.91'  
 Stan = 66.68'  
 Ltan = 133.35'  
 S = 3%  
 BST = 21+75.60  
 BFS = 23+75.60

STA. 23+75.00, 24.31' LT. BEGIN  
 6' CHAIN LINK FENCE. FENCE TO  
 BE INSTALLED AT 2' CLEAR  
 BEHIND POWER POLES

L = 200.00'  
 Theta = 02°30'09"  
 X = 199.96'  
 Y = 2.91'  
 Stan = 66.68'  
 Ltan = 133.35'  
 S = 3%  
 EFS = 23+75.60  
 EST = 25+75.60

OUTFALL P-13 INTO EXISTING  
 STRUCTURE. LOCATION APPROXIMATE.  
 FIELD VERIFY LOCATION

**NOTE:** INDICATES LOCATION OF  
 COLLECTION BASIN, SEE DETAILS ON SHT. 39

STA. 22+17, 23' RT., RE-DIRECT  
 EXISTING 8" METAL PIPE INTO  
 NEW COLLECTION BASIN

STA. 22+20, 23' RT., RE-DIRECT  
 EXISTING 3" PVC PIPE INTO  
 NEW COLLECTION BASIN

STA. 22+30, RELOCATE 3"x5"  
 CONCRETE PAD W/ FENCE  
 TRASH ENCLOSURE TO 24' RT.

STA. 24+17.34, RT. CONSTRUCT  
 FIRE HYDRANT PAD & RELOCATE  
 FIRE HYDRANT. SEE DETAILS ON  
 SHEET 37

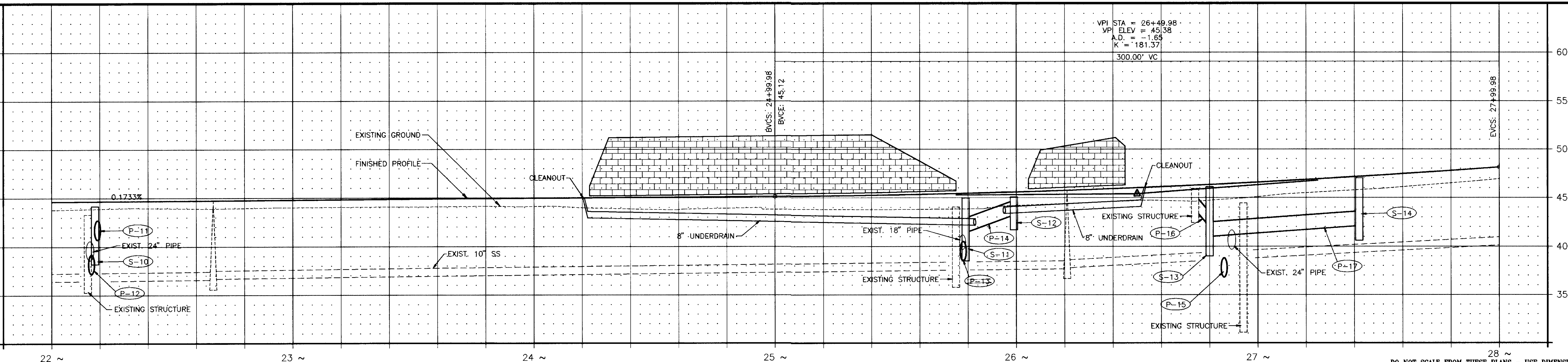
STA. 24+53, 27' RT., CONNECT  
 EXISTING 4" METAL PIPE TO  
 NEW MODIFIED UNDERDRAIN

STA. 24+58, 16' RT., CONNECT  
 EXISTING 2" METAL PIPE TO  
 NEW MODIFIED UNDERDRAIN

STA. 25+51, 17' RT., CONNECT  
 EXISTING 4" PVC PIPE TO  
 NEW MODIFIED UNDERDRAIN

STA. 25+79, 16' RT.,  
 CONNECT EXISTING PIPE TO  
 NEW CURB INLET S-11

STA. 25+99, 16' RT.,  
 CONNECT EXISTING PIPE TO  
 NEW CURB INLET S-12



BY:	DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 22+00.00 TO STA. 28+00.00

**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

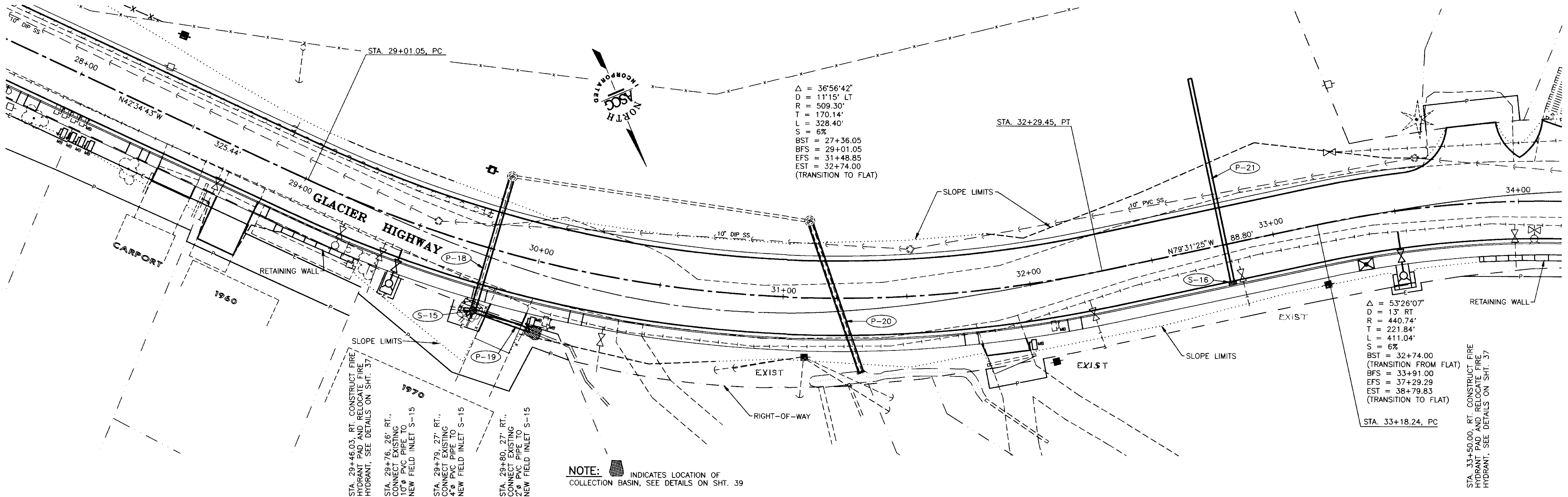
DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.:  
 69062  
 DATE:  
 MARCH 1994  
 SHEET 19 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

STA. 28+20.67, 32.18' LT, END  
6" CHAIN LINK FENCE TO  
BE INSTALLED AT 2' CLEAR  
BEHIND POWER POLES

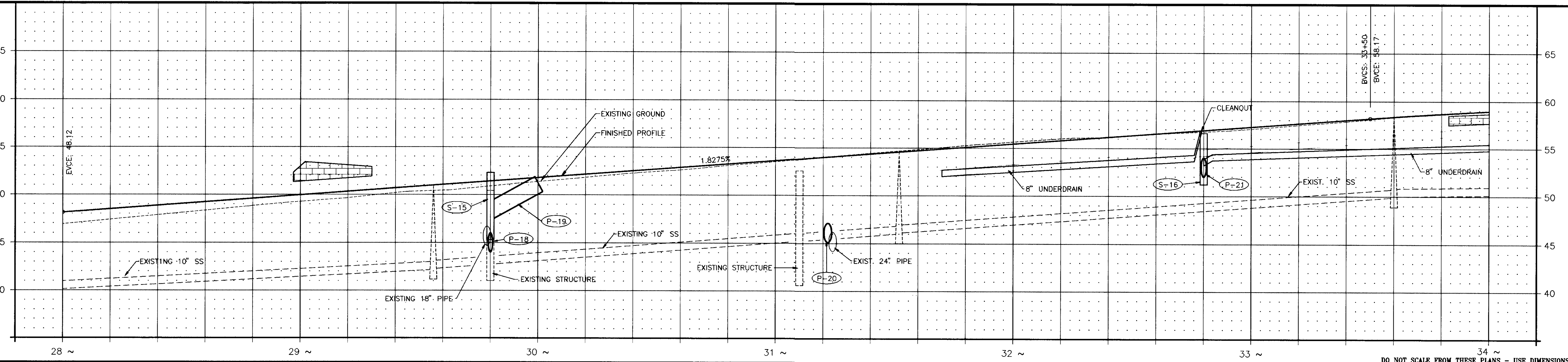


NOTE: INDICATES LOCATION OF COLLECTION BASIN, SEE DETAILS ON SHT. 39

STA. 29+46.03, RT. CONSTRUCT FIRE HYDRANT PAD AND RELOCATE FIRE HYDRANT, SEE DETAILS ON SHT. 37  
STA. 29+76, 26' RT., CONNECT EXISTING 10" PVC PIPE TO NEW FIELD INLET S-15  
STA. 29+79, 27' RT., CHANGE EXISTING 4" PVC PIPE TO NEW FIELD INLET S-15  
STA. 29+80, 27' RT., CONNECT EXISTING 2" PVC PIPE TO NEW FIELD INLET S-15

$\Delta = 53'26'07''$   
 $D = 13' RT$   
 $R = 440.74'$   
 $T = 221.84'$   
 $L = 411.04'$   
 $S = 6\%$   
BST = 32+74.00  
(TRANSITION FROM FLAT)  
BFS = 33+91.00  
EFS = 37+29.29  
EST = 38+79.83  
(TRANSITION TO FLAT)  
STA. 33+18.24, PC

STA. 33+50.00, RT. CONSTRUCT FIRE HYDRANT PAD AND RELOCATE FIRE HYDRANT, SEE DETAILS ON SHT. 37



NO.	DATE	DESCRIPTION OF CHANGE

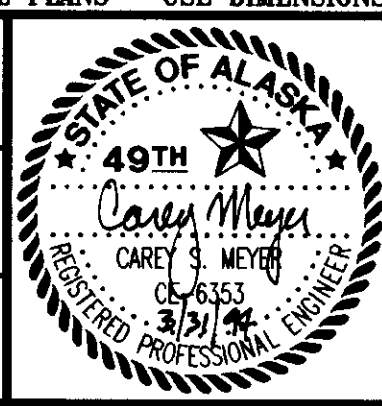
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
PLAN AND PROFILE  
STA. 28+00.00 TO STA. 34+00.00

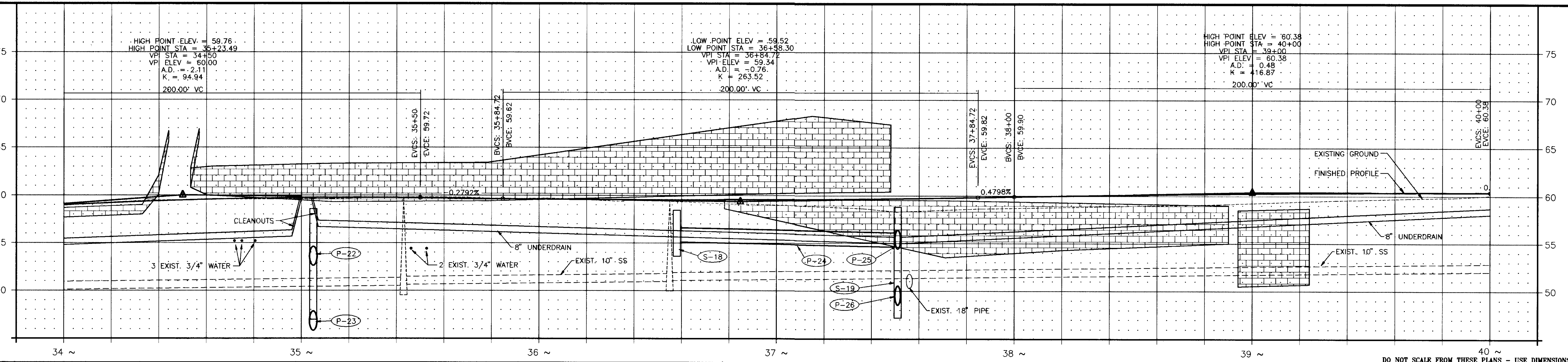
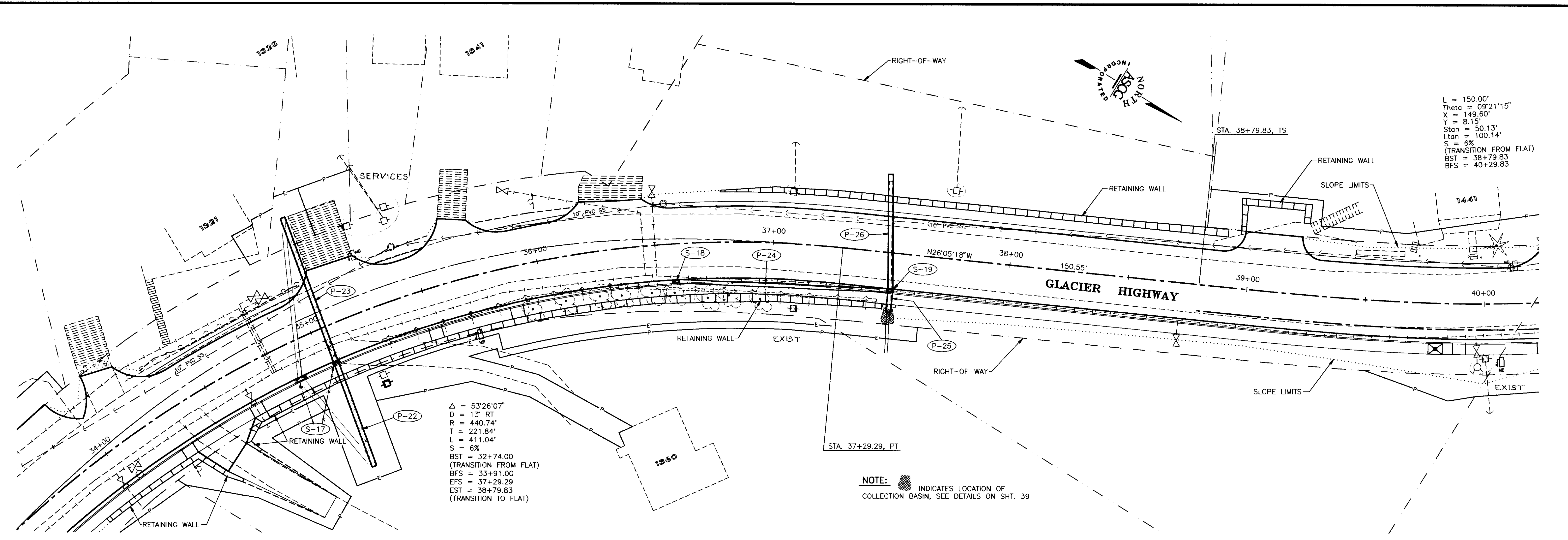


DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 20 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



NO.	DATE	DESCRIPTION OF CHANGE

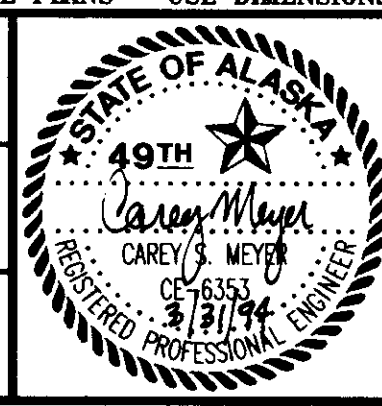
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 34+00.00 TO STA. 40+00.00

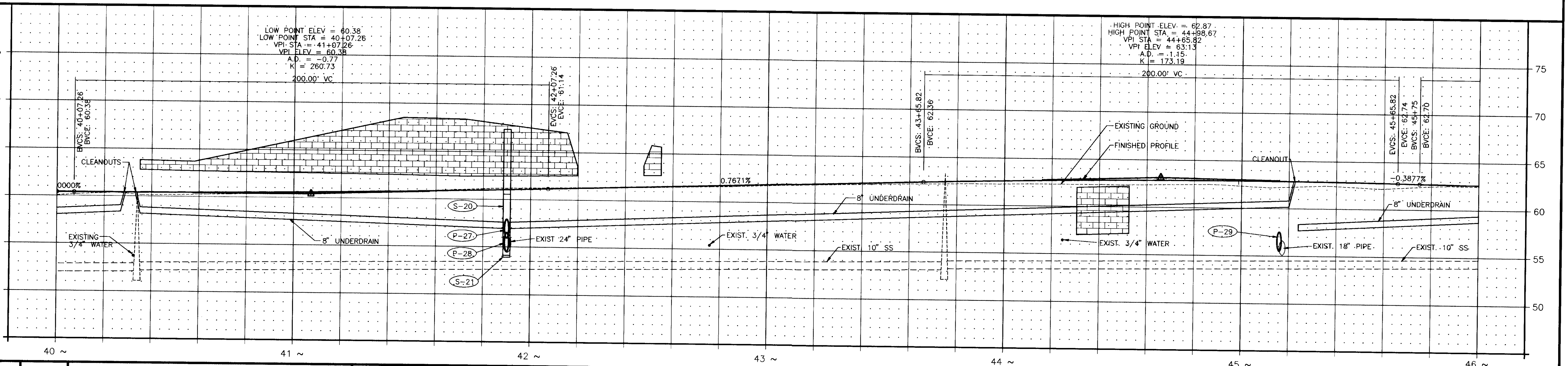
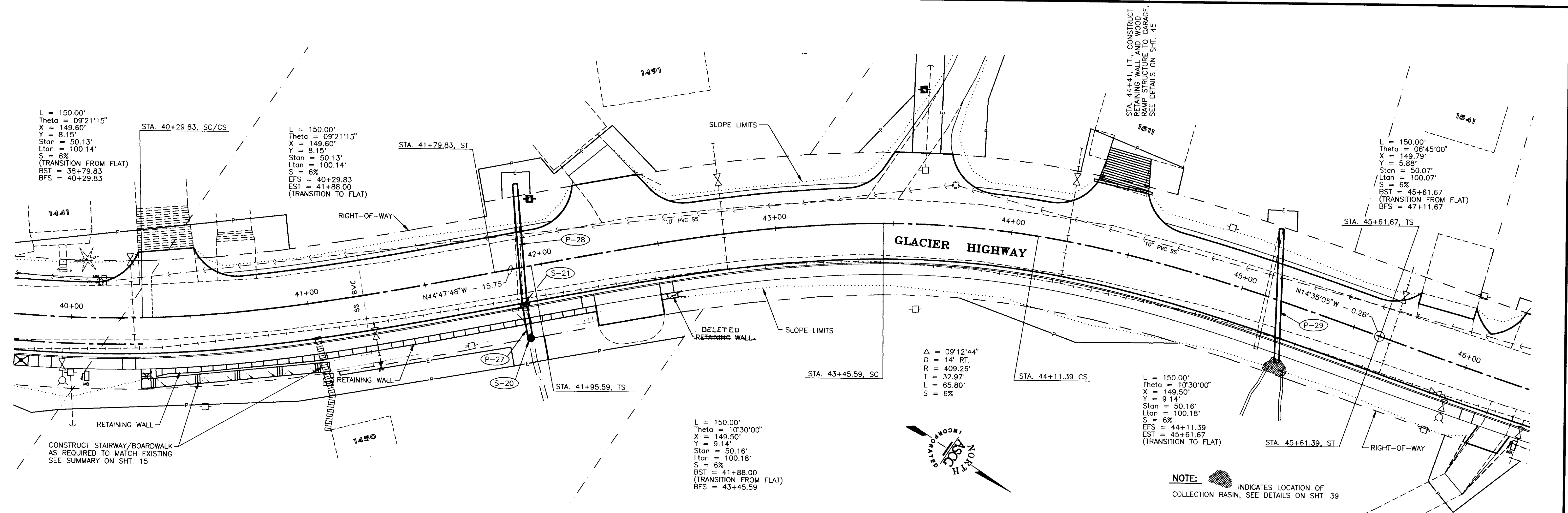


DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 21 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



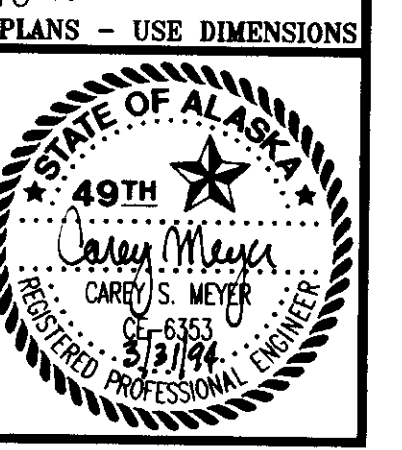
NO.	DATE	DESCRIPTION OF CHANGE

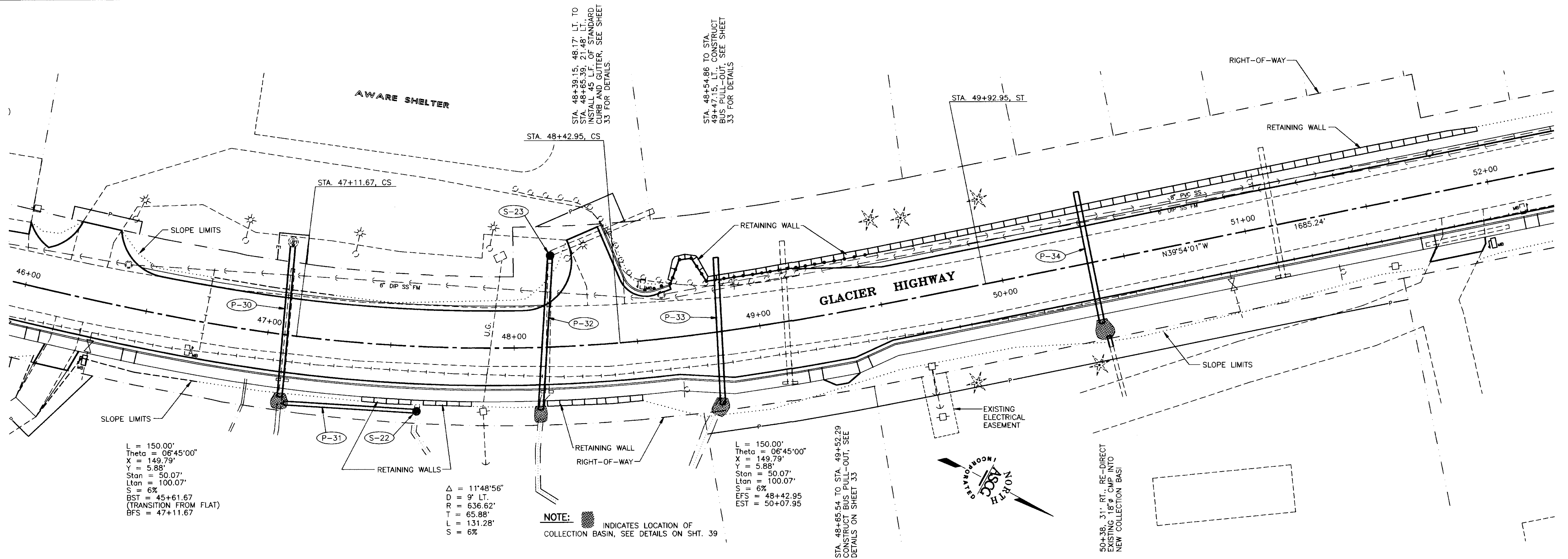
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 40+00.00 TO STA. 46+00.00



DESIGNED BY:	D.L.M.	PROJECT No.:	69062
DRAWN BY:	J.E.M.	DATE:	MARCH 1994
CHECKED BY:	D.L.M.	SHEET	22 OF 55





L = 150.00'  
 Theta = 06°45'00"  
 X = 149.79'  
 Y = 5.88'  
 Stan = 50.07'  
 Ltan = 100.07'  
 S = 6%  
 BST = 45+61.67  
 (TRANSITION FROM FLAT)  
 BFS = 47+11.67

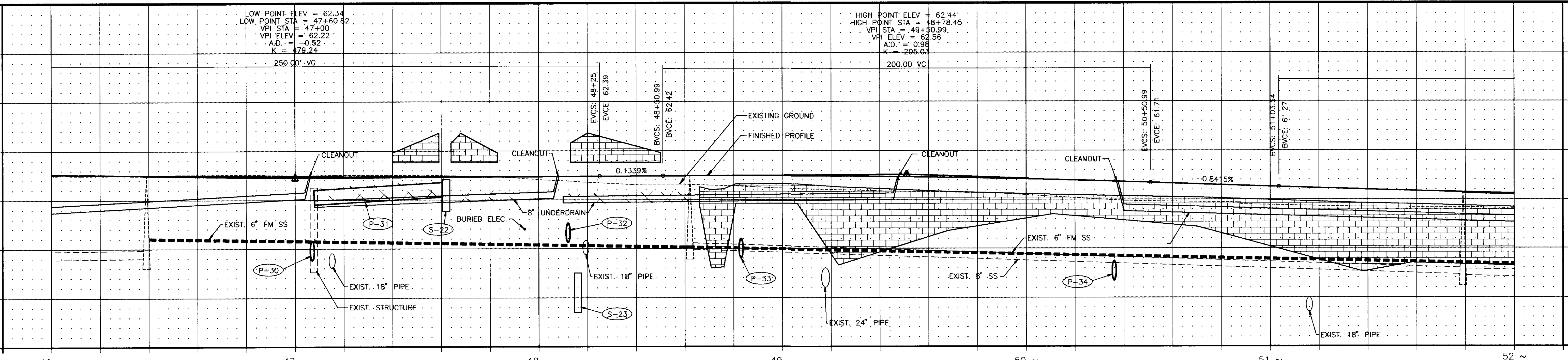
$\Delta = 11°48'56"$   
 D = 9' LT.  
 R = 636.62'  
 T = 65.88'  
 L = 131.28'  
 S = 6%

**NOTE:** INDICATES LOCATION OF COLLECTION BASIN, SEE DETAILS ON SHT. 39

L = 150.00'  
 Theta = 06°45'00"  
 X = 149.79'  
 Y = 5.88'  
 Stan = 50.07'  
 Ltan = 100.07'  
 S = 6%  
 EFS = 48+42.95  
 EST = 50+07.95

STA. 48+85.54 TO STA. 49+52.29  
 CONSTRUCT BUS PULL-OUT, SEE  
 DETAILS ON SHEET 33

50+38.31' RT. RE-DIRECT  
 EXISTING 18" CUP INTO  
 NEW COLLECTION BASIN



46 ~ 47 ~ 48 ~ 49 ~ 50 ~ 51 ~ 52 ~

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

BY:	DATE:	DESCRIPTION OF CHANGE:
<b>RECORD OF REVISIONS</b>		

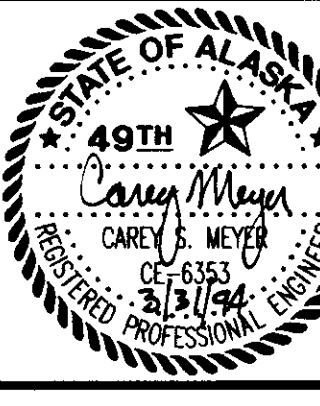
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

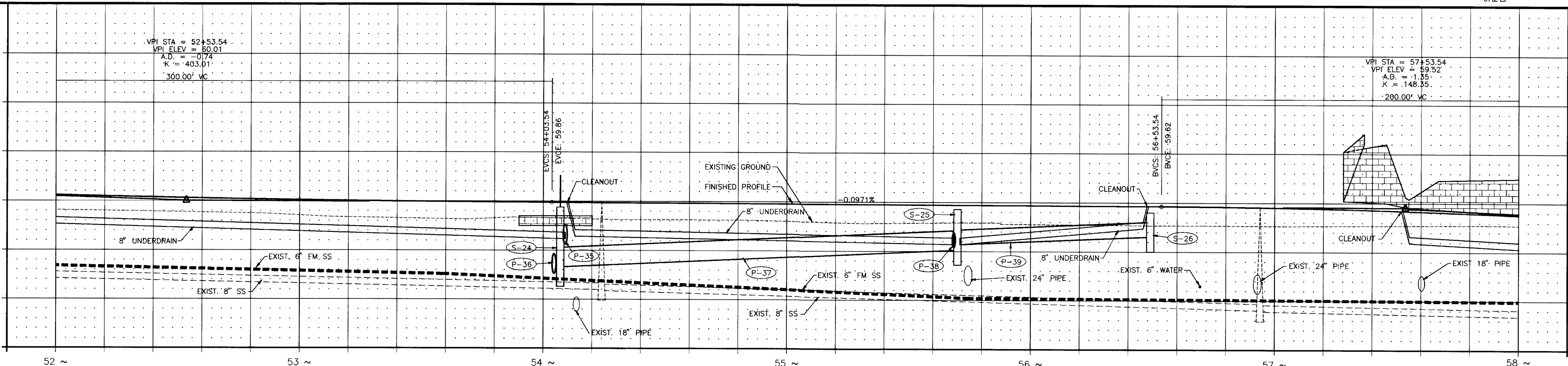
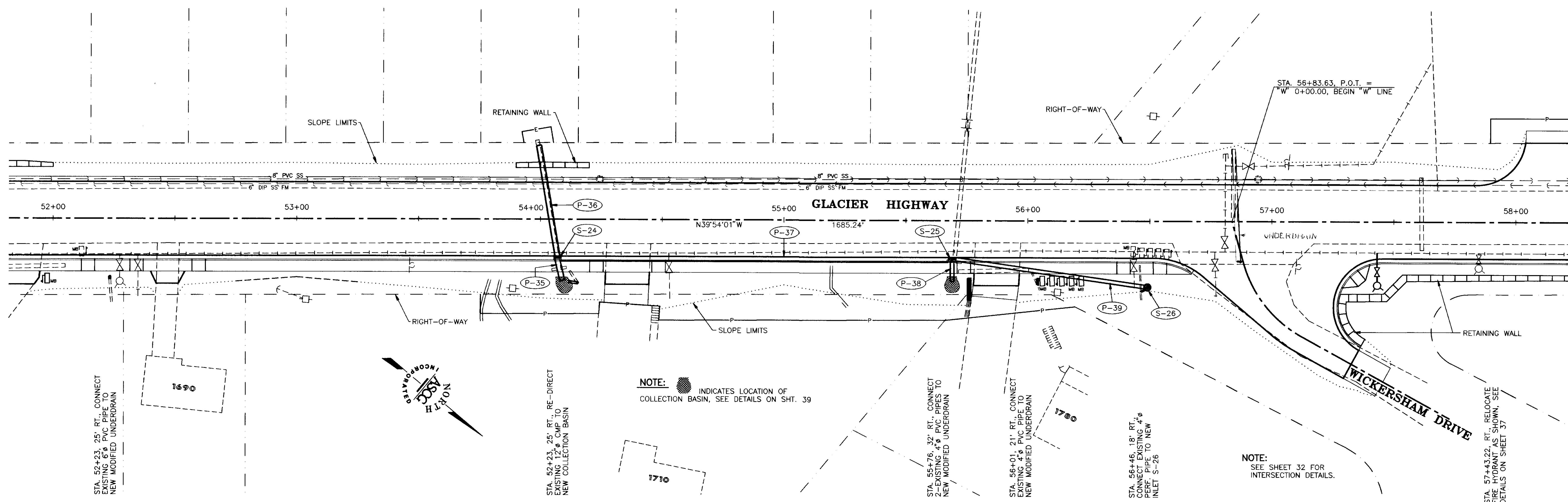
JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 46+00.00 TO STA. 52+00.00



DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 23 OF 55





BY:	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

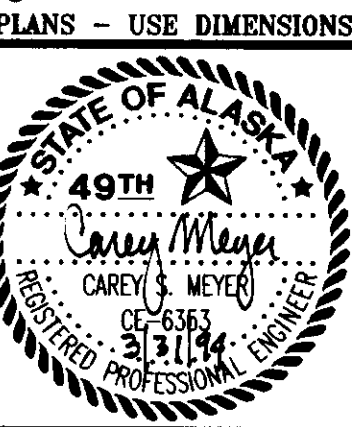
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

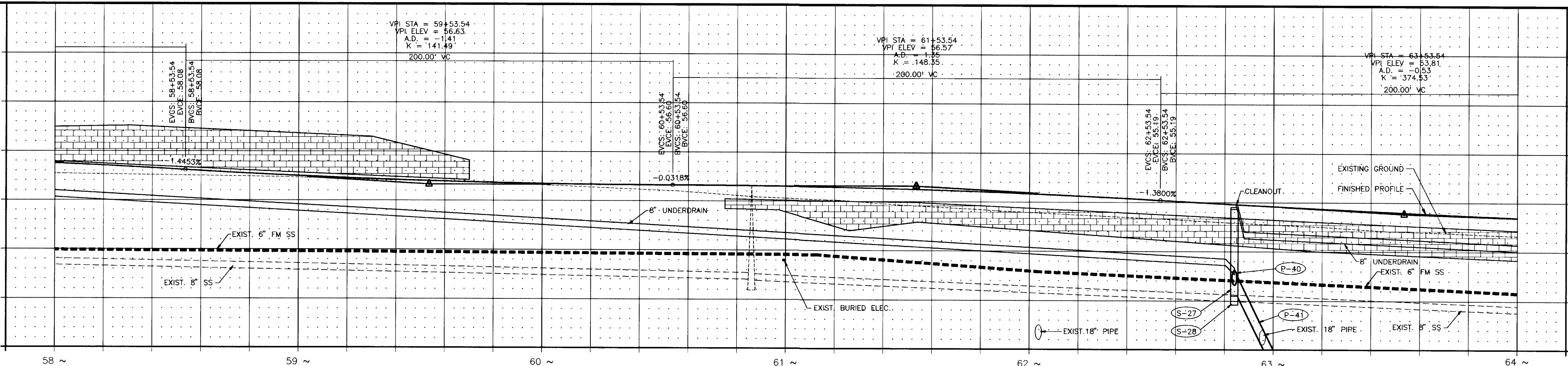
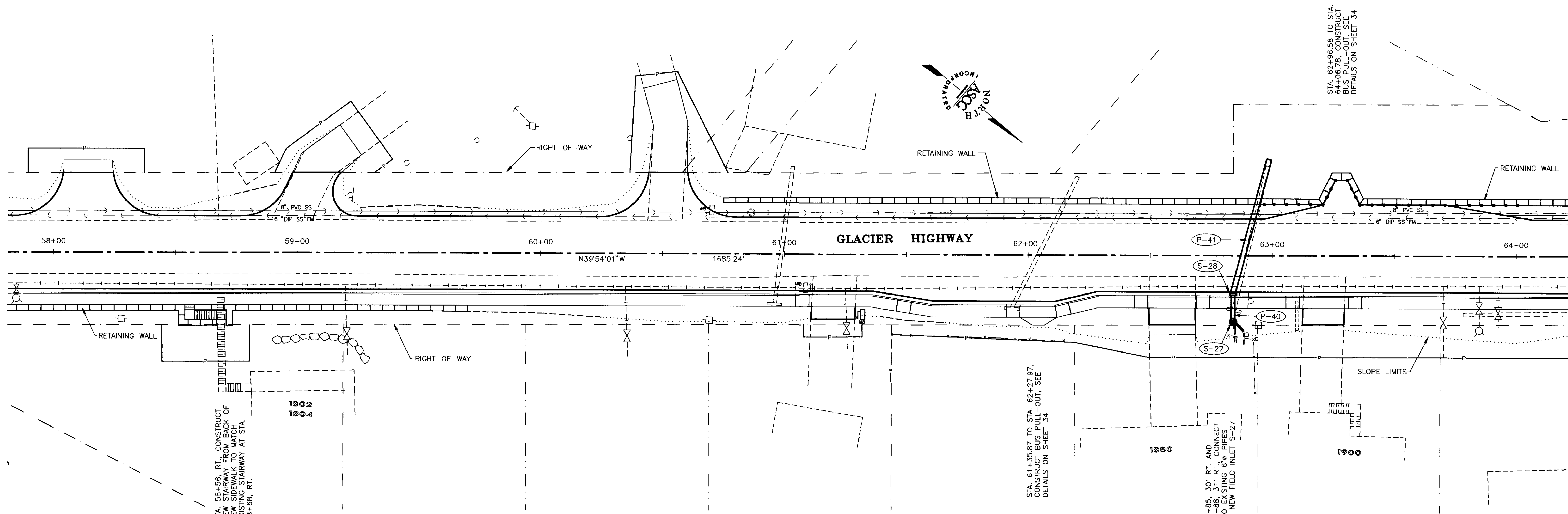
JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
PLAN AND PROFILE  
STA. 52+00.00 TO STA. 58+00.00



DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 24 OF 55





NO.	BY:	DATE:	DESCRIPTION OF CHANGE:

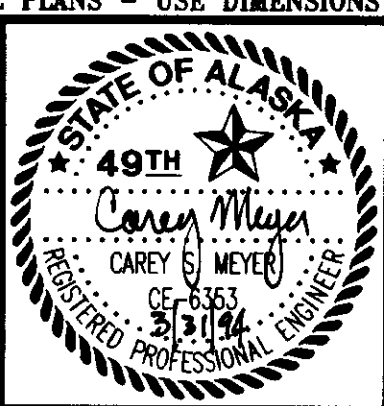
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 58+00.00 TO STA. 64+00.00



DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

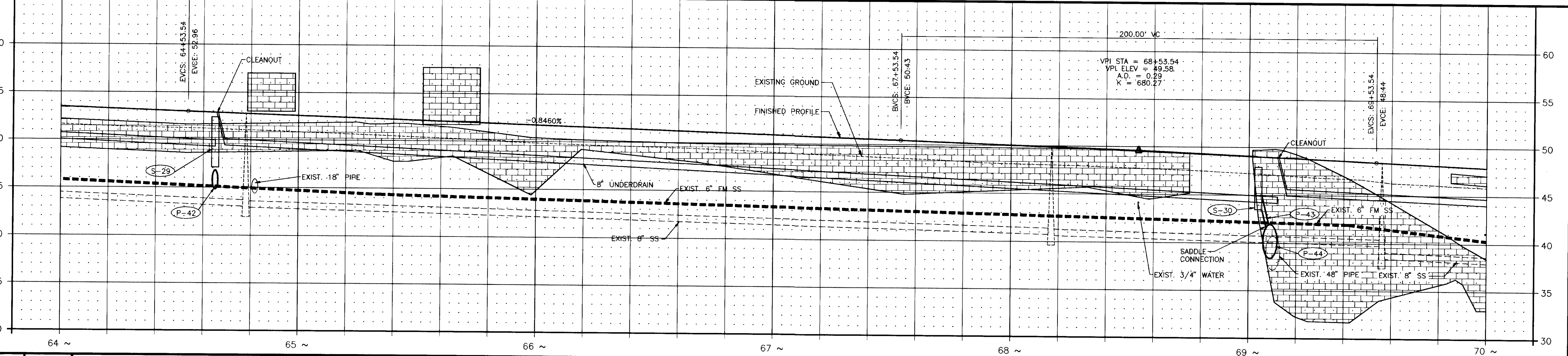
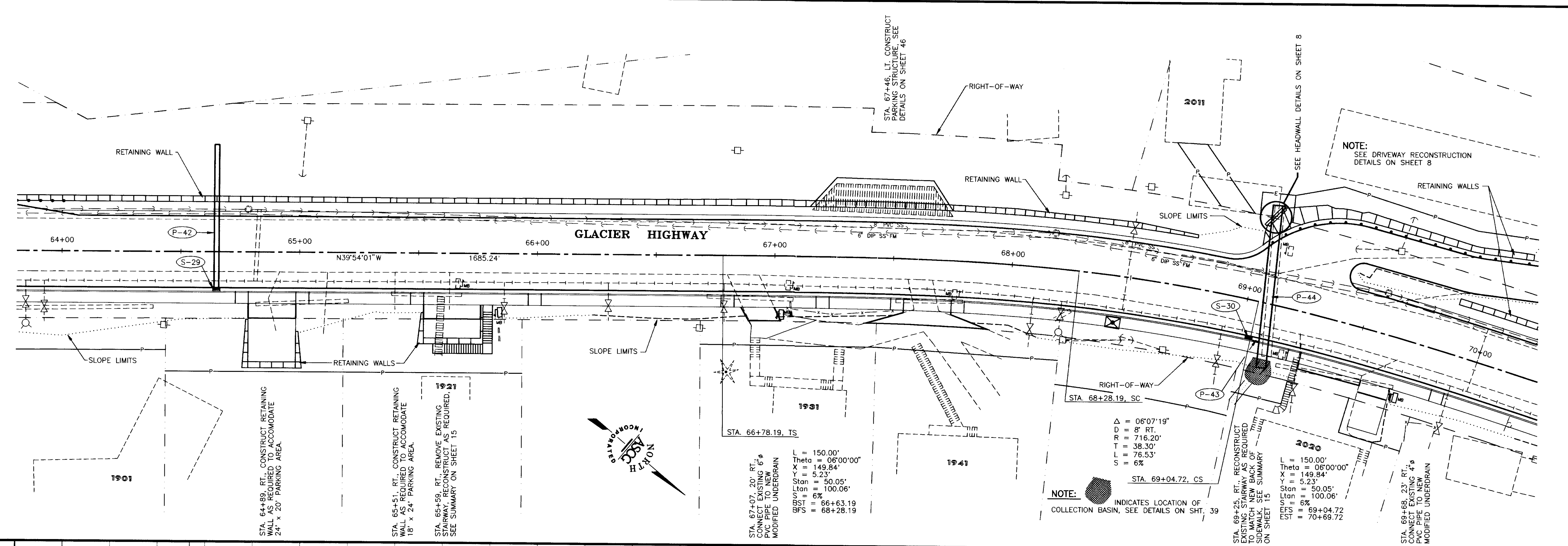
PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 25 OF 55



STA. 62+96.58 TO STA. 64+06.78, CONSTRUCT BUS PULL-OUT. SEE DETAILS ON SHEET 34

STA. 61+35.87 TO STA. 62+27.97, CONSTRUCT BUS PULL-OUT. SEE DETAILS ON SHEET 34

62+85, 30' RT. AND 62+88, 31' RT. CONNECT TWO EXISTING 6\"/>



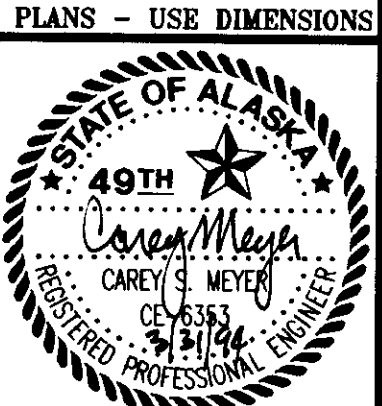
NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

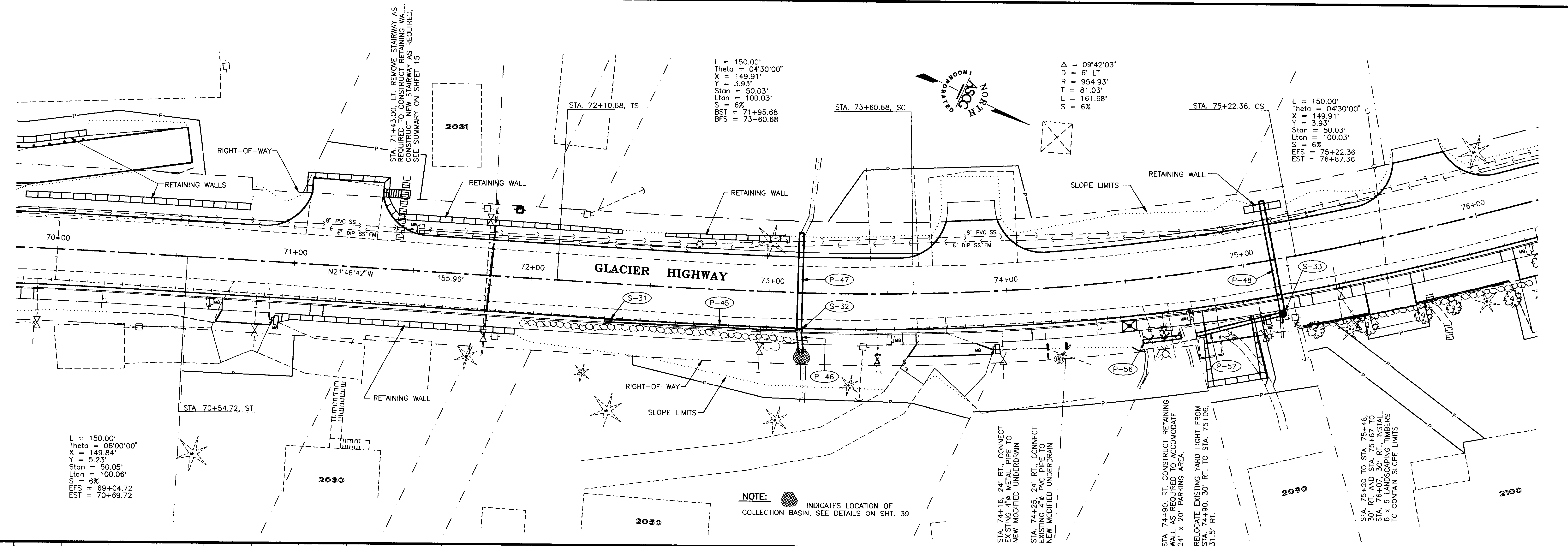
JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 64+00.00 TO STA. 70+00.00



DESIGNED BY:	D.L.M.	PROJECT No.:	69062
DRAWN BY:	J.E.M.	DATE:	MARCH 1994
CHECKED BY:	D.L.M.	SHEET	26 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



L = 150.00'  
 Theta = 06°00'00"  
 X = 149.84'  
 Y = 5.23'  
 Ltan = 50.05'  
 S = 6%  
 EFS = 69+04.72  
 EST = 70+69.72

L = 150.00'  
 Theta = 04°30'00"  
 X = 149.91'  
 Y = 3.93'  
 Ltan = 50.03'  
 S = 6%  
 BST = 71+95.68  
 BFS = 73+60.68

Δ = 09°42'03"  
 D = 6' LT.  
 R = 954.93'  
 T = 81.03'  
 L = 161.68'  
 S = 6%

L = 150.00'  
 Theta = 04°30'00"  
 X = 149.91'  
 Y = 3.93'  
 Ltan = 50.03'  
 S = 6%  
 EFS = 75+22.36  
 EST = 76+87.36

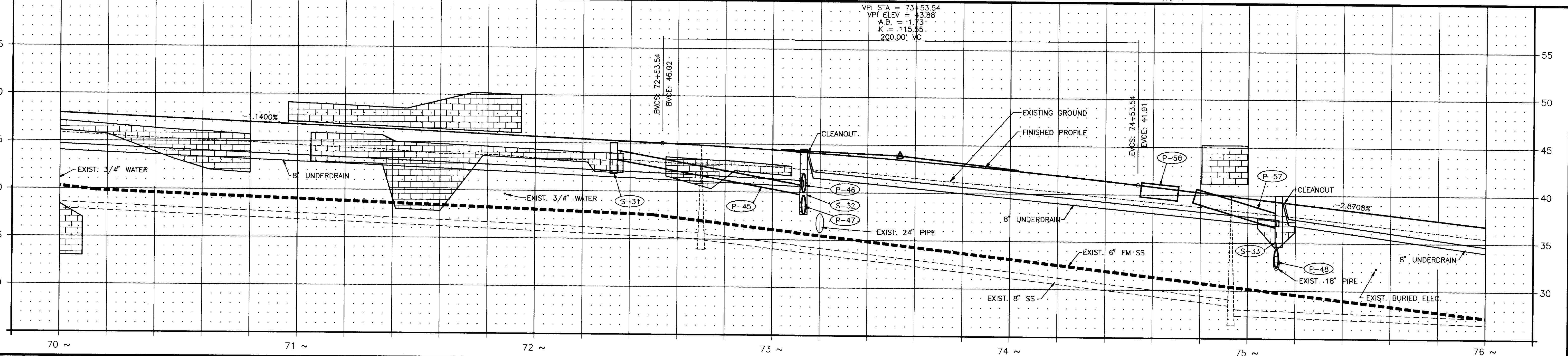
NOTE: INDICATES LOCATION OF COLLECTION BASIN, SEE DETAILS ON SHT. 39

STA. 74+16, 24' RT. CONNECT EXISTING 4\"/>

STA. 74+25, 24' RT. CONNECT EXISTING 4\"/>

STA. 74+90, RT. CONSTRUCT RETAINING WALL AS REQUIRED TO ACCOMMODATE 24' x 20' PARKING AREA.  
 RELOCATE EXISTING YARD LIGHT FROM STA. 74+90, 30' RT. TO STA. 75+06, 31.5' RT.

STA. 75+20 TO STA. 75+48, 30' RT. AND STA. 75+67 TO STA. 76+07, 30' RT. INSTALL 6 x 6 LANDSCAPING TIMBERS TO CONTAIN SLOPE LIMITS



NO.	DATE	DESCRIPTION OF CHANGE

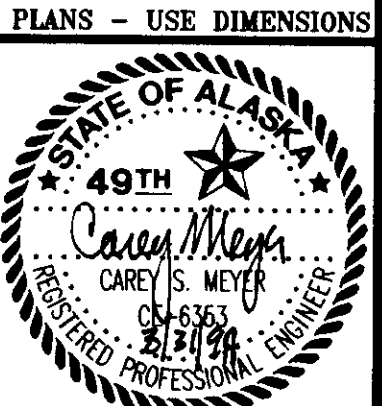
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 70+00.00 TO STA. 76+00.00



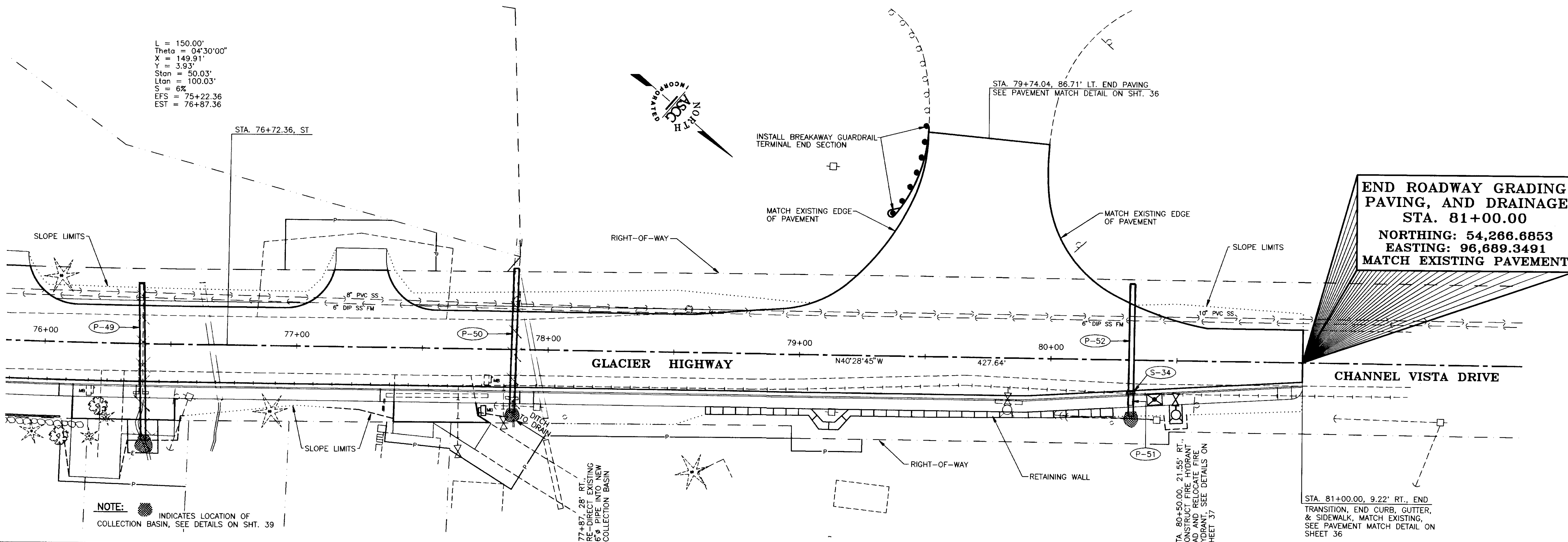
DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 27 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

L = 150.00'  
 Theta = 04°30'00"  
 X = 149.91'  
 Y = 3.93'  
 Stan = 50.03'  
 Ltan = 100.03'  
 S = 6%  
 EFS = 75+22.36  
 EST = 76+87.36

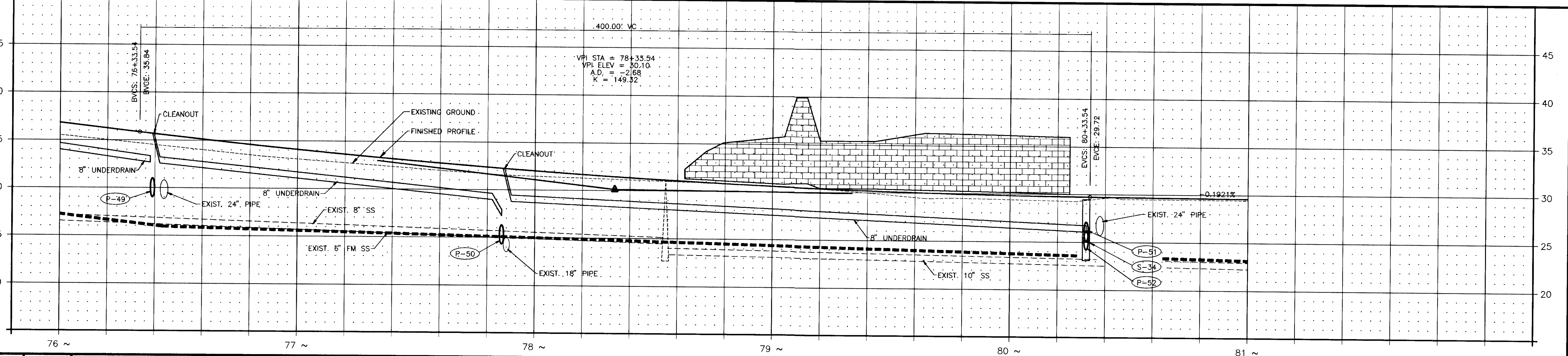


**END ROADWAY GRADING,  
 PAVING, AND DRAINAGE  
 STA. 81+00.00  
 NORTHING: 54,266.6853  
 EASTING: 96,689.3491  
 MATCH EXISTING PAVEMENT**

**NOTE:** ● INDICATES LOCATION OF COLLECTION BASIN, SEE DETAILS ON SHT. 39

STA. 80+50.00, 21.55' RT., CONSTRUCT FIRE HYDRANT PAD AND RELOCATE FIRE HYDRANT, SEE DETAILS ON SHEET 37

STA. 81+00.00, 9.22' RT., END TRANSITION, END CURB, GUTTER, & SIDEWALK, MATCH EXISTING, SEE PAVEMENT MATCH DETAIL ON SHEET 36



NO.	DATE	DESCRIPTION OF CHANGE

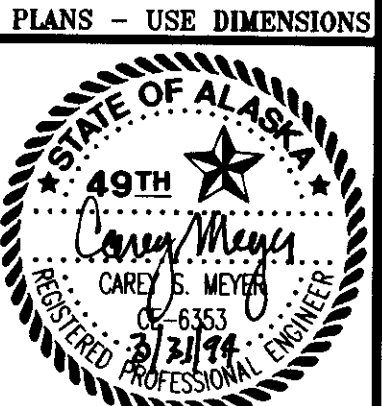
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 PLAN AND PROFILE  
 STA. 76+00.00 TO STA. 81+00.00

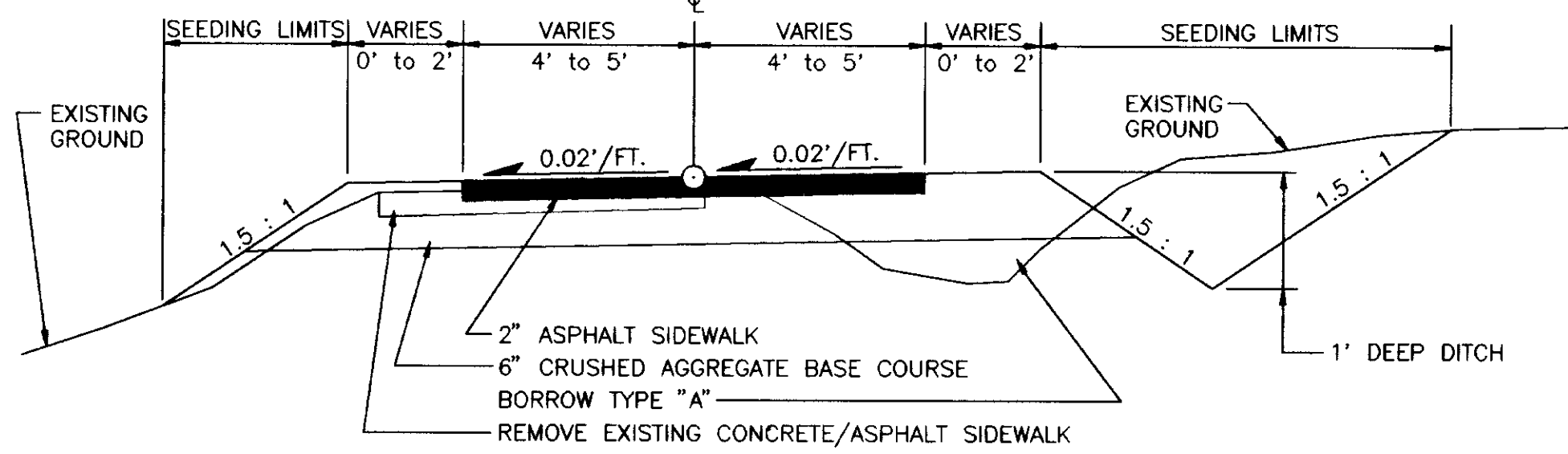


DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 28 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

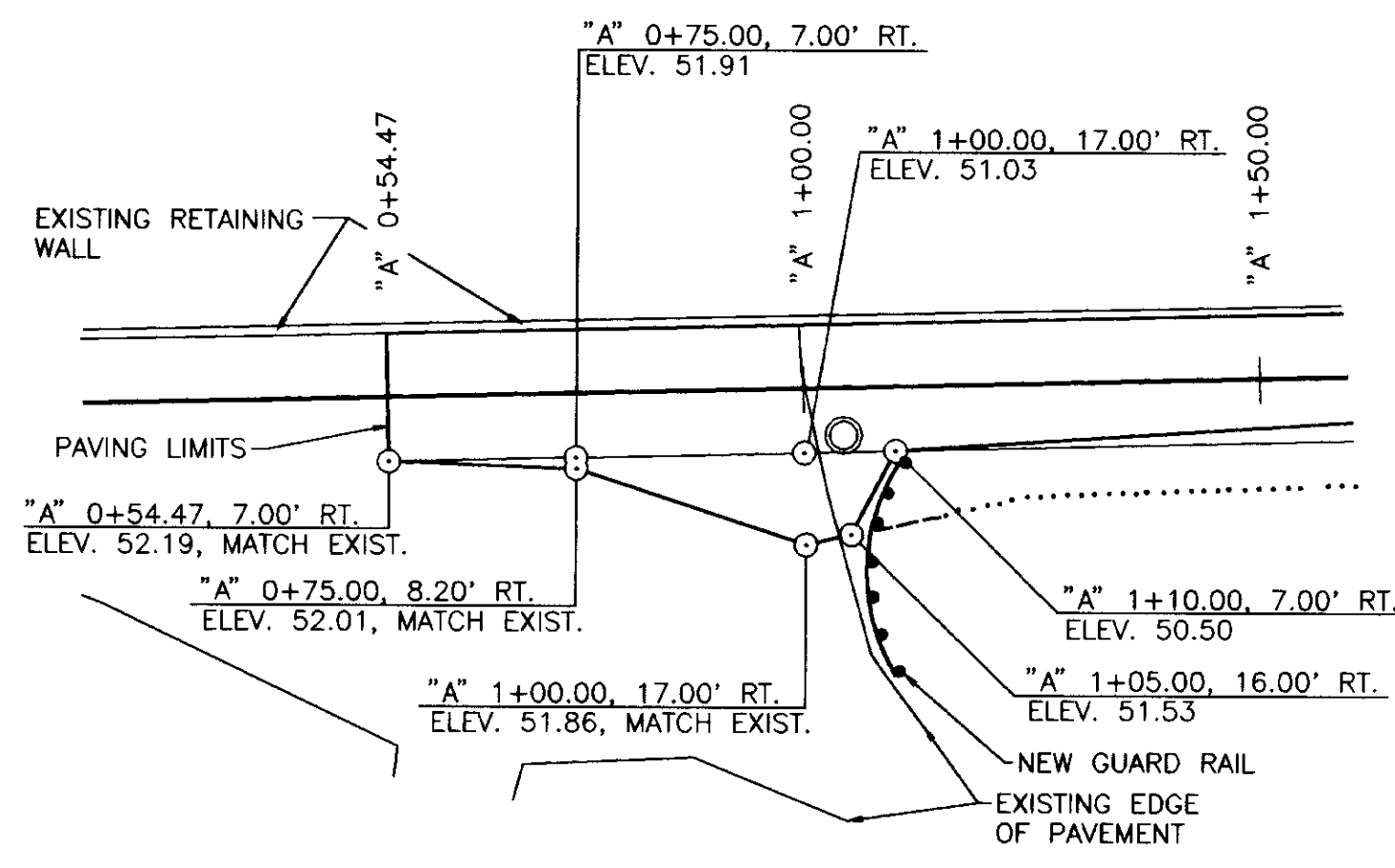


**TYPICAL SECTION FOR BIKEPATHS "A" & "B"**

ON BIKEPATH "A" FROM STATION 0+54.47 TO STATION 3+00 CROSS SLOPE SHALL BE 2% FROM LEFT TO RIGHT  
TRANSITION TO 2% RIGHT TO LEFT FROM STATION 3+00 TO STATION 3+50

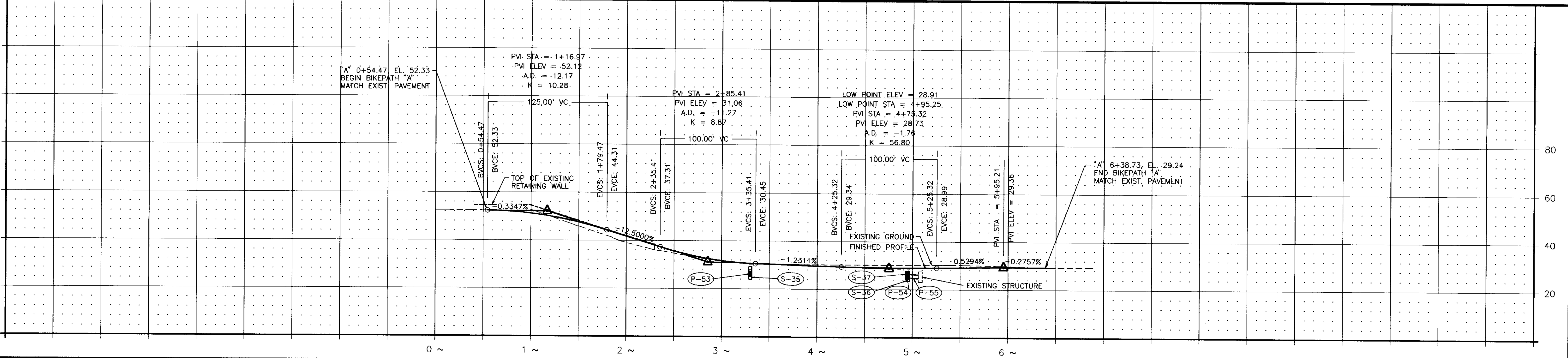
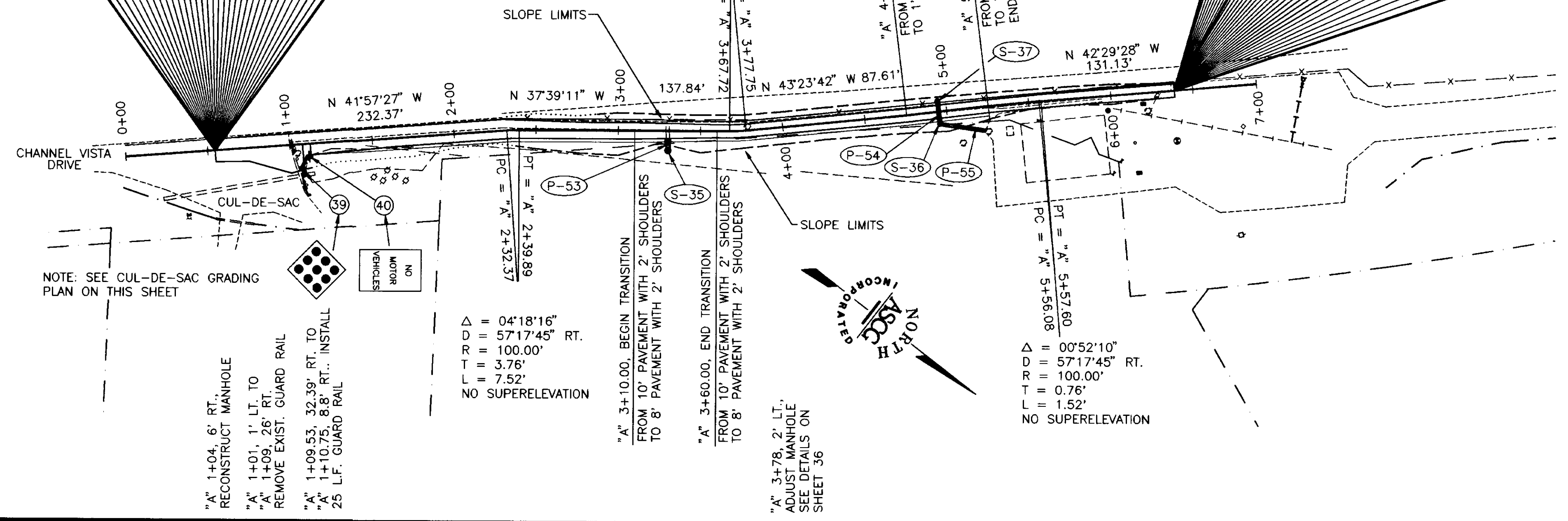
**BEGIN BIKEPATH "A"**  
**"A" 0+54.47, EL. 52.33**  
 10' PAVEMENT WITH 2' SHOULDERS EACH SIDE  
 ("A" 1+60.00, SEE CUL-DE-SAC GRADING PLAN)  
 NORTHING: 56,243.7613  
 EASTING: 95,111.4541  
 MATCH EXISTING PAVEMENT

**END BIKEPATH "A"**  
**"A" 6+38.73, EL. 29.24**  
 8' PAVEMENT WITH 1' LT. SHLD. AND NO RT. SHLD.  
 NORTHING: 56,680.5022  
 EASTING: 94,724.0758  
 MATCH EXISTING PAVEMENT



**CUL-DE-SAC GRADING PLAN**

NOTE:  
 TAPER PAVEMENT FROM 7' RT. AT "A" 1+10.00 TO 5' RT. AT "A" 1+60.00,  
 TAPER UNPAVED SHOULDER FROM 0' AT "A" 1+10 TO 2' AT "A" 1+60.00



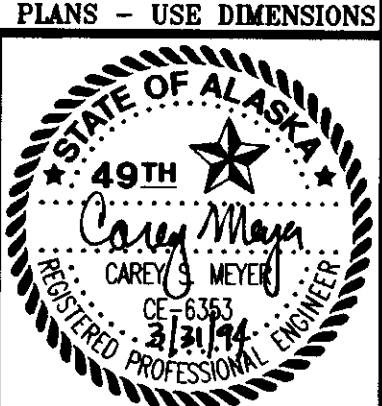
NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 BIKEPATH PLAN AND PROFILE  
 "A" 0+54.47 TO "A" 6+38.73

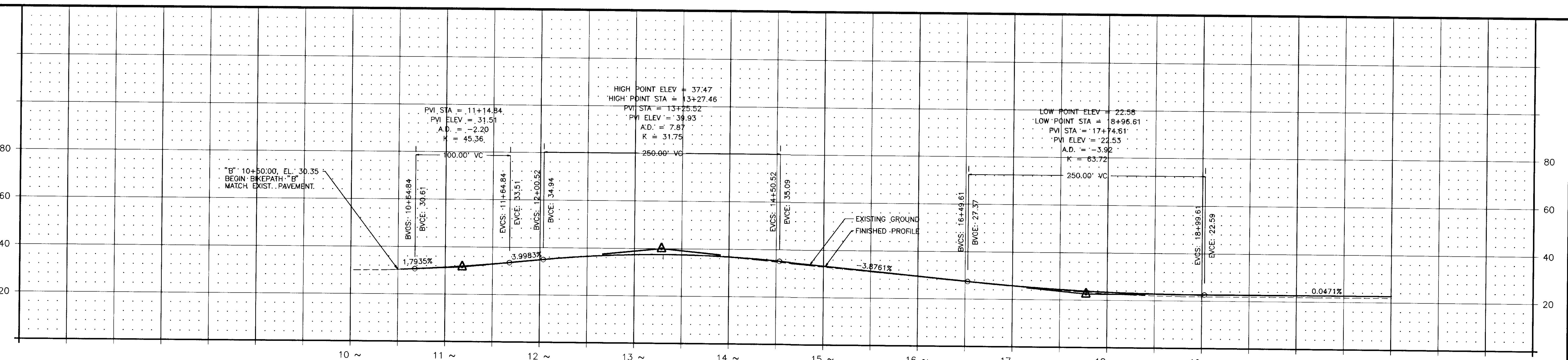
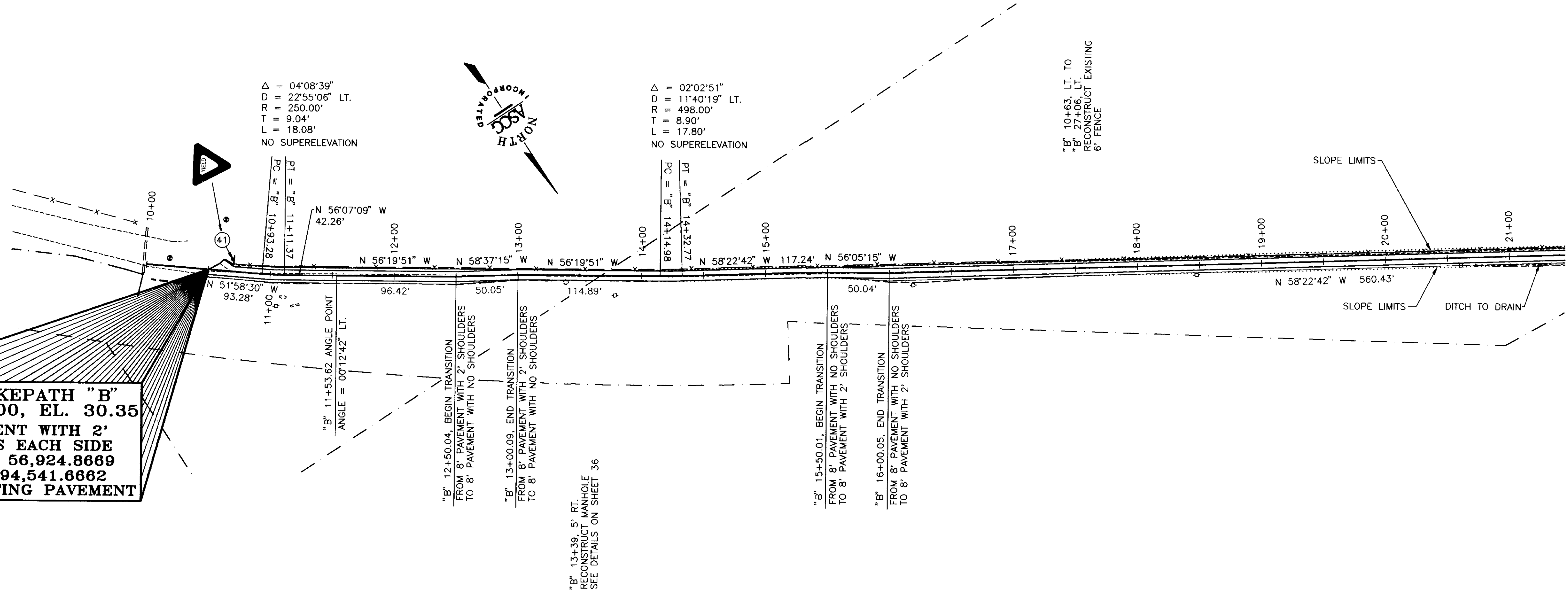


DESIGNED BY:	J.E.M.	PROJECT No.:	69062
DRAWN BY:	J.E.M.	DATE:	MARCH 1994
CHECKED BY:	D.L.M.	SHEET	29 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

**BEGIN BIKEPATH "B"**  
**"B" 10+50.00, EL. 30.35**  
**8' PAVEMENT WITH 2'**  
**SHOULDERS EACH SIDE**  
**NORTHING: 56,924.8669**  
**EASTING: 94,541.6662**  
**MATCH EXISTING PAVEMENT**



NO.	DATE	DESCRIPTION OF CHANGE

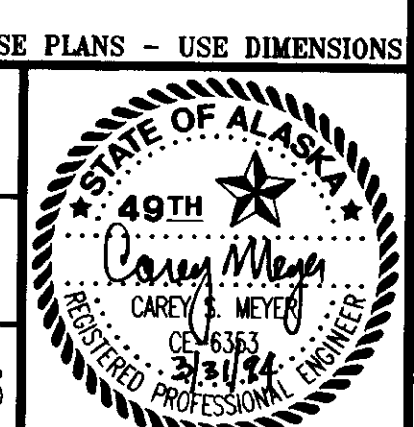
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 BIKEPATH PLAN AND PROFILE  
 "B" 10+00.00 TO "B" 21+00.00

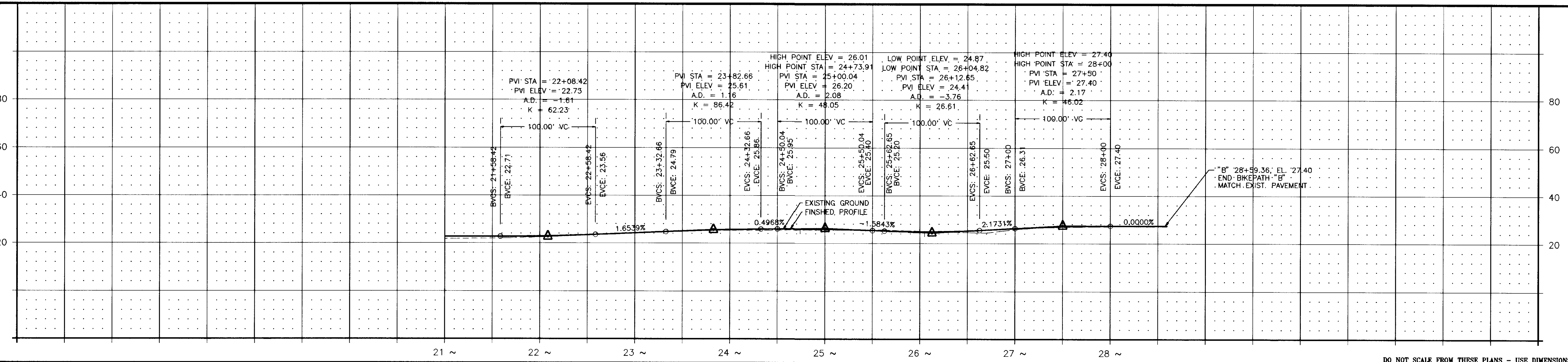
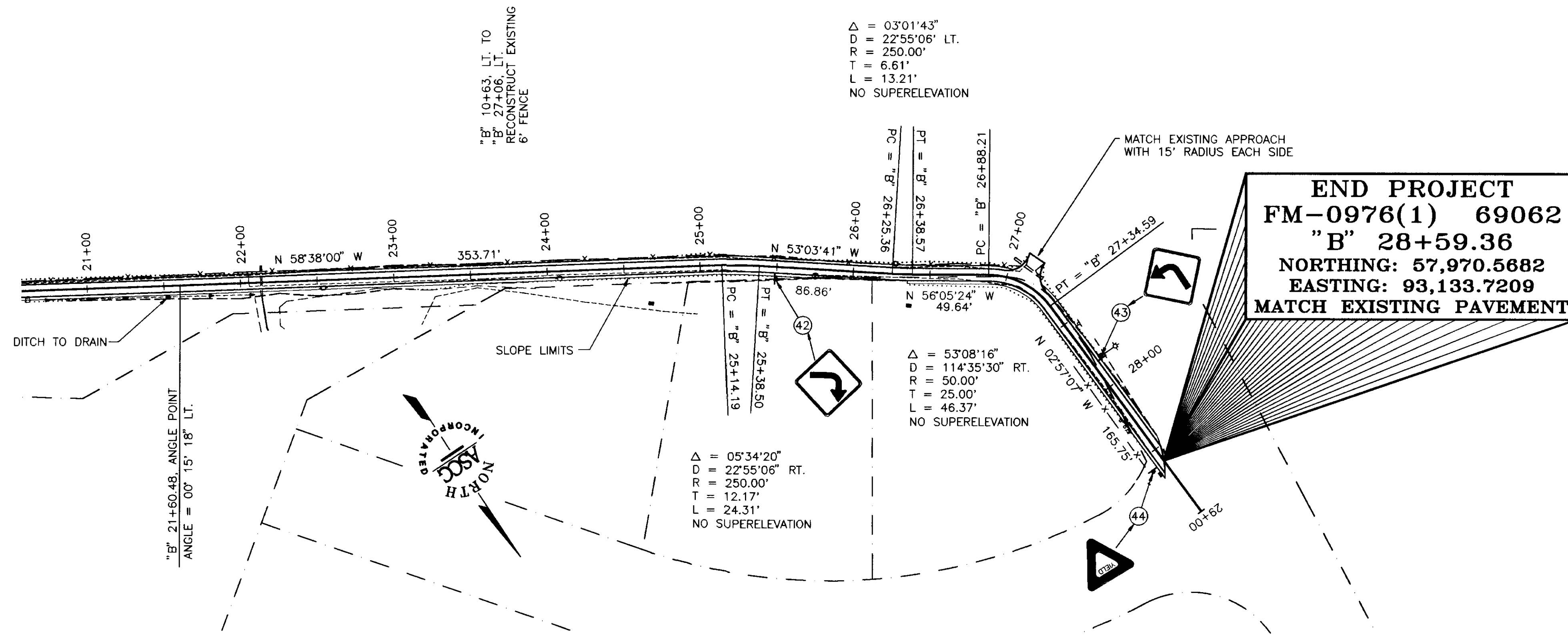


DESIGNED BY: J.E.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 30 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



BY:	DATE:	DESCRIPTION OF CHANGE:

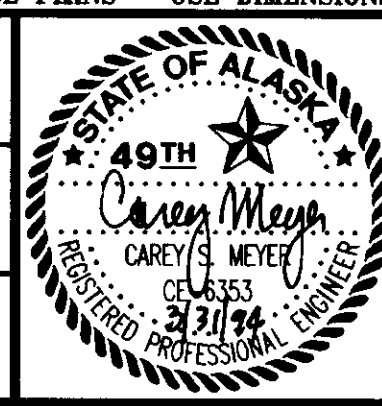
**RECORD OF REVISIONS**

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

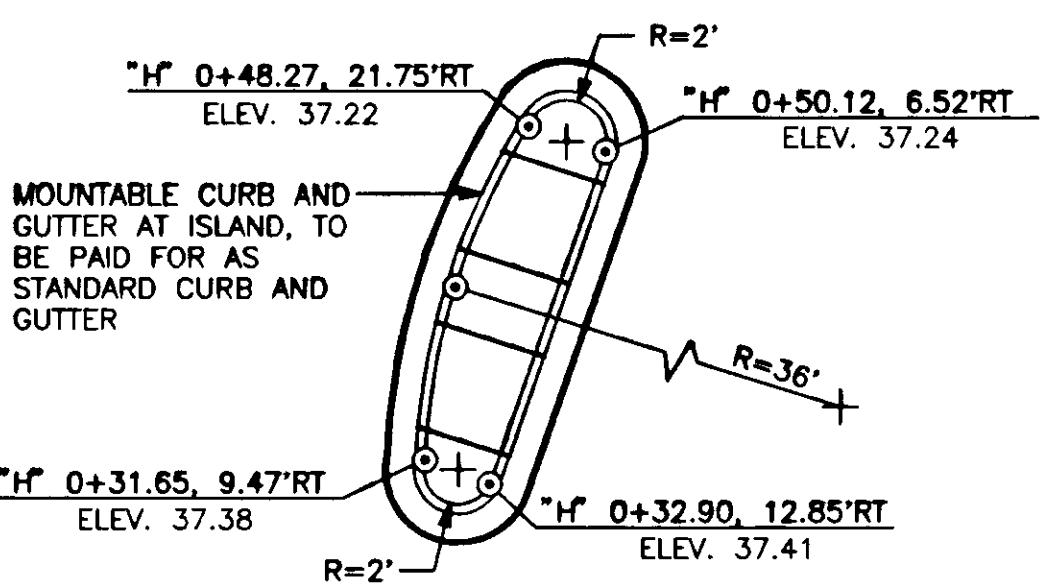
JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 BIKEPATH PLAN AND PROFILE  
 "B" 21+00.00 TO "B" 28+59.36



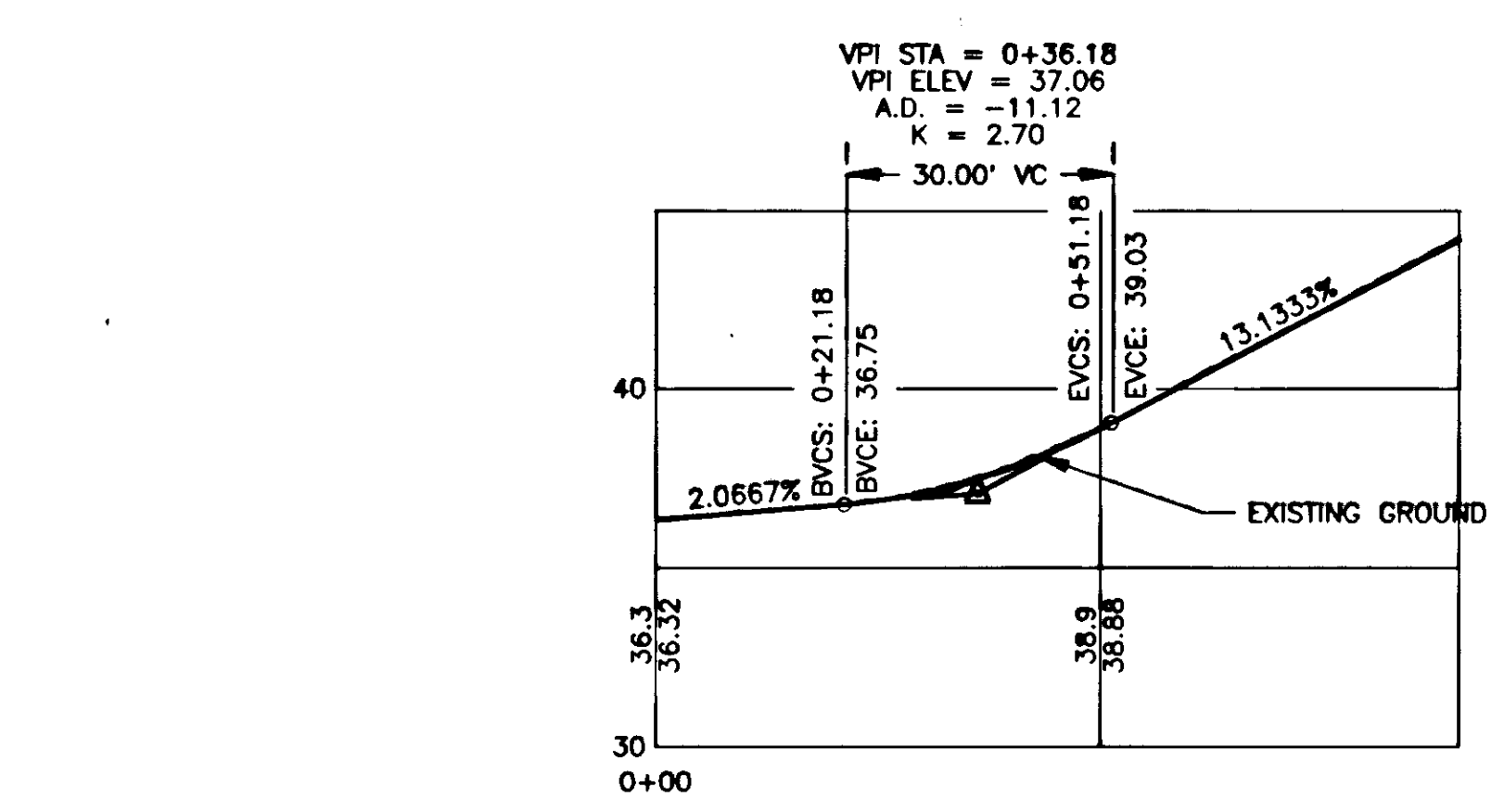
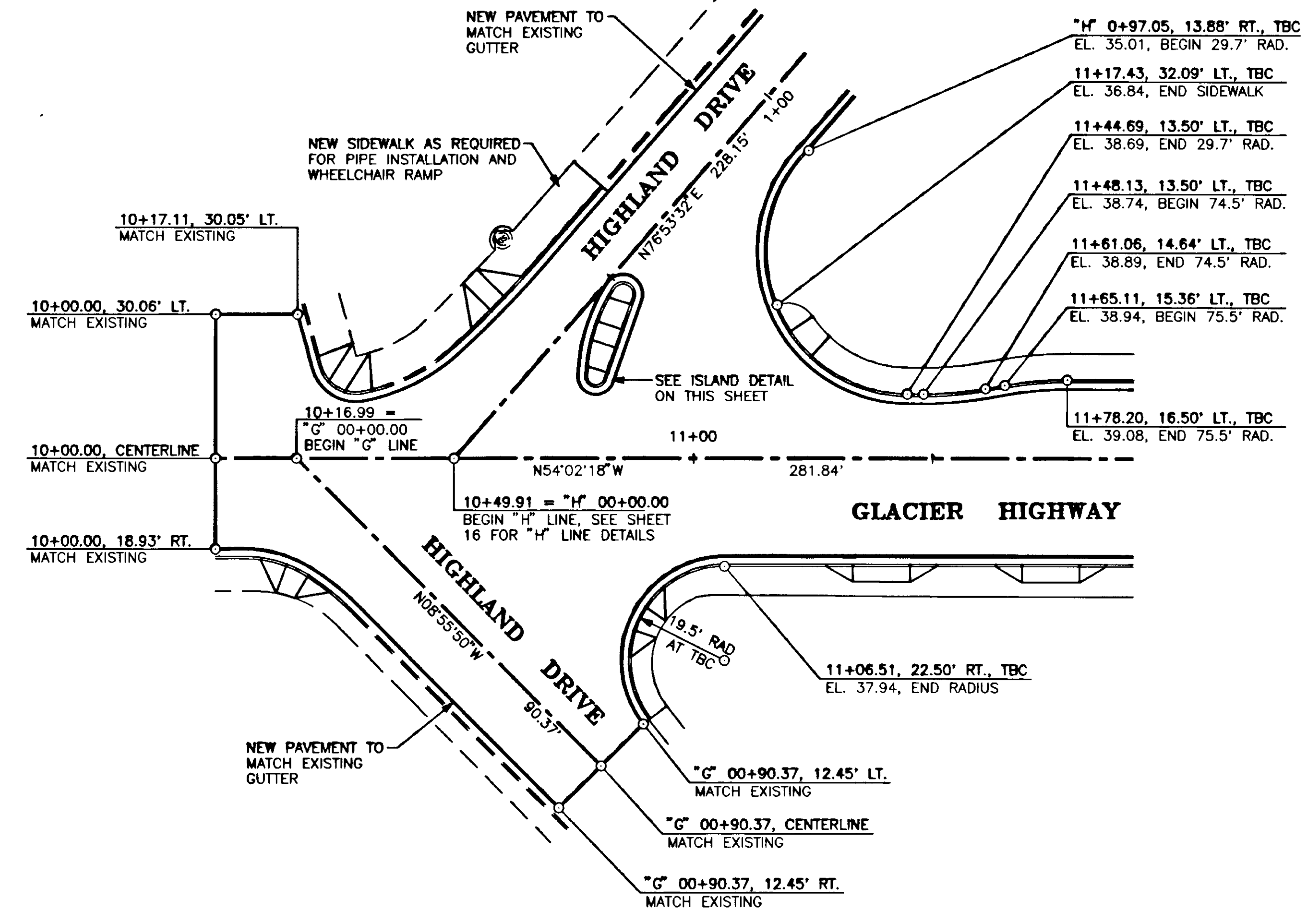
DESIGNED BY:	J.E.M.	PROJECT No.:	69062
DRAWN BY:	J.E.M.	DATE:	MARCH 1994
CHECKED BY:	D.L.M.	SHEET	31 OF 55



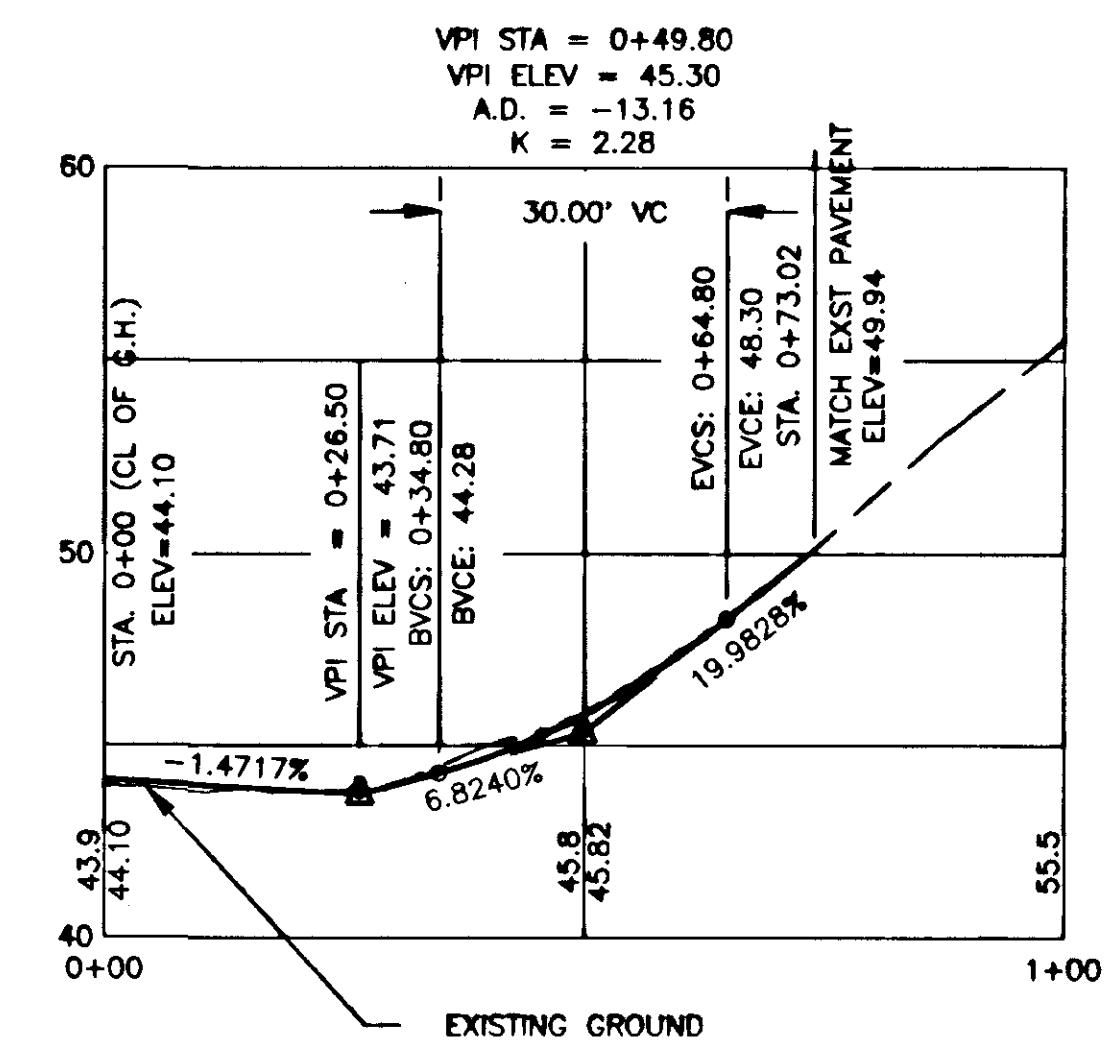
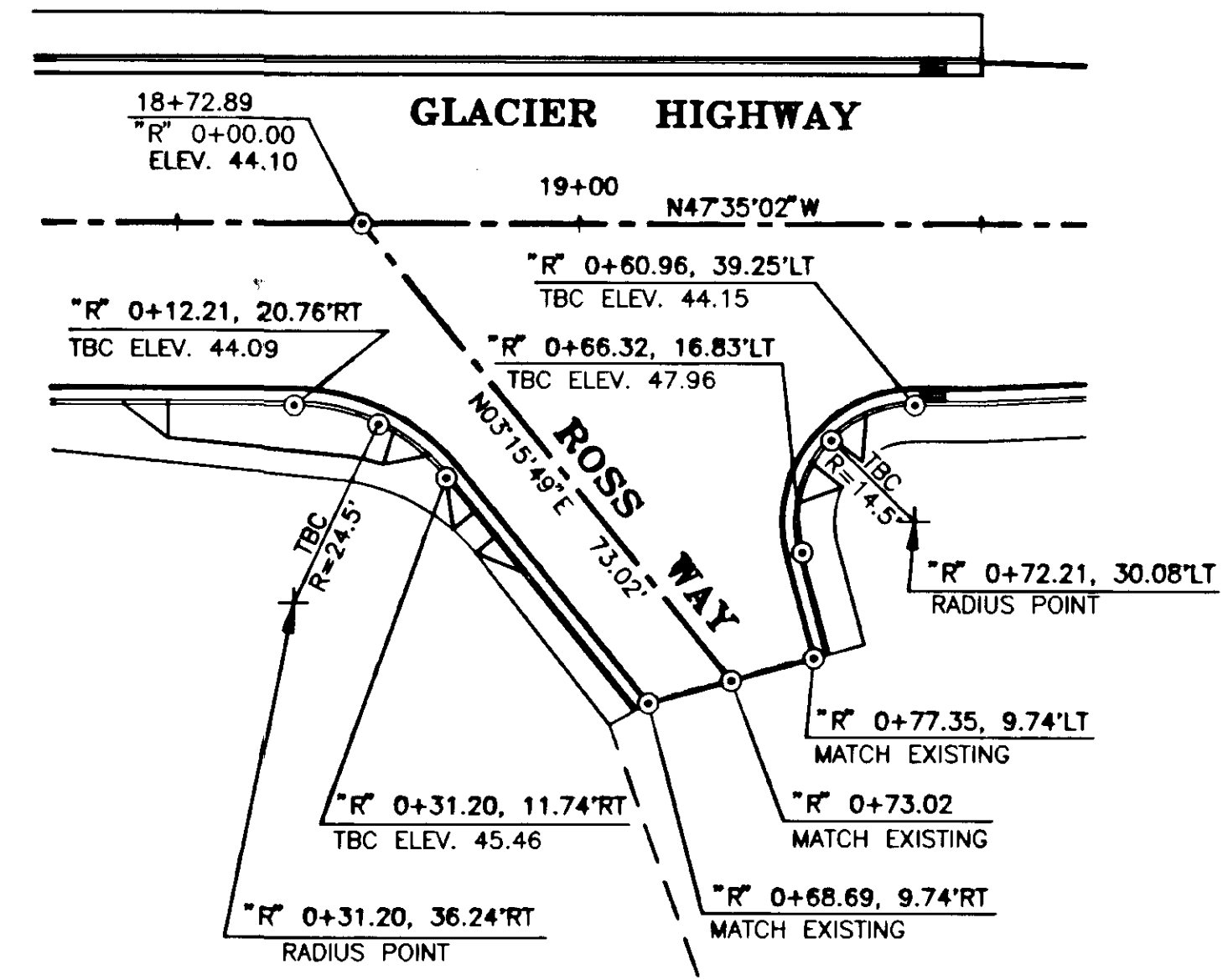
DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



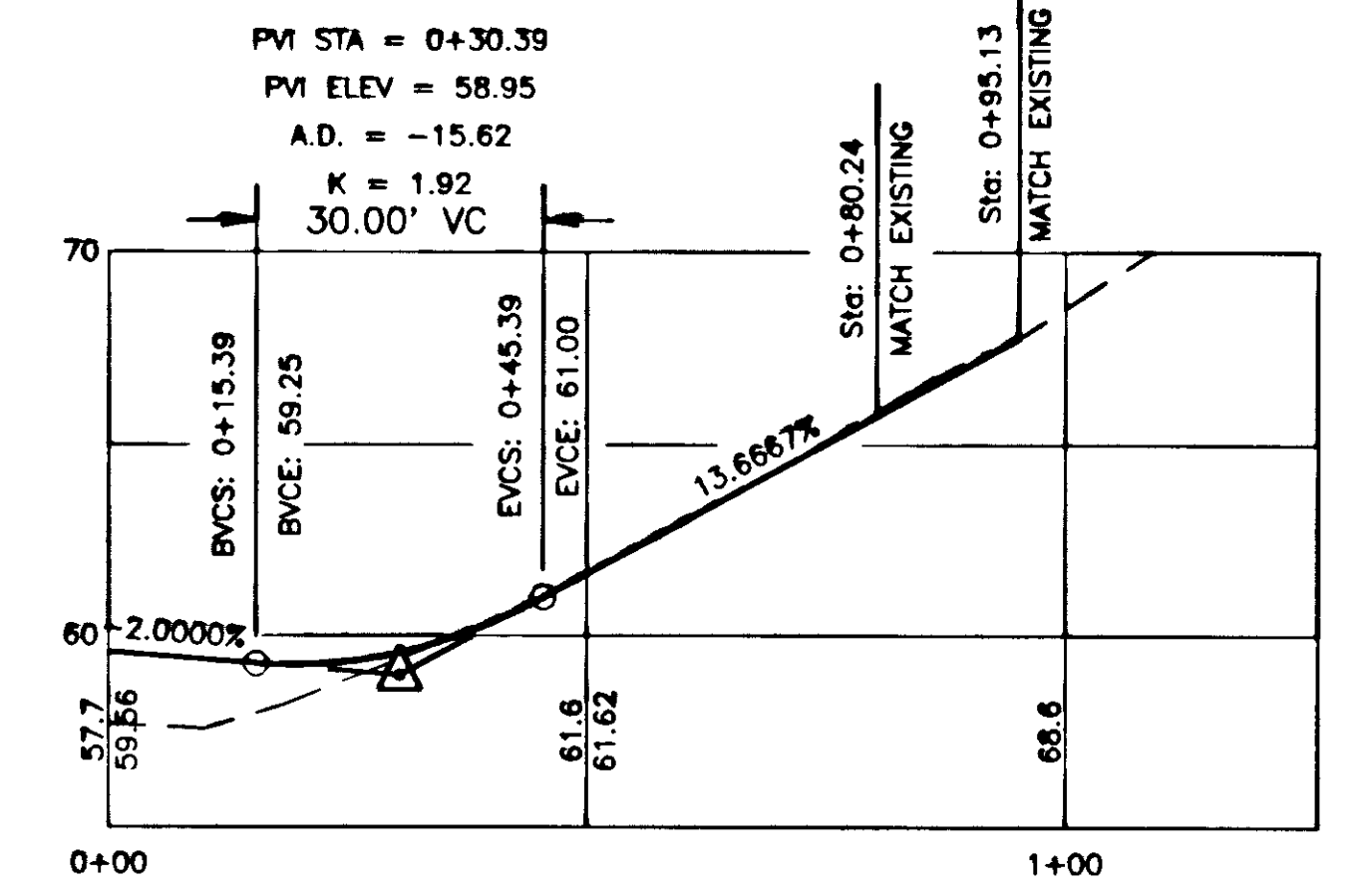
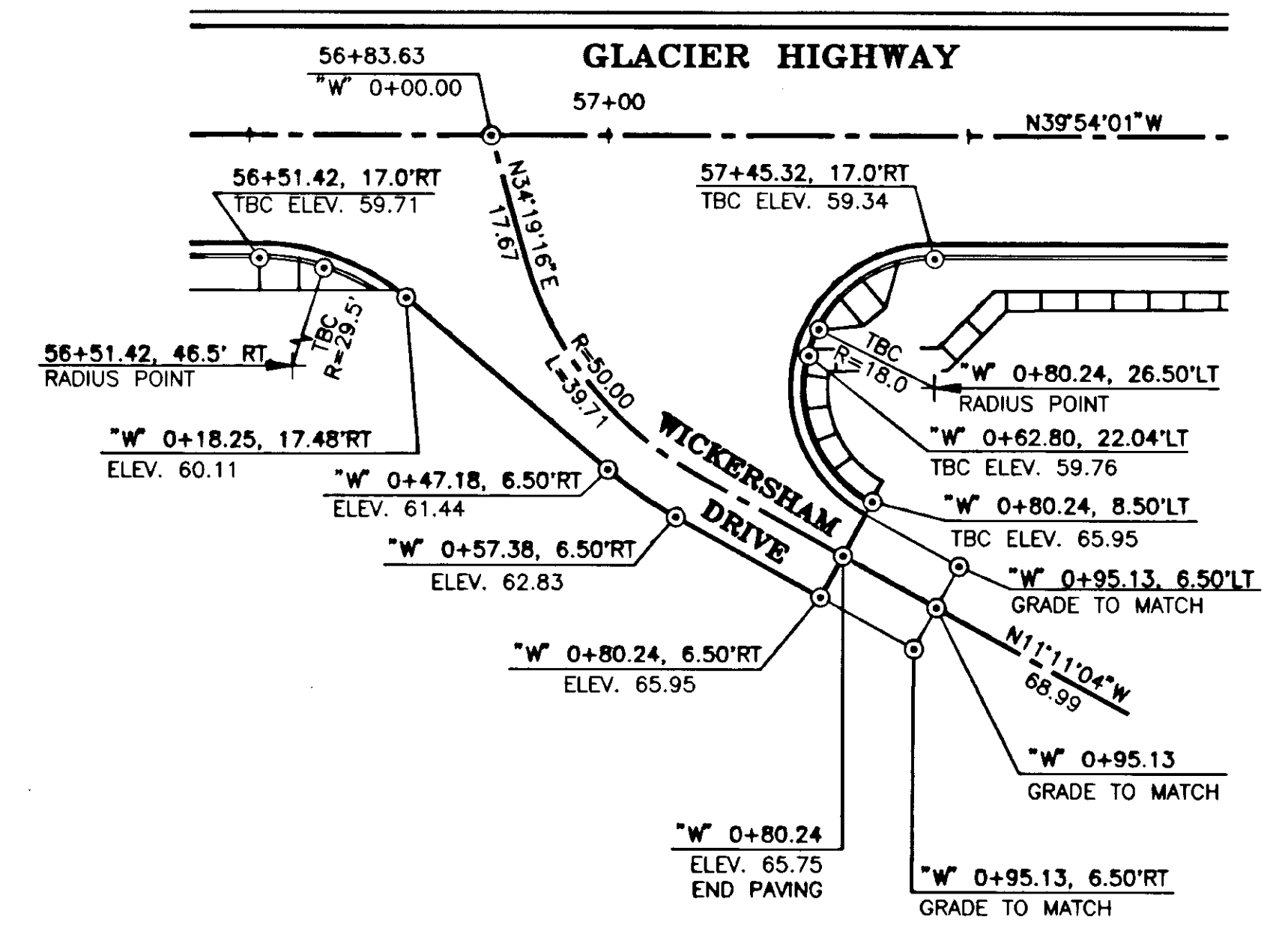
**ISLAND DETAIL**



**"G" LINE PROFILE**



**"R" LINE PROFILE**



**"W" LINE PROFILE**

BY:	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

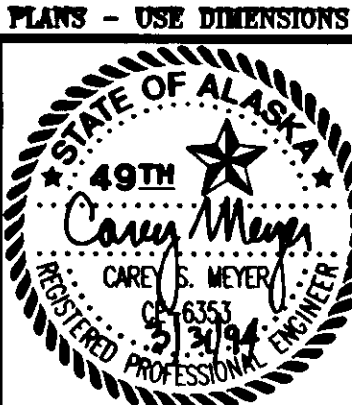
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 INTERSECTION DETAILS



DESIGNED BY: C.M.B.  
 DRAWN BY: C.M.B.  
 CHECKED BY: D.L.M.

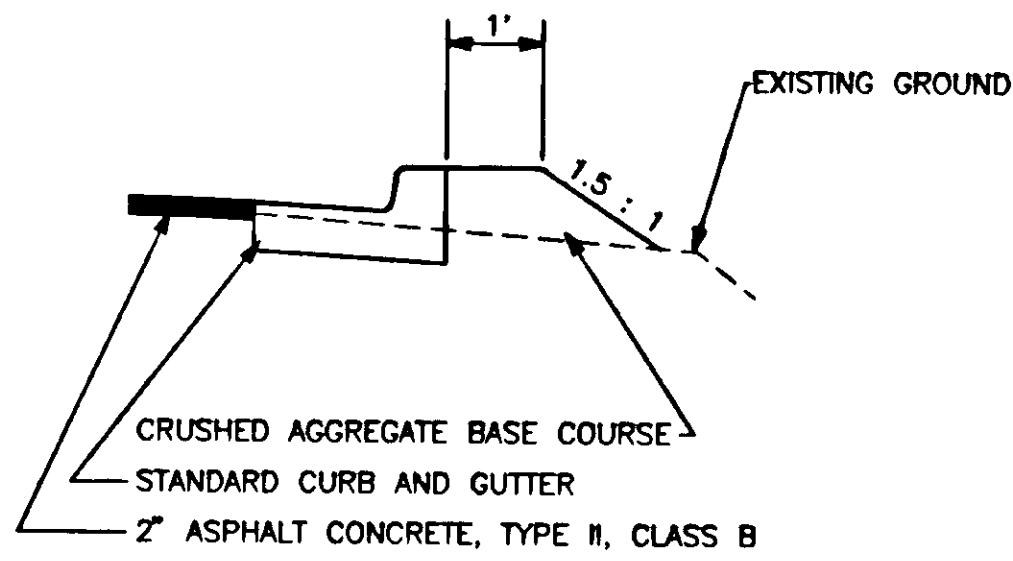
PROJECT NO:  
 69062  
 DATE:  
 MARCH 1994  
 SHEET 32 OF 55



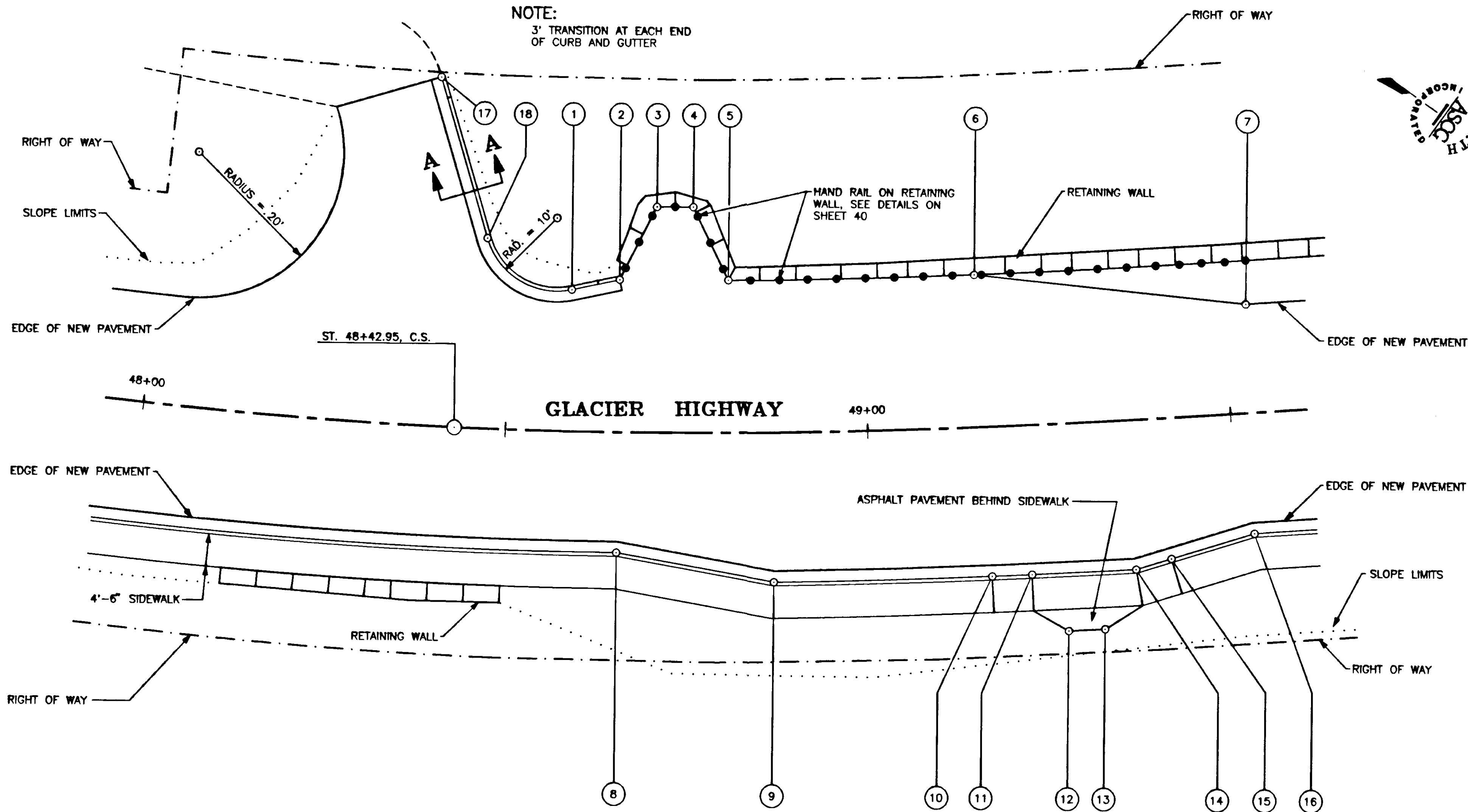
DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

**AWARE SHELTER**

**NOTE:**  
3' TRANSITION AT EACH END OF CURB AND GUTTER



**SECTION A-A**



POINT No.	STATION	OFFSET		POINT DESCRIPTION
		LEFT	RIGHT	
(1)	48+58.74	19.52'		TOP FACE OF CURB, END 10' RADIUS
(2)	48+65.51	21.00'		ANGLE POINT, TOP FACE OF CURB, END CURB AND GUTTER, BEGIN HAND RAIL
(3)	48+70.72	31.00'		ANGLE POINT
(4)	48+75.92	31.00'		ANGLE POINT
(5)	48+80.91	21.00'		ANGLE POINT
(6)	49+15.50	21.00'		ANGLE POINT BEGIN TRANSITION
(7)	49+53.04	15.00'		ANGLE POINT END TRANSITION, END HAND RAIL AT 21' LT.
(8)	48+65.54		16.50'	TOP FACE OF CURB, ANGLE POINT, BEGIN TRANSITION
(9)	48+86.77		20.50'	TOP FACE OF CURB, ANGLE POINT, END TRANSITION
(10)	49+16.35		20.50'	TOP FACE OF CURB, BEGIN CURB AND SIDEWALK TRANSITION TO DEPRESSED CURB
(11)	49+21.76		20.50'	TOP FACE OF CURB, END CURB AND SIDEWALK TRANSITION TO DEPRESSED CURB
(12)	49+26.74		28.50'	ANGLE POINT
(13)	49+31.62		28.50'	ANGLE POINT
(14)	49+35.91		20.50'	TOP FACE OF CURB, BEGIN CURB AND SIDEWALK TRANSITION FROM DEPRESSED CURB, BEGIN TRANSITION
(15)	49+40.78		19.29'	TOP FACE OF CURB, END CURB AND SIDEWALK TRANSITION FROM DEPRESSED CURB
(16)	49+52.29		16.50'	ANGLE POINT, END TRANSITION
(17)	48+38.64	48.00'		TOP FACE OF CURB, BEGIN CURB AND GUTTER
(18)	48+46.48	26.18'		TOP FACE OF CURB, BEGIN 10' RADIUS

NO.	DATE:	DESCRIPTION OF CHANGE:

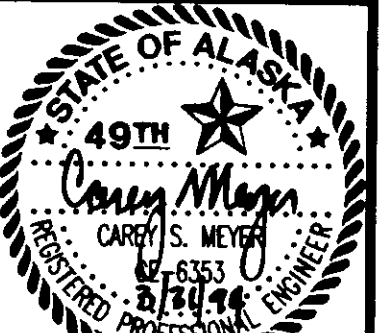
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
BUS PULL-OUT DETAILS

**ASCC**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

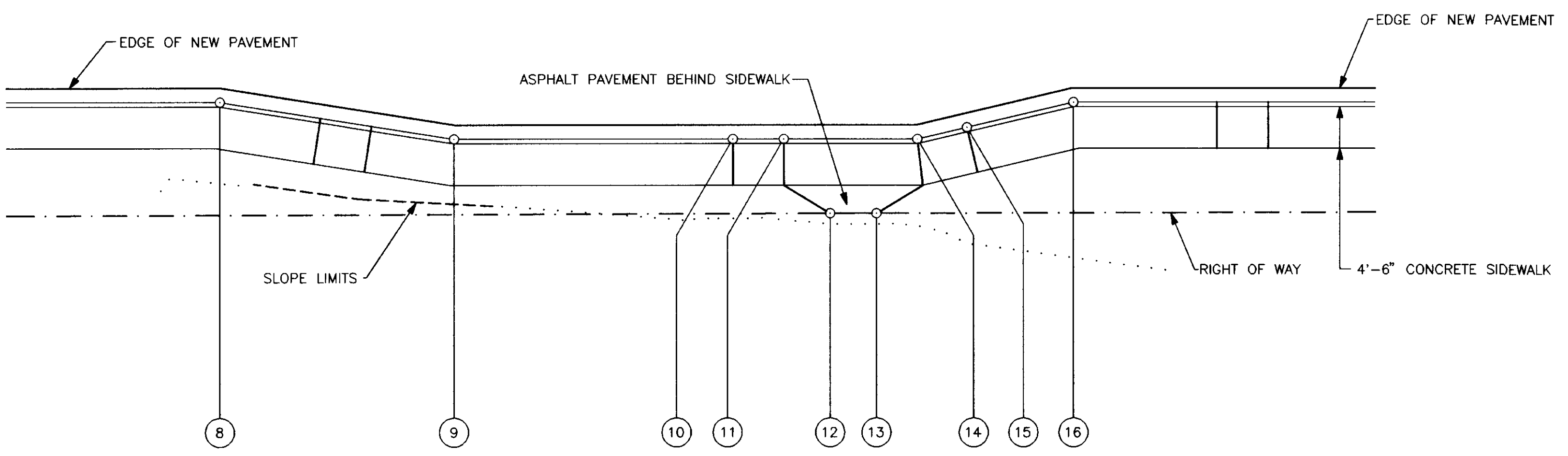
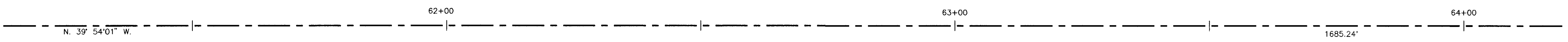
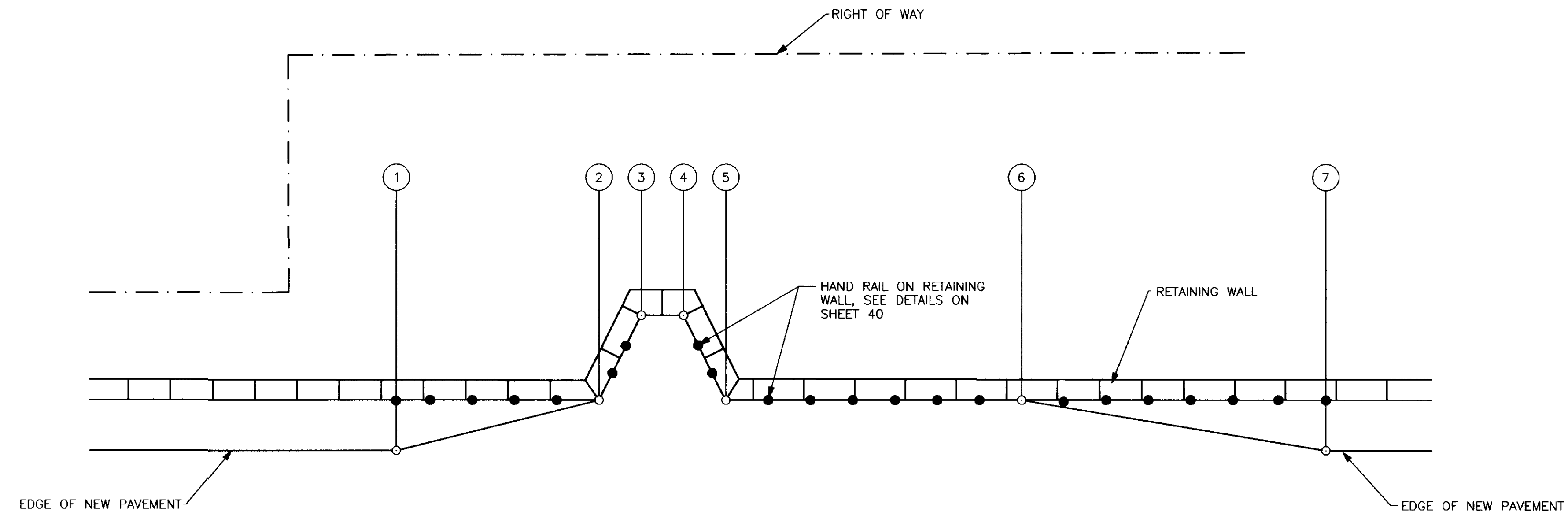
PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 33 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



# GLACIER HIGHWAY



POINT LOCATIONS AND DESCRIPTIONS				
POINT No.	STATION	OFFSET		POINT DESCRIPTION
		LEFT	RIGHT	
(1)	62+96.58	15.00'		ANGLE POINT, BEGIN TRANSITION, BEGIN HAND RAIL AT 21' LT.
(2)	62+20.78	21.00'		ANGLE POINT, END TRANSITION
(3)	63+25.78	31.00'		ANGLE POINT
(4)	63+30.78	31.00'		ANGLE POINT
(5)	63+35.78	21.00'		ANGLE POINT
(6)	63+70.78	21.00'		ANGLE POINT, BEGIN TRANSITION
(7)	64+06.78	15.00'		ANGLE POINT, END TRANSITION, END HAND RAIL AT 21' LT.
(8)	61+35.87		16.50'	TOP FACE OF CURB, ANGLE POINT, BEGIN TRANSITION
(9)	61+61.13		20.50'	TOP FACE OF CURB, ANGLE POINT, END TRANSITION
(10)	61+91.22		20.50'	TOP FACE OF CURB, BEGIN CURB AND SIDEWALK TRANSITION TO DEPRESSED CURB
(11)	61+96.72		20.50'	TOP FACE OF CURB, END CURB AND SIDEWALK TRANSITION TO DEPRESSED CURB
(12)	62+01.72		28.50'	ANGLE POINT
(13)	62+06.72		28.50'	ANGLE POINT
(14)	62+11.13		20.50'	TOP FACE OF CURB, BEGIN CURB AND SIDEWALK TRANSITION FROM DEPRESSED CURB, BEGIN TRANSITION
(15)	62+16.48		19.23'	TOP FACE OF CURB, END CURB AND SIDEWALK TRANSITION FROM DEPRESSED CURB
(16)	62+27.97		16.50'	ANGLE POINT, END TRANSITION

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

BY:	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0978(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 BUS PULL-OUT DETAILS

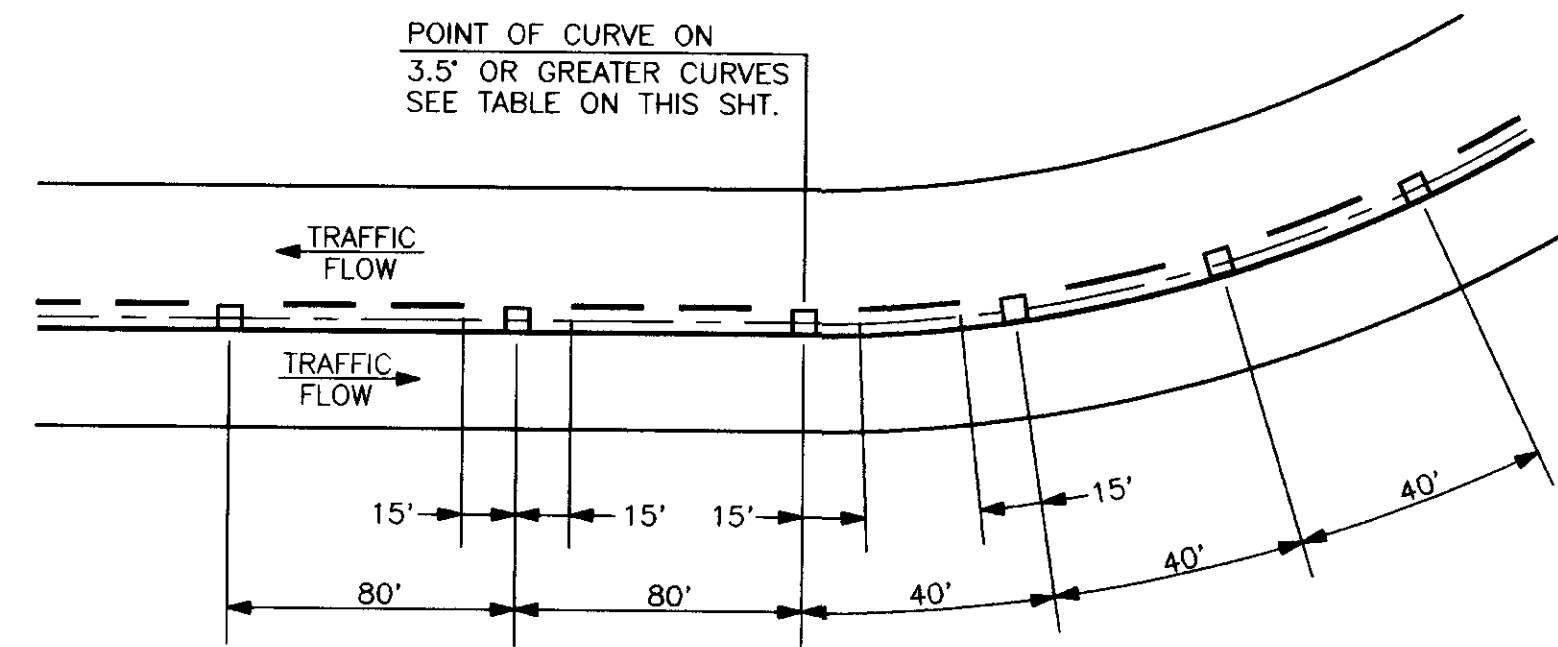


DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 34 OF 55



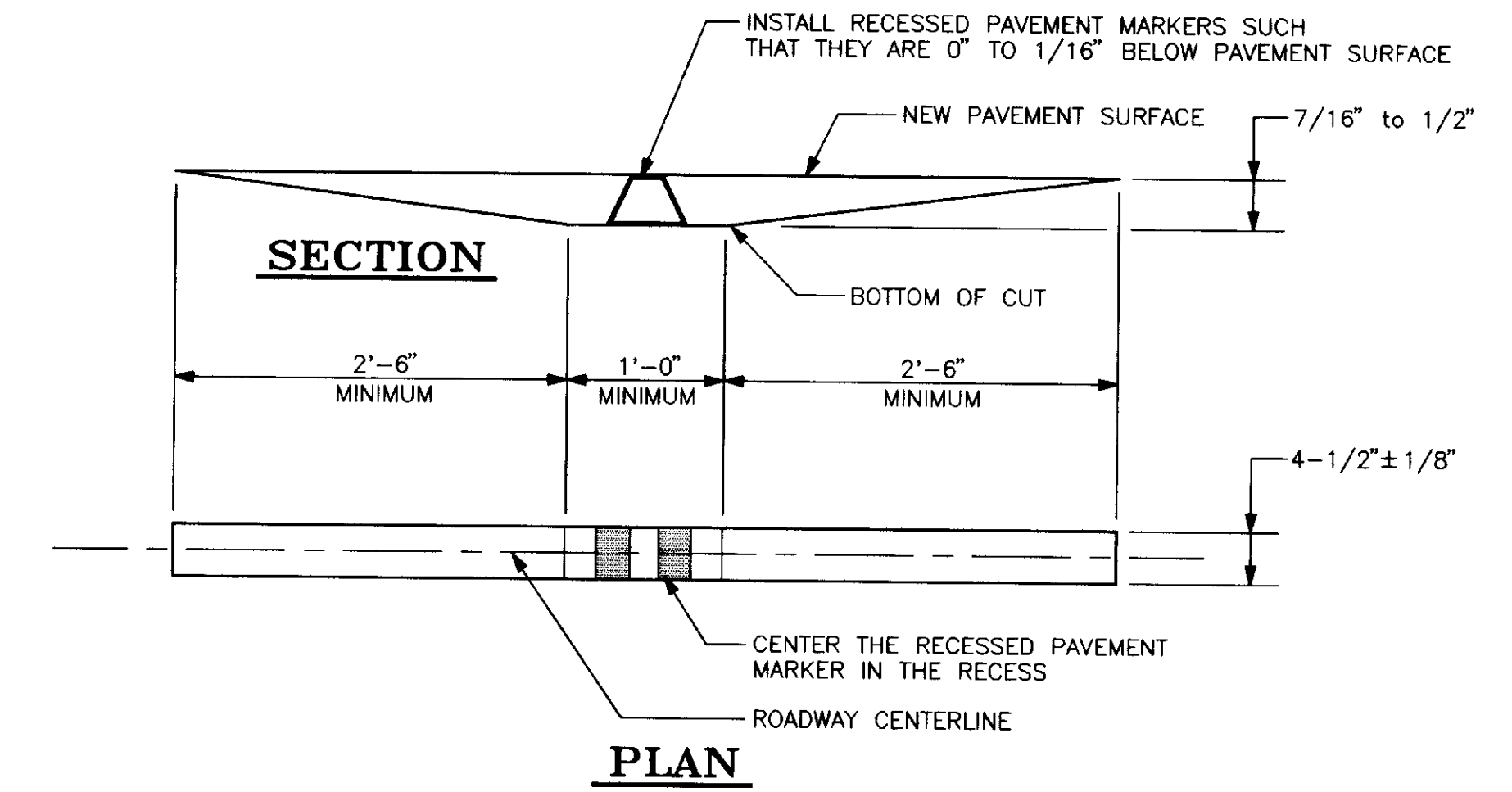
RECESSED PAVEMENT MARKER SUMMARY				
STATION		SPACING		No. REQ'D.
FROM	TO	40'	80'	
"H" 0+00	"H" 2+38	X		4
STA. 10+00	STA. 29+20		X	28
STA. 29+20	STA. 32+40	X		8
STA. 32+40	STA. 33+20		X	1
STA. 33+20	STA. 37+20	X		10
STA. 37+20	STA. 38+80		X	2
STA. 38+80	STA. 41+20	X		6
STA. 41+20	STA. 42+00		X	1
STA. 42+00	STA. 45+20	X		8
STA. 45+20	STA. 46+00		X	1
STA. 46+00	STA. 49+20	X		8
STA. 49+20	STA. 67+60		X	26
STA. 67+60	STA. 70+00	X		6
STA. 70+00	STA. 73+00		X	4
STA. 73+00	STA. 75+60	X		8
STA. 75+60	STA. 81+00		X	6



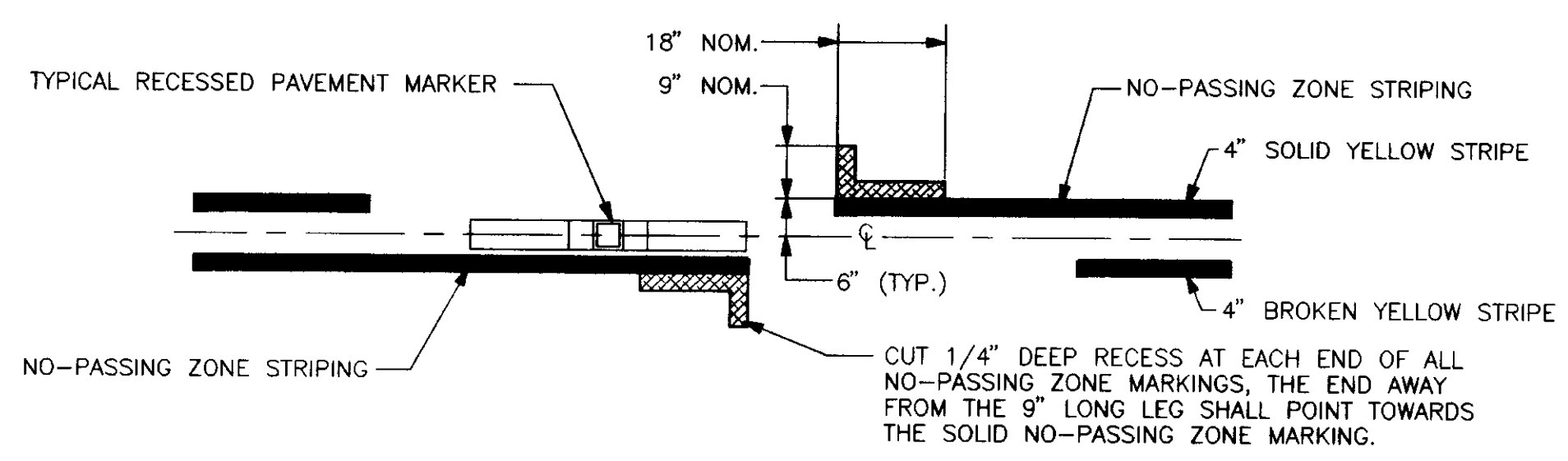
**RECESSED PAVEMENT MARKER DETAIL FOR CURVES OF 3.5° OR GREATER**

**NOTES**

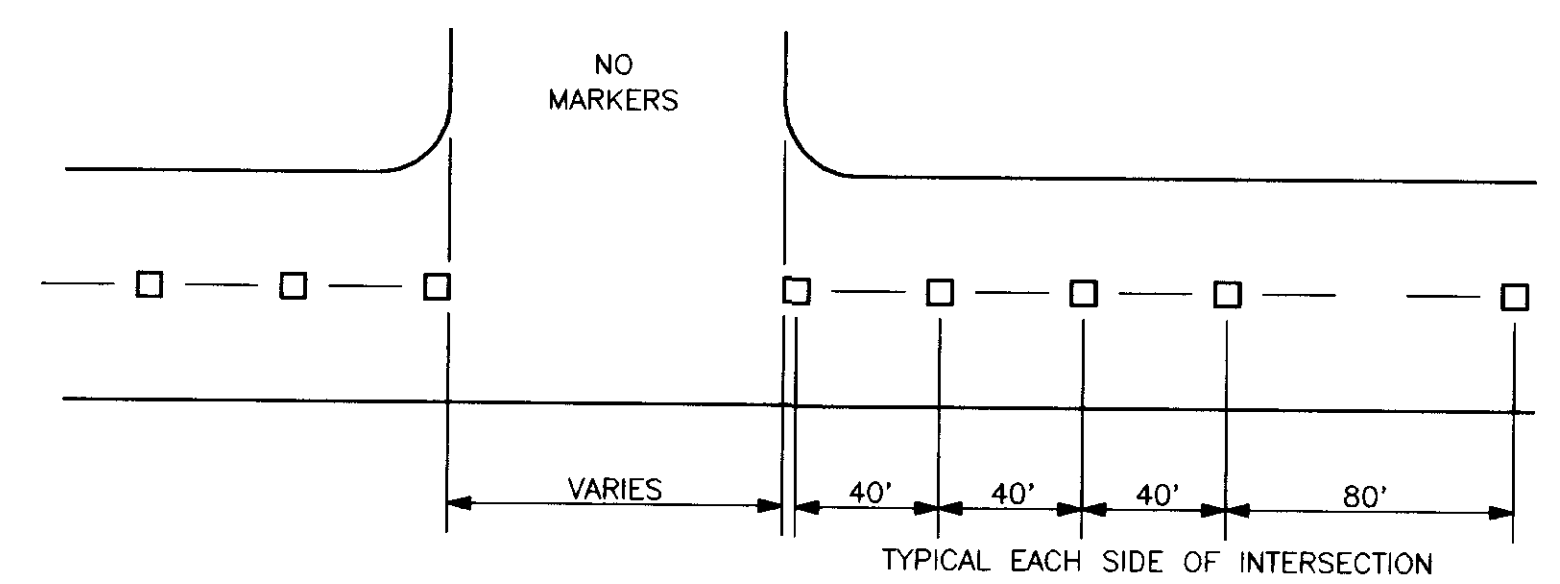
- RECESSED PAVEMENT MARKERS (R.P.M.'s) SHALL BE INSTALLED BETWEEN THE B.O.P. AND THE E.O.P.
- R.P.M.'s SHALL BE SPACED AT EVERY 80' ON TANGENT SECTIONS OF ROADWAY AND ON CURVES WITH A DEGREE OF CURVATURE LESS THAN 3.5°. ON CURVES WITH A DEGREE OF CURVATURE OF 3.5° OR GREATER THE R.P.M.'s SHALL BE SPACED AT EVERY 40'. SEE DETAILS AND SUMMARY ON THIS SHEET.
- R.P.M.'s SHALL NOT BE PLACED IN THE INTERSECTIONS WITH HIGHLAND DR., ROSS WAY, WICKERSHAM DR., OR AT THE E.O.P.. USE THE RECESSED PAVEMENT MARKER DETAIL FOR INTERSECTION APPROACHES ON THIS SHEET AT THESE LOCATIONS.
- ON ALL ROADWAY SECTIONS WITH DOUBLE LINES (EITHER BROKEN OR SOLID) R.P.M.'s SHALL BE PLACED BETWEEN THE LINES, ON SECTIONS OF ROADWAY WITH SINGLE BROKEN LINES THE R.P.M.'s SHALL BE PLACED ON THE CENTERLINE BETWEEN THE STRIPES.
- THE LOCATIONS OF ALL PASSING AND NO-PASSING ZONES SHALL BE DETERMINED AND LOCATED IN THE FIELD BY THE ENGINEER.



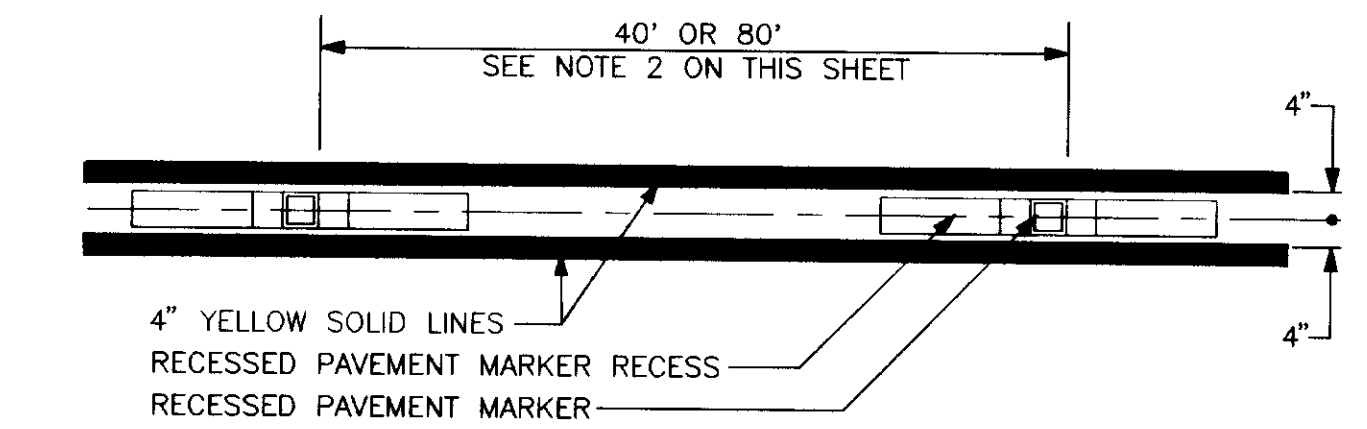
**RECESSED PAVEMENT MARKER DETAIL**



**NO-PASSING ZONE MARKING GROOVE DETAIL**



**RECESSED PAVEMENT MARKER DETAIL FOR INTERSECTION APPROACHES**  
(THIS DETAIL APPLIES AT HIGHLAND DR., ROSS WAY, WICKERSHAM DR., & THE E.O.P.)



**RECESSED PAVEMENT MARKER INSTALLATION DETAILS**

NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
CONSTRUCTION DETAILS

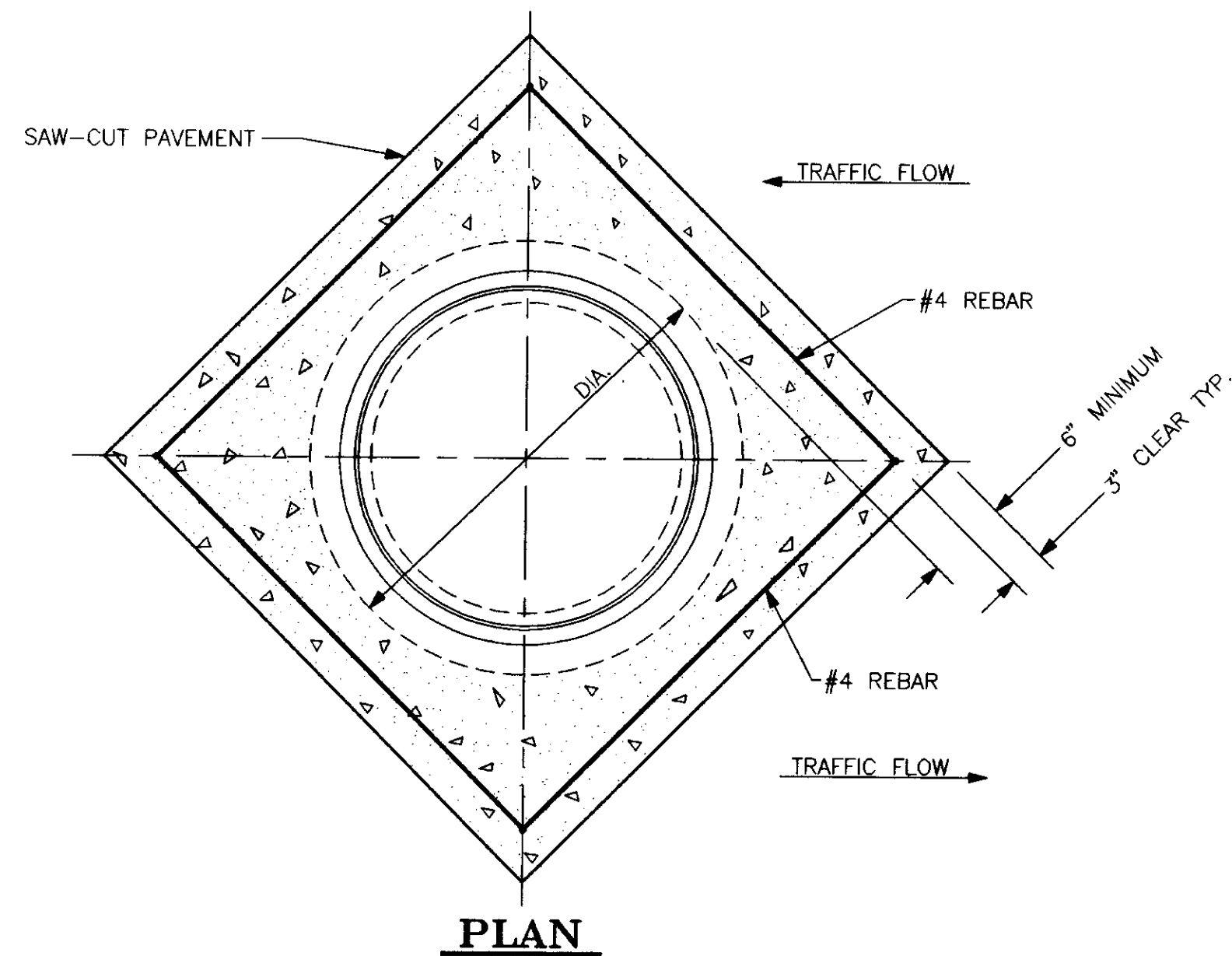
**ASCC**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

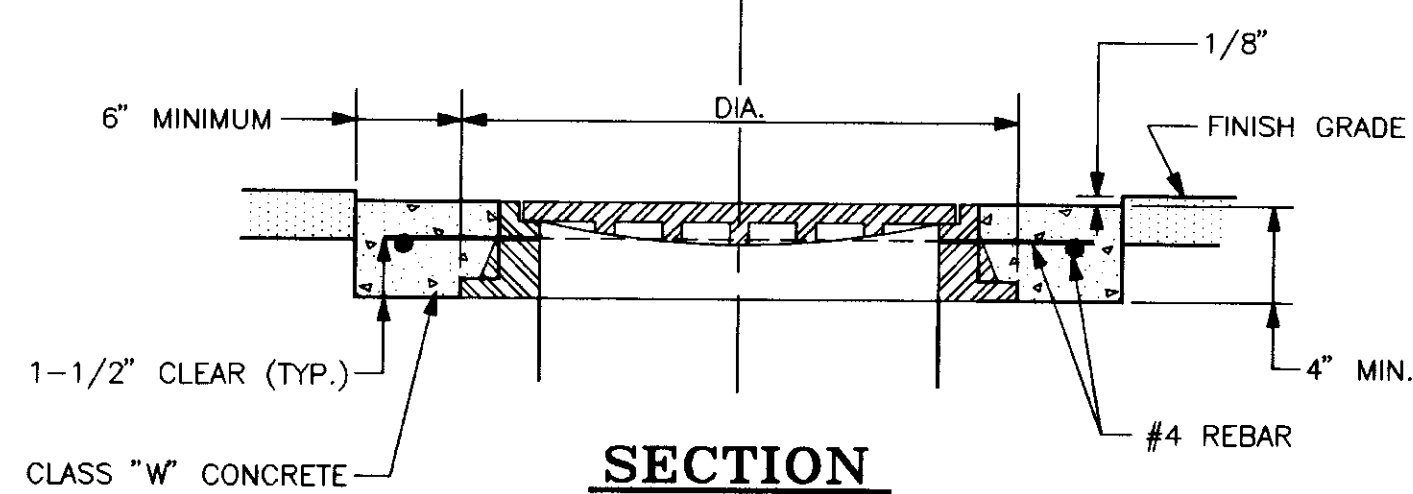
DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

PATH = E:\ACAD\WORKSPACE\DRAWING IN AUTOCAD RELEASE 11  
DRAWING PLOT SCALE: 1 = 1  
DRAWING LAST UPDATED: 3/17/1994

PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 35 OF 55



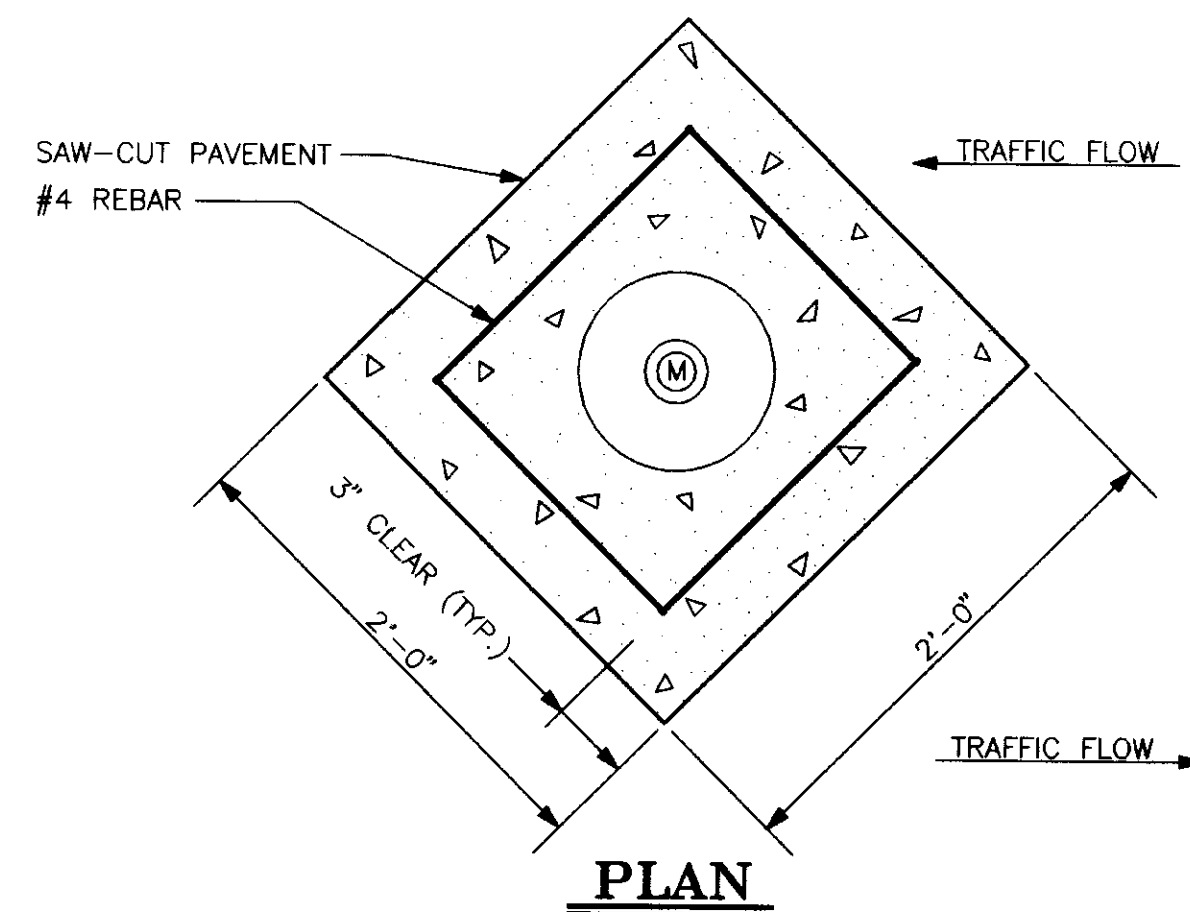
**PLAN**



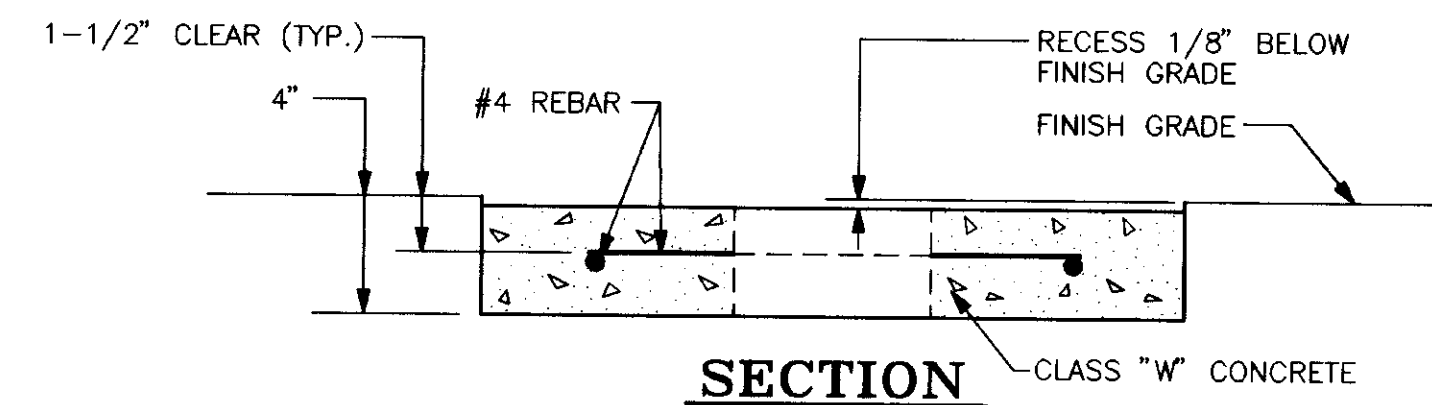
**SECTION**

**MANHOLE ENCASEMENT DETAIL**

NOTE: PAVEMENT SAW-CUTS SHALL BE SUBSIDIARY TO ITEM 604(3)



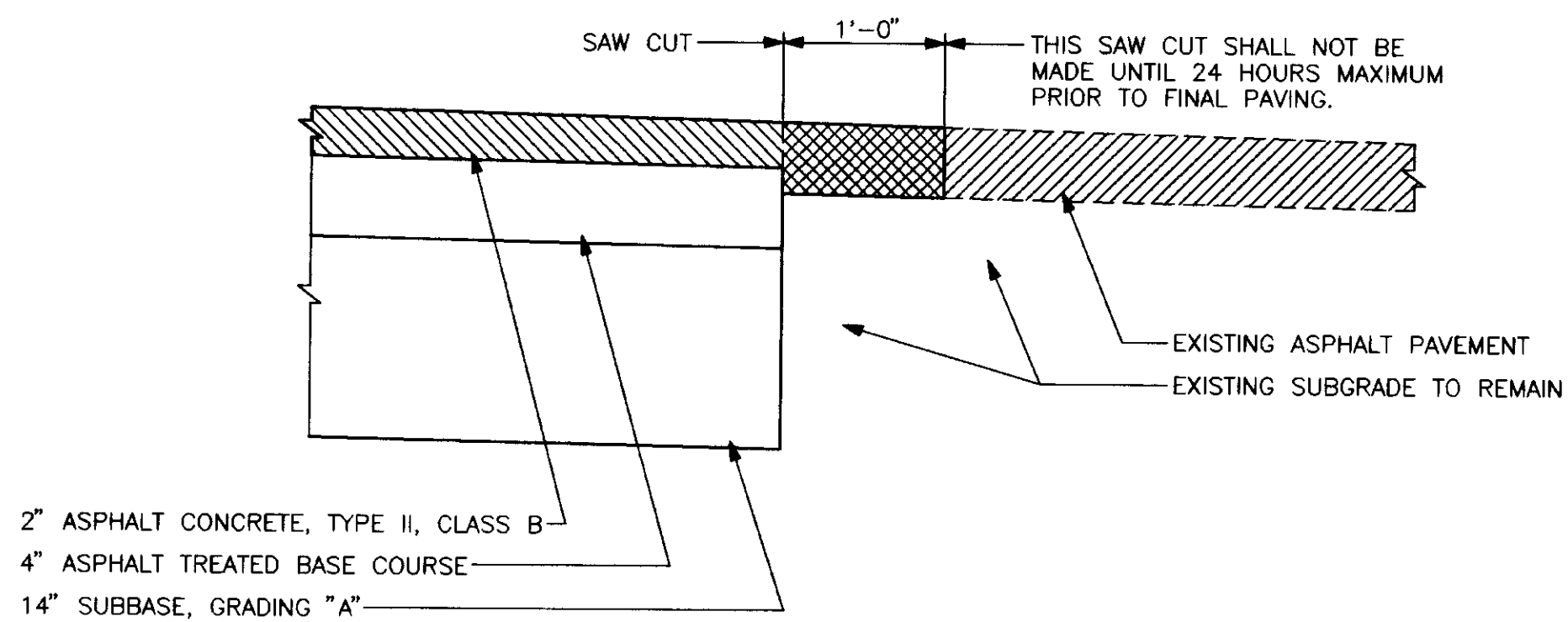
**PLAN**



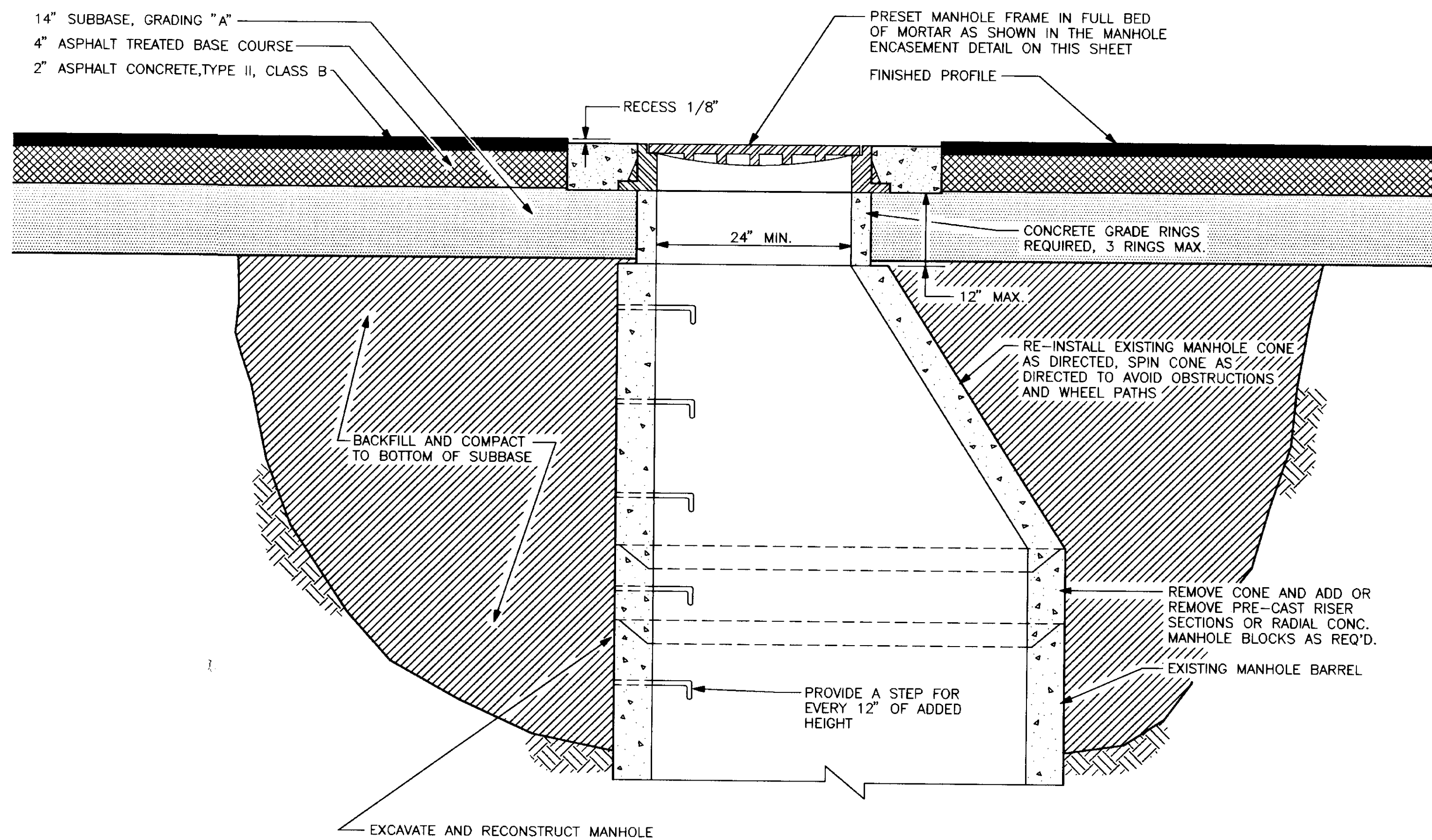
**SECTION**

**MONUMENT ENCASEMENT DETAIL**

NOTE: PAVEMENT SAW-CUTS SHALL BE SUBSIDIARY TO ITEM 614(4).



**PAVEMENT MATCH DETAIL**



**MANHOLE RECONSTRUCTION DETAILS**

NOTE: PAVEMENT SAW-CUTS SHALL BE SUBSIDIARY TO ITEM 604(3).

RECORD OF REVISIONS		
BY:	DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 CONSTRUCTION DETAILS

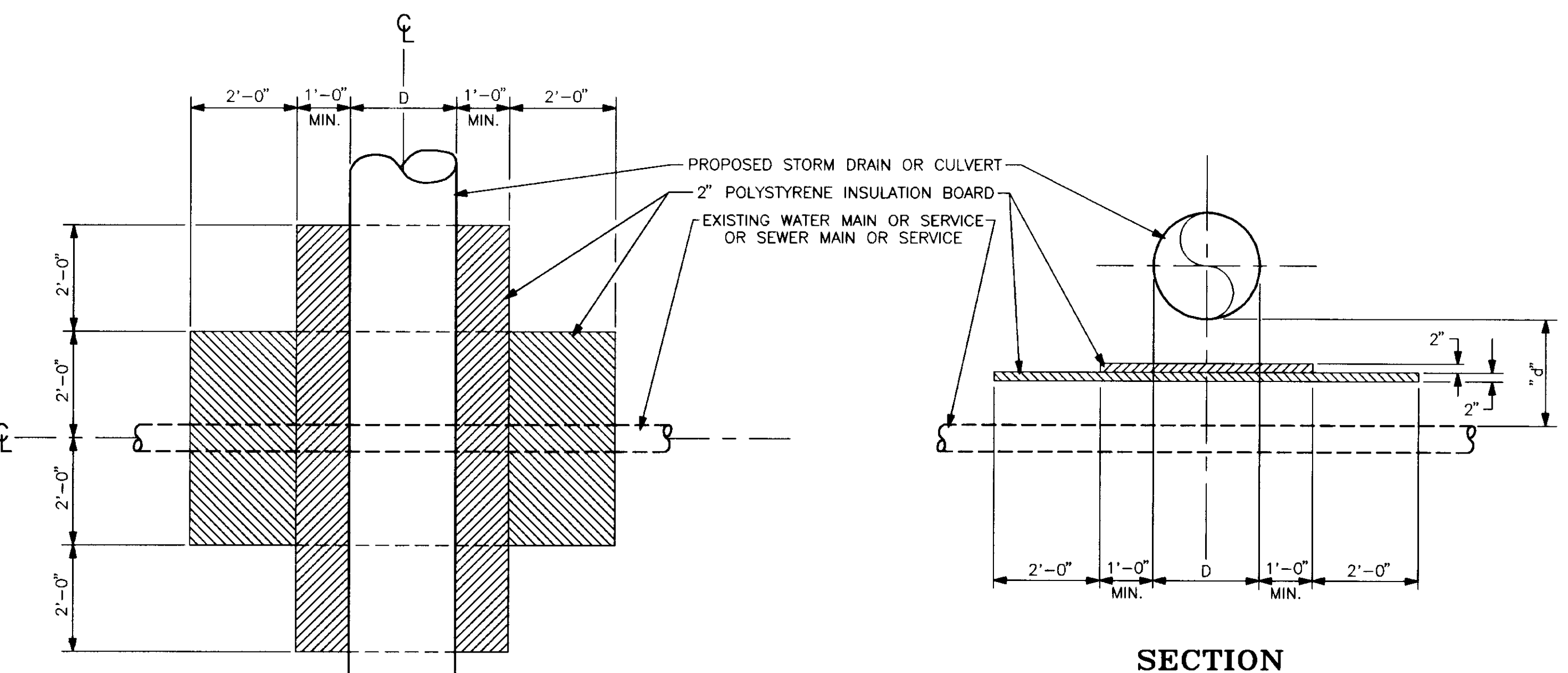
**ASCC**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 36 OF 55

STATE OF ALASKA  
 49TH  
 Carey Meyer  
 CAREY & MEYER  
 ENGINEERS  
 PROFESSIONAL

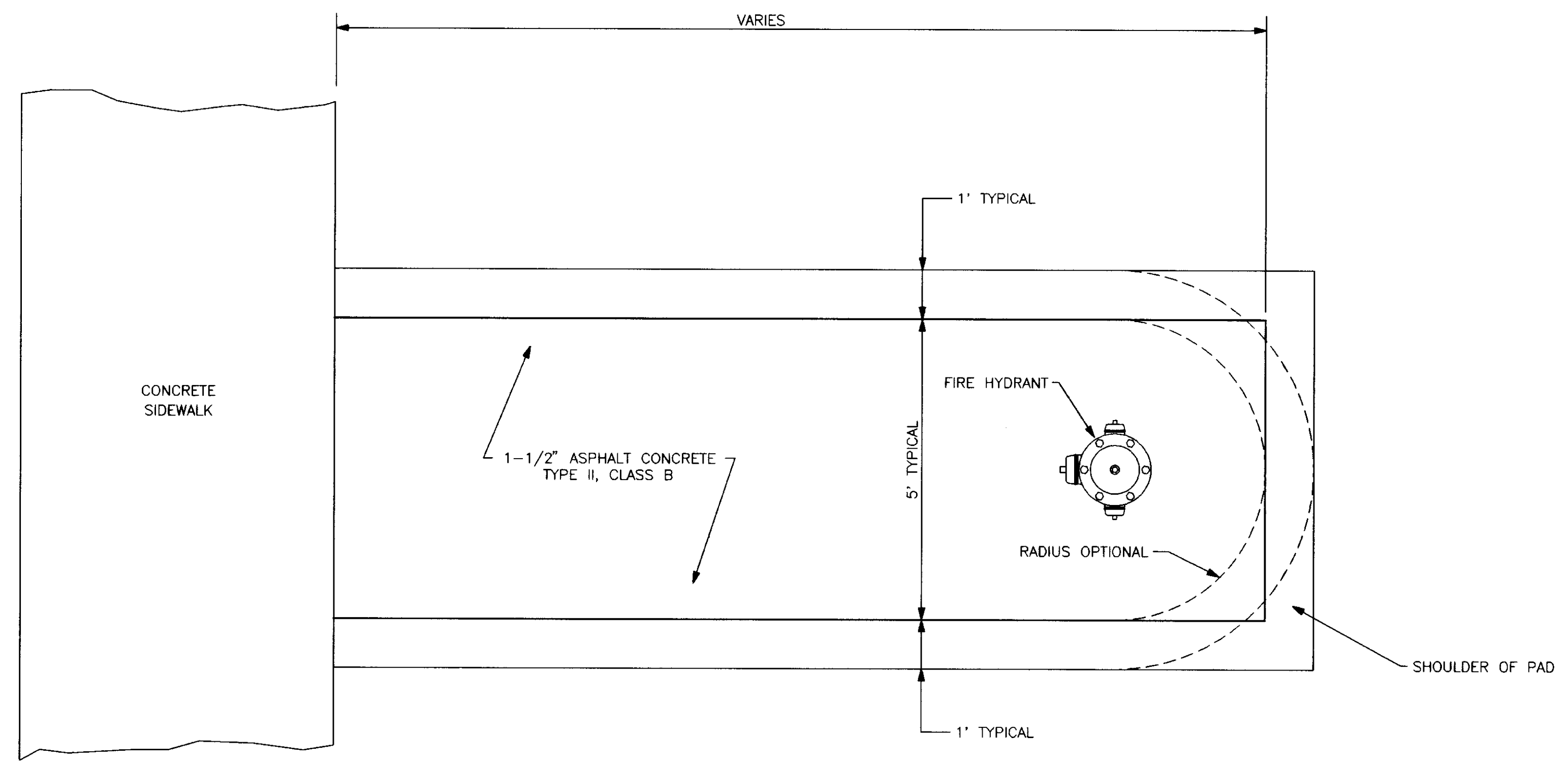


**PLAN**

**SECTION**

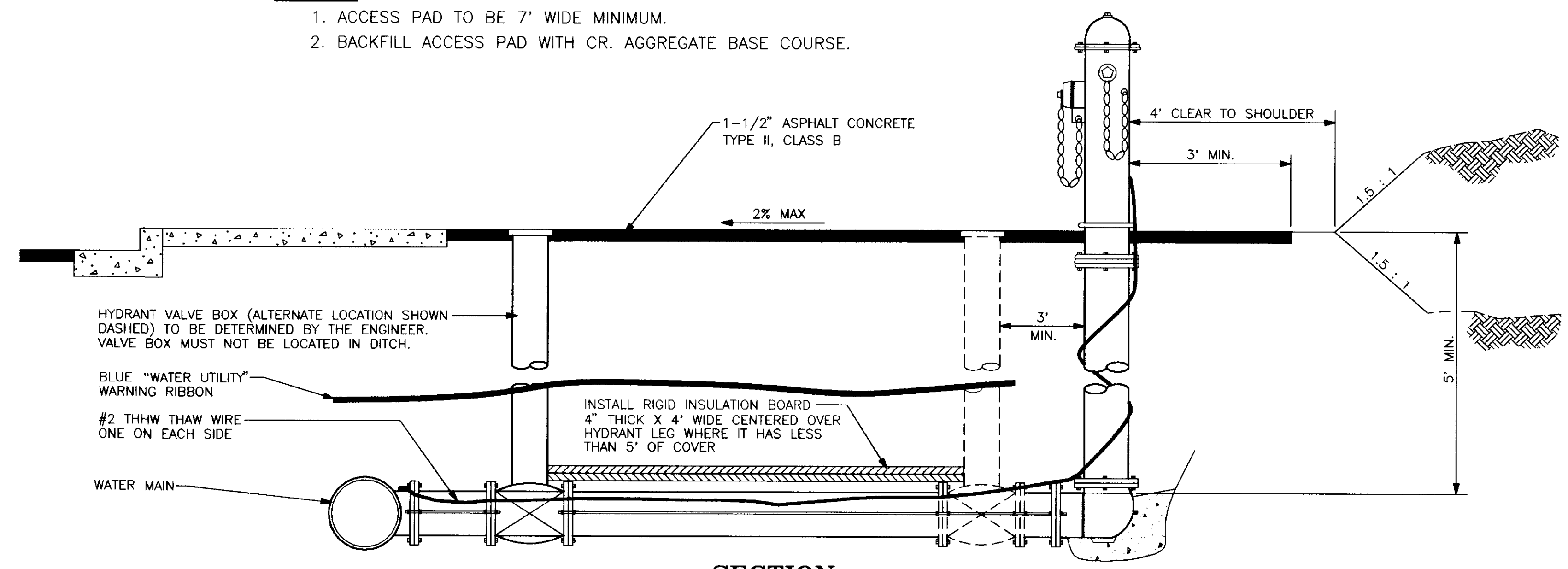
- NOTES:**
1. INSTALL 2" POLYSTYRENE INSULATION BOARD AS SHOWN WHEN "d" IS LESS THAN 2'-0".
  2. WRAP-AROUND INSULATION WITH AN R-FACTOR EQUAL TO 4" OF RIGID INSULATION BOARD MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.

**WATER LINE INSULATION AT CULVERT DETAIL**



**PLAN**

- NOTES:**
1. ACCESS PAD TO BE 7' WIDE MINIMUM.
  2. BACKFILL ACCESS PAD WITH CR. AGGREGATE BASE COURSE.



**SECTION**

**FIRE HYDRANT RELOCATION & ACCESS PAD DETAILS**

INSULATION BOARD SUMMARY							
STATION	OFFSET	APPROX. SQ. FT.	REMARKS	STATION	OFFSET	APPROX. M.B.M.	REMARKS
12+11	CENTERLINE	.13	SEWER MAIN	47+08	11' RT.	.13	WATER MAIN
17+02	2' RT.	.13	SEWER MAIN	48+12	12' RT.	.13	WATER MAIN
18+79	2' RT.	.13	SEWER MAIN	48+15	22' LT.	.13	SEWER MAIN
20+63	5' LT.	.13	SEWER MAIN	48+83	13' RT.	.13	WATER MAIN
20+66	11' RT.	.13	WATER MAIN	48+85	18' LT.	.26	2-SEWER MAINS
22+16	10' LT.	.13	SEWER MAIN	50+36	16' RT.	.13	WATER MAIN
22+17	6' RT.	.13	WATER MAIN	54+02	16' LT.	.26	2-SEWER MAINS
25+78	10' LT.	.13	SEWER MAIN	54+06	13' RT.	.13	WATER MAIN
25+80	15' RT.	.13	WATER MAIN	62+84	13' RT.	.13	WATER MAIN
26+83	13' RT.	.13	WATER MAIN	64+65	12' RT.	.13	WATER MAIN
26+89	11' LT.	.13	SEWER MAIN	64+65	17' LT.	.26	2-SEWER MAINS
29+79	10' LT.	.13	SEWER MAIN	69+08	13' LT.	.51	2-SEWER MAINS/48" PIPE
29+80	15' RT.	.13	WATER MAIN	69+10	13' RT.	.26	WATER MAIN
31+14	22' LT.	.13	SEWER MAIN	73+13	15' RT.	.13	WATER MAIN
31+27	19' RT.	.13	WATER MAIN	73+13	21' LT.	.26	2-SEWER MAINS
32+80	7' RT.	.13	WATER MAIN	74+63	25' RT.	.13	FIRE HYDRANT LEG
32+80	21' LT.	.13	SEWER MAIN	75+12	16' LT.	.26	2-SEWER MAINS
35+06	15' LT.	.13	SEWER MAIN	75+13	15' RT.	.13	WATER MAIN
37+49	13' LT.	.13	SEWER MAIN	76+28	19' LT.	.26	2-SEWER MAINS
41+89	13' RT.	.13	WATER MAIN	76+29	15' RT.	.13	WATER MAIN
41+91	14' LT.	.13	SEWER MAIN	77+86	14' RT.	.13	WATER MAIN
45+14	10' LT.	.13	SEWER MAIN	77+87	17' LT.	.26	2-SEWER MAINS
45+21	15' LT.	.13	WATER MAIN	80+32	17' LT.	.26	2-SEWER MAINS

RECORD OF REVISIONS		
BY:	DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 CONSTRUCTION DETAILS

**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

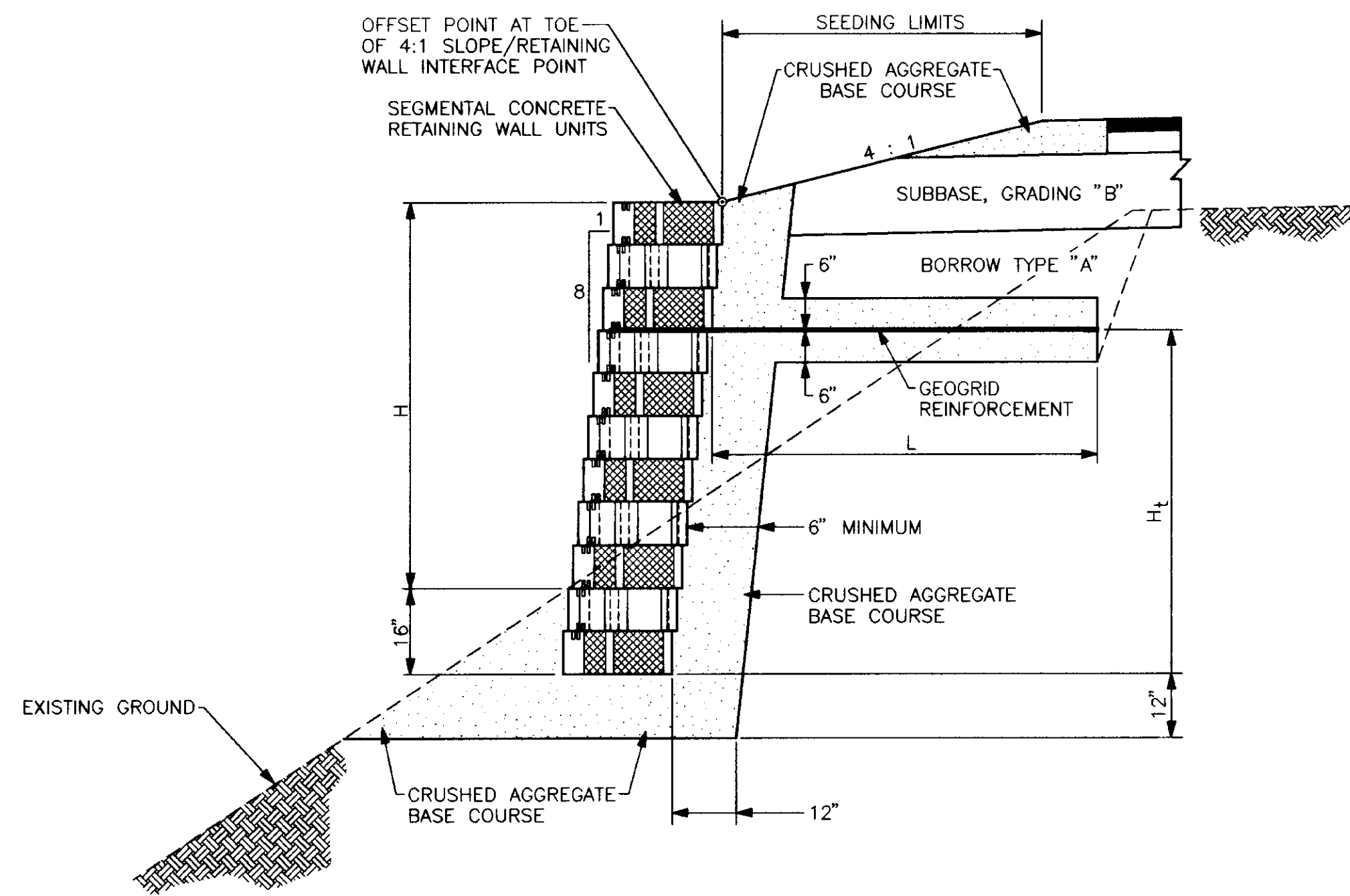
DESIGNED BY: D.L.M.  
 DRAWN BY: C.M.B.  
 CHECKED BY: D.L.M.

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

PATH = E:\ACAD\NORWAP1  
 DRAWN IN AUTOCAD RELEASE 11  
 DRAWING PLOT SCALE: 1" = 1'  
 DRAWING LAST UPDATED: 3/17/1994

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 37 OF 55

STATE OF ALASKA  
 19th  
 Carey S. Meyer  
 04-665  
 9/21/94  
 REGISTERED PROFESSIONAL ENGINEER

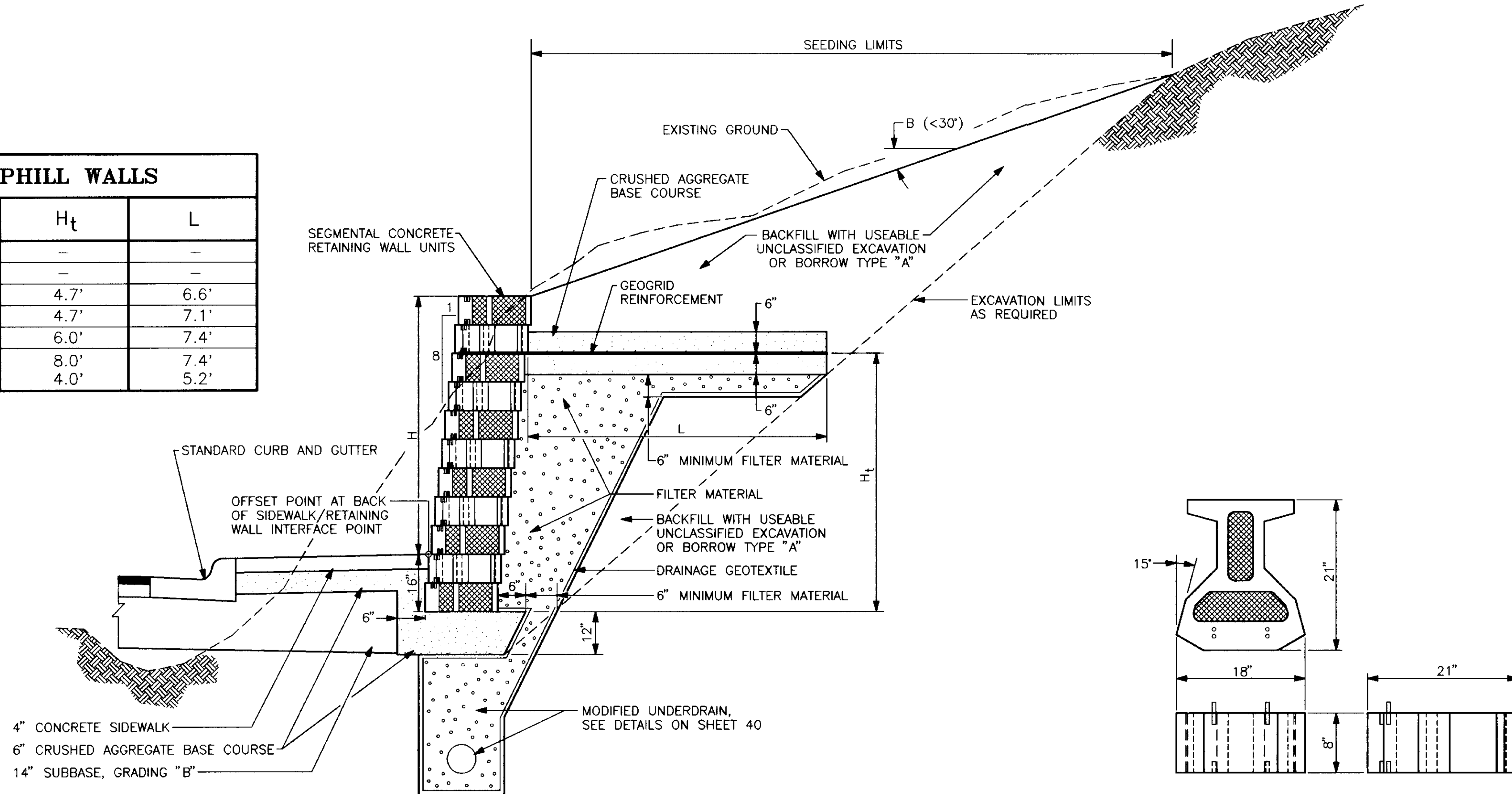


REINFORCEMENT FOR DOWNHILL WALLS			
H	GEOGRID LAYERS	H <sub>t</sub>	L
4' OR LESS	NONE	-	-
4'-5'	1 LAYER	4.7'	5.5'
5'-6'	1 LAYER	5.3'	6.0'
6'-7'	1 LAYER	6.0'	7.0'
7'-8'	2 LAYERS	7.3'	7.5'
		3.3'	5.0'
10'-12'	3 LAYERS	10.0'	8.0'
		5.3'	7.0'
		2.7'	7.0'
13'	4 LAYERS	12.0'	9.0'
		8.7'	7.5'
		5.3'	7.5'
		2.7'	7.5'
14' AND 15'	5 LAYERS	13.3'	10.0'
		10.0'	8.5'
		6.7'	8.5'
		4.0'	8.5'
		2.0'	8.5'
16' AND 17'	6 LAYERS	15.3'	11.0'
		12.0'	9.5'
		8.7'	9.5'
		6.0'	9.5'
		4.0'	9.5'
		2.0'	9.5'

**TYPICAL RETAINING WALL DETAIL FOR DOWNHILL SIDE OF ROAD**

NOT TO SCALE

REINFORCEMENT FOR UPHILL WALLS				
H	B	GEOGRID LAYERS	H <sub>t</sub>	L
4' OR LESS	<30'	NONE	-	-
4'-5'	<20'	NONE	-	-
4'-5'	>20', <30'	1 LAYER	4.7'	6.6'
5'-6'	>20', <30'	1 LAYER	4.7'	7.1'
6'-7'	<20'	1 LAYER	6.0'	7.4'
7'-8'	<20'	2 LAYERS	8.0'	7.4'
			4.0'	5.2'



**TYPICAL RETAINING WALL DETAIL FOR UPHILL SIDE OF ROAD**

NOT TO SCALE

**SEGMENTAL CONCRETE RETAINING WALL UNIT DETAIL**

NOT TO SCALE

NO.	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
**CONSTRUCTION DETAILS**

**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

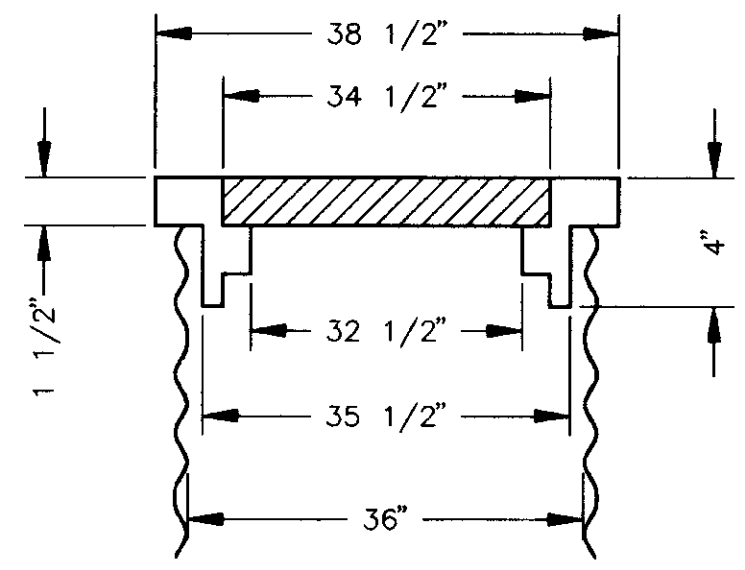
DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

PATH = E:\ASCG\WORK\99\69062\69062.DWG  
 DRAWN IN AUTOCAD RELEASE 11  
 DRAWING PLOT SCALE: 1" = 1'  
 DRAWING LAST UPDATED: 3/17/1994

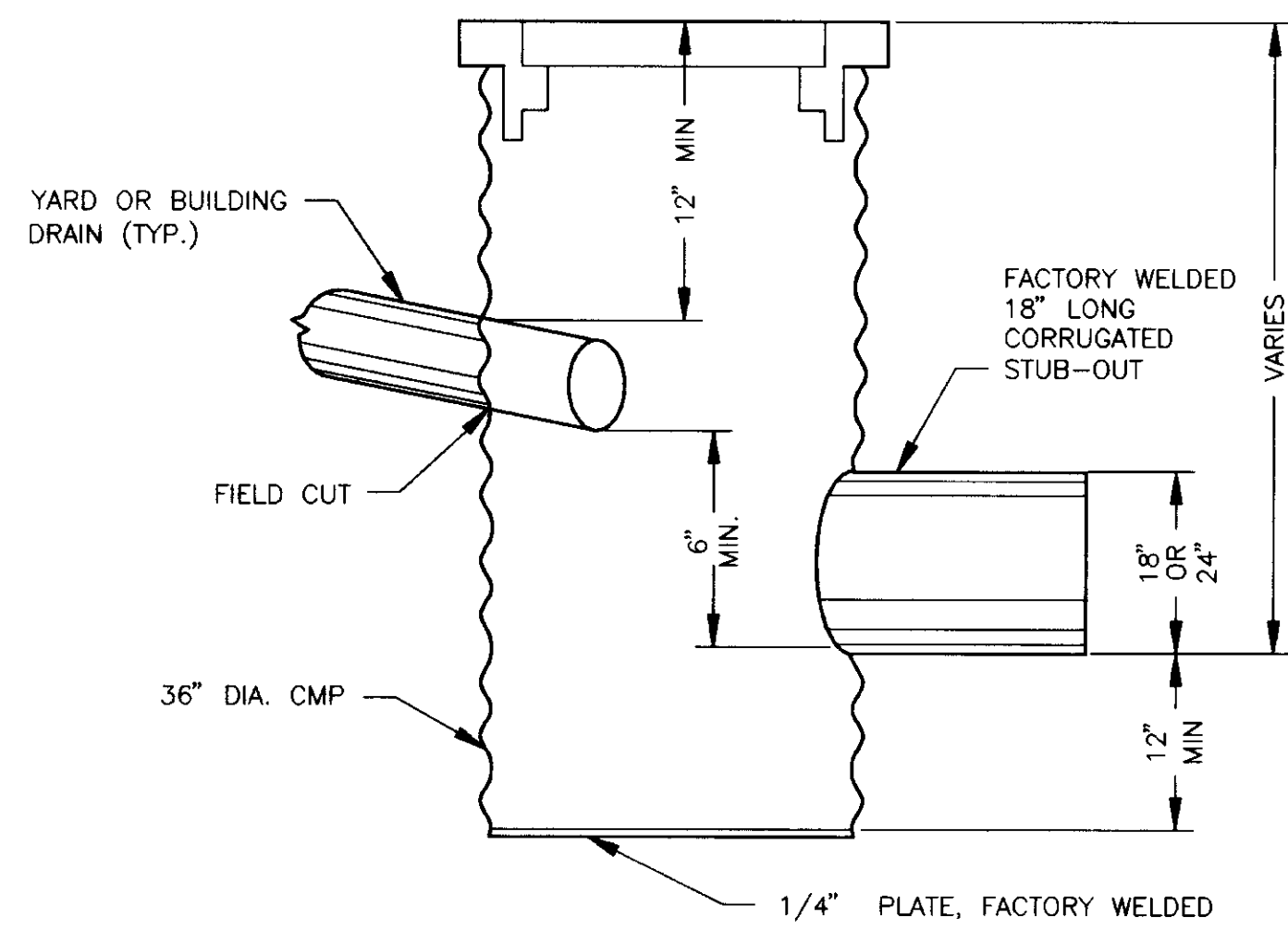
PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 38 OF 55

STATE OF ALASKA  
 49TH  
 Cary S. Meyer  
 REGISTERED PROFESSIONAL ENGINEER



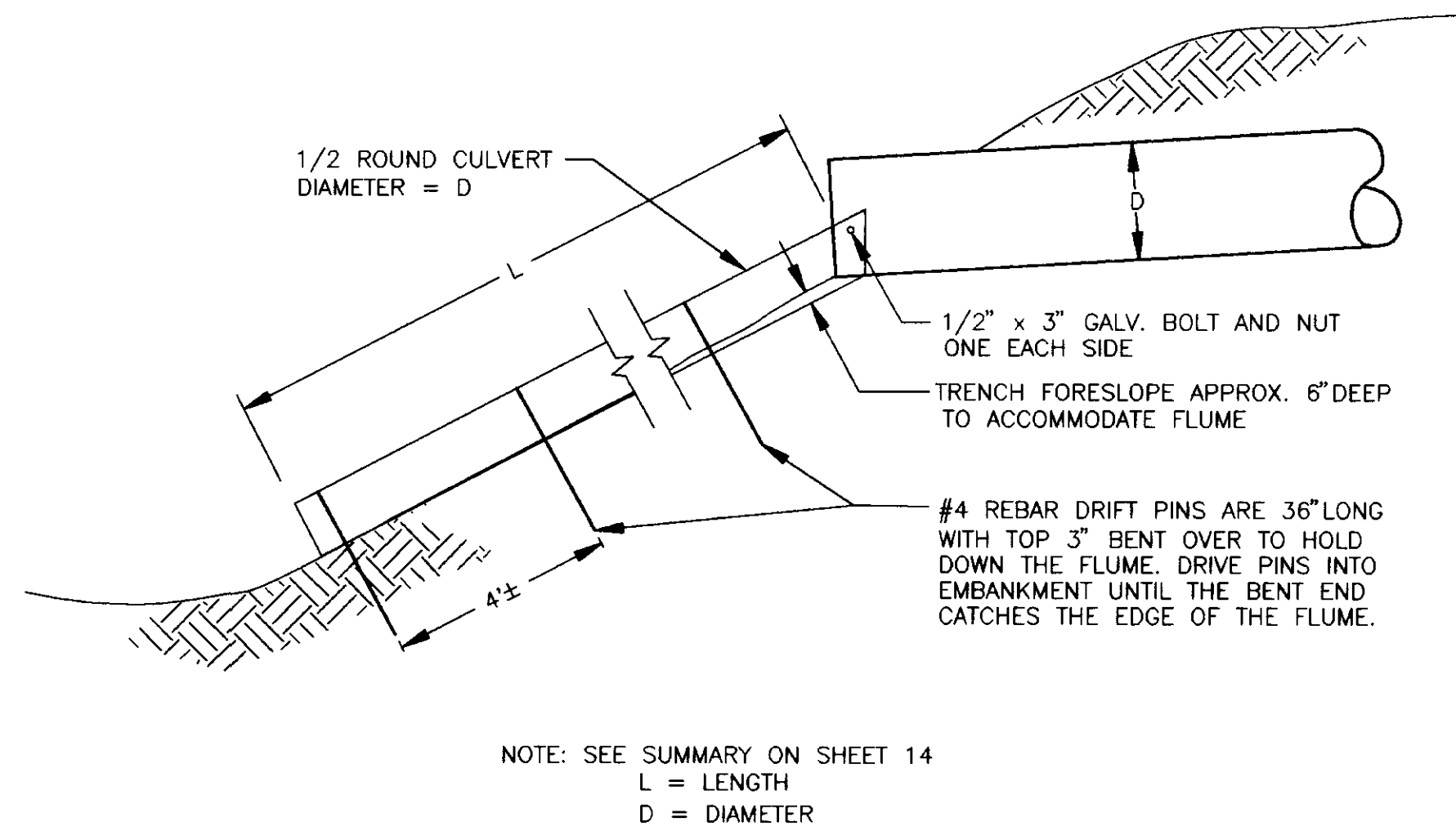
NOTE: FRAME AND OPEN GRATE LID SHALL WEIGH A MINIMUM OF 300 POUNDS. NEENAH NO. R-5901-H OR APPROVED EQUAL

**FIELD INLET FRAME & OPEN GRATE LID DETAIL**



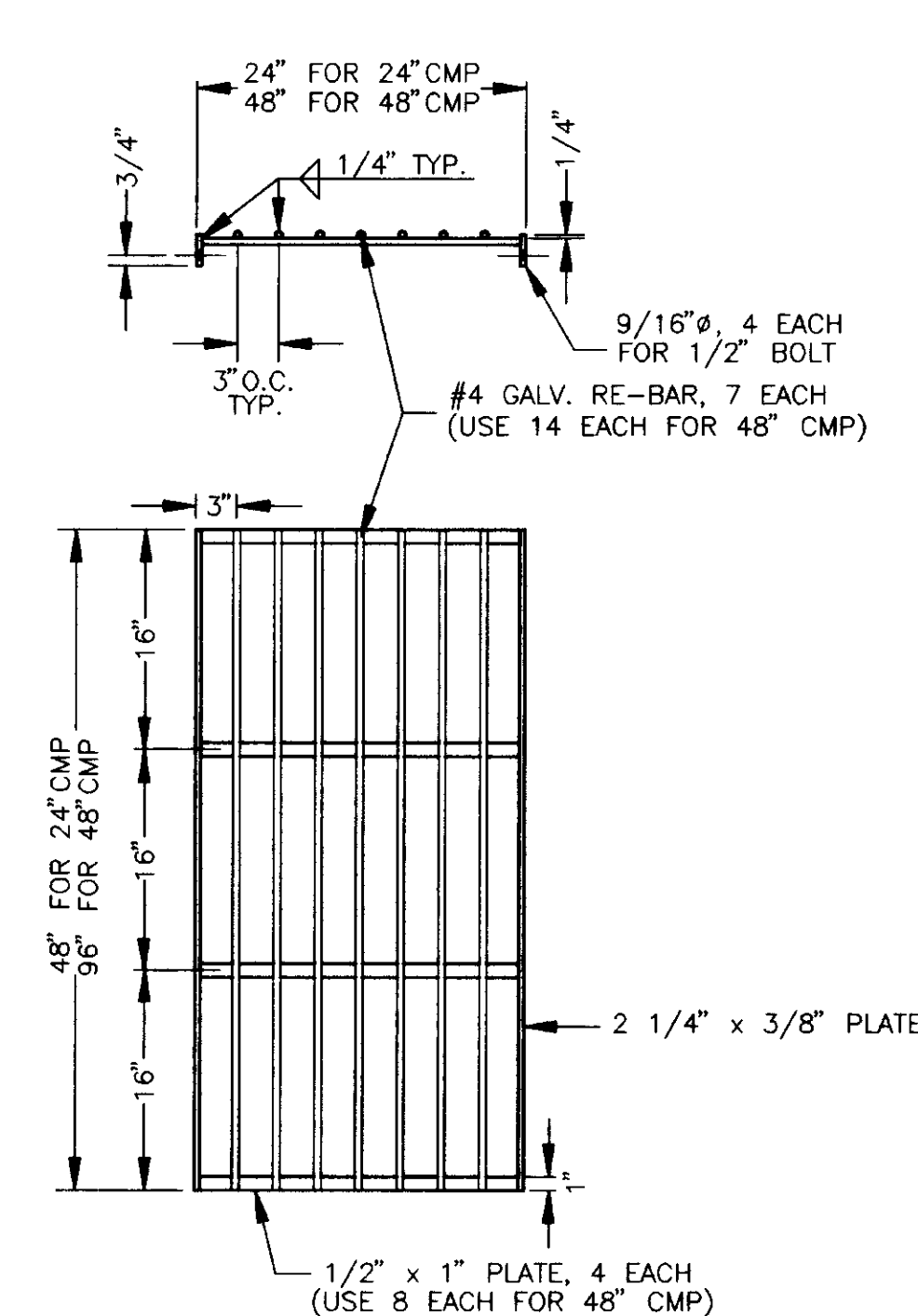
NOTE: SEE DRAINAGE SUMMARY FOR LOCATIONS OF FIELD INLETS

**36" FIELD INLET DETAILS**

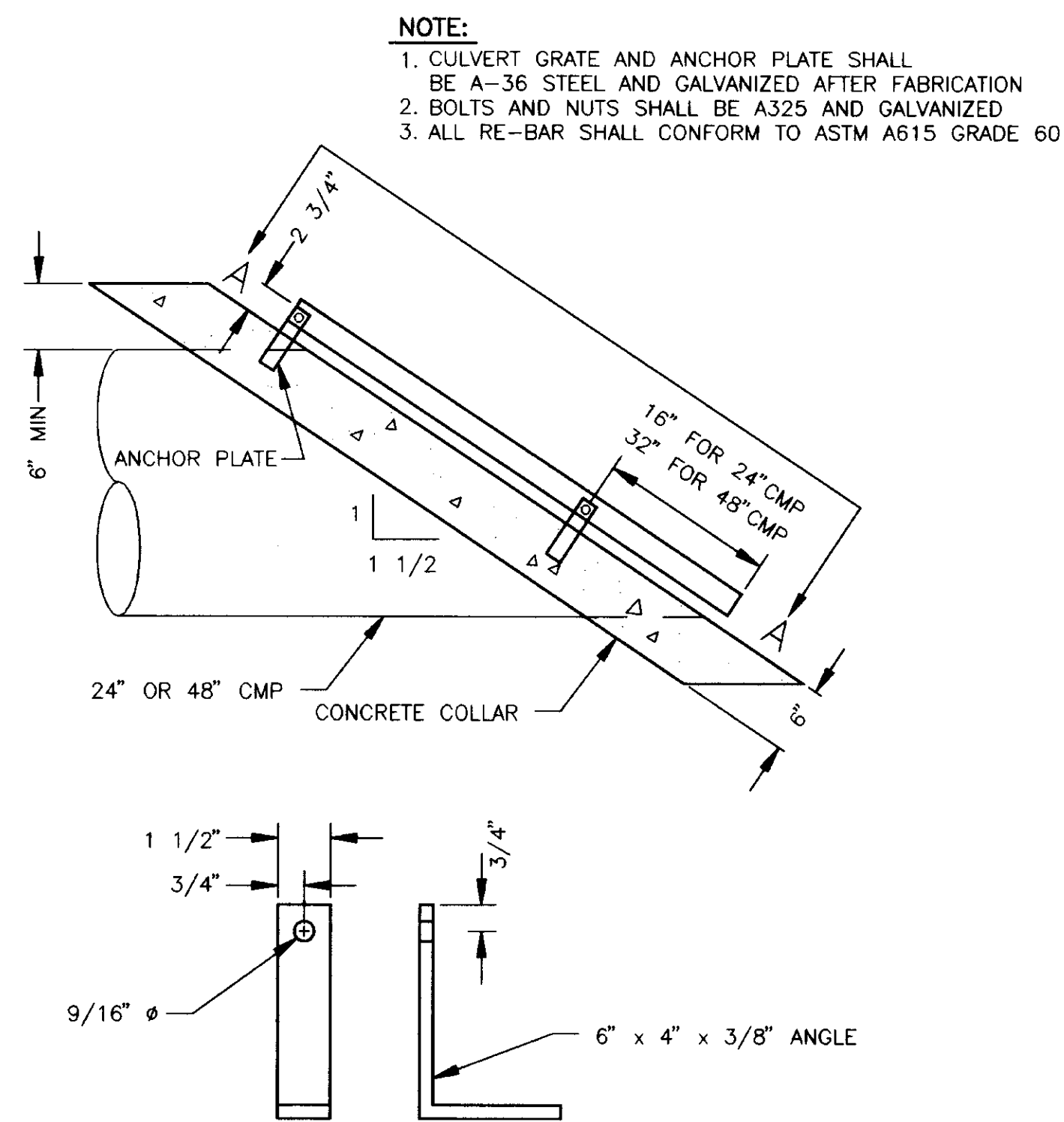


NOTE: SEE SUMMARY ON SHEET 14  
L = LENGTH  
D = DIAMETER

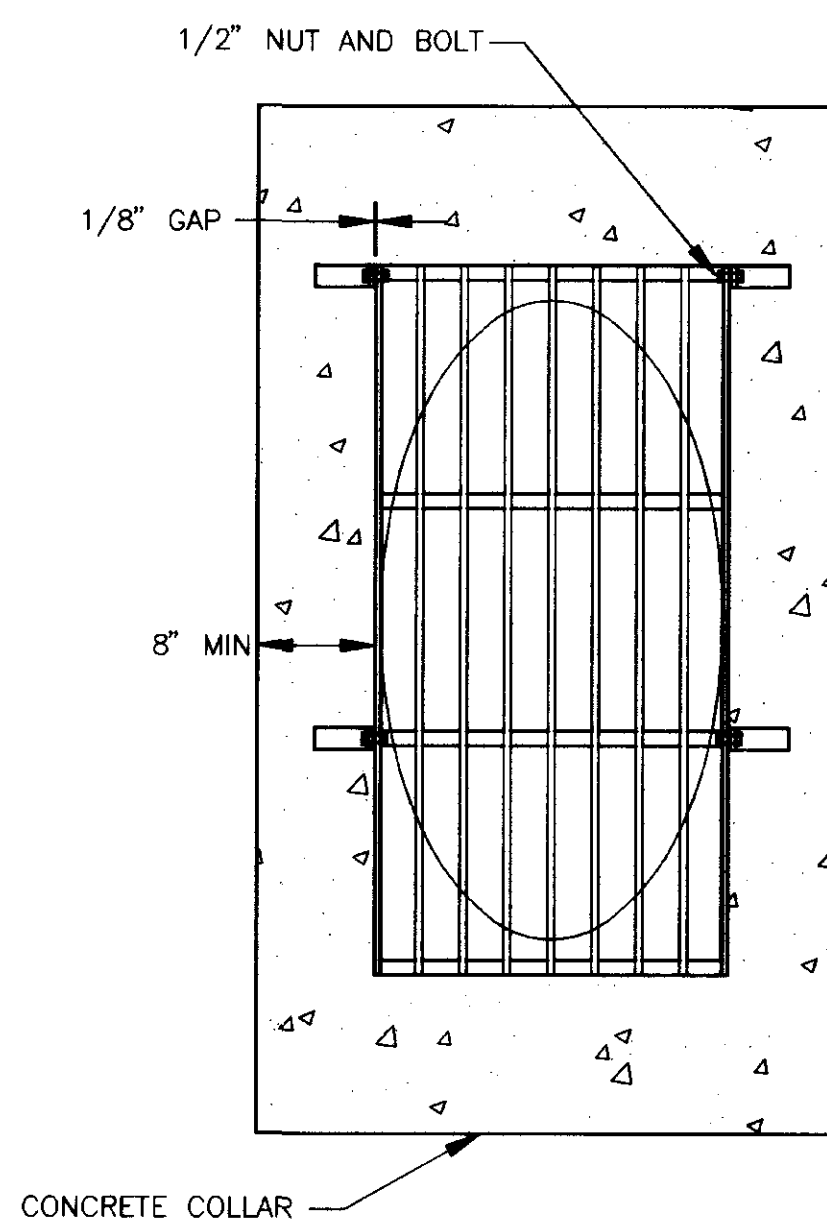
**METAL FLUME DOWN DRAIN DETAIL**



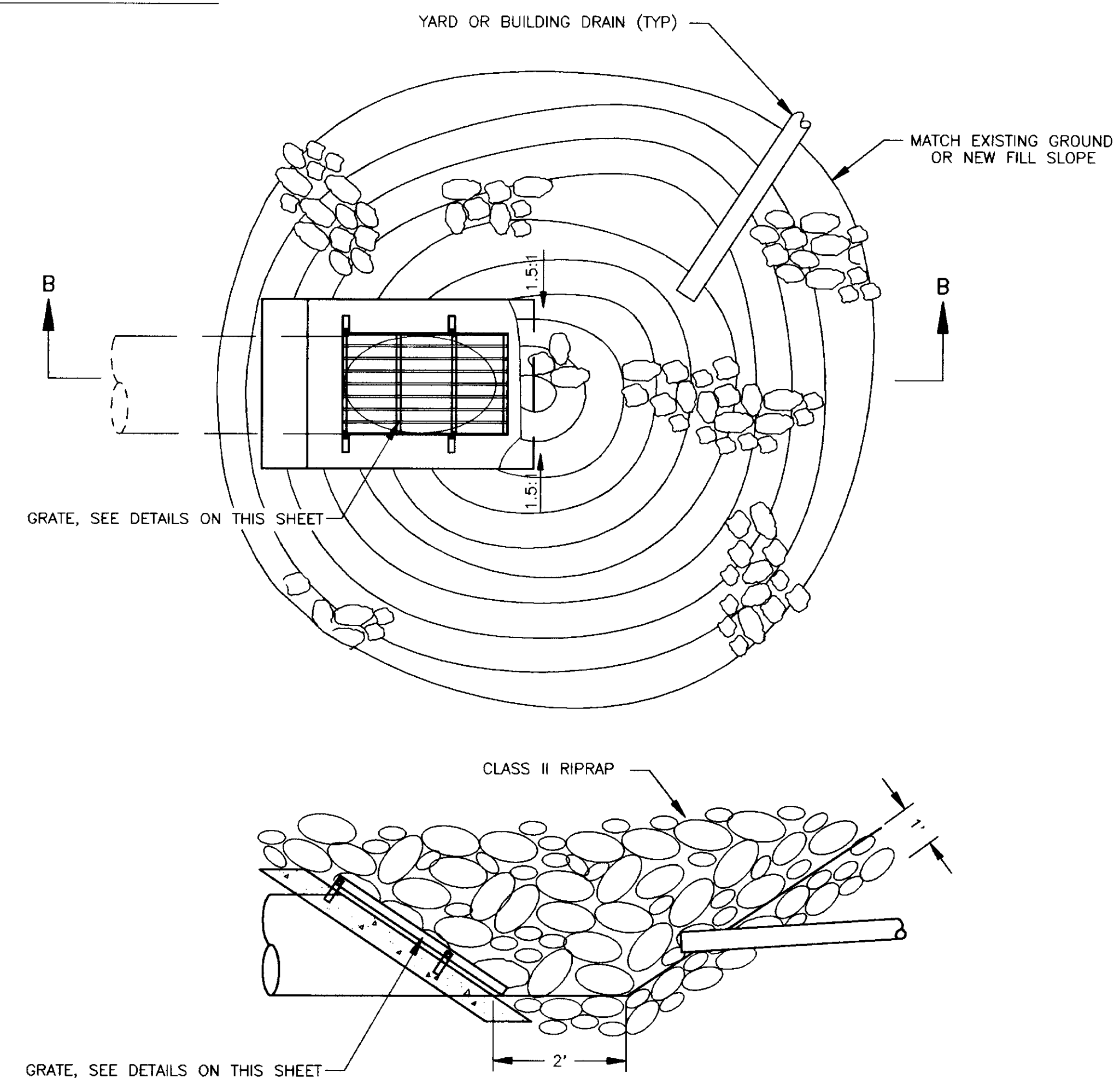
**CULVERT GRATE**



**ANCHOR PLATE**



**SECTION A-A**



**SECTION B-B**

**COLLECTION BASIN DETAILS**

**COLLECTION BASIN GRATE DETAILS**

BY:	DATE:	DESCRIPTION OF CHANGE:

**RECORD OF REVISIONS**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

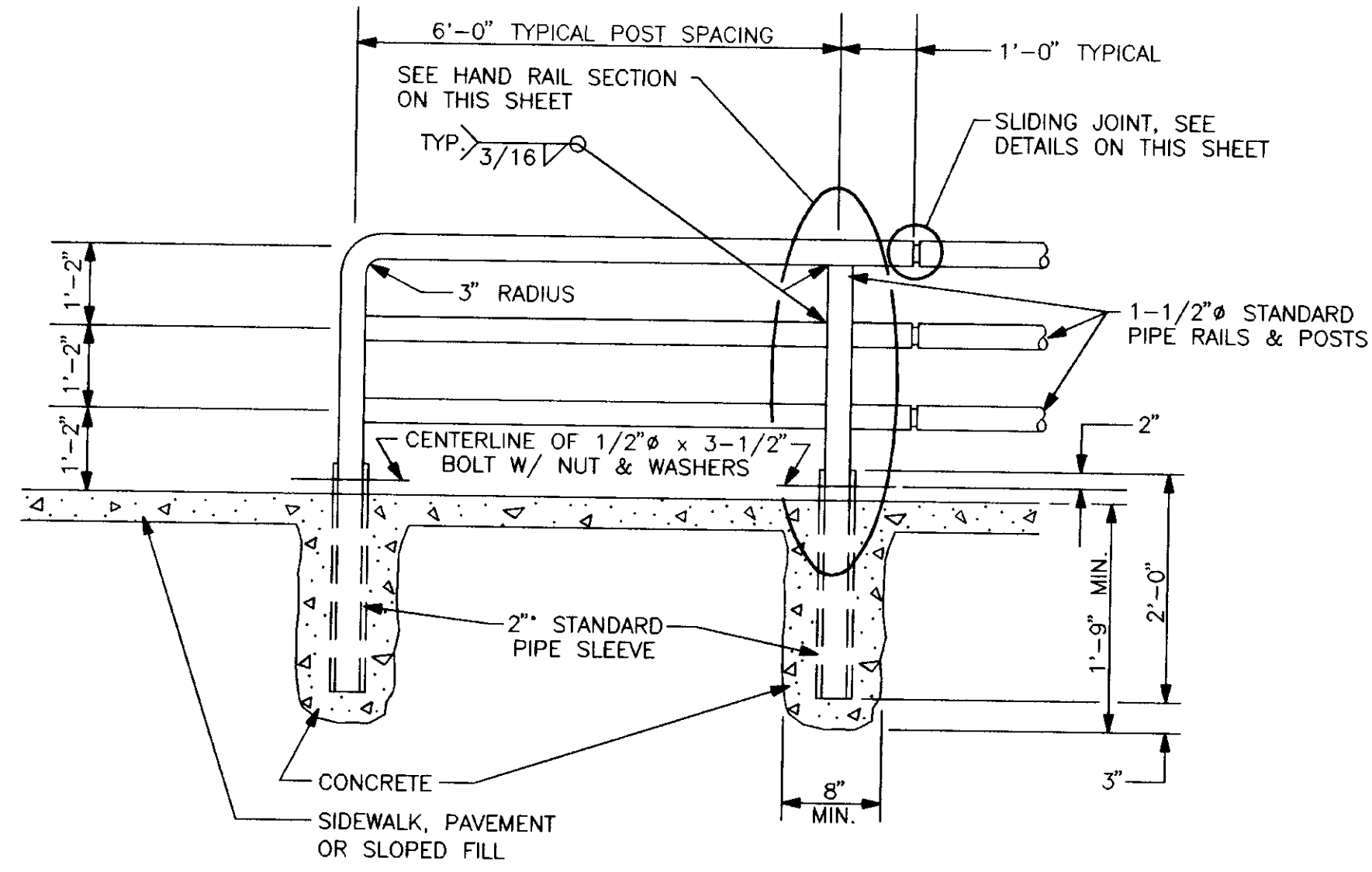
JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
CONSTRUCTION DETAILS

**ASCG**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

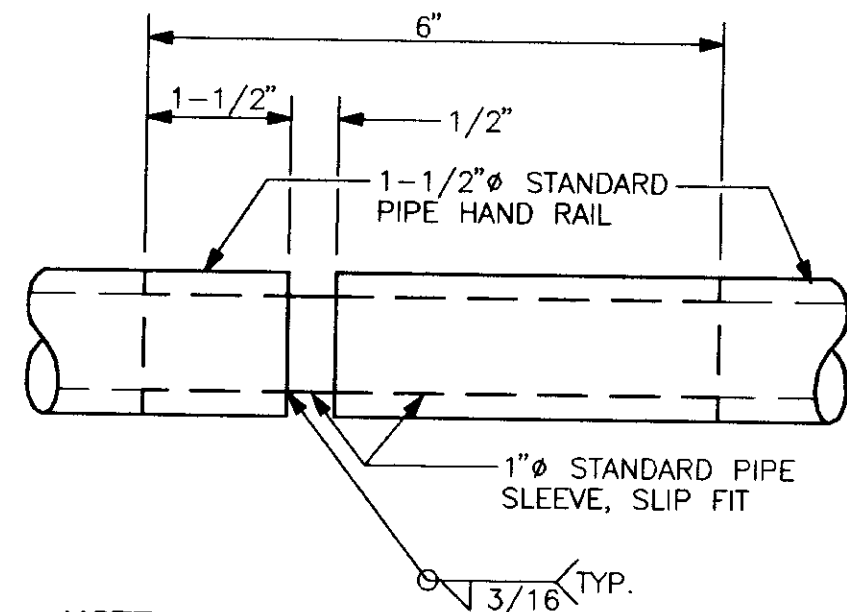
DESIGNED BY: D.L.M.  
DRAWN BY: G.L.J.  
CHECKED BY: D.L.M.

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS  
PATH = E:\ASCG\NORWAYPIT  
DRAWN IN AUTOCAD RELEASE 11  
DRAWING PLOT SCALE: 1" = 1'  
PROJECT LAST UPDATED: 3/17/1994  
PROJECT NO. 69062  
DATE: MARCH 1994  
SHEET 39 OF 55

STATE OF ALASKA  
49th  
CAREY S. MEYER  
REGISTERED PROFESSIONAL ENGINEER

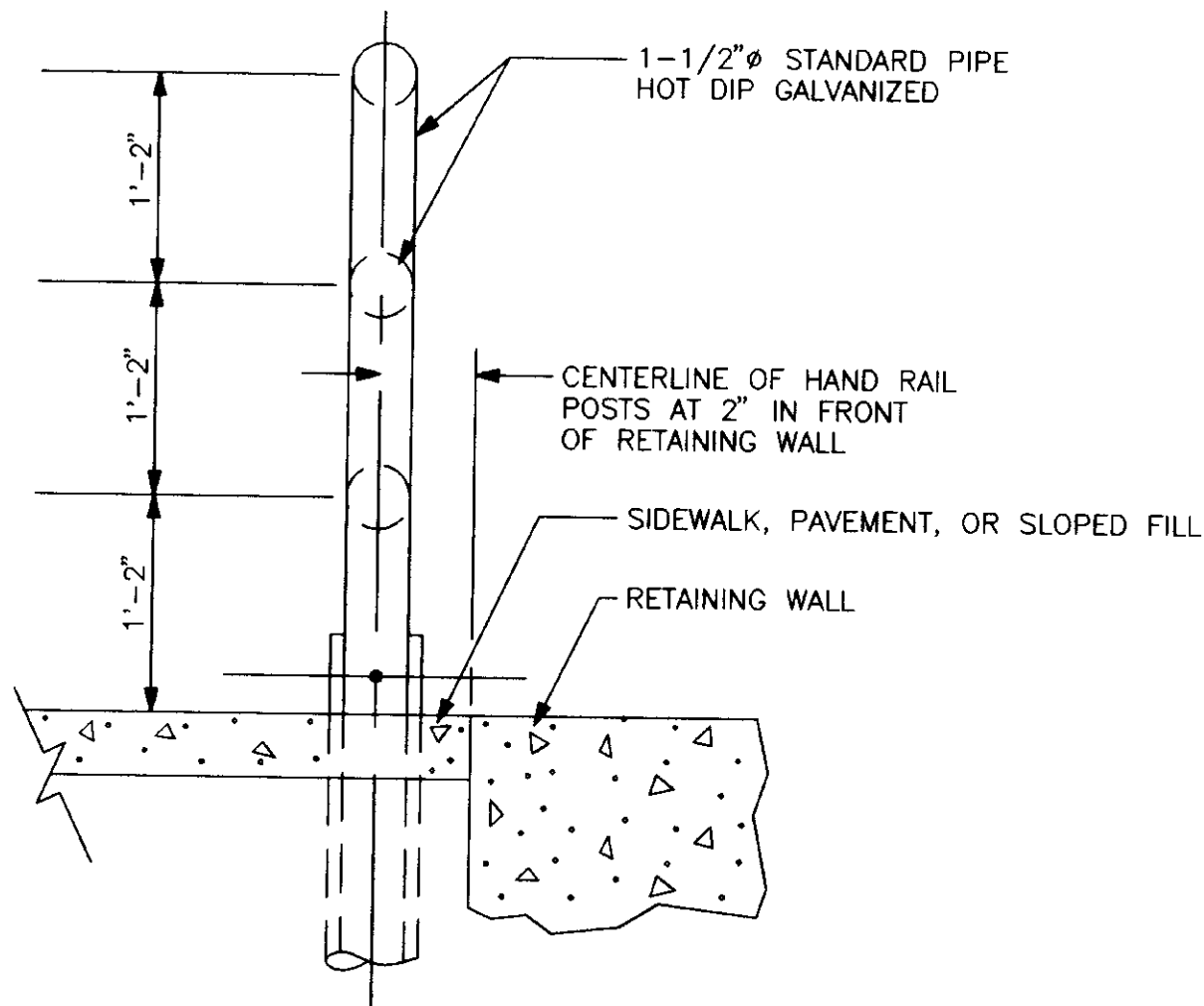


**TYPICAL HAND RAIL INSTALLATION DETAIL**

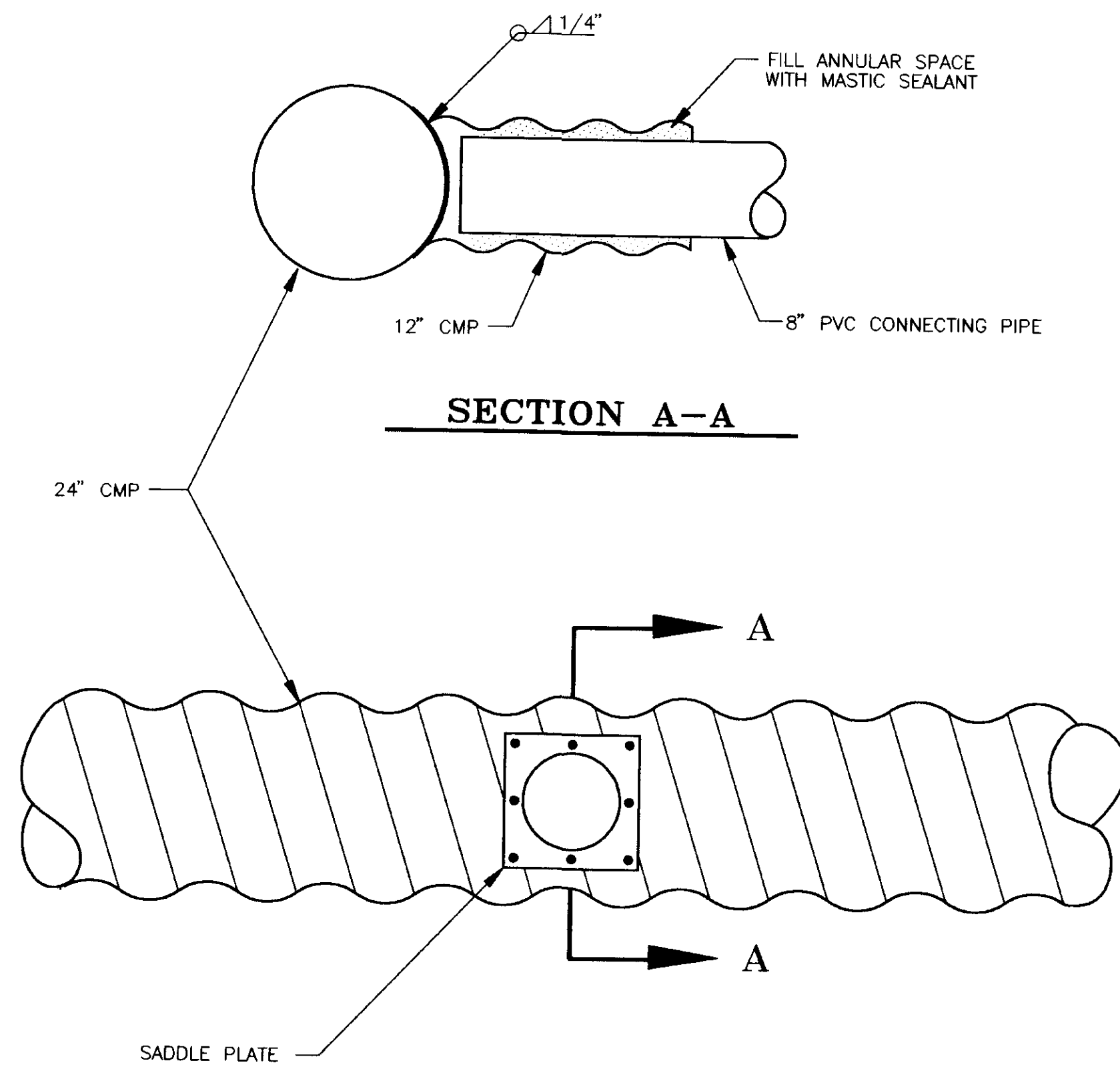


NOTE:  
SLIDING JOINT SHALL BE LOCATED EVERY THIRD POST

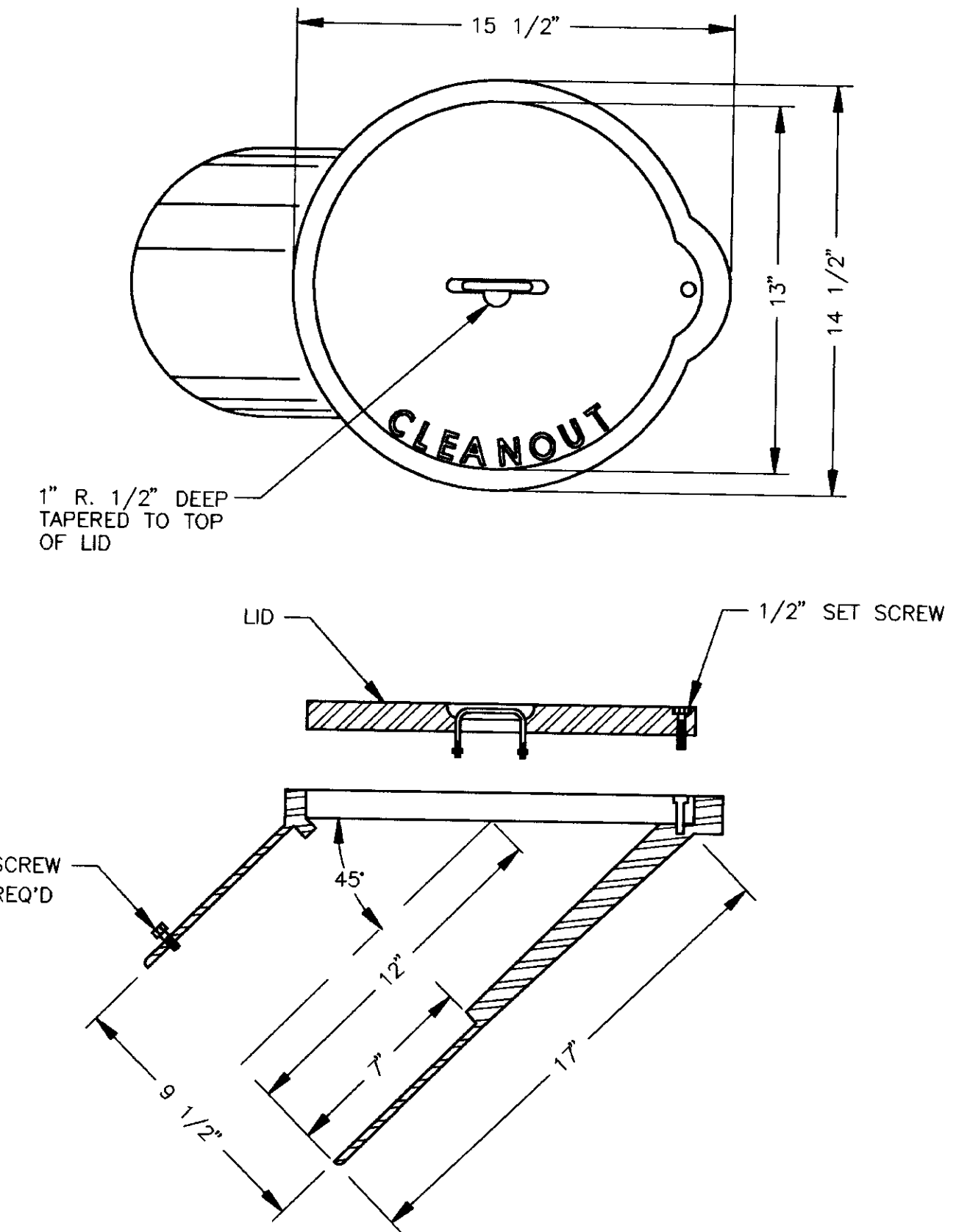
**SLIDING JOINT DETAIL**



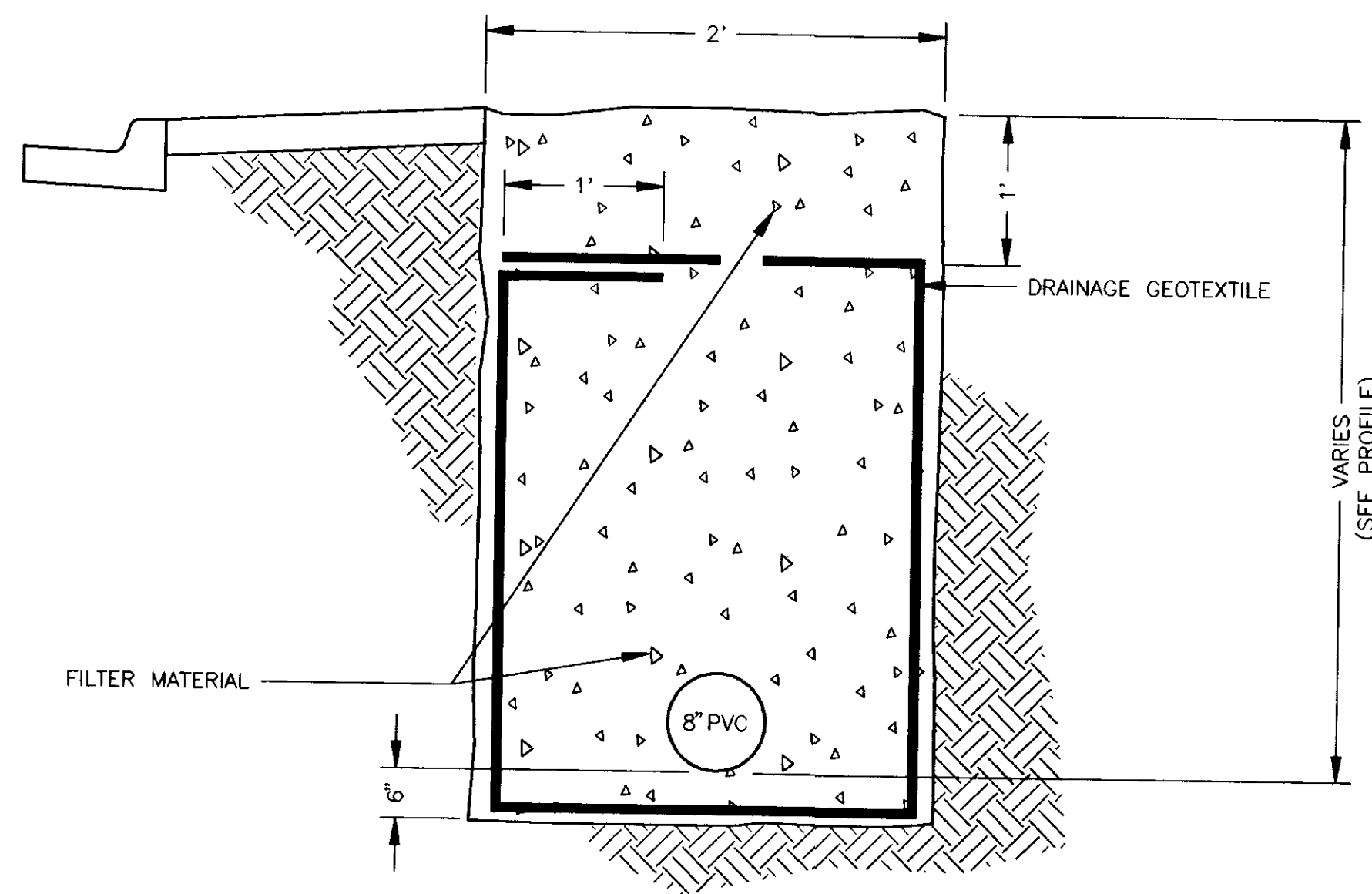
**HAND RAIL SECTION**



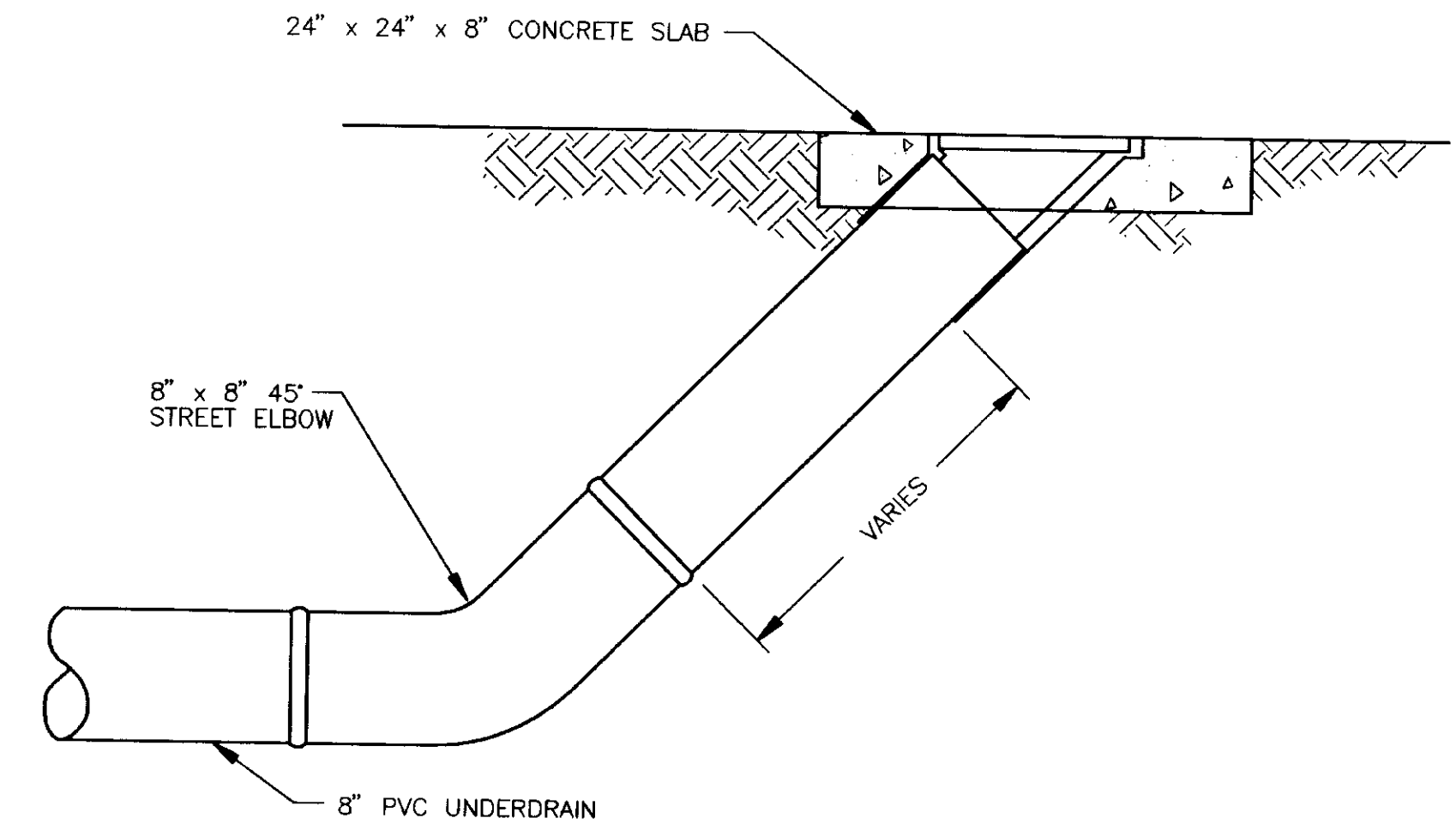
**SADDLE PLATE UNIVERSAL CONNECTION**



**CLEANOUT FRAME AND COVER**



**MODIFIED UNDERDRAIN**



**UNDER DRAIN CLEANOUT**

NO.	DATE	DESCRIPTION OF CHANGE

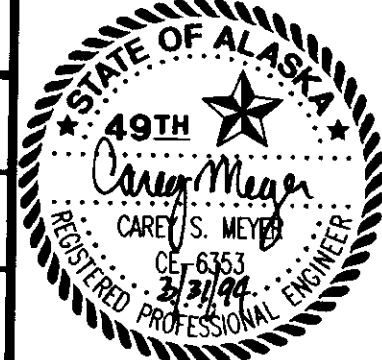
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

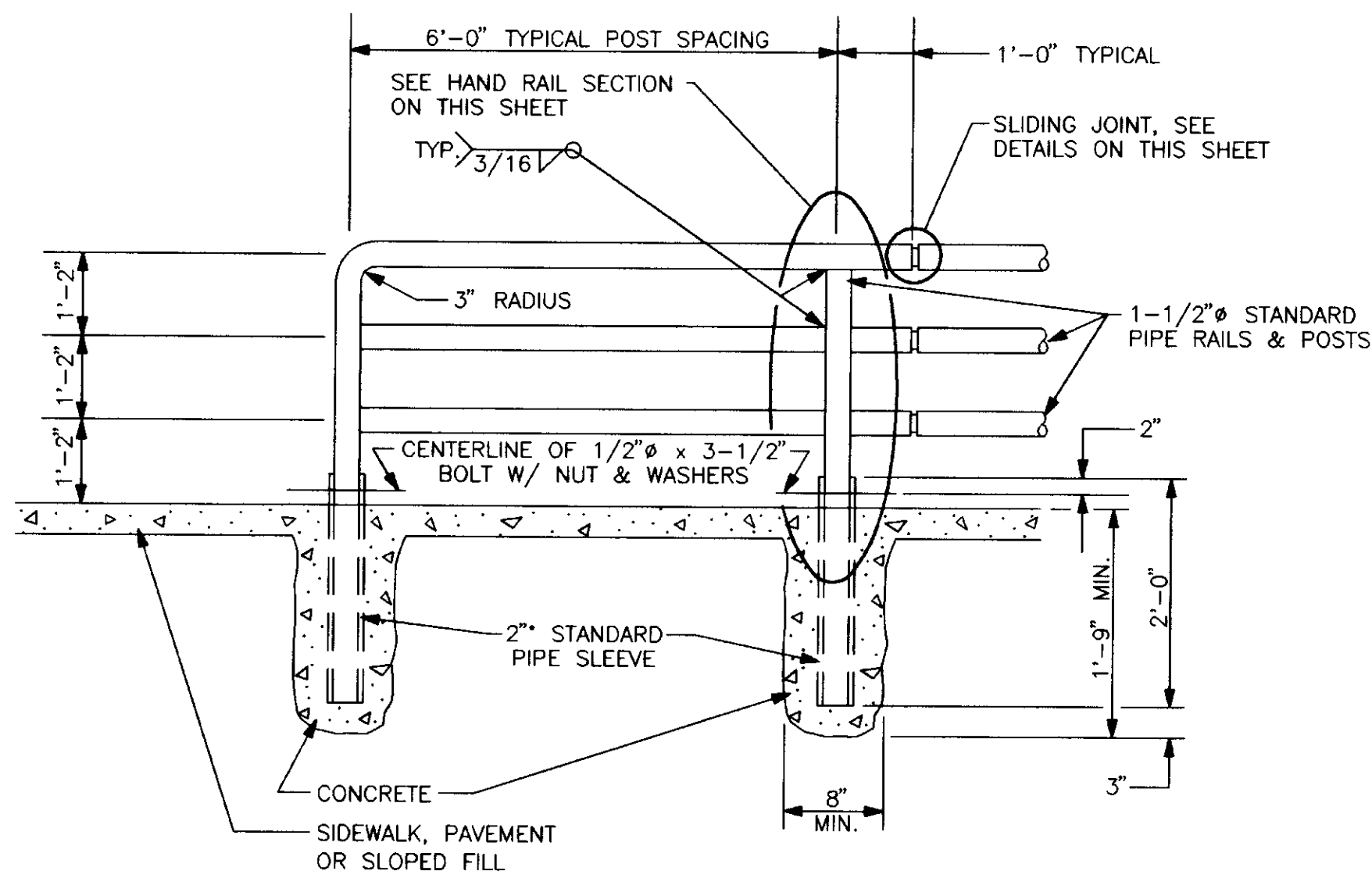
JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
CONSTRUCTION DETAILS

**ASCG**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

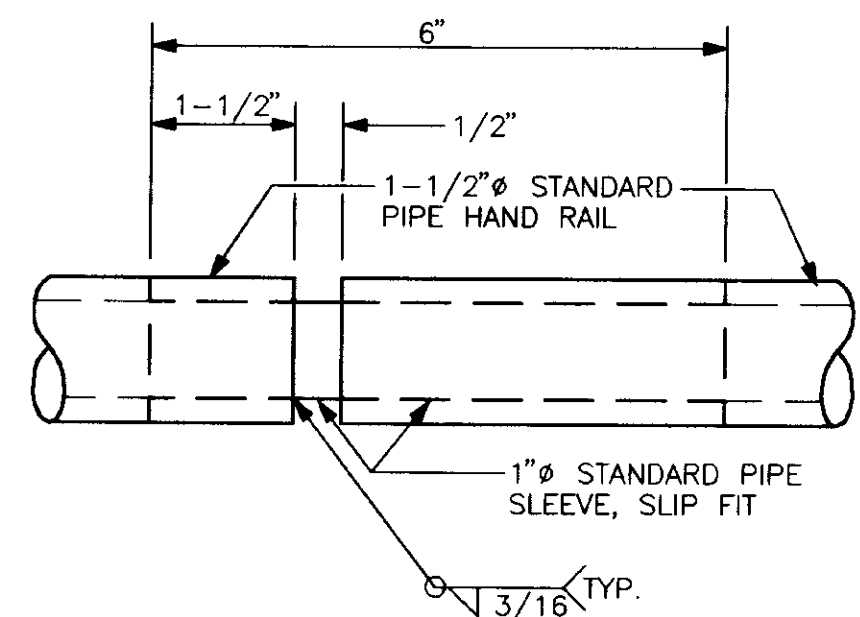
DESIGNED BY: D.L.M.  
DRAWN BY: C.M.B.  
CHECKED BY: D.L.M.

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS  
PLOT IN AUTOCAD RELEASE 11  
DRAWING PLOT SCALE: 1" = 1'  
DRAWING LAST UPDATED: 3/17/1994  
PROJECT NO: 69062  
DATE: MARCH 1994  
SHEET 40 OF 55



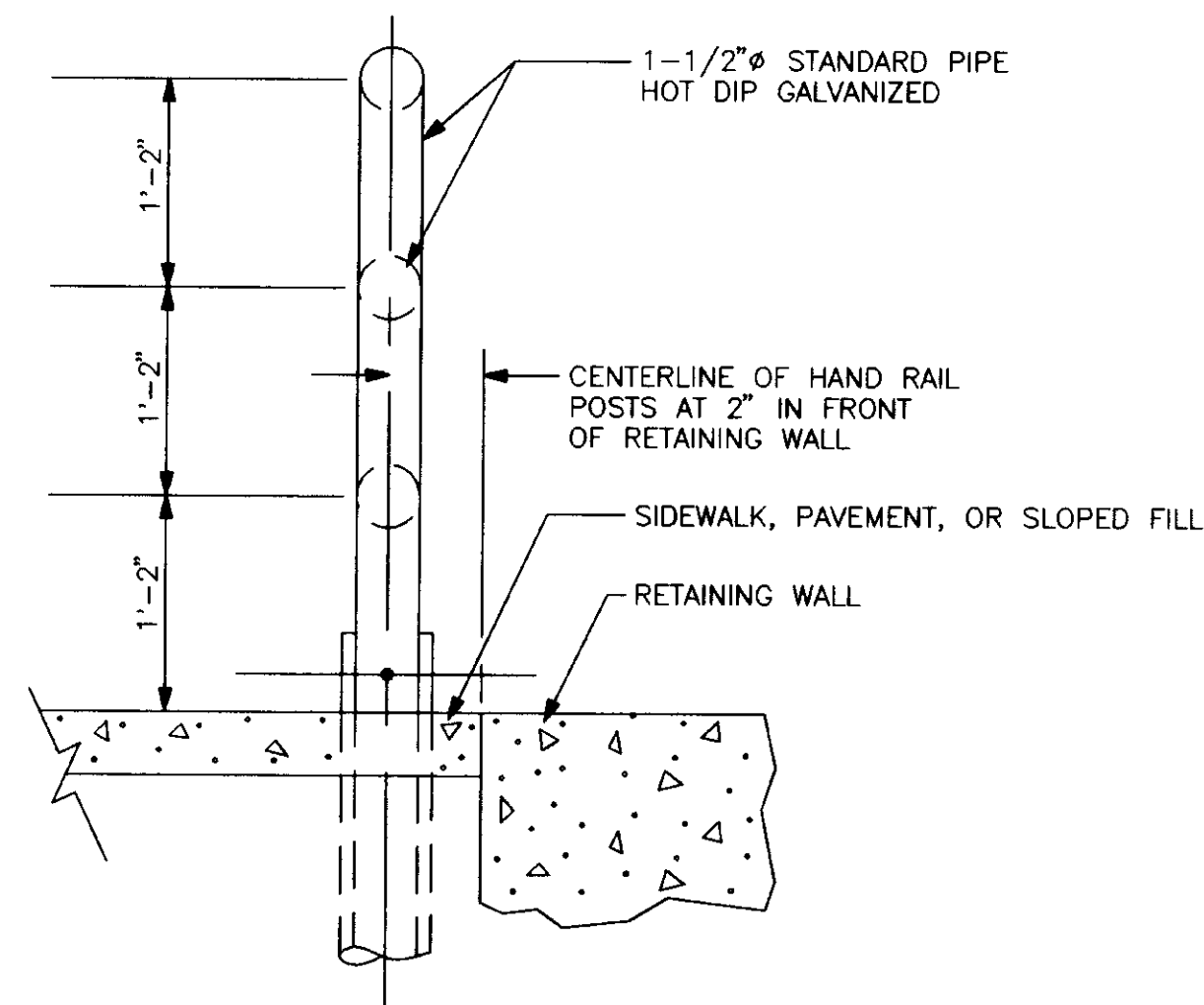


**TYPICAL HAND RAIL INSTALLATION DETAIL**

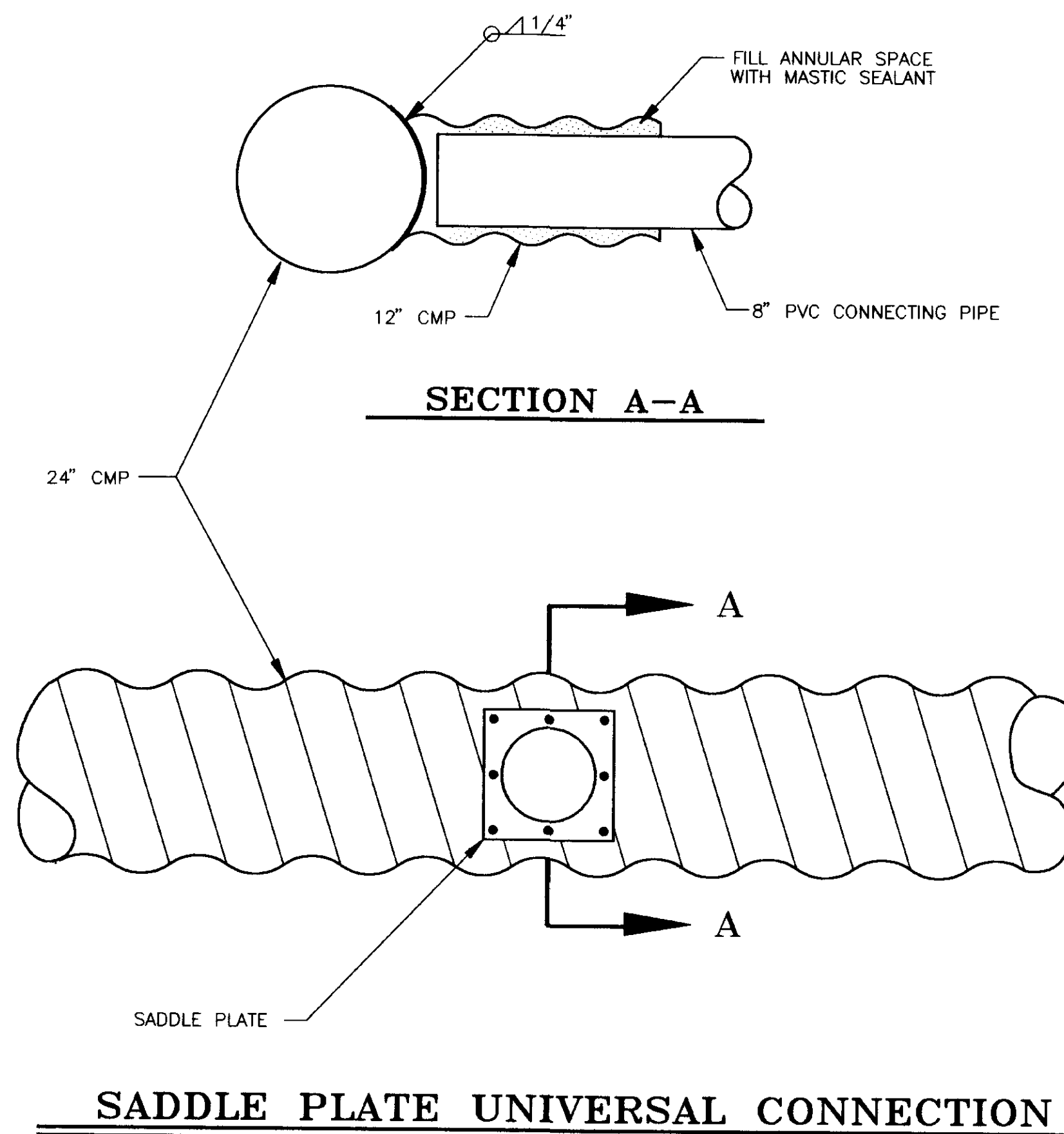


NOTE: SLIDING JOINT SHALL BE LOCATED EVERY THIRD POST

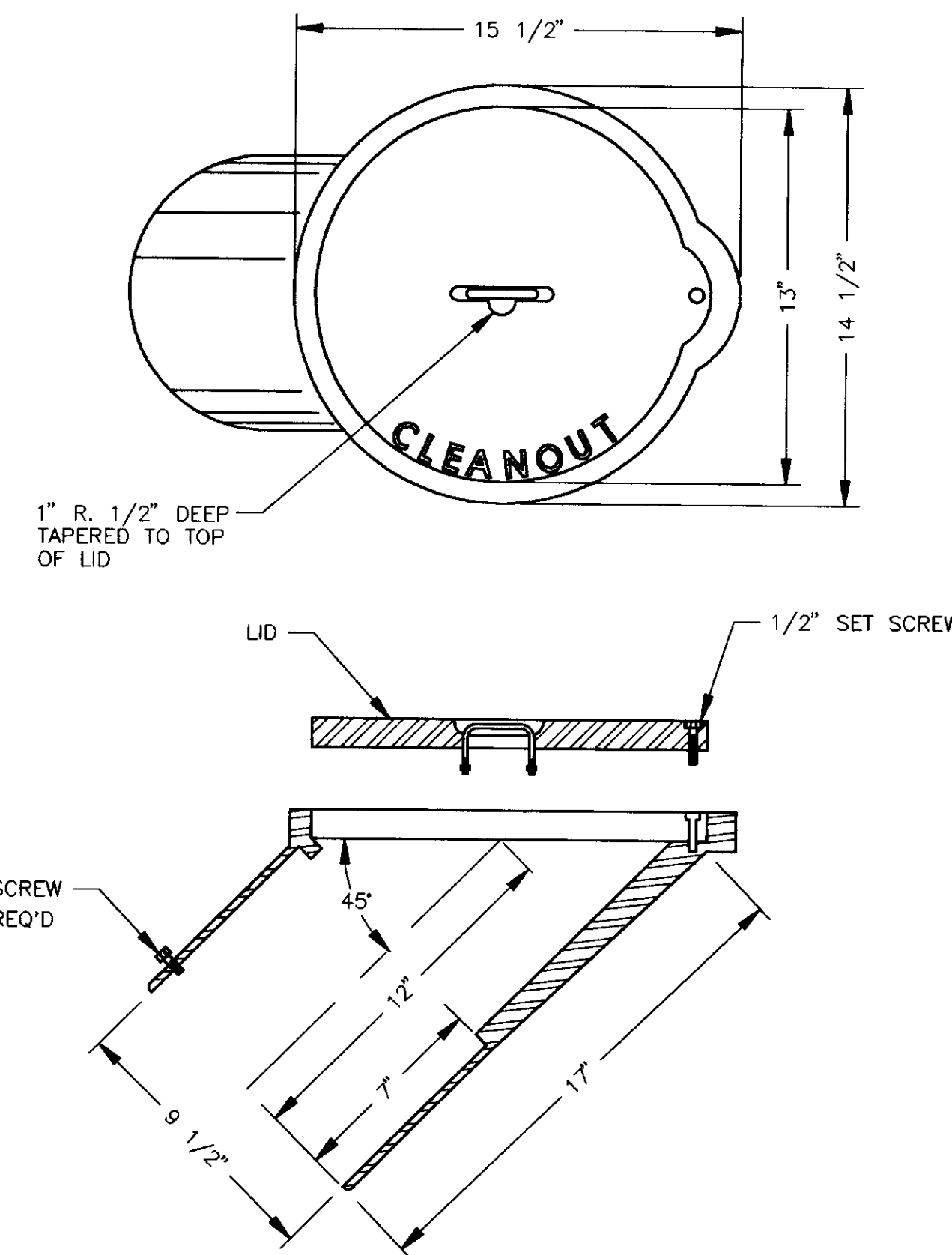
**SLIDING JOINT DETAIL**



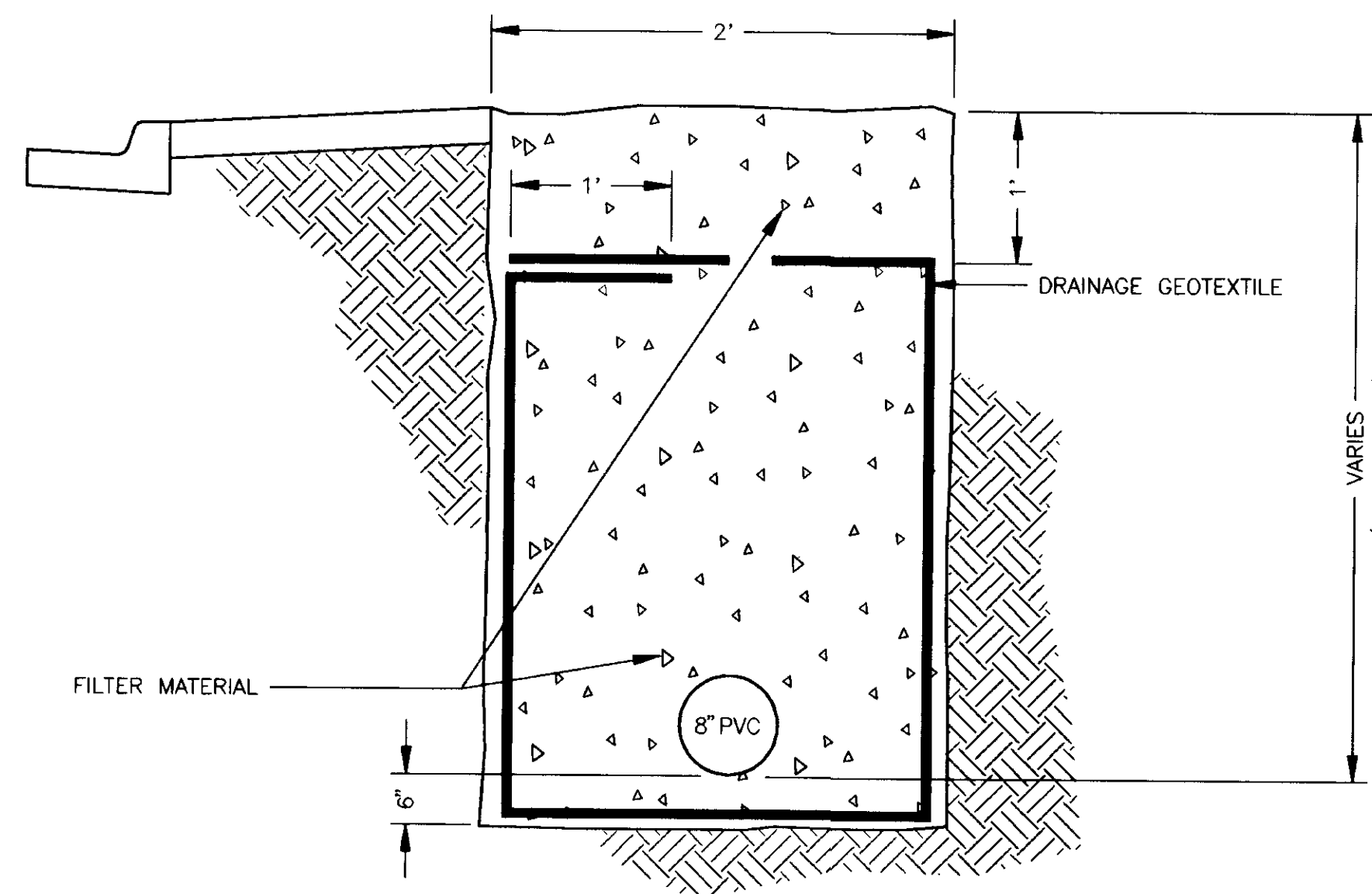
**HAND RAIL SECTION**



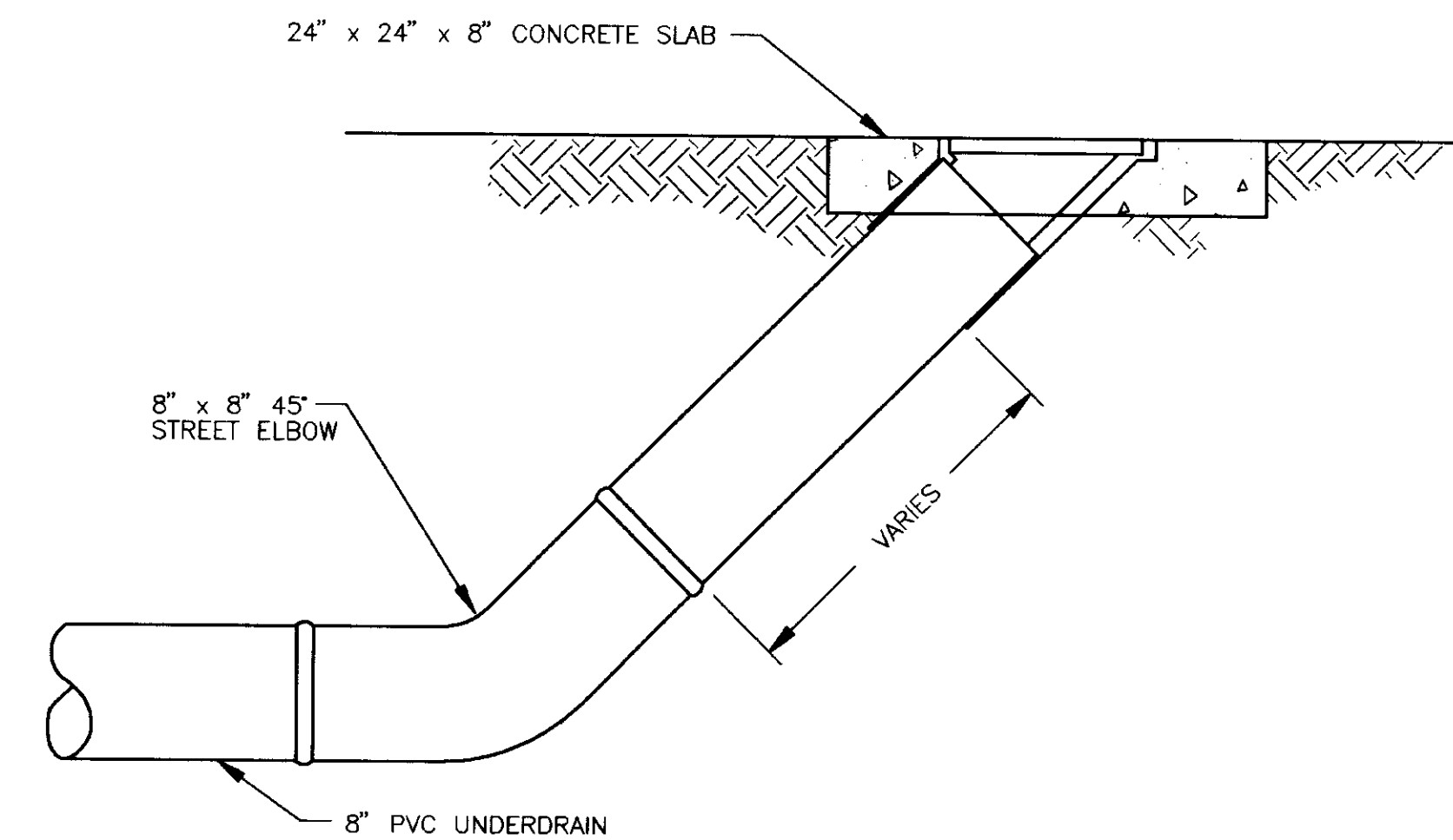
**SADDLE PLATE UNIVERSAL CONNECTION**



**CLEANOUT FRAME AND COVER**



**MODIFIED UNDERDRAIN**



**UNDER DRAIN CLEANOUT**

NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

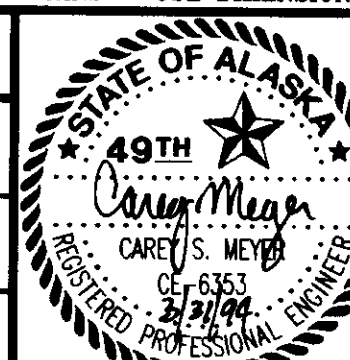
JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 CONSTRUCTION DETAILS

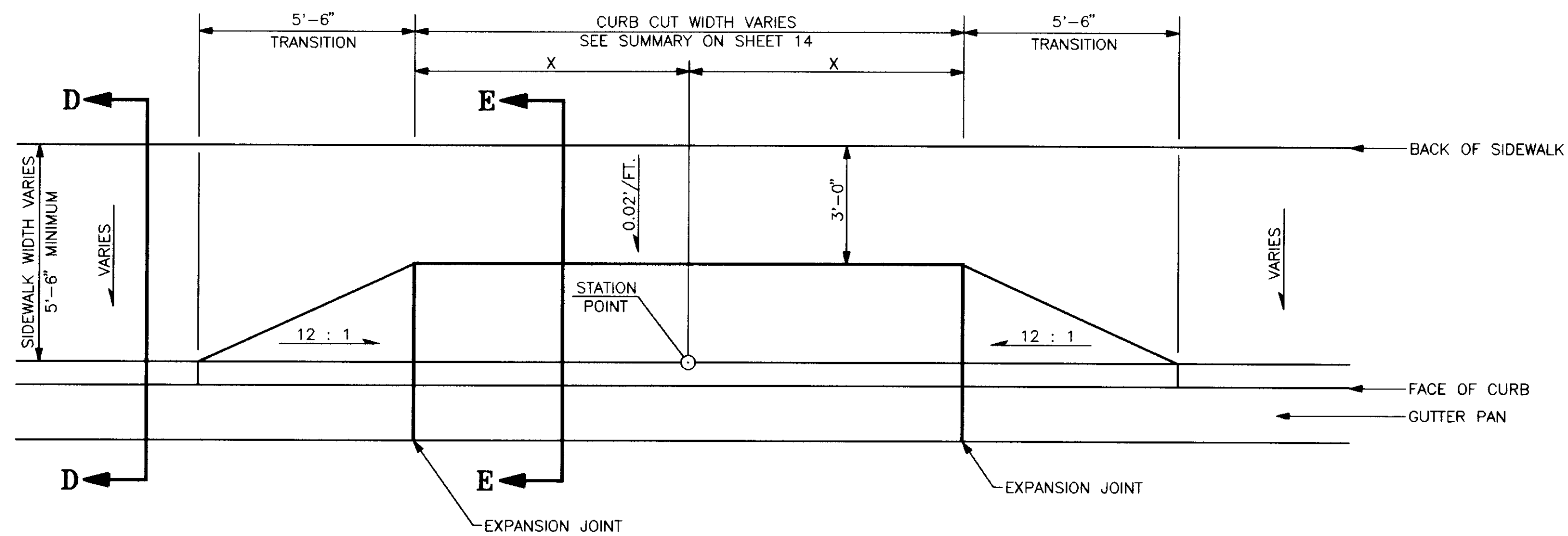
**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
 DRAWN BY: C.M.B.  
 CHECKED BY: D.L.M.

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

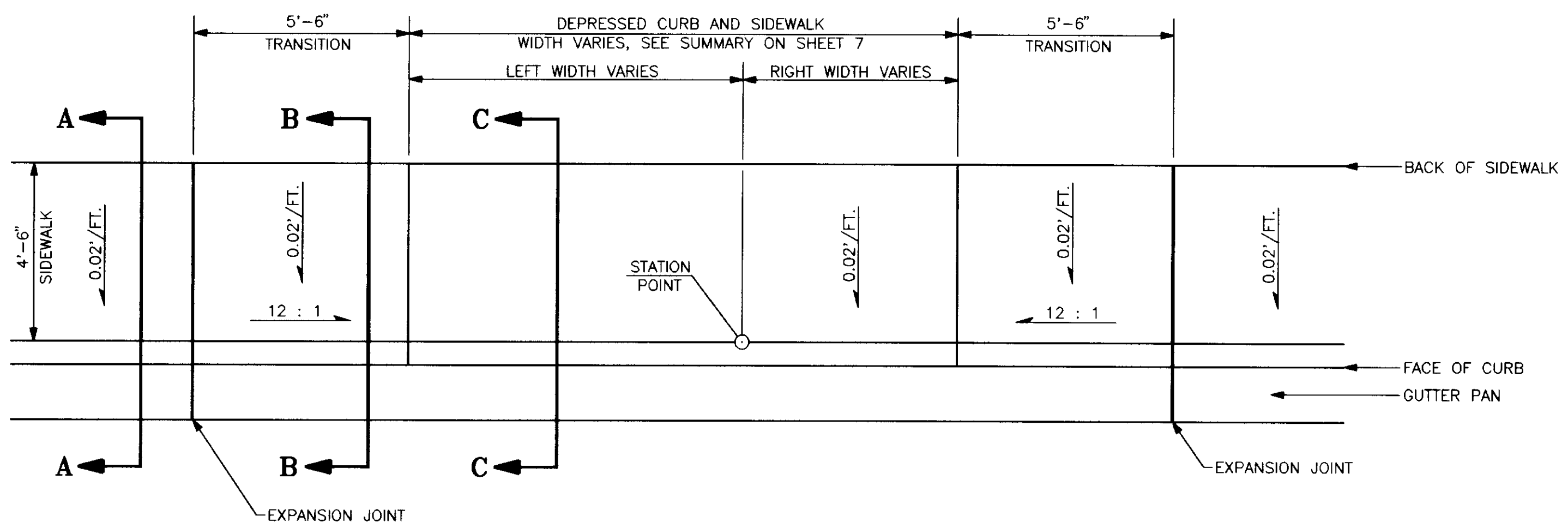
PROJECT NO:  
 69062  
 DATE:  
 MARCH 1994  
 SHEET 40 OF 55





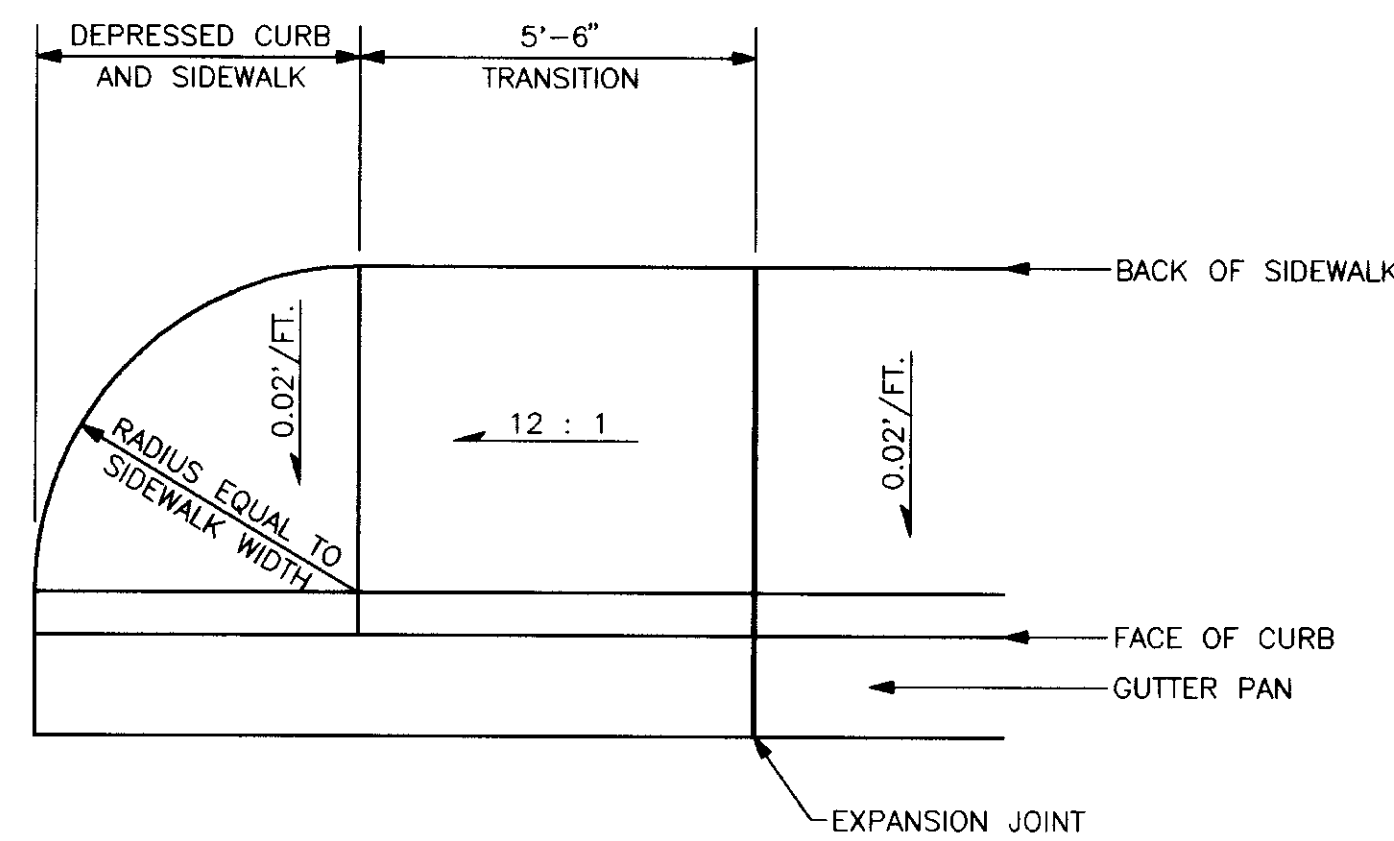
**CURB CUT AT DRIVEWAYS**

B.O.P. TO ROSS WAY, RIGHT

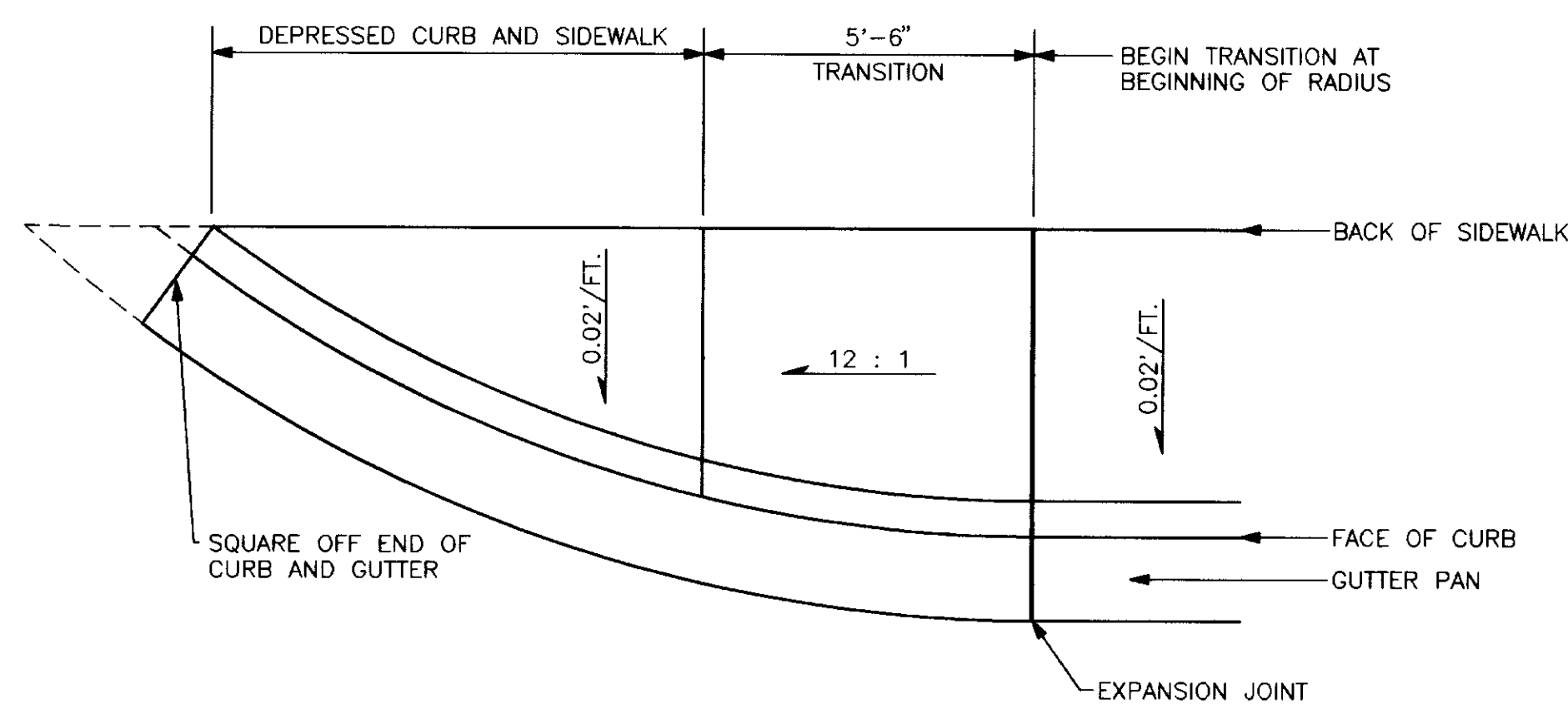


**DEPRESSED CURB AND SIDEWALK AT DRIVEWAYS**

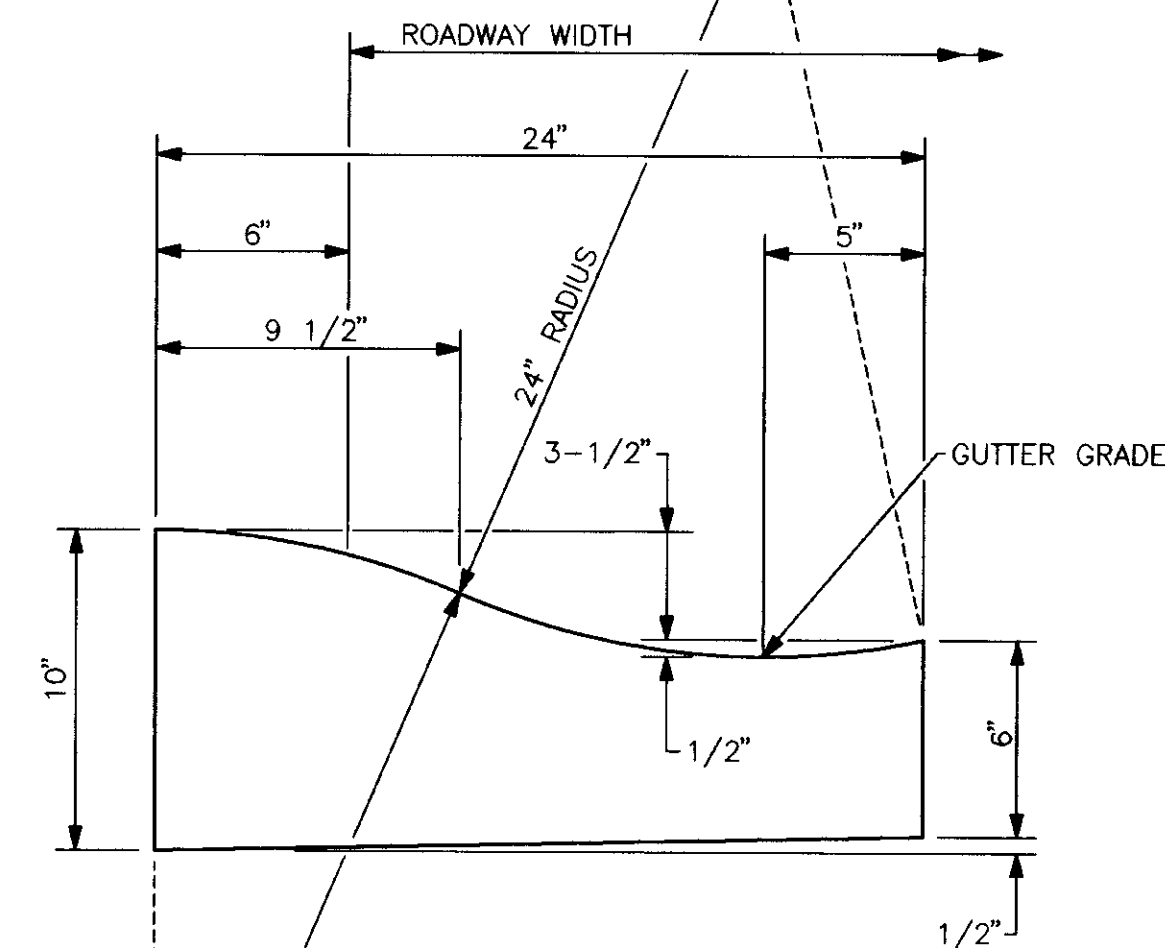
ROSS WAY TO E.O.P., RIGHT



**WHEELCHAIR RAMP AT SIDEWALK END**

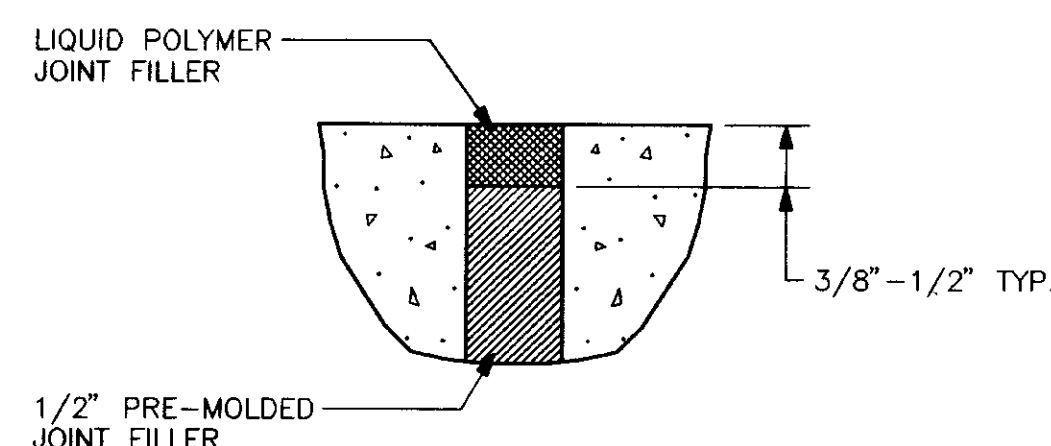


**WHEELCHAIR RAMP AT SIDEWALK END**

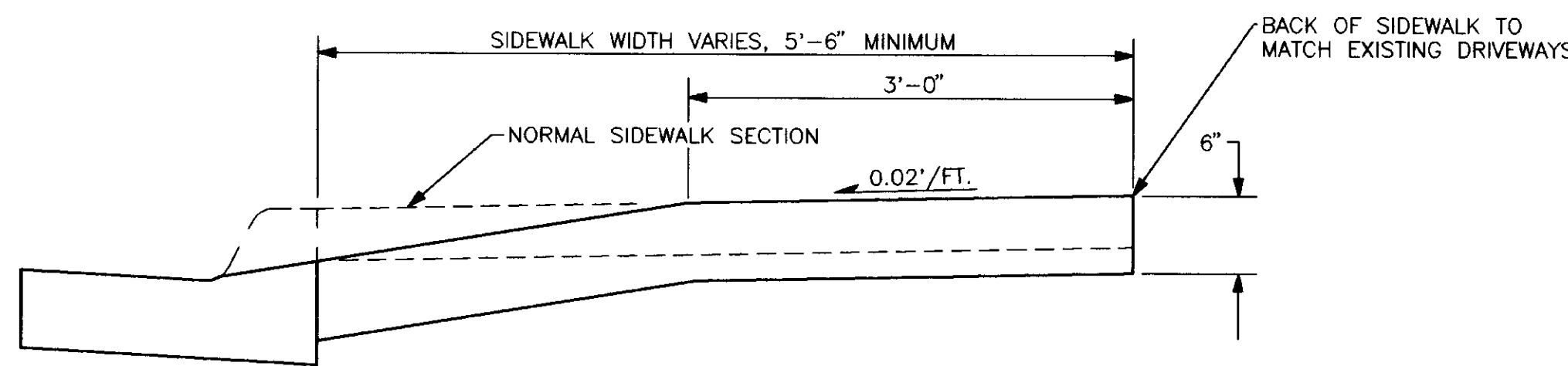


**ROLLED CURB DETAIL**

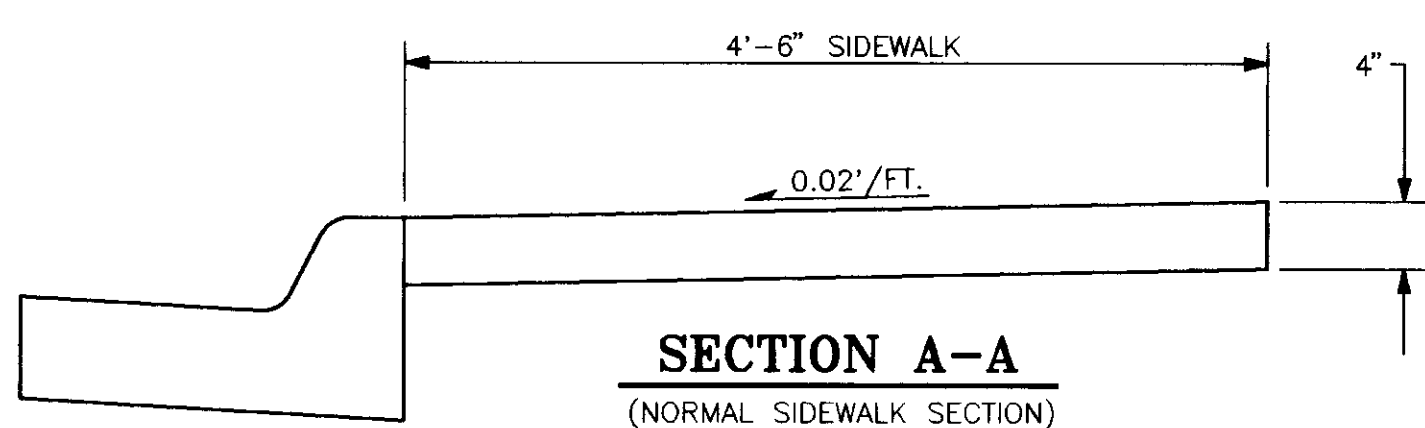
STA. 11+44.69 TO STA. 13+62.68, LEFT



**EXPANSION JOINT DETAIL**

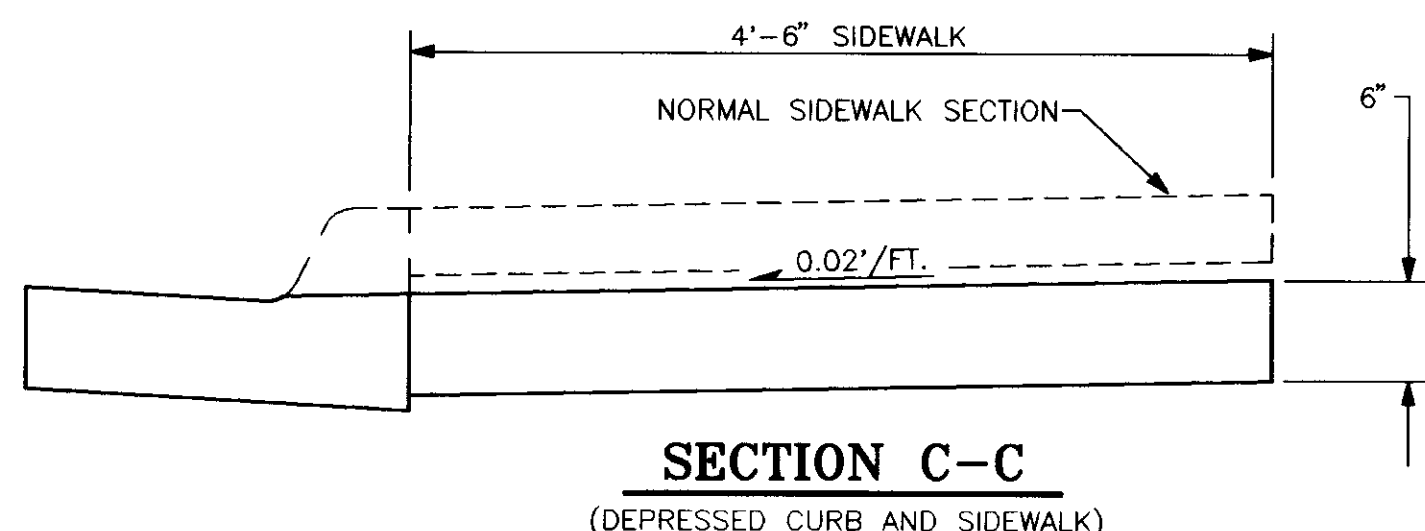


**SECTION E-E**



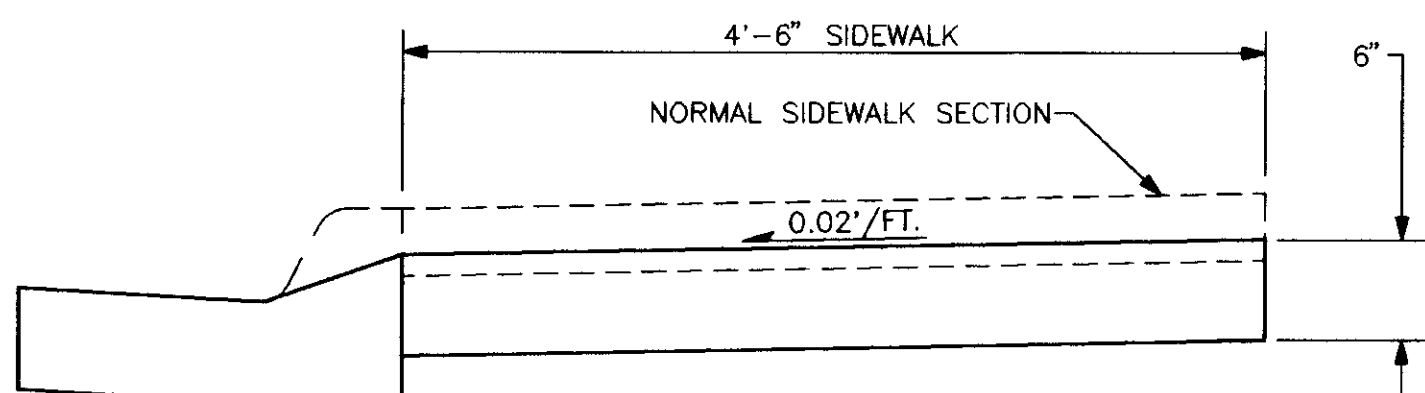
**SECTION A-A**

(NORMAL SIDEWALK SECTION)



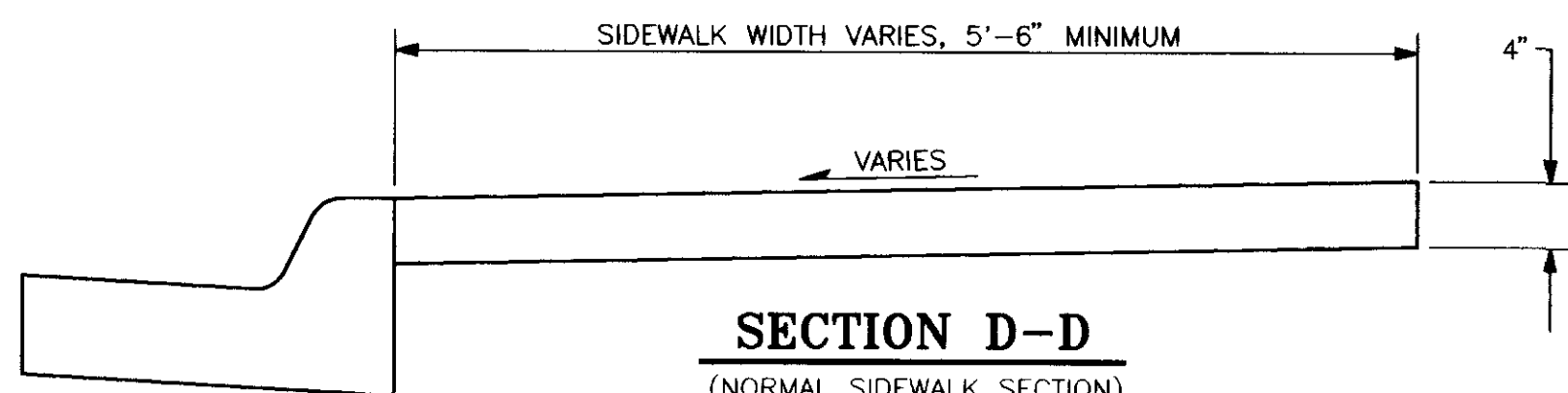
**SECTION C-C**

(DEPRESSED CURB AND SIDEWALK)



**SECTION B-B**

(TRANSITION AREA)



**SECTION D-D**

(NORMAL SIDEWALK SECTION)

NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
CONSTRUCTION DETAILS

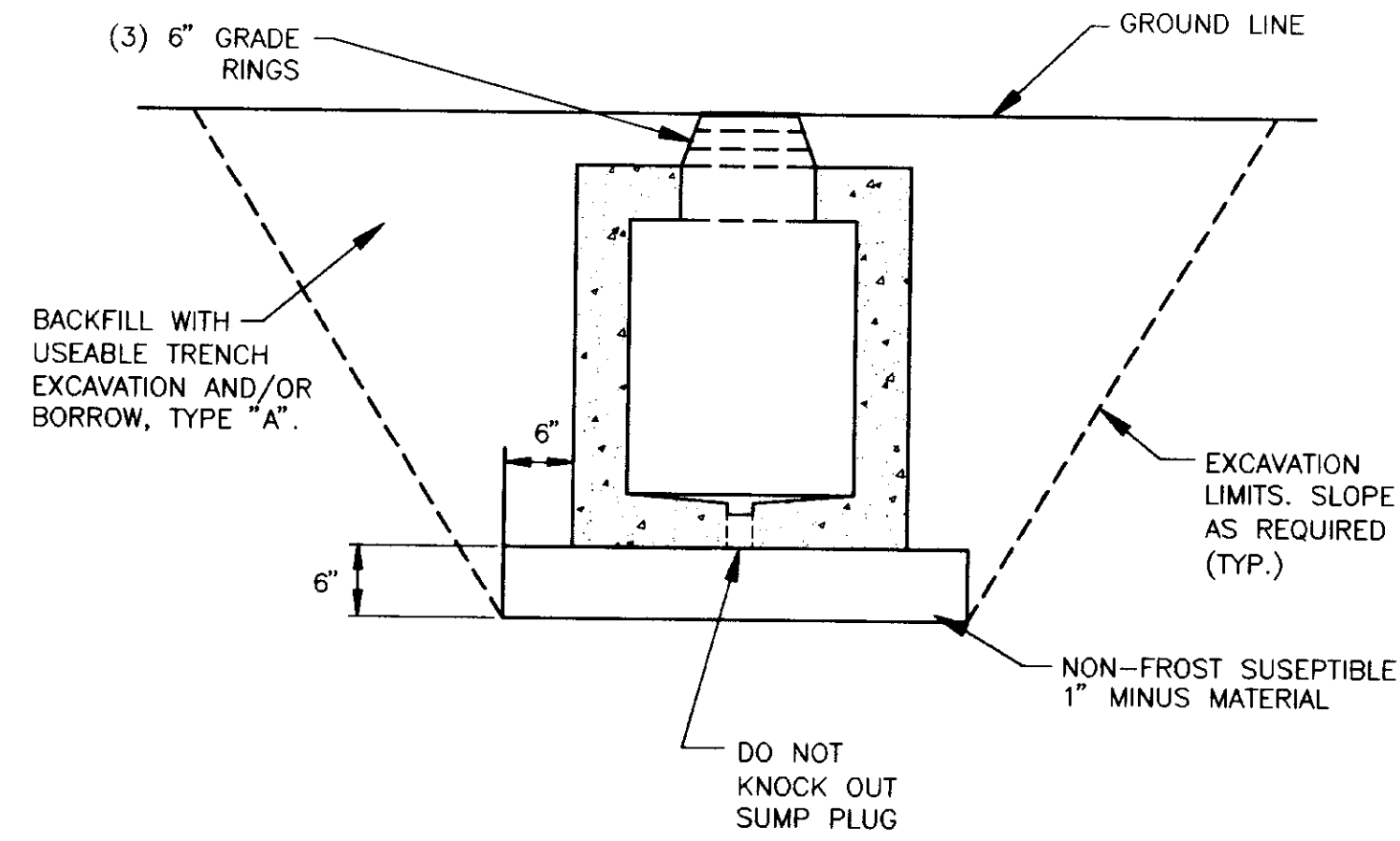
**ASCG**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

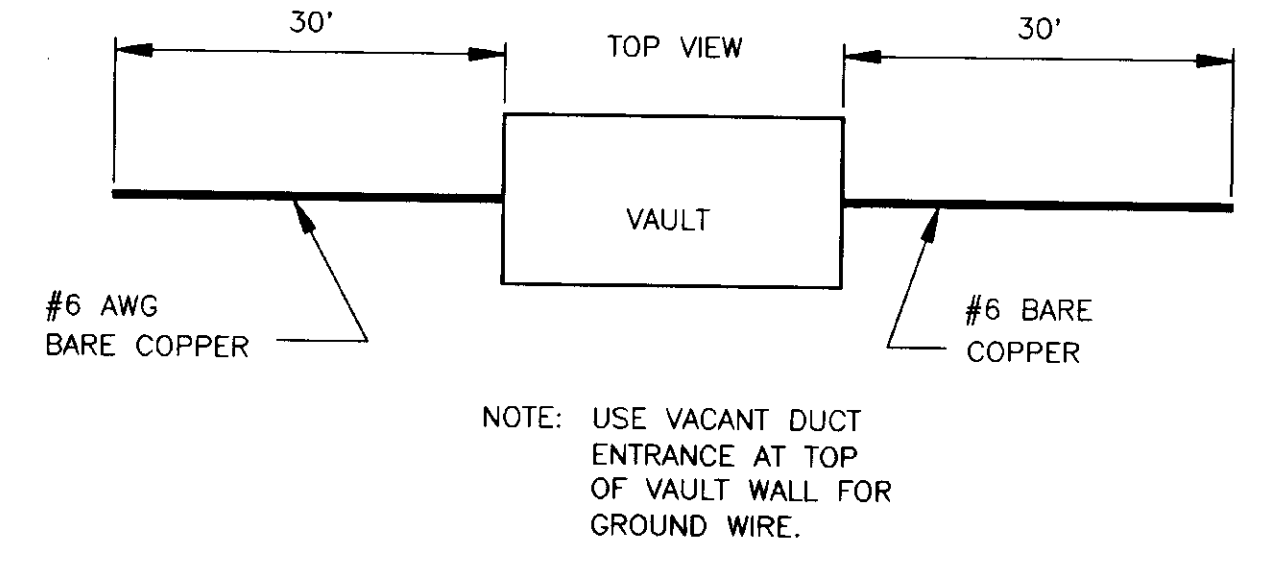
PROJECT NO:  
69062  
DATE:  
MARCH 1994  
SHEET 41 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



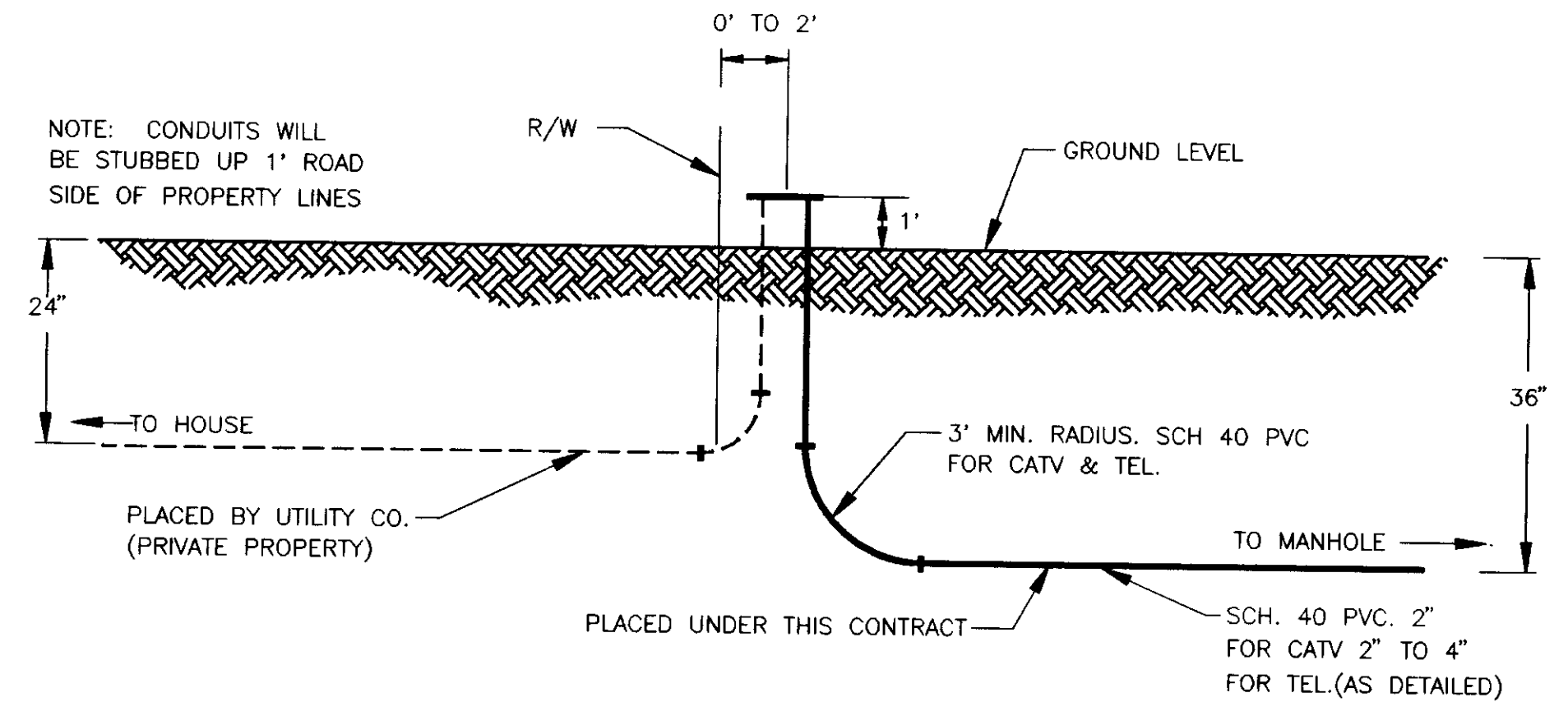
**VAULT EXCAVATION DETAIL  
(TELEPHONE)**



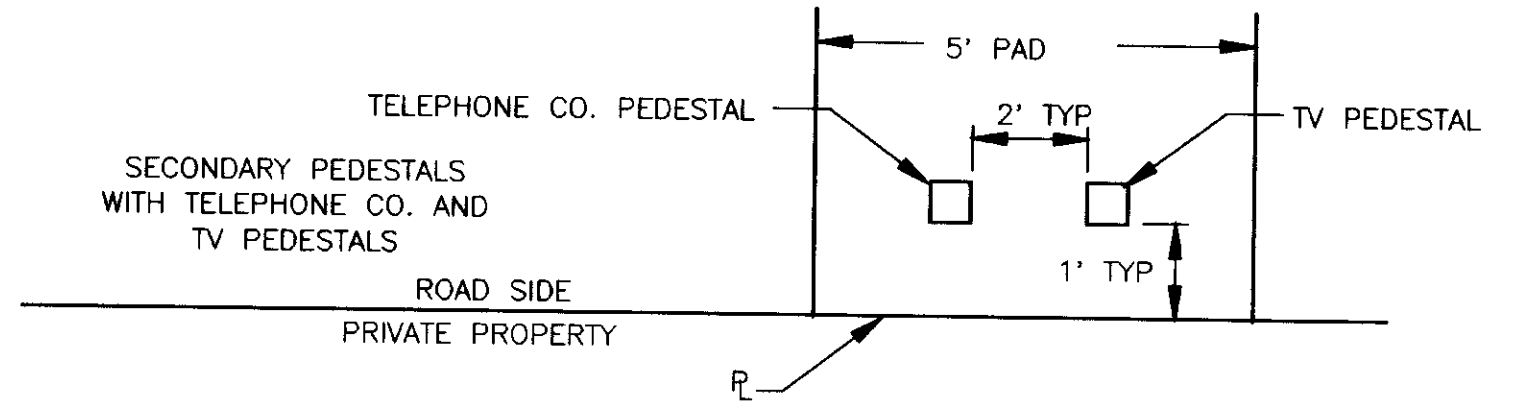
**GROUNDING DETAIL  
(TELEPHONE)**

**TELEPHONE VAULT NOTES:**

1. EACH VAULT WILL BE COMPLETE WITH:  
 3 EA - 6" GRADE RINGS  
 1 EA - BOLT-ON LADDER  
 1 EA - CAST IRON FRAME & COVER  
 TYPE 4210 MARKED "TELEPHONE"  
 1 EA - RACKING PACKAGE  
 8" PERFORATED DRAIN PIPE (SEE DETAIL - INCLUDES PIPE ALL AROUND VAULT, OUTFALL PIPE AND ALL CONNECTIONS)  
 SUBSURFACE DRAINAGE GEOTEXTILE.
2. ALL VAULTS WILL BE GROUTED WITH SINK-A-PLUG OR EQUIVALENT SEALER.
3. INSTALL DUCT SEALS OR PLUGS AS REQUIRED.
4. EACH VAULT WILL BE GROUNDED (SEE DETAIL).



**DETAIL "B"**



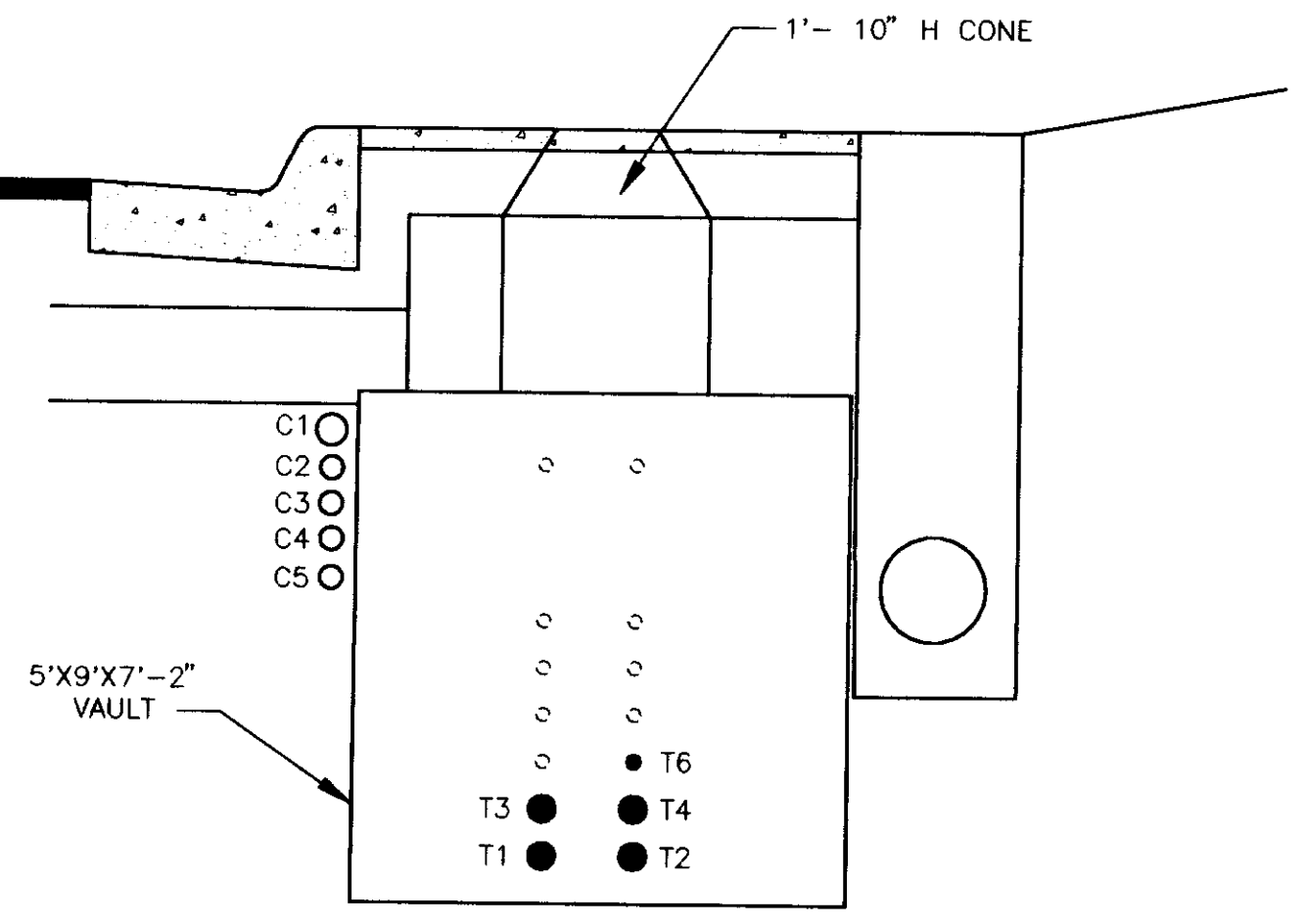
**DETAIL "A"**  
(PRIMARY AND SECONDARY LOCATED IN R.O.W.)

**GENERAL CONDUIT NOTES:**

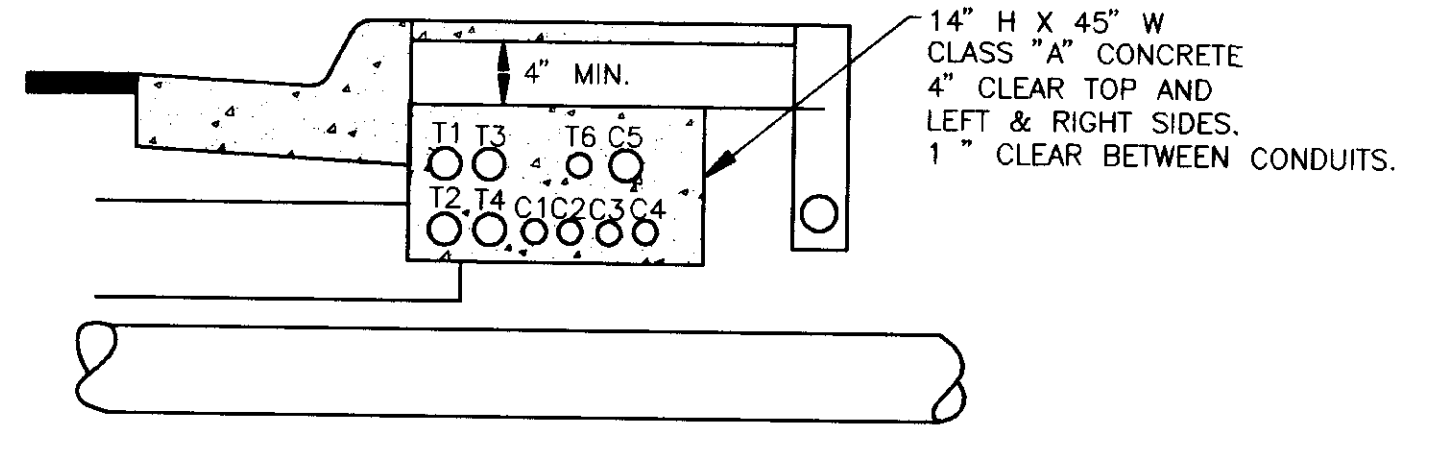
1. ALL DUCTS WILL BE RODDED & CLEANED WITH A TEST MANDREL 1/4" SMALLER IN DIA. THAN THE INSIDE DIA. OF THE DUCT.
2. ALL DUCTS WILL INCLUDE PULL STRINGS.
3. ENCASEMENT OF CRUSHED AGGREGATE WILL CONSIST OF A 4" MIN. COVER OVER THE CONDUITS, 1.5" MIN. AT THE SIDES AND 3" MIN. FOR THE BASE.
4. CONDUIT WILL BE SCHEDULE 40 PVC WITH 3' MIN. BEND RADIUS. SIZE OF CONDUIT WILL BE FROM THE CONDUIT TABLES.
5. THE TELEPHONE & TV PEDESTALS WILL BE ARRANGED AS SHOWN ON DETAILS "A" & "B".
5. OTHER CONDUIT ARRANGEMENTS MAY BE PRESENTED BY THE CONTRACTOR FOR THE ENGINEER'S CONSIDERATION.

**NOTE:**  
SEE SHEET 38 FOR RETAINING WALL SECTION

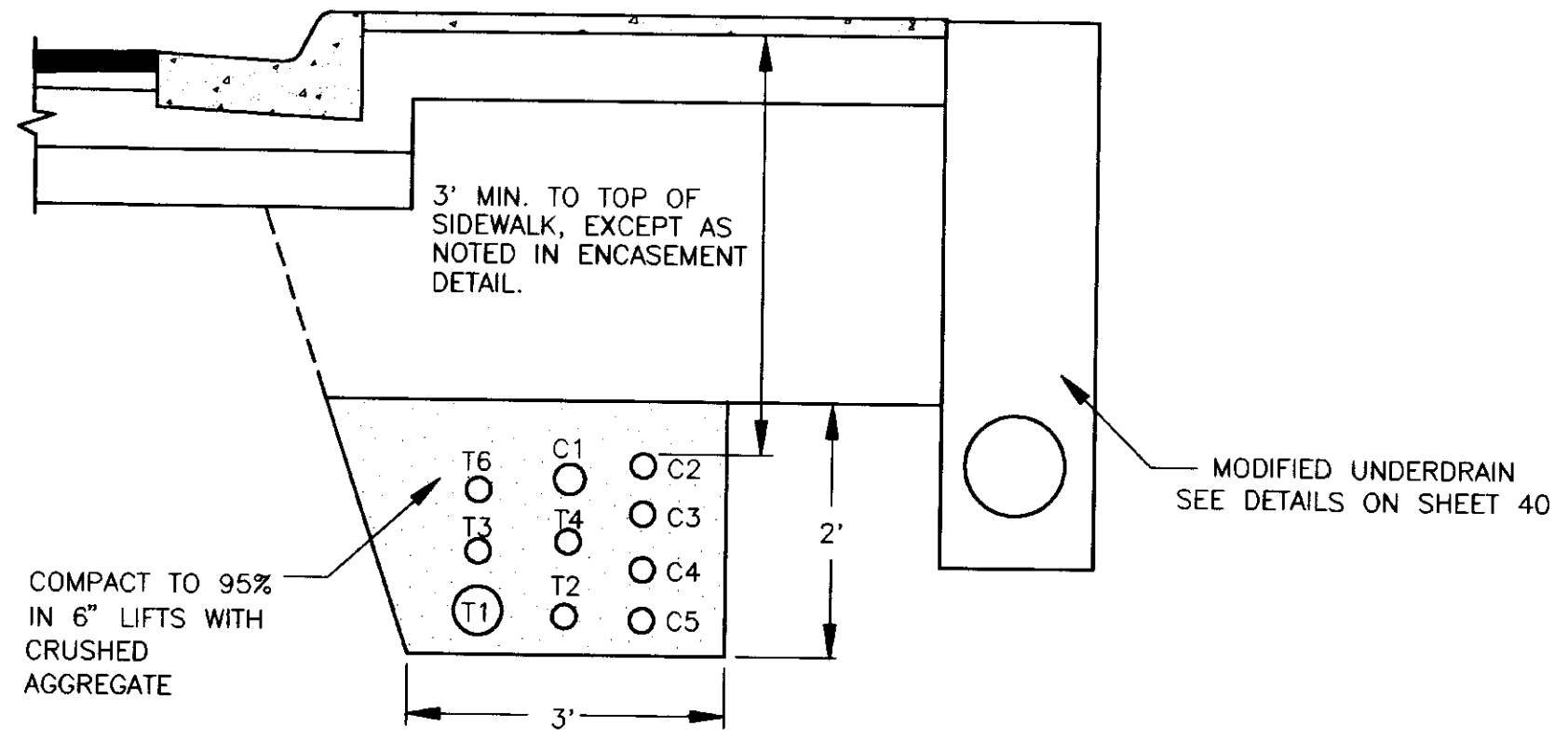
- TV CONDUITS**  
 C1 = TRUNK (3" PVC SCH. 40)  
 C2 = DISTRIBUTION (2" PVC SCH 40)  
 C3 = RETURN (2" PVC SCH 40)  
 C4 = FIBER (2" PVC SCH 40)  
 C5 = SPARE (2" PVC SCH 40)
- TELEPHONE CONDUITS**  
 T1 THRU T4 ARE CONTINUOUS 4" PVC SCH 40  
 T6 = DISTRIBUTION (2" PVC SCH 40)



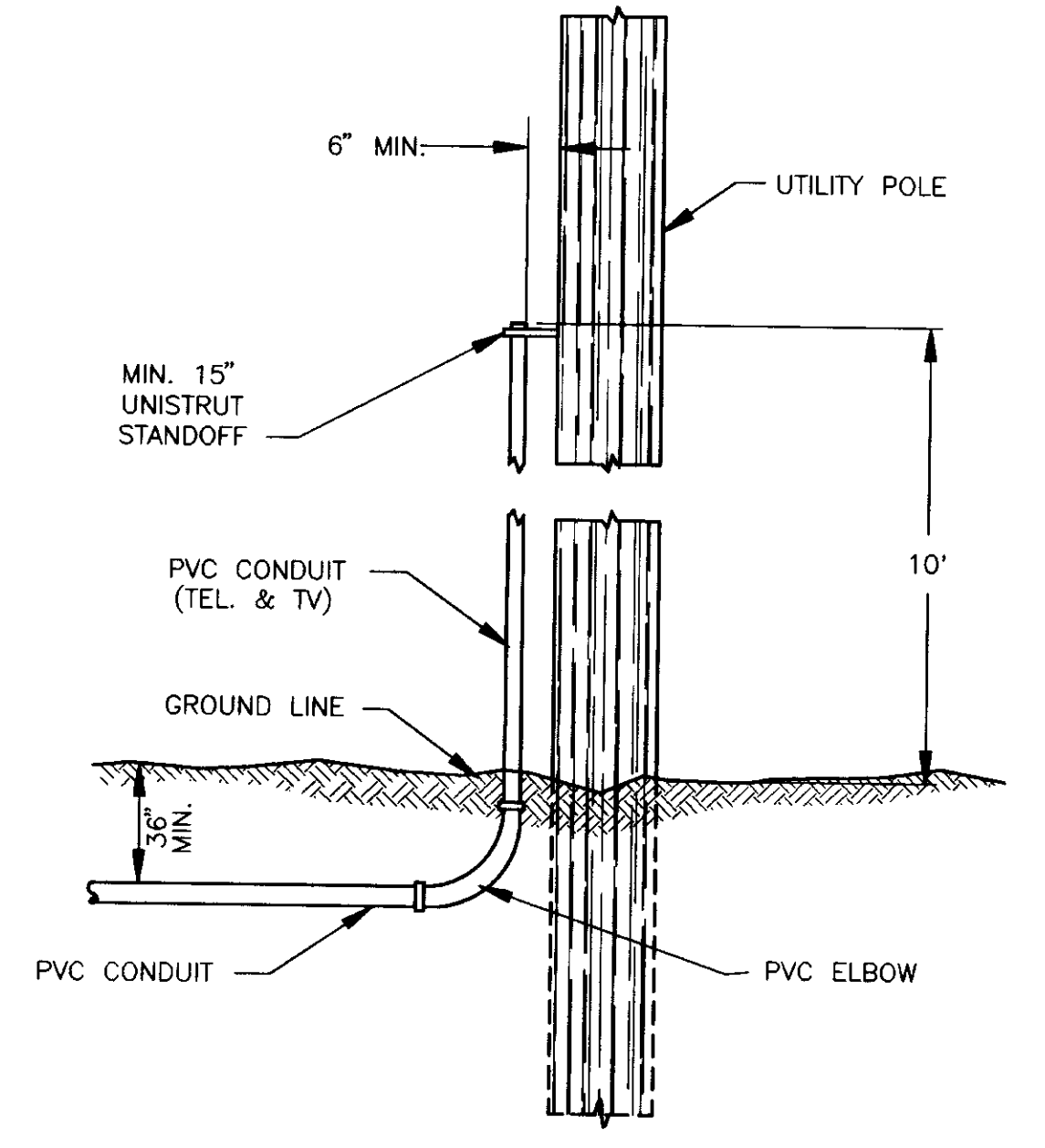
**TYPICAL UTILITIES ROUTING  
AROUND TELEPHONE VAULT**



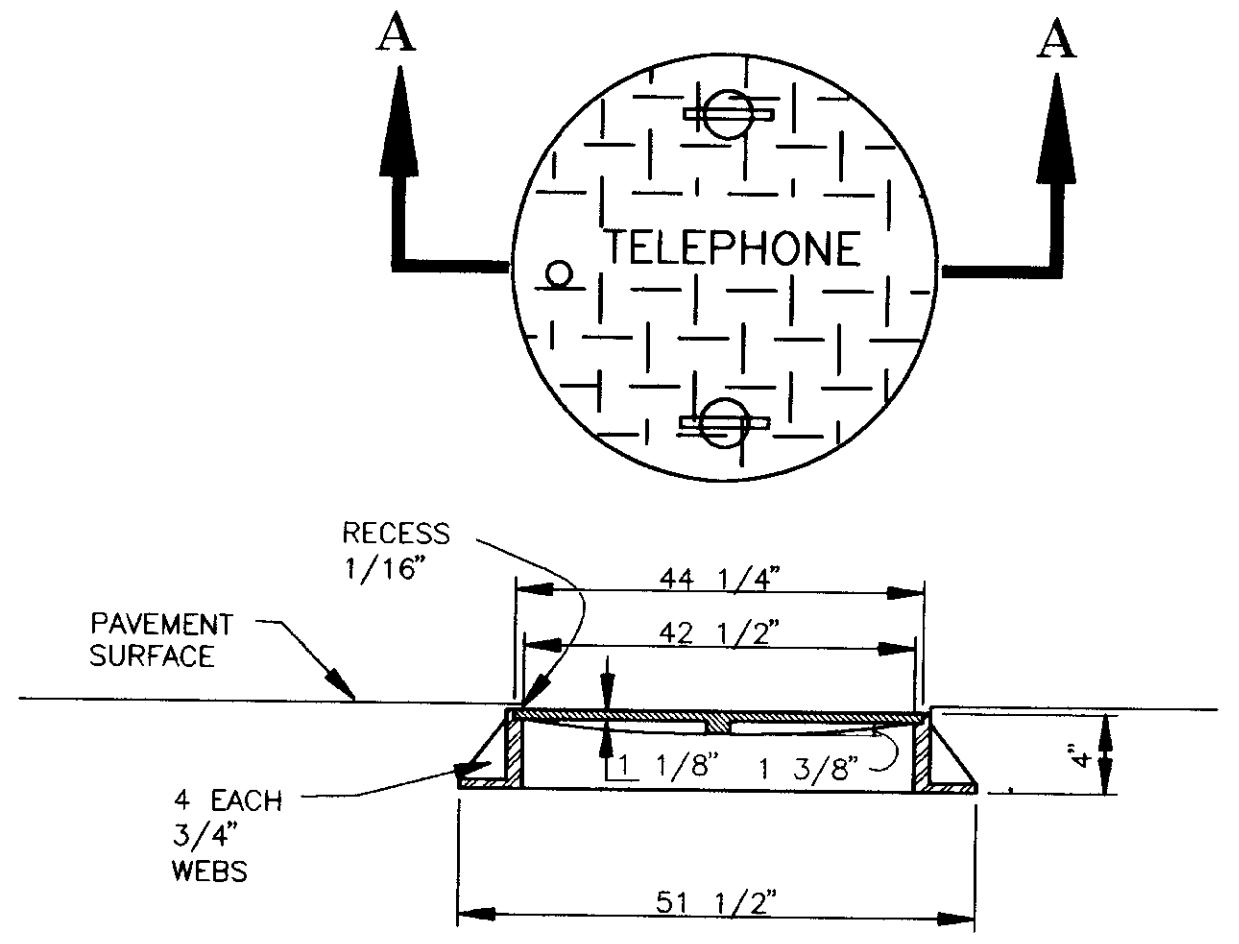
**UTILITIES ENCASEMENT  
OVER SHALLOW CMP CROSSINGS**



**CONDUIT BEDDING DETAIL**



**TYPICAL RISER**



**HIGHWAY VAULT COVER  
UTILITY VAULT 4210-S OR EQUAL**

RECORD OF REVISIONS	
DATE:	DESCRIPTION OF CHANGE:

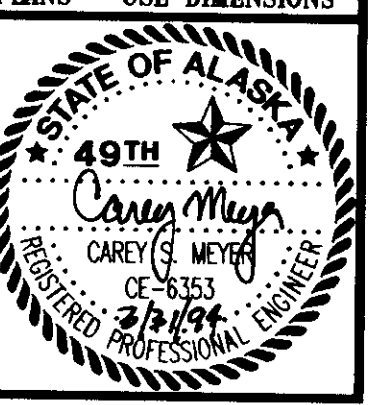
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976 (1) AND 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 UTILITY DETAILS

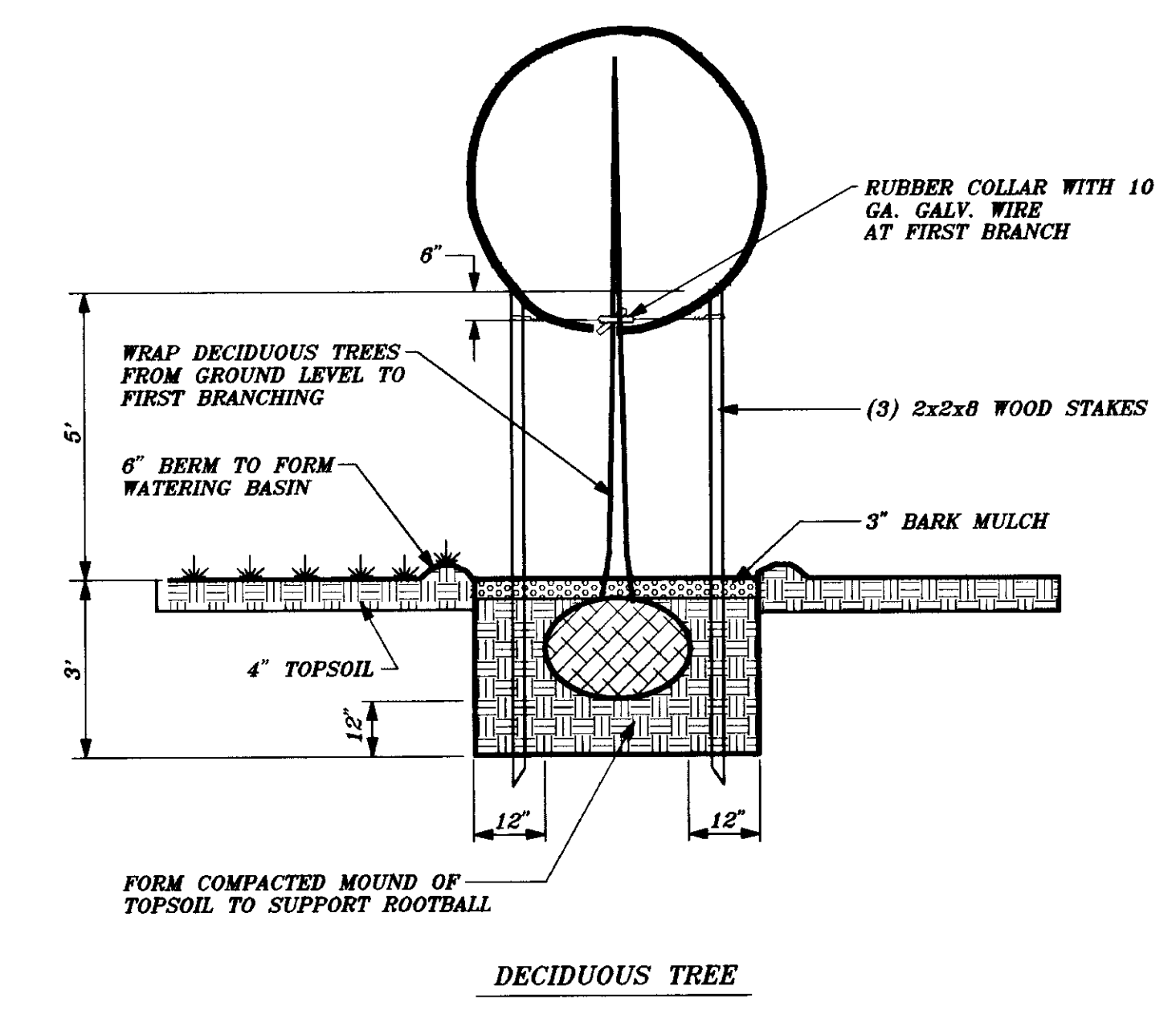
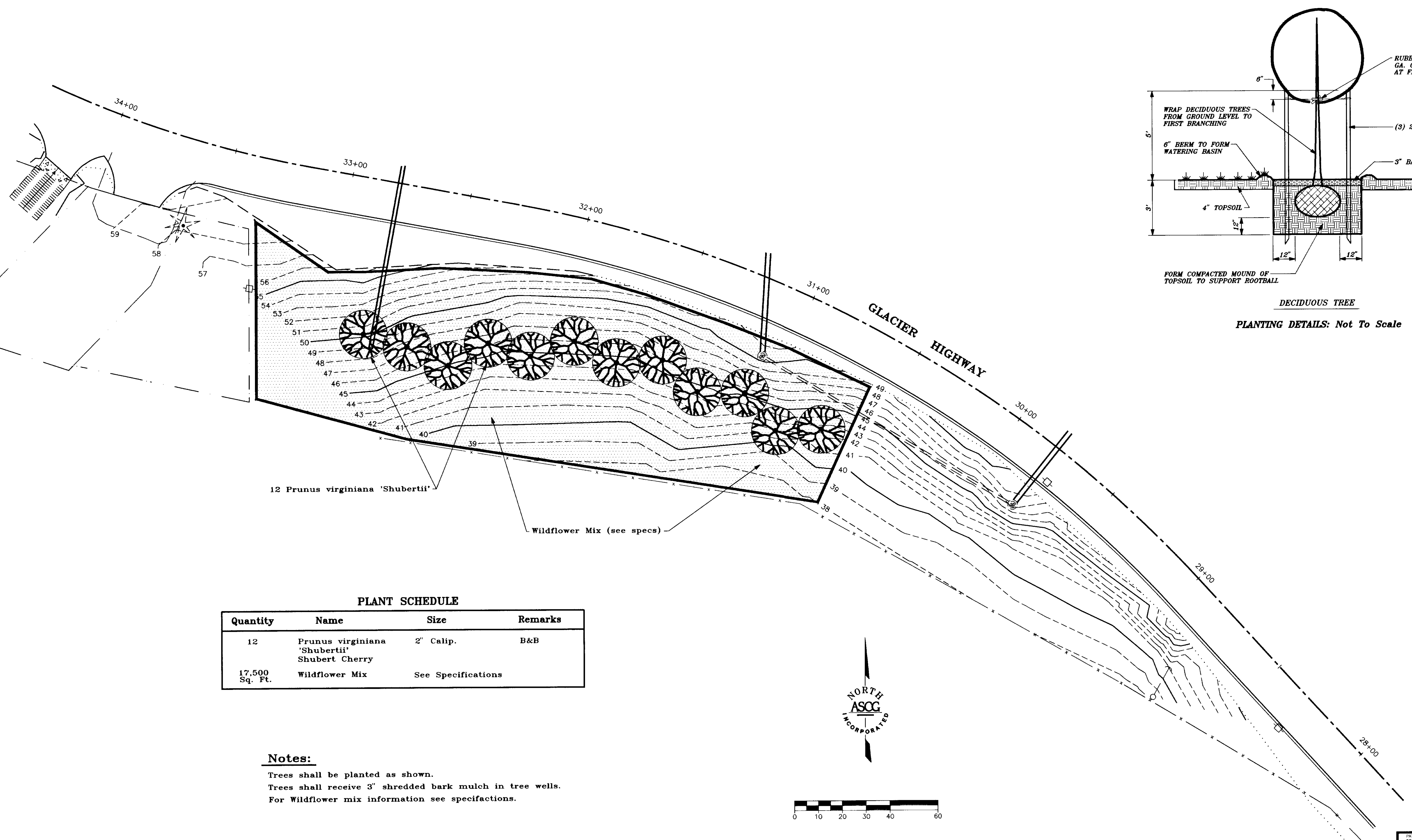
**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
 DRAWN BY: C.M.B.  
 CHECKED BY: D.L.M.

PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 42 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



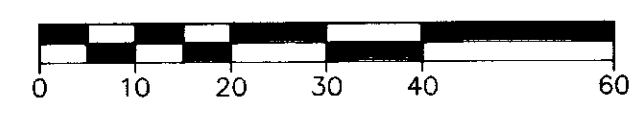
12 *Prunus virginiana* 'Shubertii'

Wildflower Mix (see specs)

**PLANT SCHEDULE**

Quantity	Name	Size	Remarks
12	<i>Prunus virginiana</i> 'Shubertii' Shubert Cherry	2" Calip.	B&B
17,500 Sq. Ft.	Wildflower Mix	See Specifications	

**Notes:**  
 Trees shall be planted as shown.  
 Trees shall receive 3" shredded bark mulch in tree wells.  
 For Wildflower mix information see specifications.



BY:	DATE:	DESCRIPTION OF CHANGE:
<b>RECORD OF REVISIONS</b>		

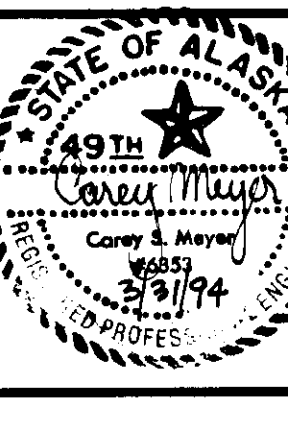
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

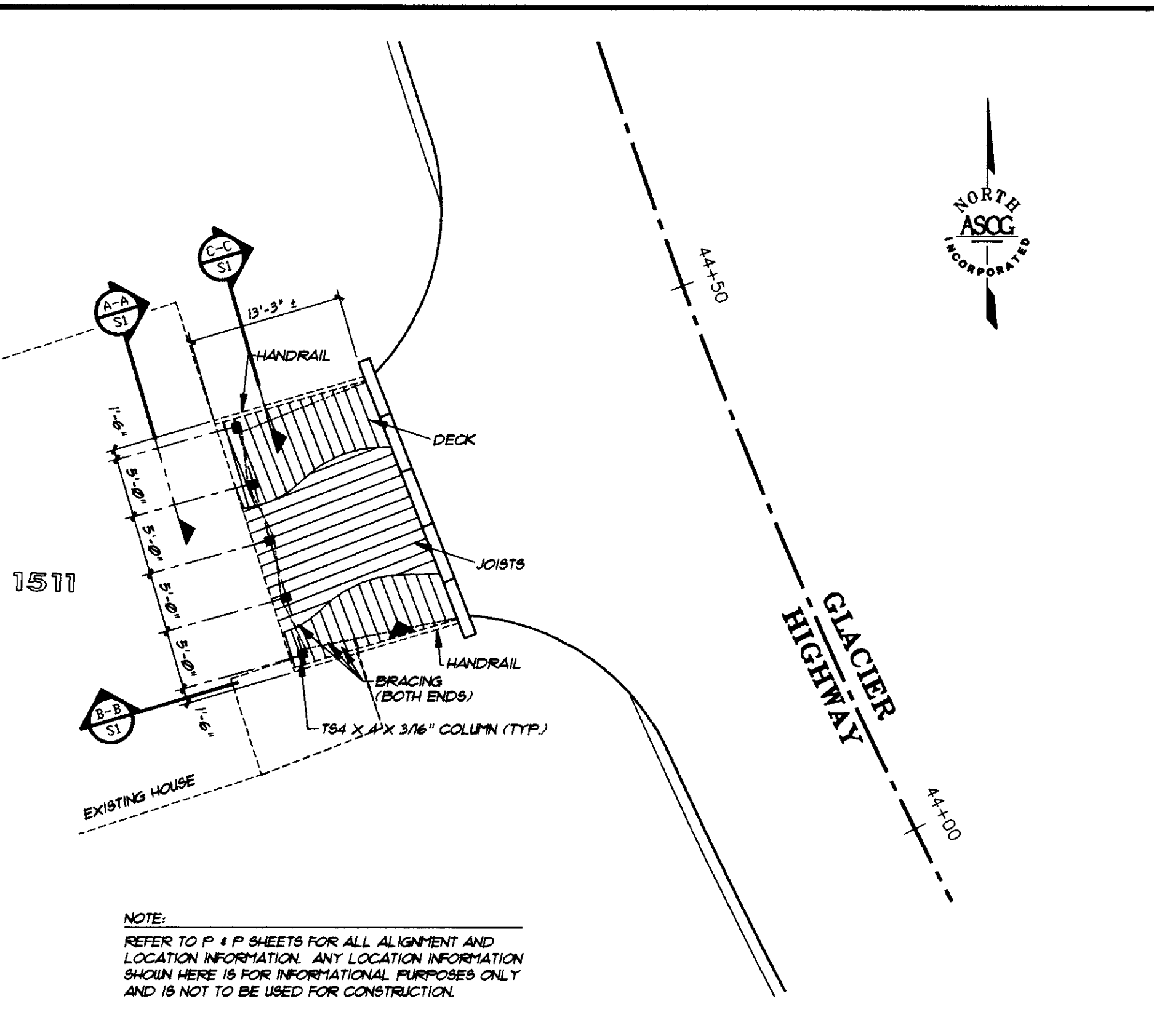
JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
**LANDSCAPING DETAILS**



DESIGNED BY: JANE CARHART & ASSOCIATES  
 DRAWN BY: J. CARHART  
 CHECKED BY: J. CARHART

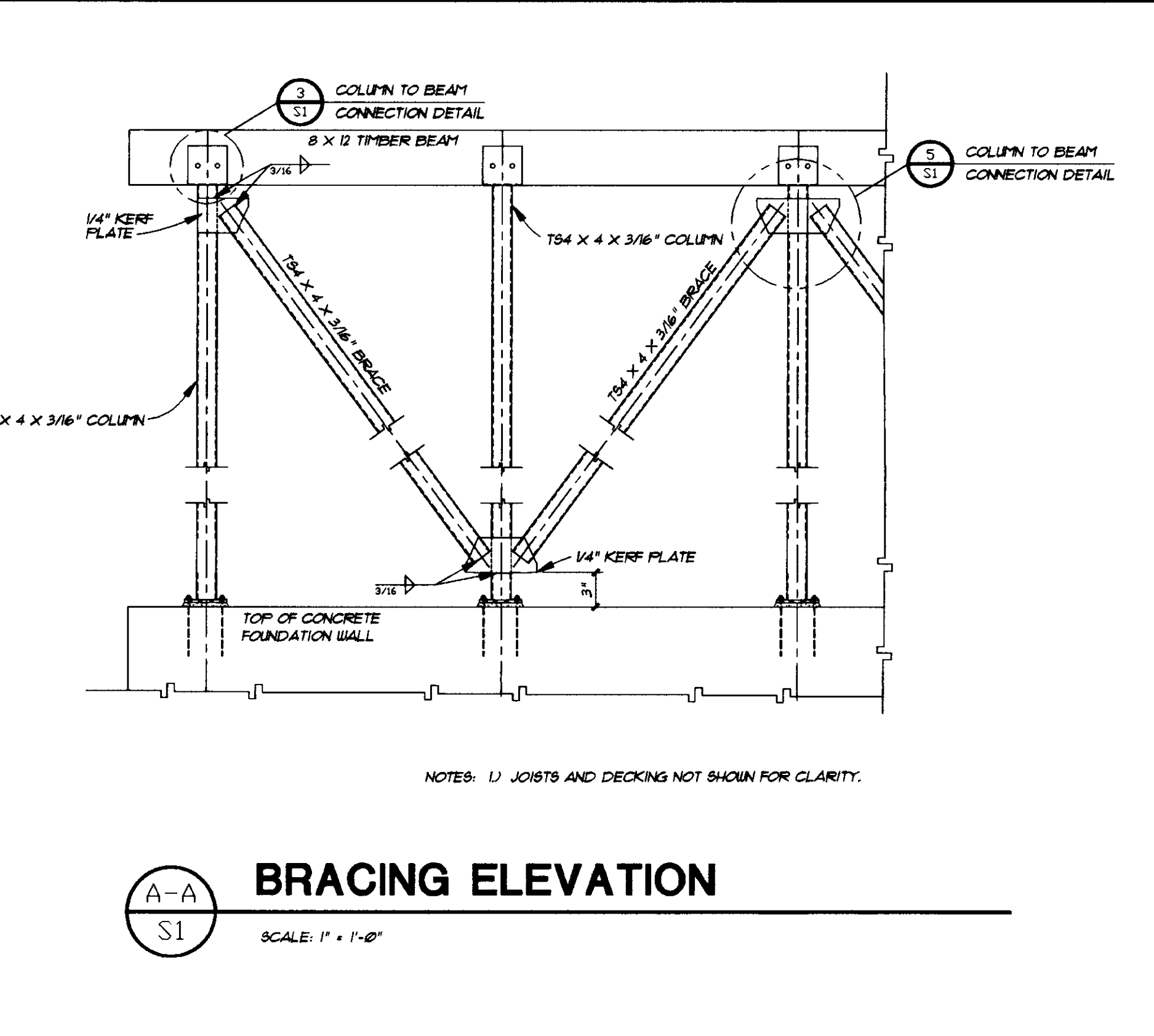
PROJECT No.: 69062  
 DATE: MARCH 1994  
 SHEET 44 OF 55





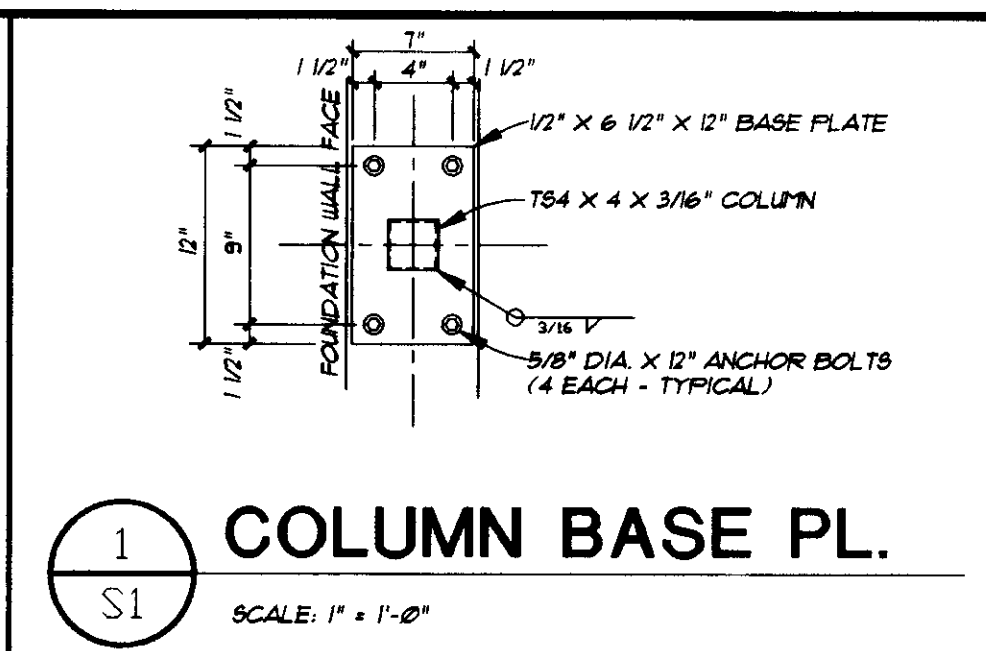
NOTE:  
REFER TO P & P SHEETS FOR ALL ALIGNMENT AND LOCATION INFORMATION. ANY LOCATION INFORMATION SHOWN HERE IS FOR INFORMATIONAL PURPOSES ONLY AND IS NOT TO BE USED FOR CONSTRUCTION.

**PLAN VIEW - STRUCTURE 1 (STA. 44+42)**  
SCALE: 1" = 10'

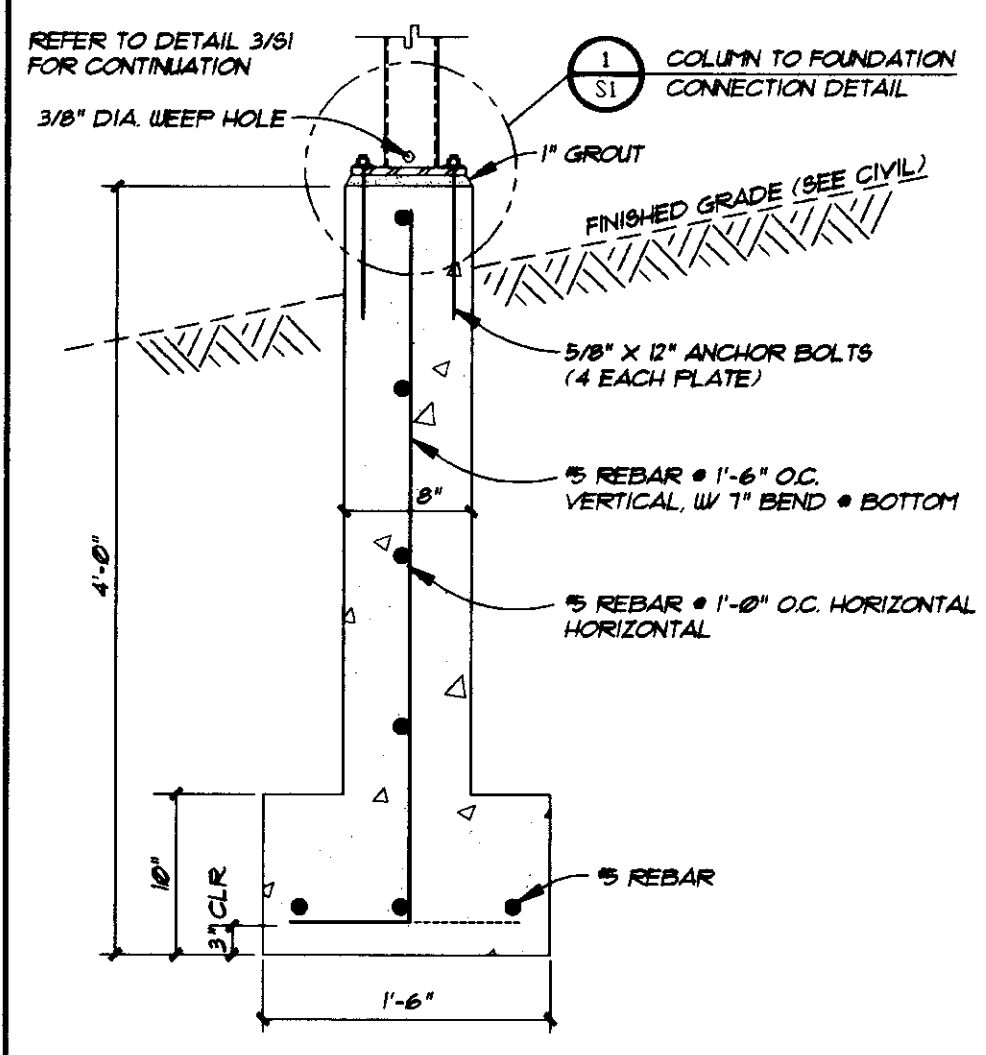


NOTES: 1. JOISTS AND DECKING NOT SHOWN FOR CLARITY.

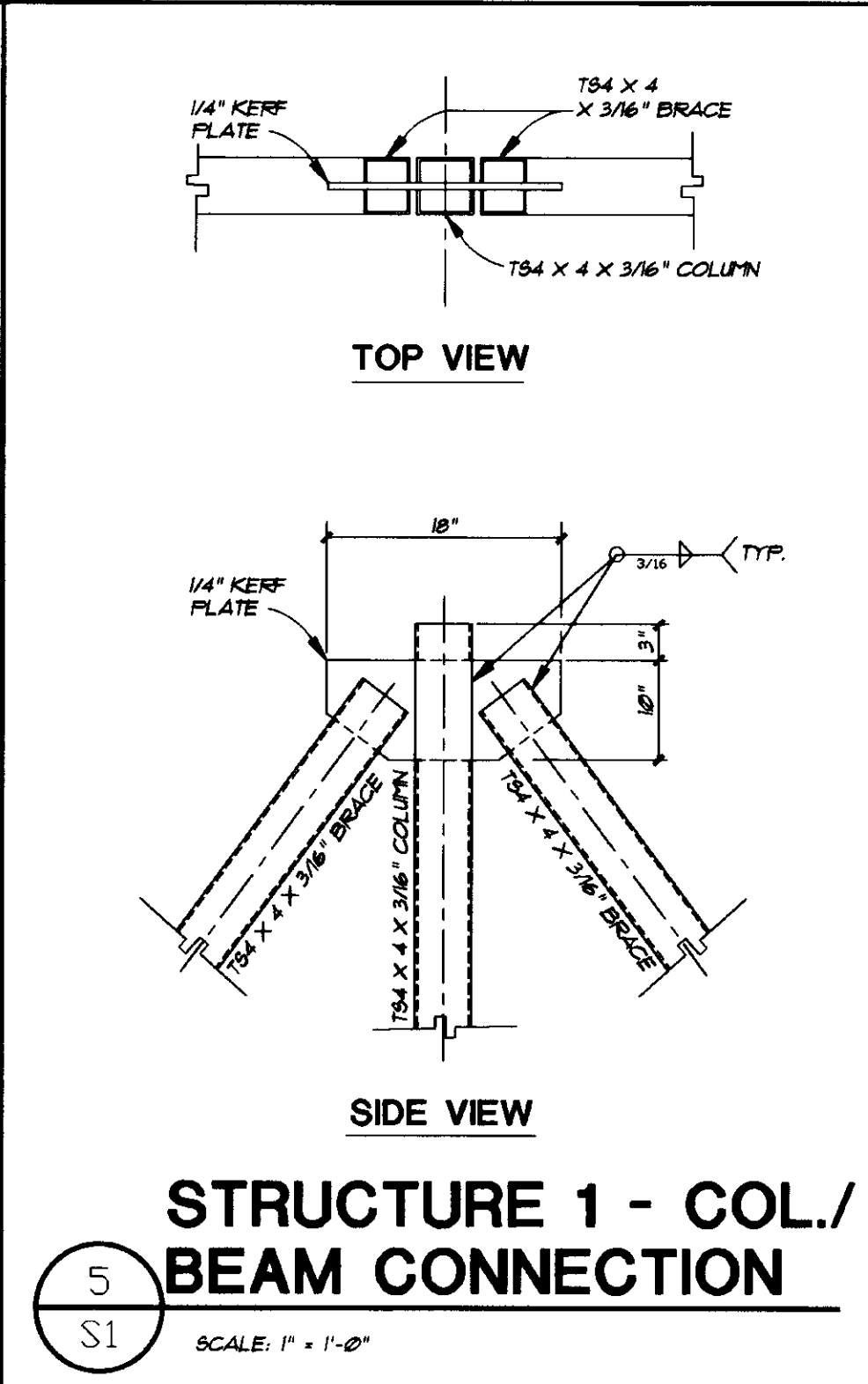
**BRACING ELEVATION**  
SCALE: 1" = 1'-0"



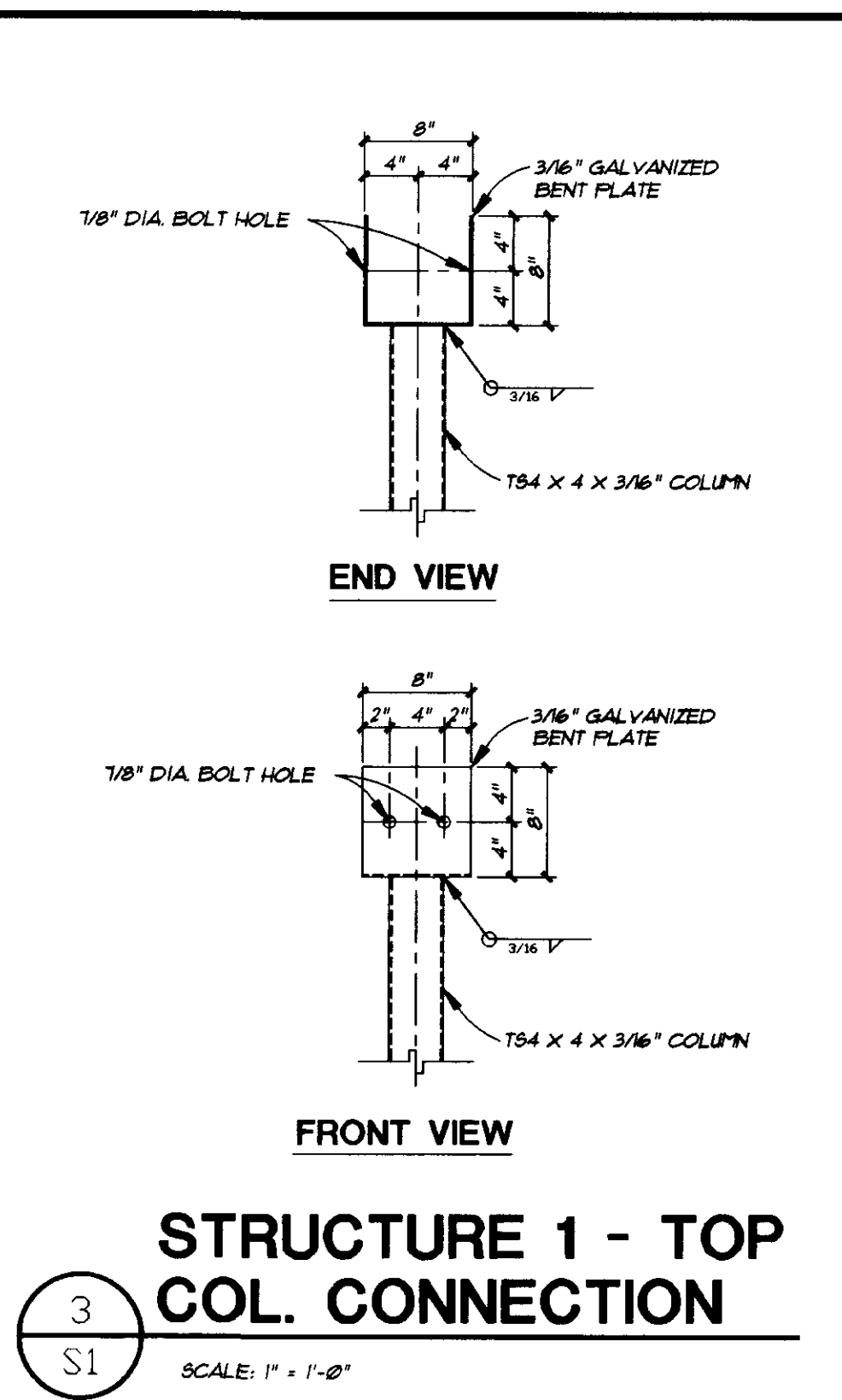
**1 COLUMN BASE PL.**  
SCALE: 1" = 1'-0"



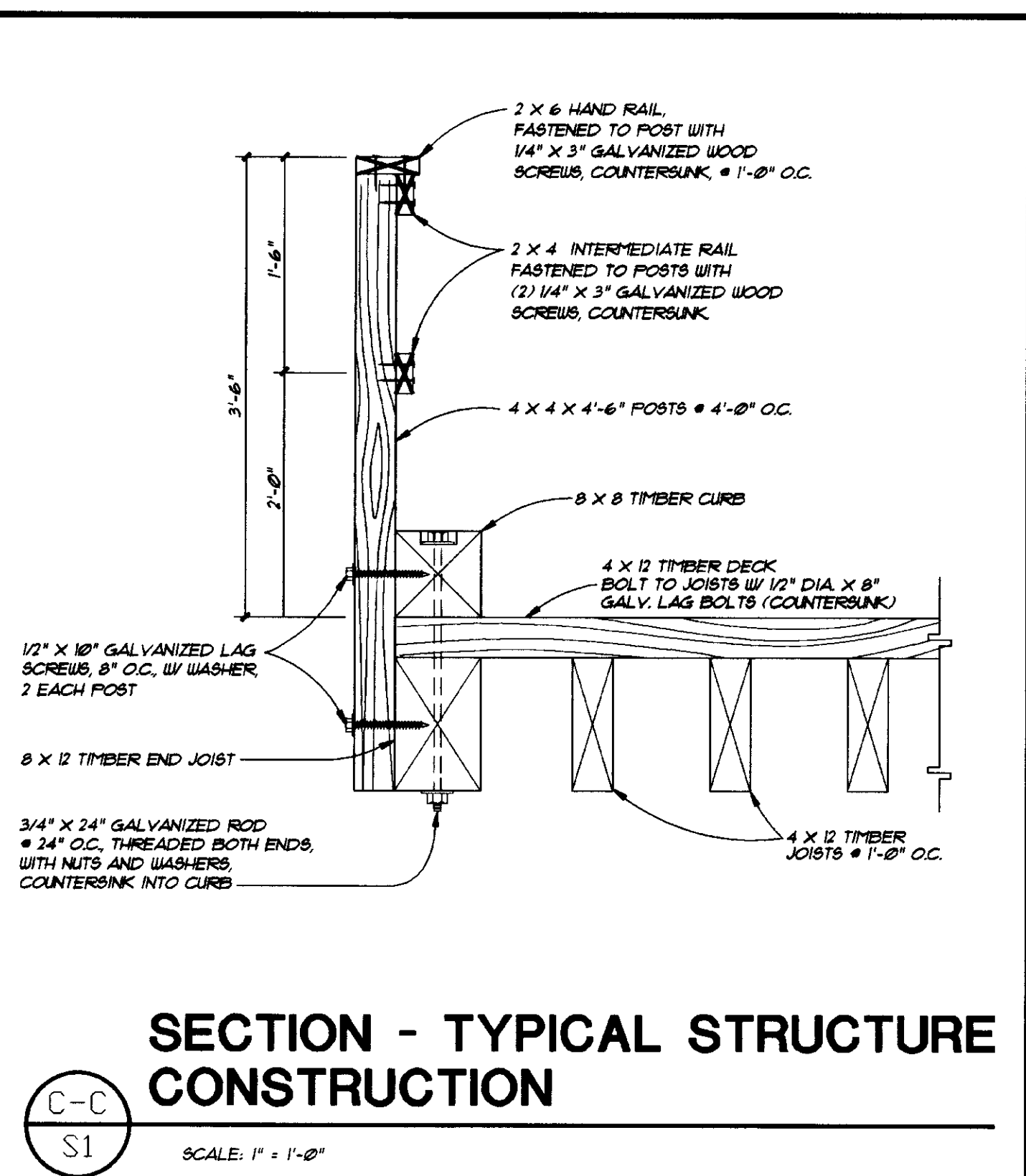
**2 STRUCTURE 1 - COL. CONNECTION**  
SCALE: 1" = 1'-0"



**5 STRUCTURE 1 - COL./BEAM CONNECTION**  
SCALE: 1" = 1'-0"



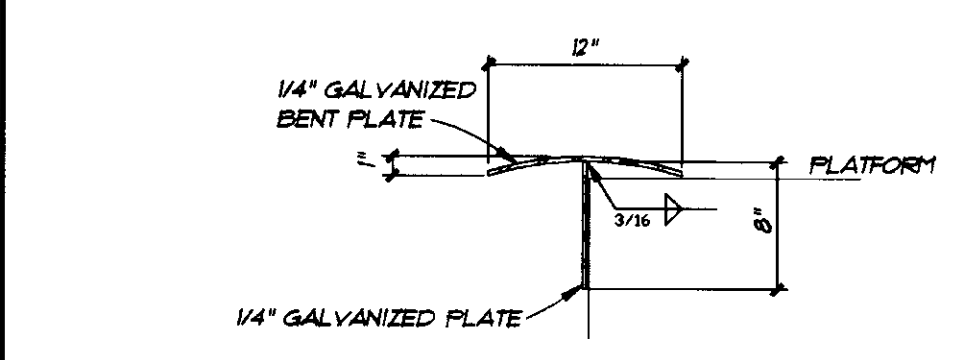
**3 STRUCTURE 1 - TOP COL. CONNECTION**  
SCALE: 1" = 1'-0"



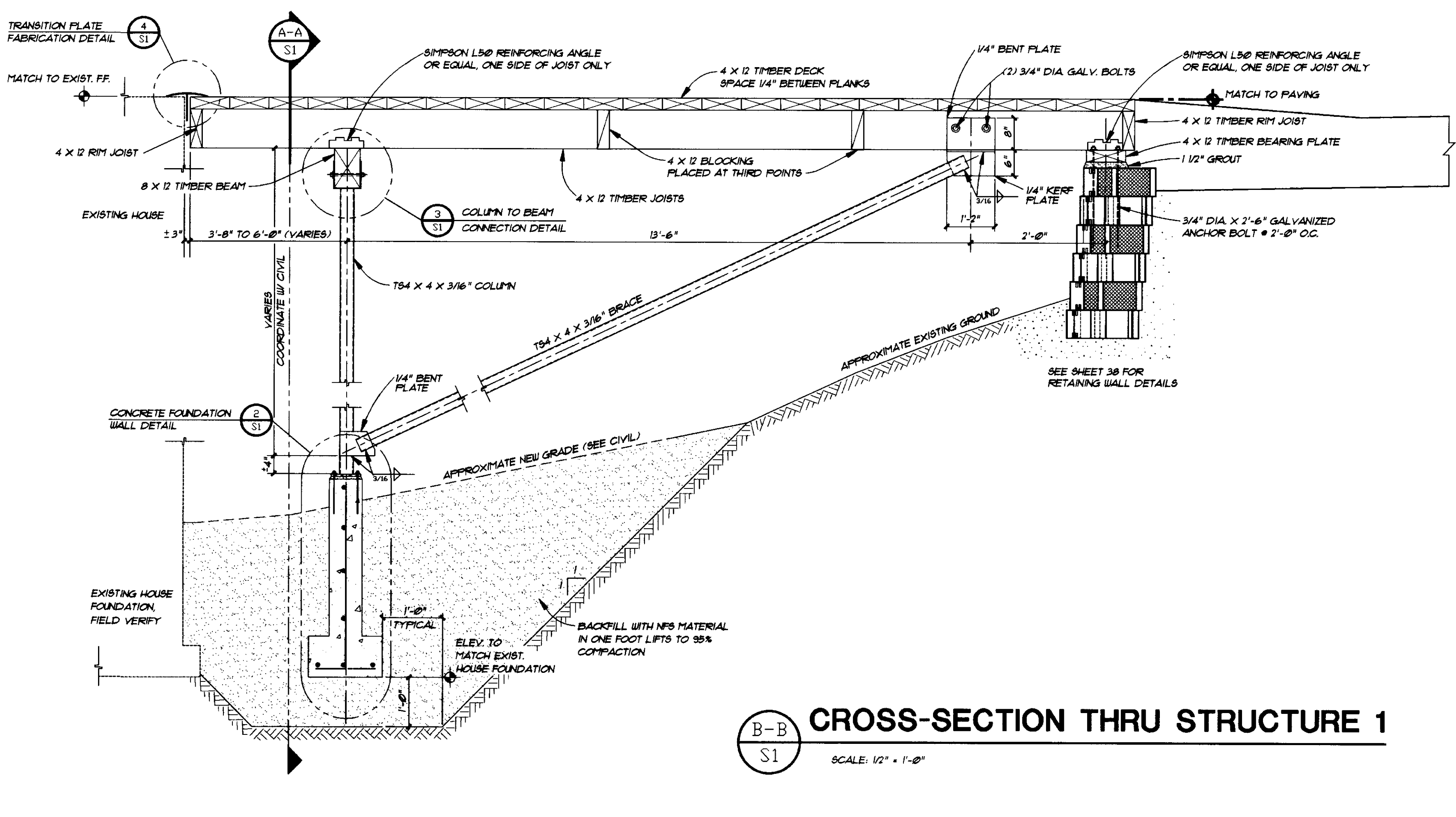
**SECTION - TYPICAL STRUCTURE CONSTRUCTION**  
SCALE: 1" = 1'-0"

**GENERAL NOTES**

- ALL STEEL SHALL BE A-36.
- ALL TIMBER SHALL BE PRESSURE TREATED. CUTS, DRILL HOLES AND OTHER MODIFICATIONS TO THE ORIGINAL TREATED TIMBER SHALL RECEIVE ADDITIONAL TREATMENT AS PER AMERICAN WOOD-PRESERVERS ASSOCIATION RECOMMENDATION. EXISTING TIMBER SHALL NOT BE REUSED.
- SEE P & P SHEETS FOR POINT ELEVATIONS & SURROUNDING FEATURE DETAIL.
- PAINT ALL STEEL WITH CARBO-ZINC 11" OR AN APPROVED EQUAL.



**4 DETAIL - TRANSITION PLATE**  
SCALE: 1" = 1'-0"



**B-B CROSS-SECTION THRU STRUCTURE 1**  
SCALE: 1/2" = 1'-0"

NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND  
PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
PARKING STRUCTURE 1

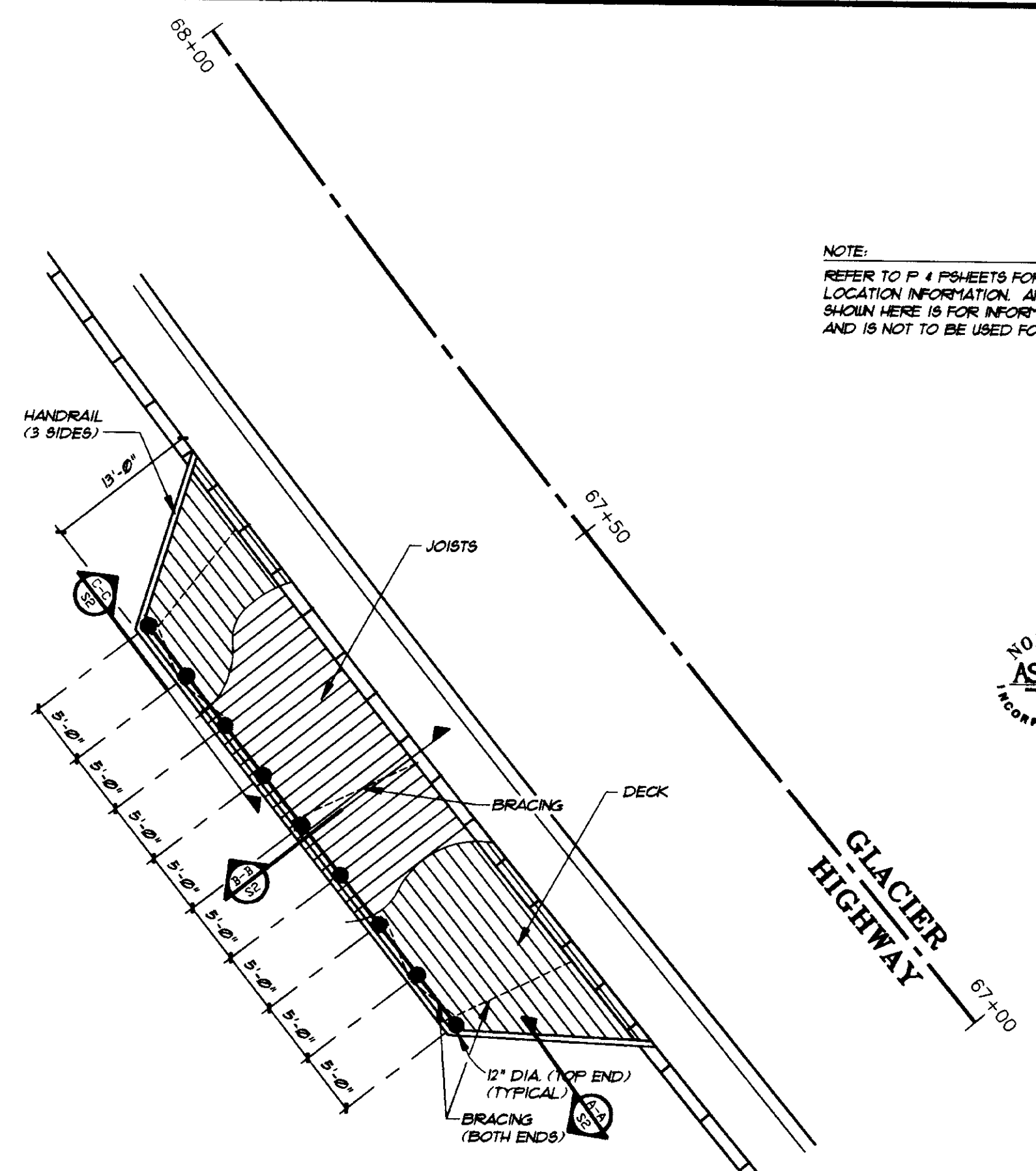
**ASCG**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: SH  
DRAWN BY: MP  
CHECKED BY: DC

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS  
PATH - J:\1400\2310\STRUCT\DRW\51  
DRAWN IN AUTOCAD RELEASE 11  
DRAWING PLOT SCALE: 1/4" = 1'-0"  
DRAWING LAST UPDATED: 3/17/1994

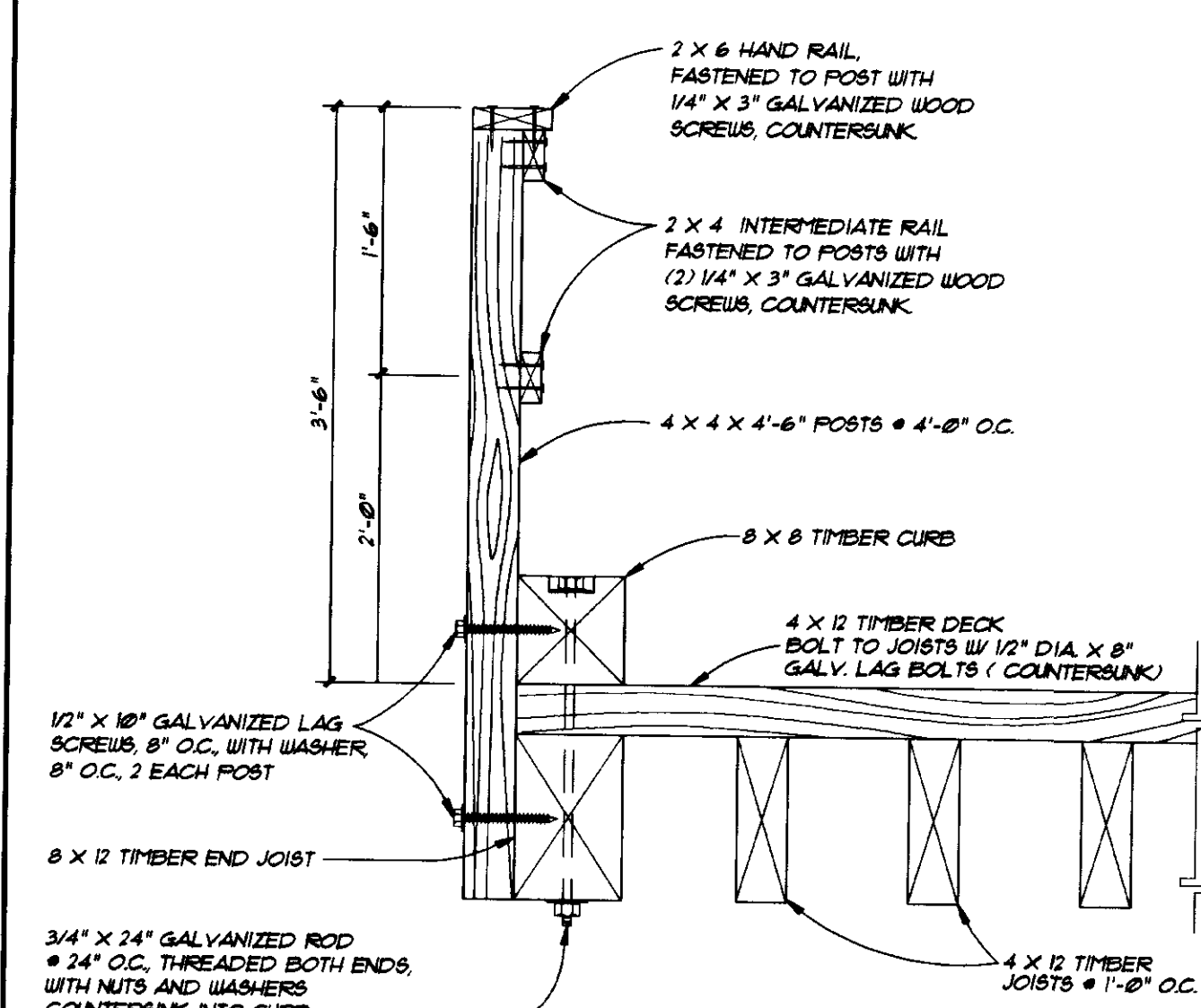
PROJECT No. 69062  
DATE: MARCH 1994  
SHEET 45 OF 55

STATE OF ALASKA  
49th  
Duane E. Carson  
REGISTERED PROFESSIONAL ENGINEER  
1980-E



PLAN VIEW - STRUCTURE 2 (STA. 67+46)

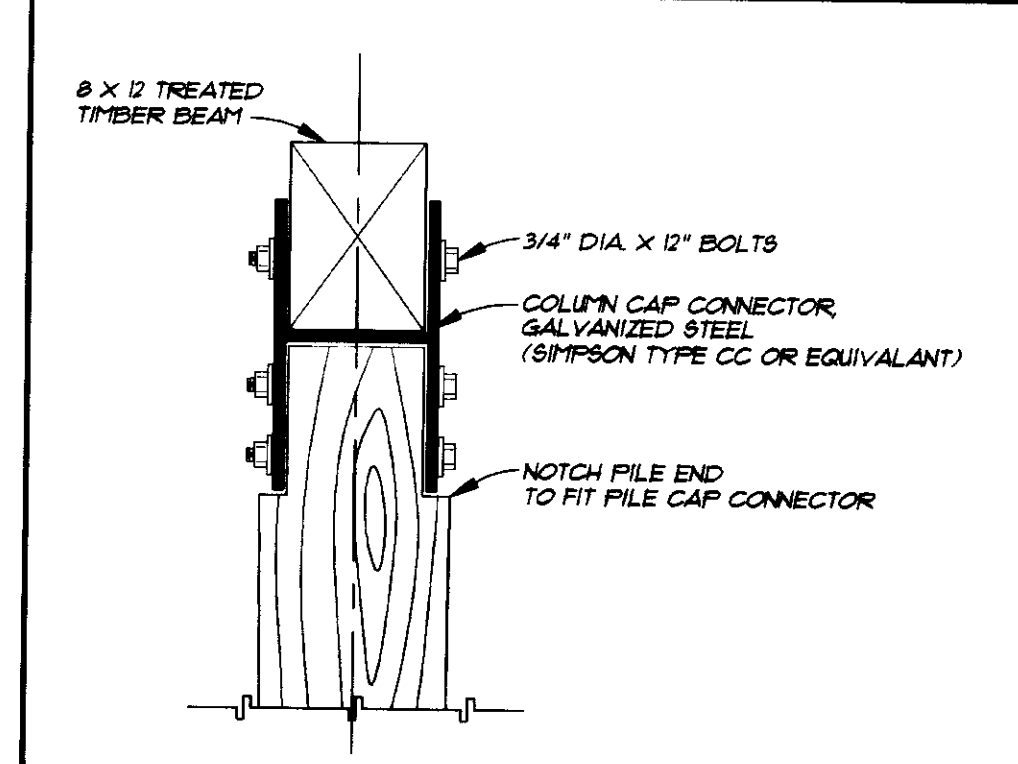
SCALE: 1" = 10'



SECTION - TYPICAL STRUCTURE CONSTRUCTION

A-A  
S2

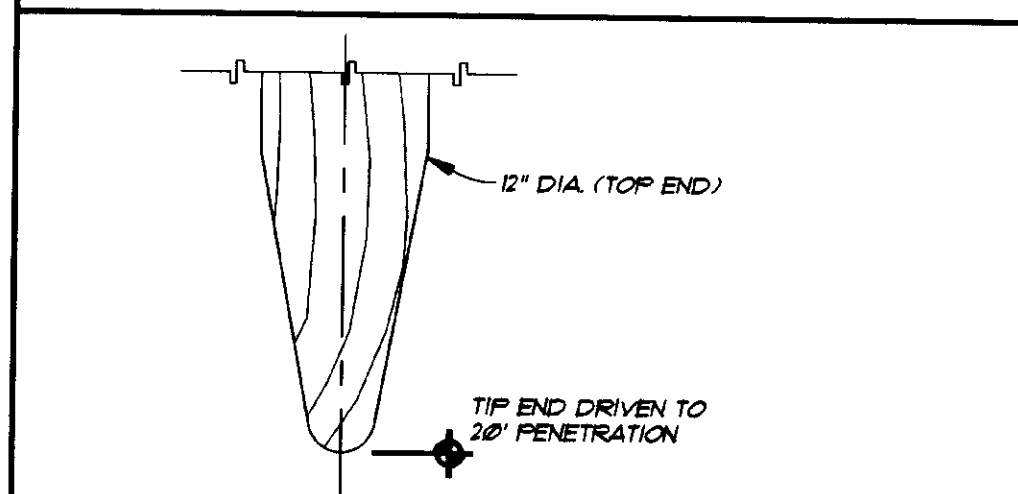
SCALE: 1" = 1'-0"



STRUCTURE 2 COL./BEAM CONN.

1  
S2

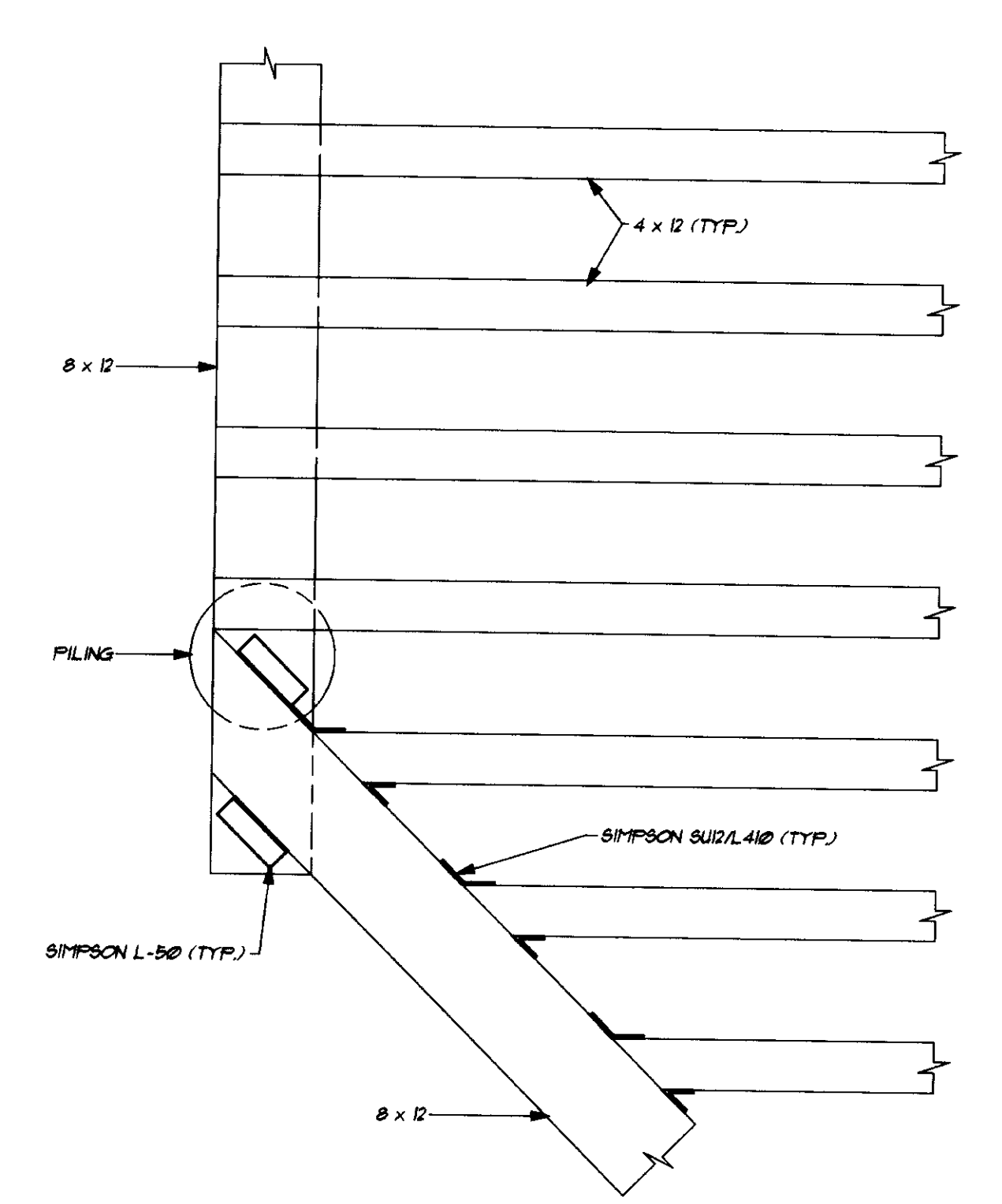
SCALE: 1" = 1'-0"



STRUCTURE 2 PILE TIP DETAIL

2  
S2

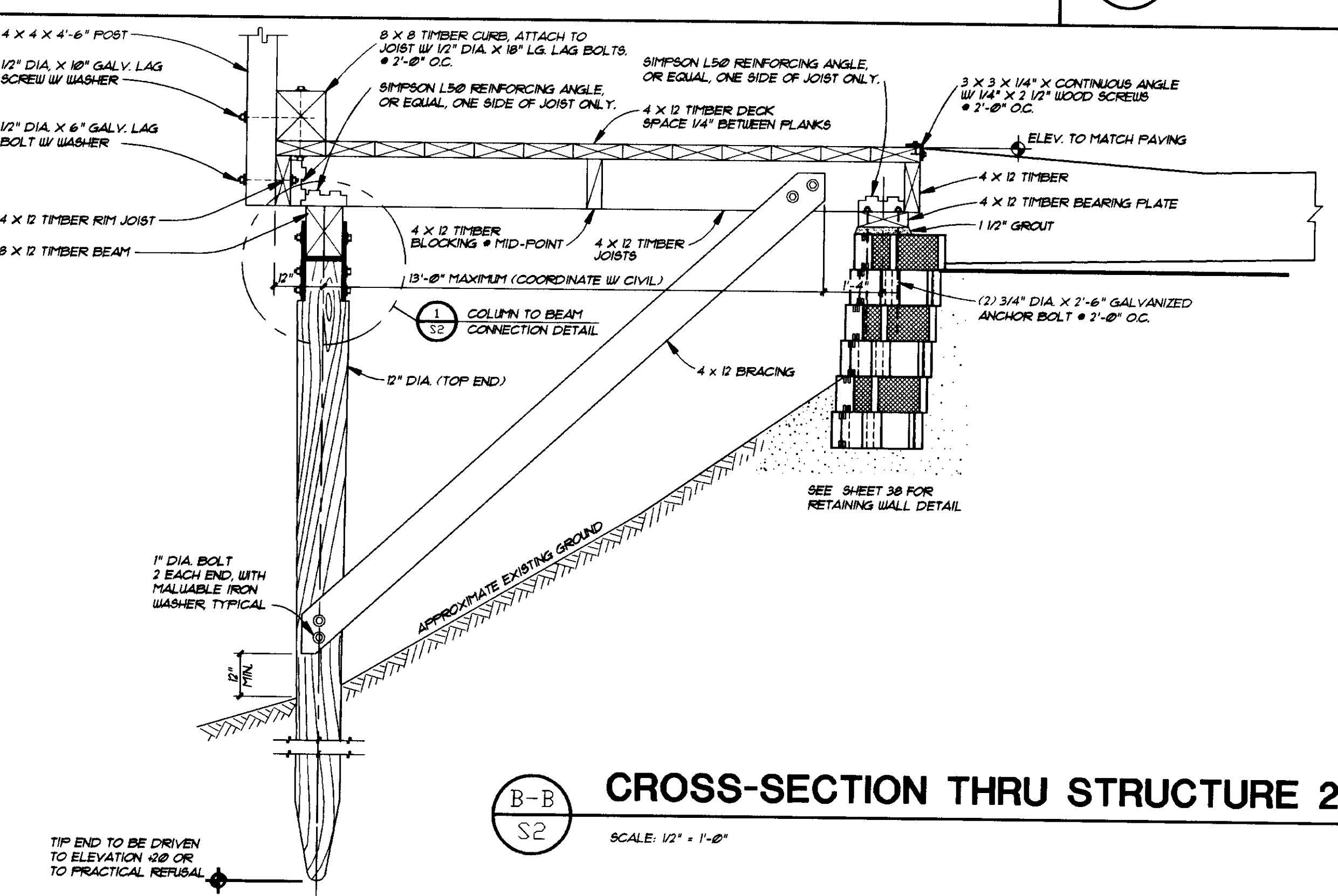
SCALE: 1" = 1'-0"



FRAMING DETAIL

3  
S2

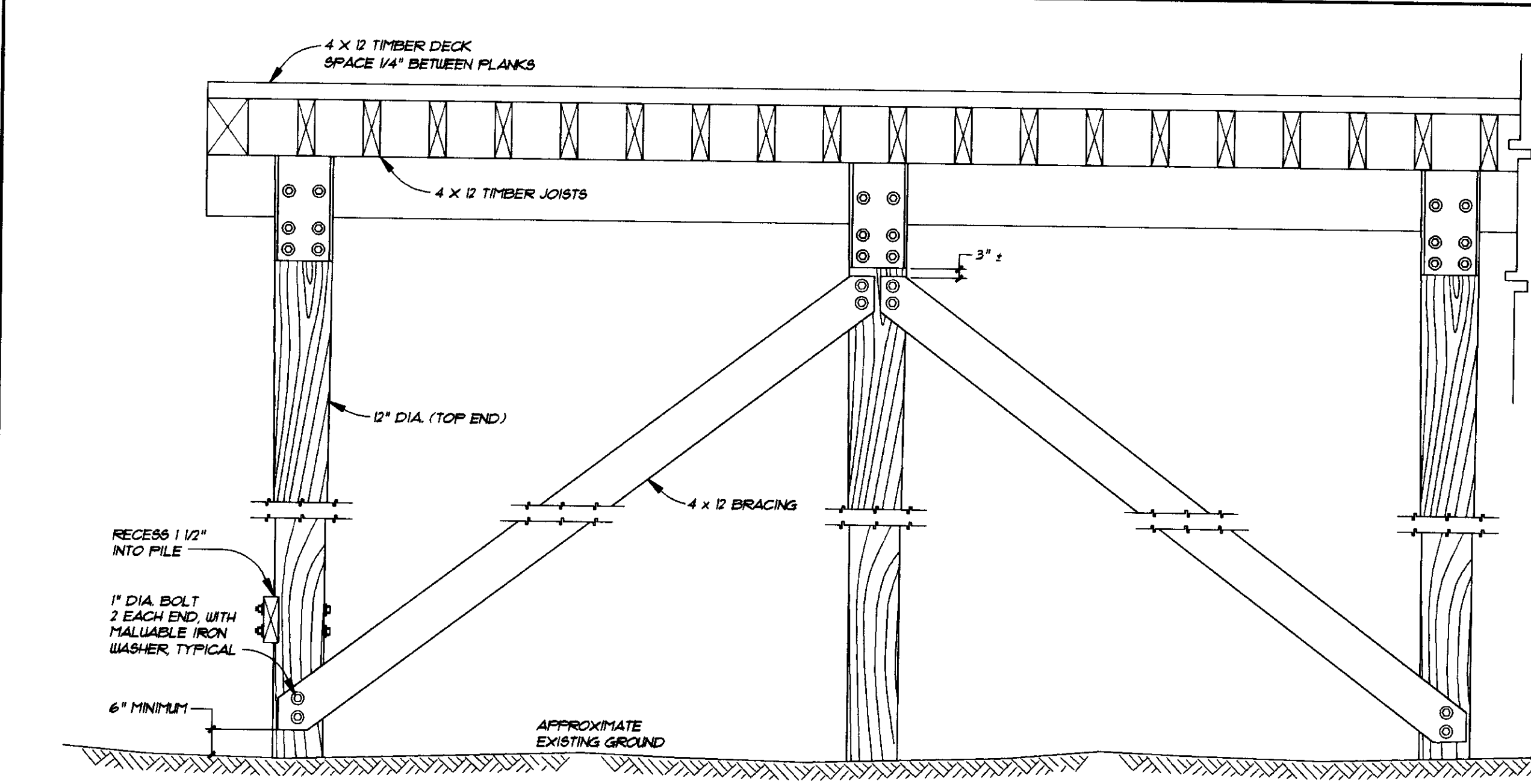
SCALE: 1" = 1'-0"



CROSS-SECTION THRU STRUCTURE 2

B-B  
S2

SCALE: 1/2" = 1'-0"



BRACING - STRUCTURE 2

C-C  
S2

SCALE: 1/2" = 1'-0"

NOTE: HANDRAIL NOT SHOWN THIS VIEW FOR CLARITY.

NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

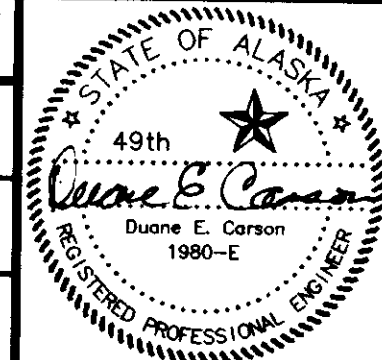
JUNEAU  
FM-0976 (1) (69062)  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
PARKING STRUCTURE 2

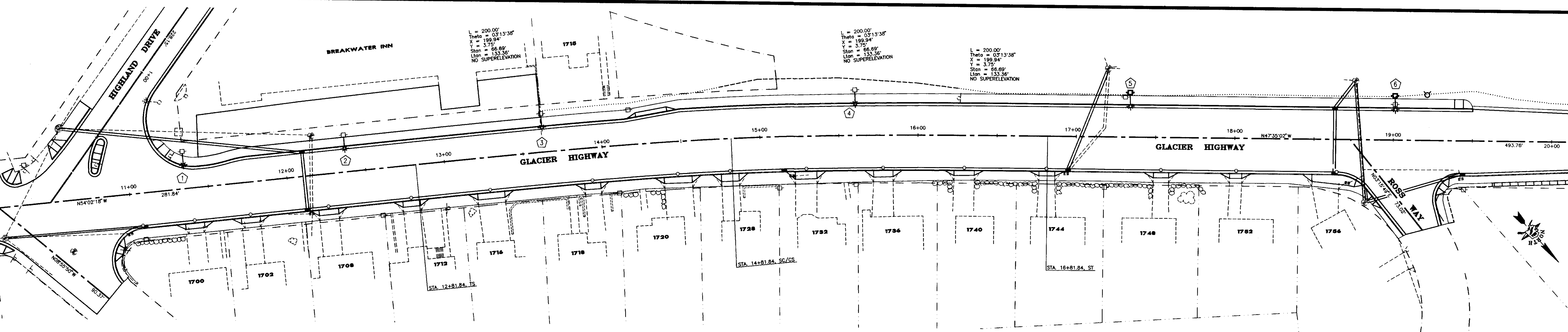
**ASCG**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: SH  
DRAWN BY: MP  
CHECKED BY: DC

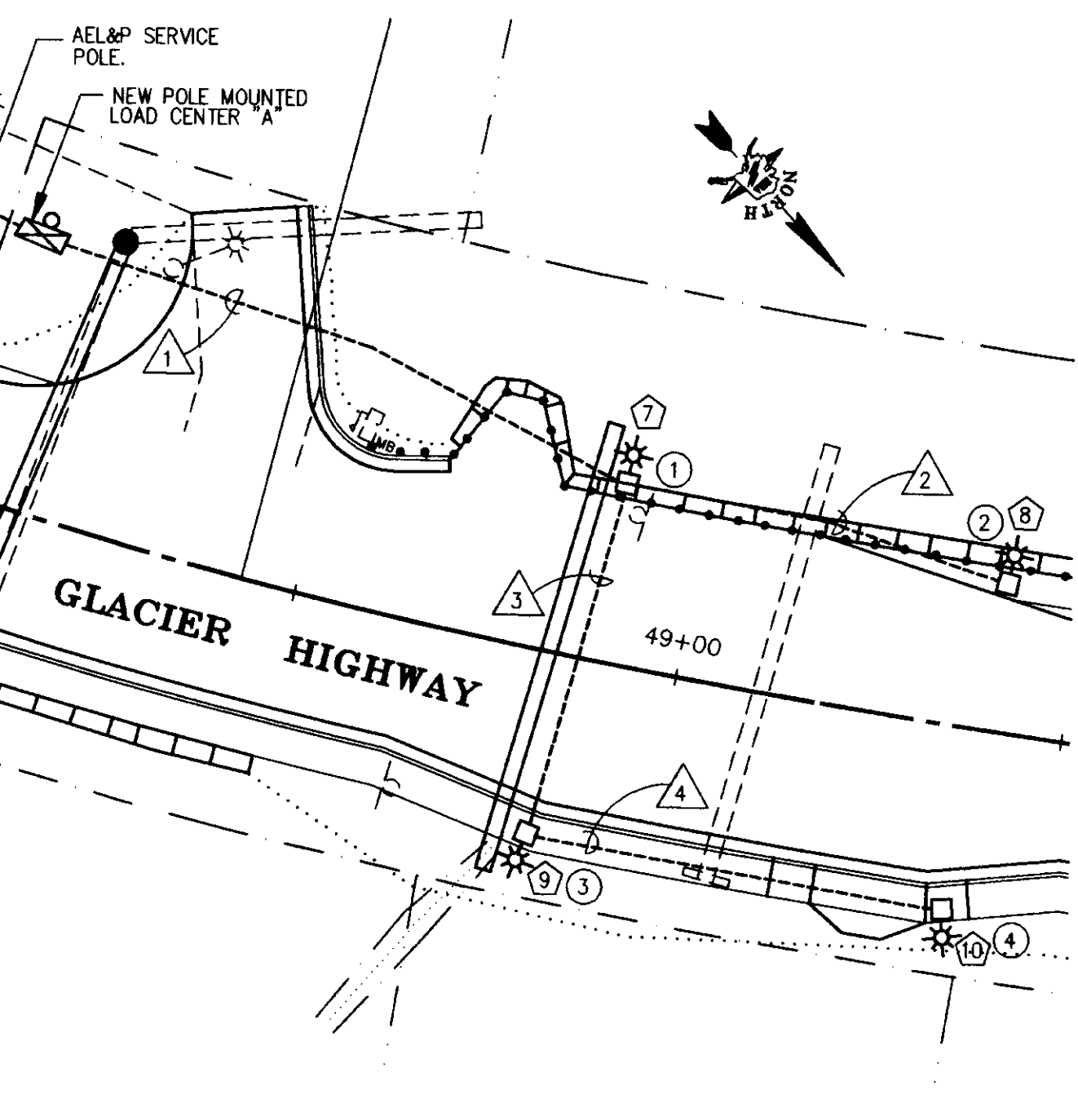
DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS  
PATH: J:\1400\2370\STRUCT\DWG\S2  
DRAWN IN AUTOCAD RELEASE 11  
DRAWING PLOT SCALE: 1"=1'  
DRAWING LAST UPDATED: 3/17/1994

PROJECT No.: 69062  
DATE: MARCH 1994  
SHEET 46 OF 55

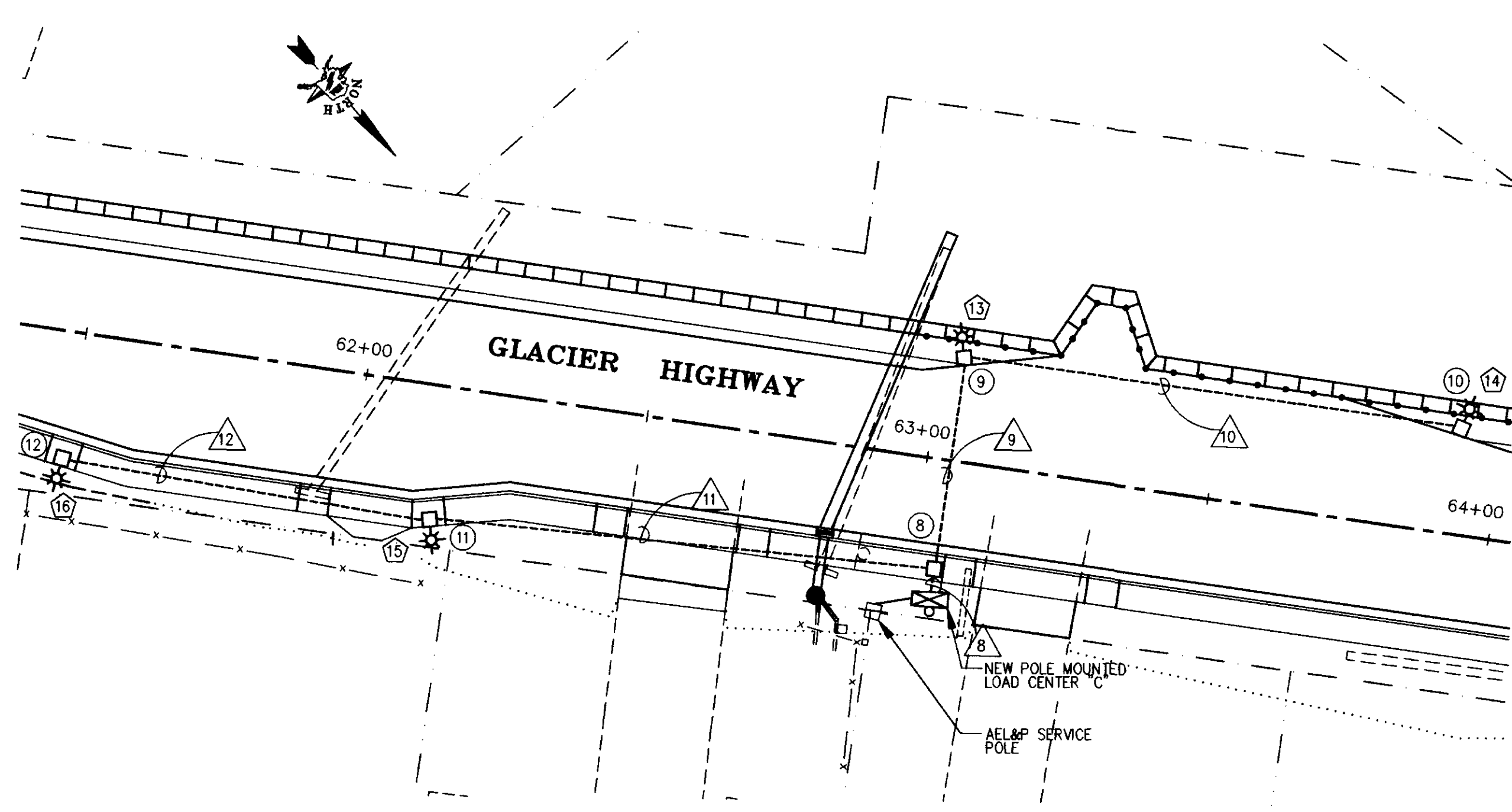




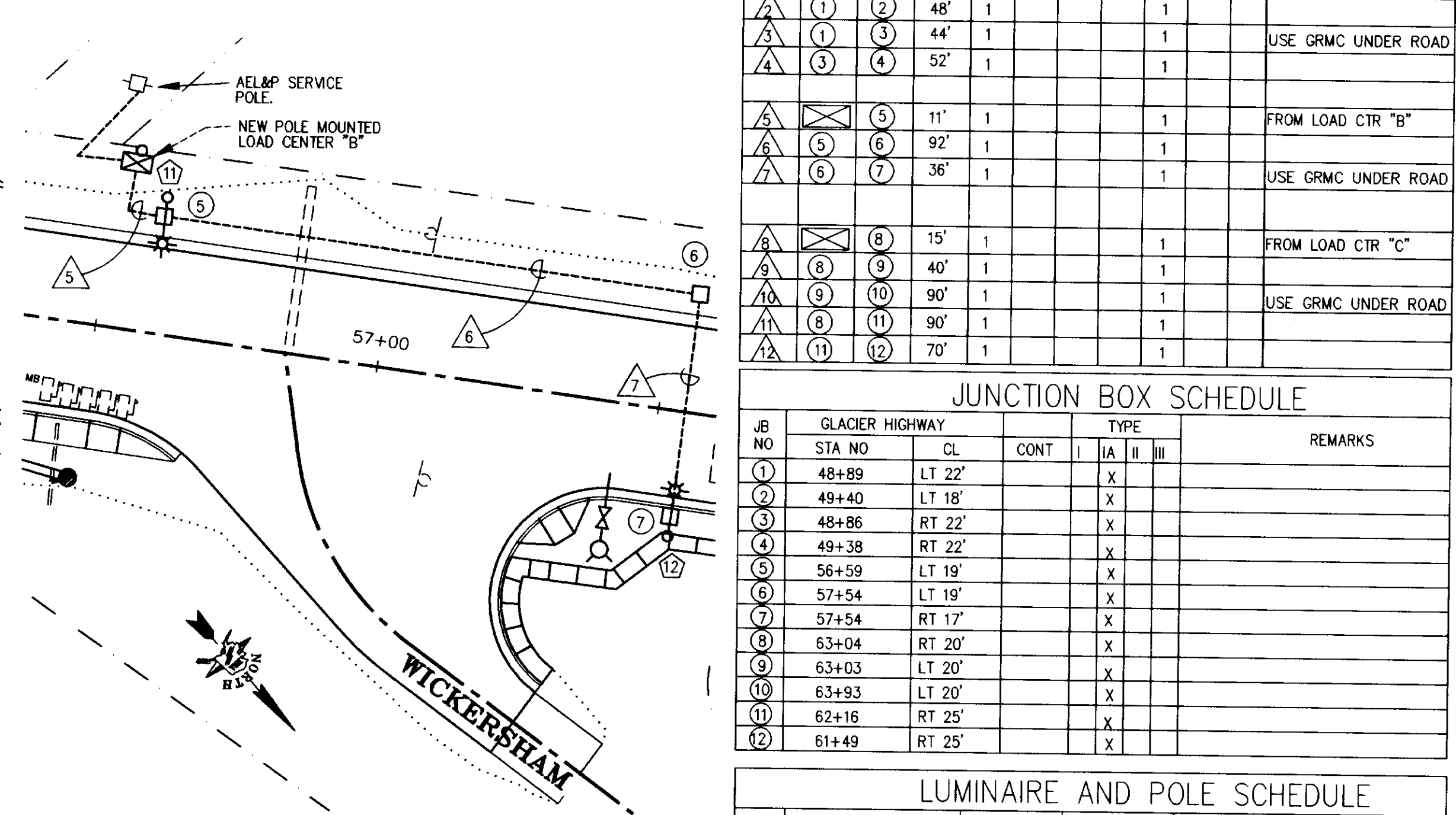
1 HIGHLAND DRIVE TO ROSS WAY LIGHTING PLAN  
SCALE: 1" = 30'



BUS TURNOUT #1 LIGHTING PLAN  
SCALE: 1" = 20'



3 BUS TURNOUT #2 LIGHTING PLAN  
SCALE: 1" = 20'



4 WICKERSHAM DRIVE AND GLACIER HWY LIGHTING PLAN  
SCALE: 1" = 20'

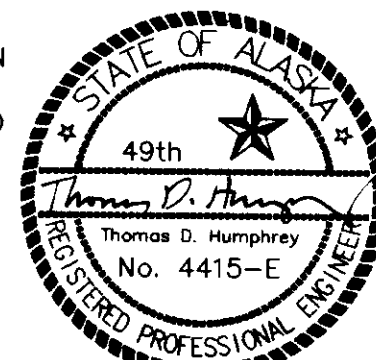
CONDUIT & WIRE SCHEDULE									
RUN	FROM	TO	LENGTH	CONDUIT			LIGHTING		REMARKS
				2"	3"	4"	3C#8	3C#6	
1	1	2	77'	1			1		FROM LOAD CTR "A"
2	2	3	48'	1			1		
3	3	4	44'	1			1		USE GRMC UNDER ROAD
4	4	5	52'	1			1		
5	5	6	11'	1			1		FROM LOAD CTR "B"
6	6	7	92'	1			1		
7	7	8	36'	1			1		USE GRMC UNDER ROAD
8	8	9	15'	1			1		FROM LOAD CTR "C"
9	9	10	40'	1			1		
10	10	11	90'	1			1		USE GRMC UNDER ROAD
11	11	12	90'	1			1		
12	12	13	70'	1			1		

JUNCTION BOX SCHEDULE							
JB NO	GLACIER HIGHWAY		CONT	TYPE			REMARKS
	STA NO	CL		I	II	III	
1	48+89	LT 22'		X			
2	49+40	LT 18'		X			
3	48+86	RT 22'		X			
4	49+38	RT 22'		X			
5	56+59	LT 19'		X			
6	57+54	LT 19'		X			
7	57+54	RT 17'		X			
8	63+04	RT 20'		X			
9	63+03	LT 20'		X			
10	63+93	LT 20'		X			
11	62+16	RT 25'		X			
12	61+49	RT 25'		X			

LUMINAIRE AND POLE SCHEDULE					
UNIT NO.	GLACIER HIGHWAY		MOUNTING HEIGHT (FT)	MAST ARM LENGTH (FT)	REMARKS
	STATION	OFFSET (FT)			
1	11+36	22'	LT 27'	7'	ADD ARM AND LUMINAIRE TO POLE
2	12+38	22'	LT 27'	EXISTING	REPLACE LUMINAIRE
3	13+63	23'	LT 27'	EXISTING	REPLACE LUMINAIRE
4	15+60	27'	LT 27'	EXISTING	REPLACE LUMINAIRE
5	17+31	24'	LT 27'	7'	ADD ARM AND LUMINAIRE TO POLE
6	19+01	21'	LT 27'	EXISTING	REPLACE LUMINAIRE
7	48+89	24'	LT 16'	4"	BUS PULLOUT LUMINAIRE
8	49+40	20'	LT 16'	4"	BUS PULLOUT LUMINAIRE
9	48+86	24'	RT 16'	4"	BUS PULLOUT LUMINAIRE
10	49+38	24'	RT 16'	4"	BUS PULLOUT LUMINAIRE
11	56+93	21'	LT 30'	7'	BREAKAWAY POLE
12	57+54	19'	RT 30'	7'	BREAKAWAY POLE
13	63+03	22'	LT 16'	4"	BUS PULLOUT LUMINAIRE
14	63+93	22'	LT 16'	4"	BUS PULLOUT LUMINAIRE
15	62+16	27'	RT 16'	4"	BUS PULLOUT LUMINAIRE
16	61+49	27'	RT 16'	4"	BUS PULLOUT LUMINAIRE

NOTES:

- HIGHLAND DRIVE TO ROSS WAY**
- LUMINAIRES SHALL BE 250W HPS, WITH TYPE III DISTRIBUTION FURNISHED AND INSTALLED BY AEL&P UNDER THEIR TARIFF. CONTRACTOR SHALL COORDINATE WITH AEL&P TO HAVE AEL&P ADD AND/OR REPLACE THESE LUMINAIRES AS SCHEDULED ON THESE PLANS AND SPECIFICATIONS.
- WICKERSHAM DRIVE AND GLACIER HWY.**
- CONTRACTOR SHALL INSTALL NEW ELECTROLIERS AT LOCATIONS INDICATED. ELECTROLIERS SHALL BE 30' POLE HEIGHT AND 7' ARM LENGTH PER STATE OF ALASKA DOT/FP STANDARD DWG L-03.01. FOOTINGS SHALL BE CAST-IN-DRILLED HOLE PER STANDARD DWG L-30.01. BREAKAWAY BASES ARE REQUIRED PER STANDARD DWG L-10.01 & L-14.00.
  - LUMINAIRE SHALL BE 250W HPS, MEDIUM CUTOFF, WITH A TYPE III DISTRIBUTION. LOAD CENTER SHALL BE INSTALLED PER STANDARD DWG L-20.01. SERVICE RISER ON EXISTING POLE. DISTRIBUTION CENTER ON METAL POLE WITH UNDERGROUND SERVICE. USE TYPE I JUNCTION BOXES; REFERENCE STANDARD DWG L-23.00. CONTRACTOR SHALL DO ALL COORDINATION WORK WITH AEL&P FOR INSTALLATION OF SERVICE.
  - LOAD CENTER A, B, AND C SHALL BE 120/240V, 1Ø, 100A WITH A MINIMUM OF 8 CIRCUIT SPARES. THREE 20/2 CIRCUITS SHALL BE INSTALLED IN EACH LOAD CENTER. CIRCUIT 1 TO BE USED FOR LIGHTING. CIRCUITS 2 AND 3 TO BE SPARES.
- BUS TURNOUT ELECTROLIERS**
- CONTRACTOR SHALL INSTALL NEW ELECTROLIER AT LOCATIONS INDICATED. ELECTROLIER SHALL BE 16' POLE HEIGHT. POLES, FOOTINGS, AND BREAKAWAY BASES SHALL BE PER STANDARD DRAWINGS AS REFERENCED ABOVE.
  - LUMINAIRES SHALL BE A "SHOE-BOX" TYPE, 150W HPS, SIMILAR TO HUBBELL RCJ MAGNUFORM II OR EQUAL.
  - BUS STOP ELECTROLIER FOUNDATIONS MAY BE TYPE "A" PER STANDARD DRAWING T-31. BOLT PATTERN SHOULD CONFORM TO MANUFACTURER'S REQUIREMENTS.

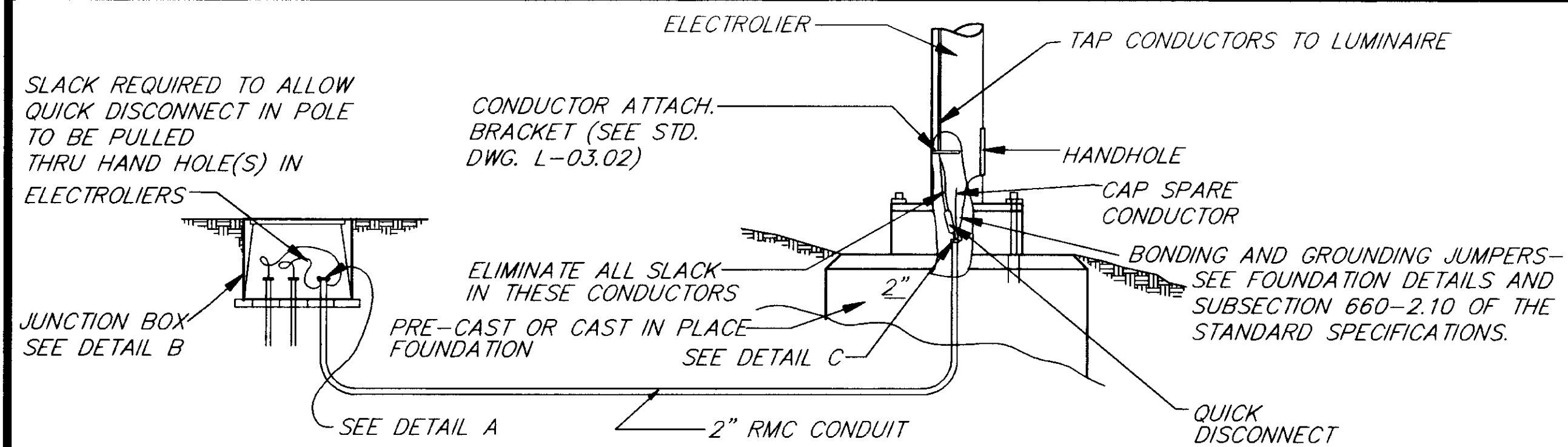


FIELD BOOKS		DESIGNED	M.C.S.
DESIGN		DRAWN	M.C.S.
STAKING		CHECKED	T.D.H.
AS-BUILT		DATE	3/21/94
SCALE		GRID	
HOR. AS NOTED		JOB NO.	AT03
DATE	BY	REV	REVISION

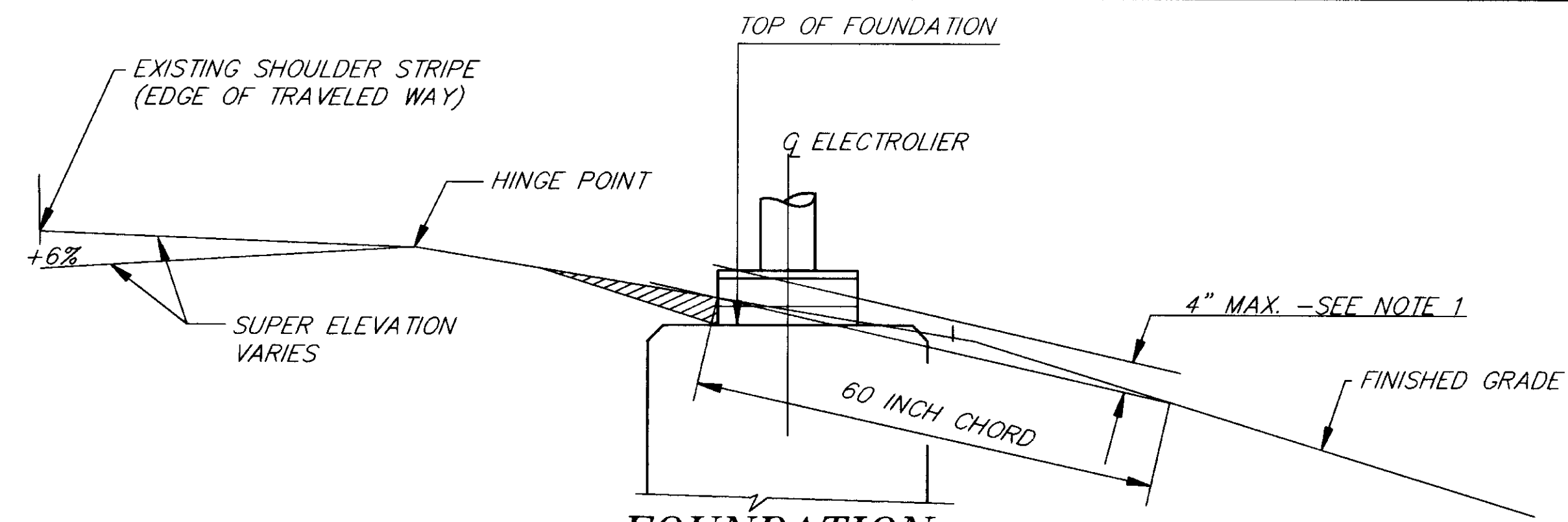
**RAJ BHARGAVA ASSOCIATES**  
ENGINEERING IN ALASKA  
301 E. FIREWEED LANE, ANCHORAGE, ALASKA 99503 (907) 276-3768

STATE OF ALASKA DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
**JUNEAU-GLACIER HIGHWAY LIGHTING PLANS**

SHEET  
**47** OF **55**  
FILE NO.  
AT03-E1



**LIGHTING SYSTEM POLE AND J-BOX WIRING DETAILS**

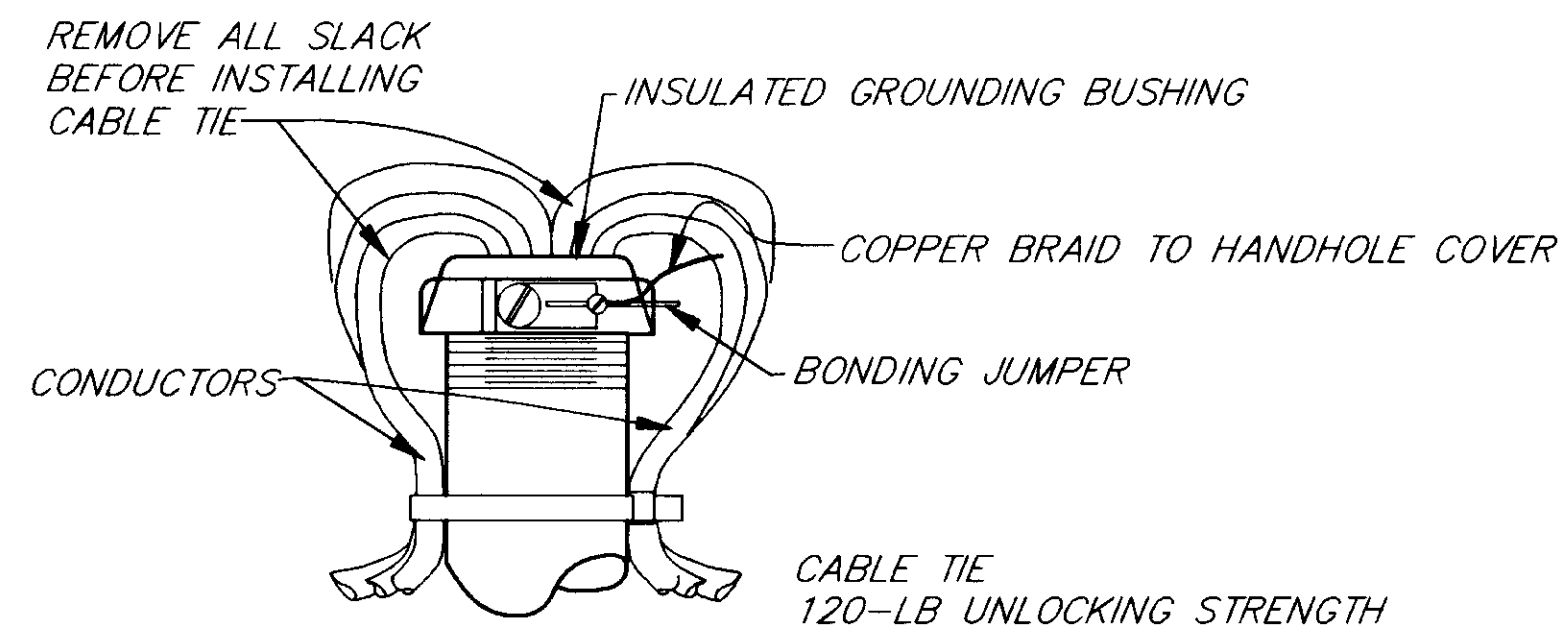


**FOUNDATION INSTALLATION DETAIL**

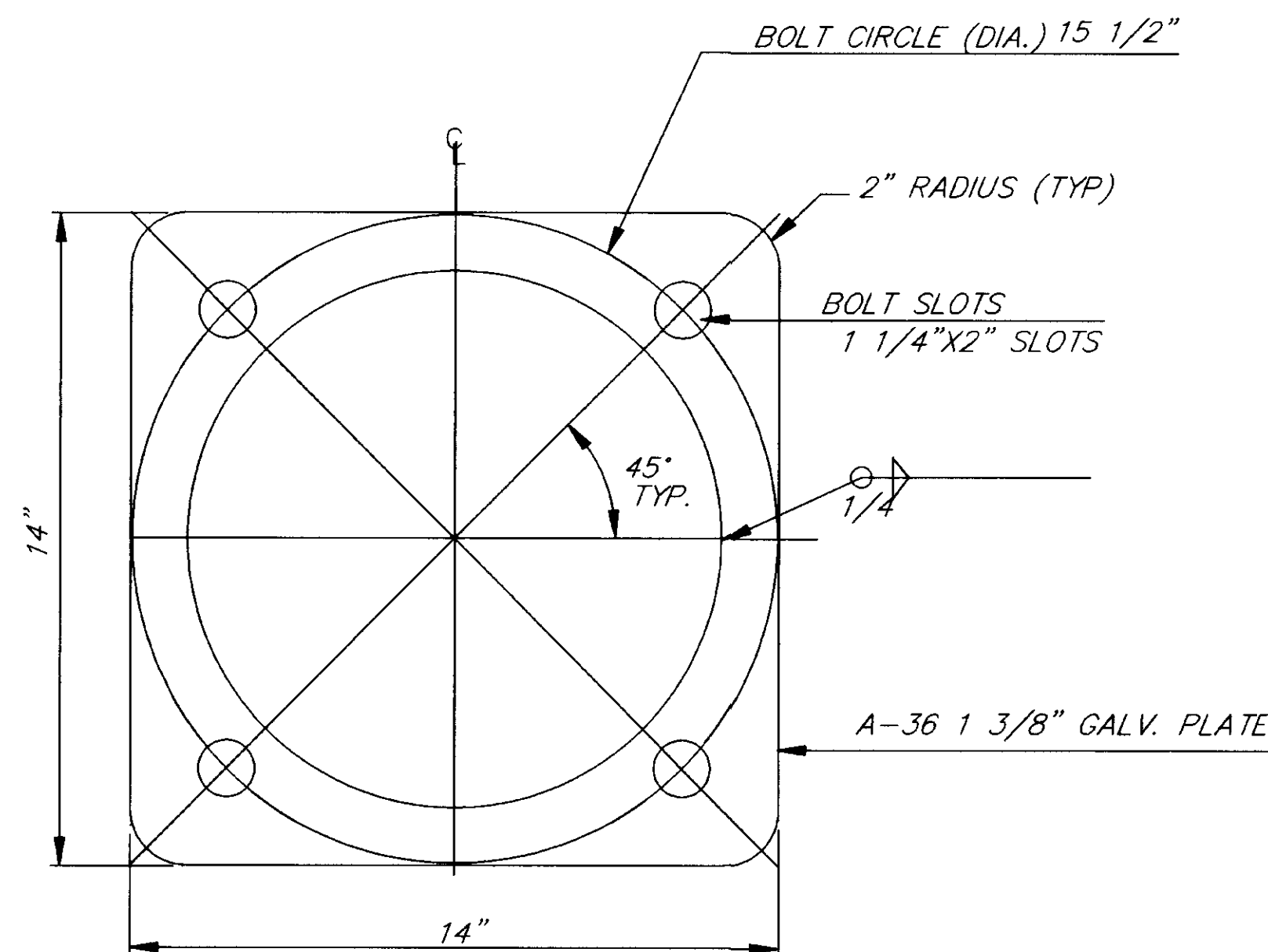
(NO SIDEWALK)

**NOTES**

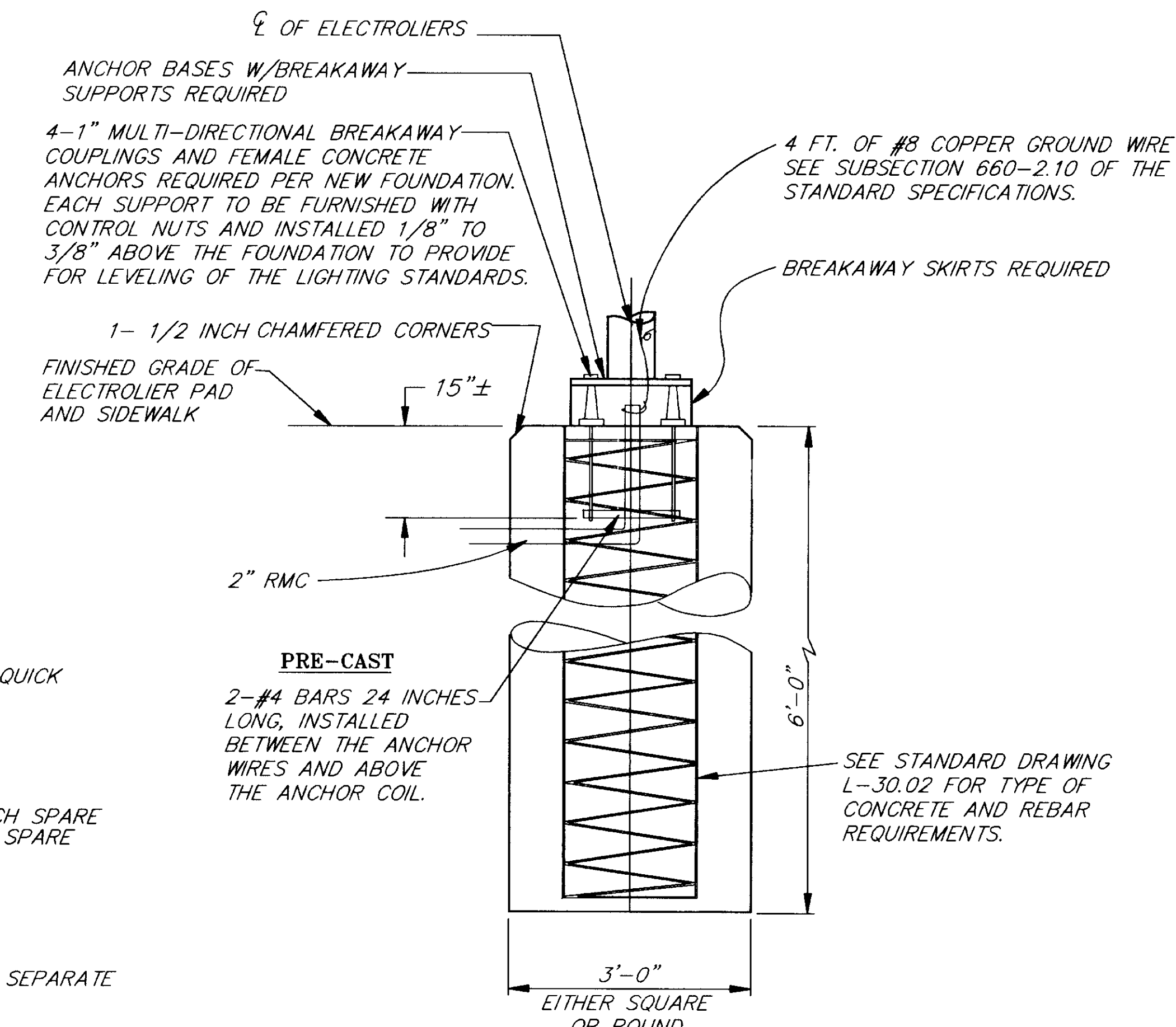
- NON BREAKAWAY PORTIONS OF FOUNDATIONS SHALL NOT PROTRUDE MORE THAN 4 INCHES ABOVE ANY 60 INCH CHORD STARTING AND ENDING ON FINISHED GRADE.
- INDICATES EMBANKMENT MATERIAL TO BE REMOVED AROUND BREAKAWAY SKIRTS.



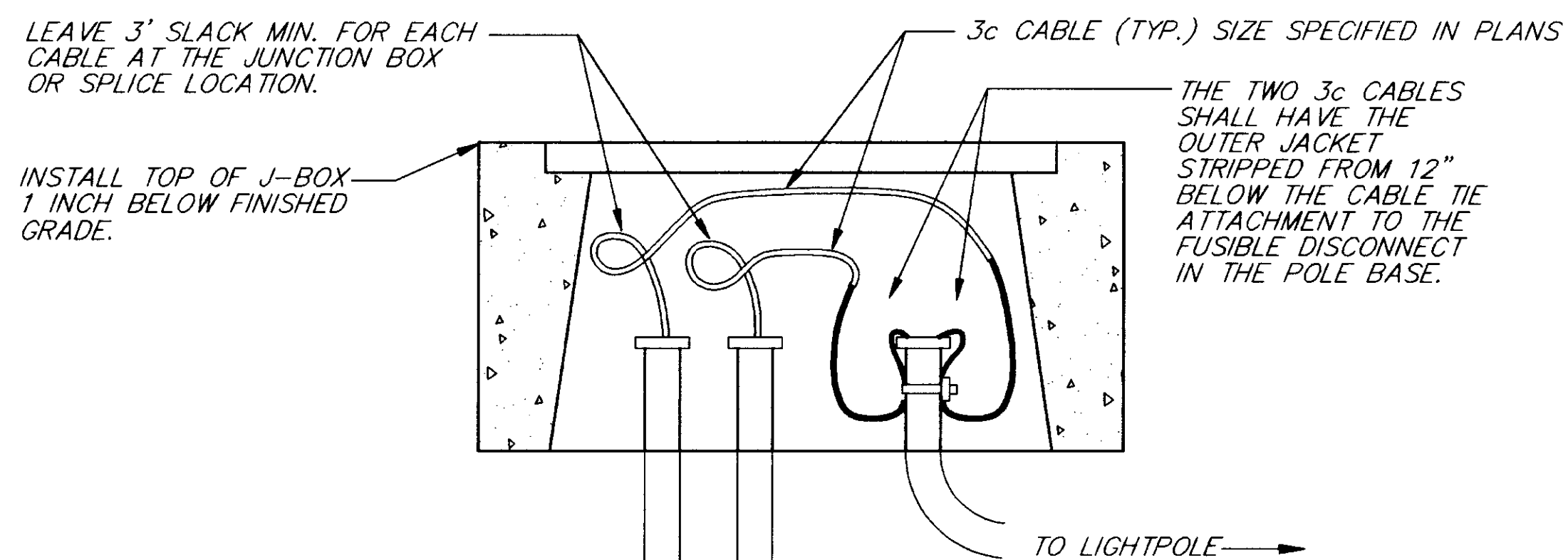
**DETAIL A**  
( IN J-BOX )



**ANCHOR BASE DETAIL**

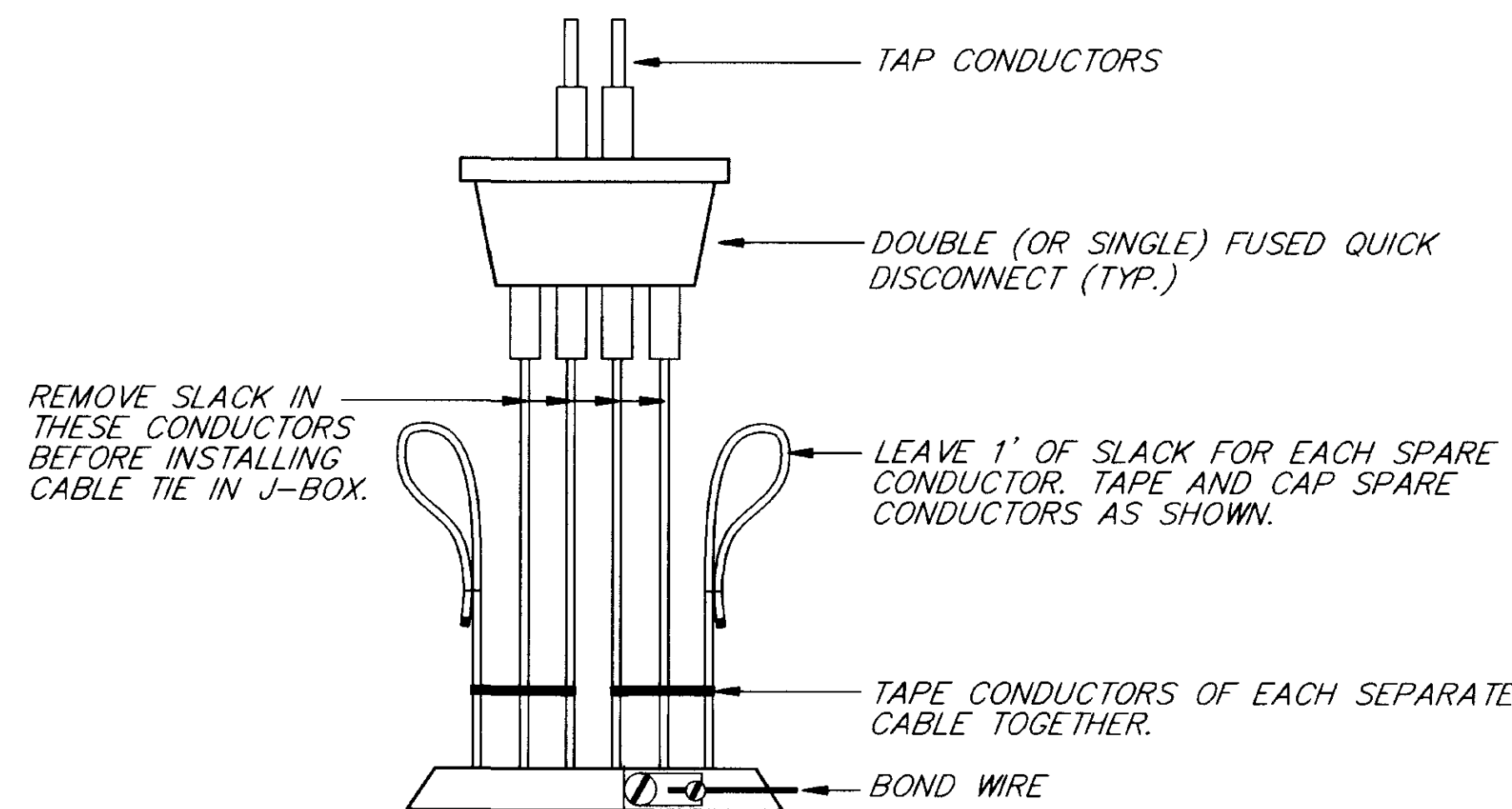


**FOUNDATION WITH BREAKAWAY COUPLINGS**



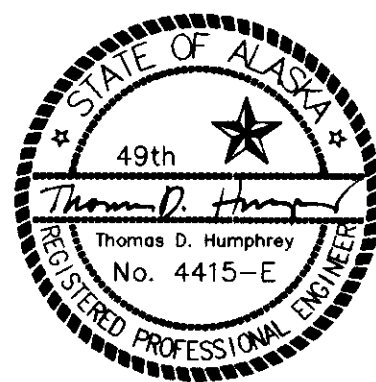
**DETAIL B**

NOTE: SEE DETAIL "A" FOR THREE-WAY SPLICE



**DETAIL C**  
( IN POLE BASE )

NOTE: DO NOT SCALE FROM THESE PLANS—USE DIMENSIONS



DATE	BY	REV	REVISION	FIELD BOOKS	DESIGNED	M.C.S.
				DESIGN	DRAWN	M.C.S.
				STAKING	CHECKED	T.D.H.
				AS-BUILT	DATE	3/21/94
				SCALE	GRID	
				HOR. AS NOTED	JOB NO.	AT03

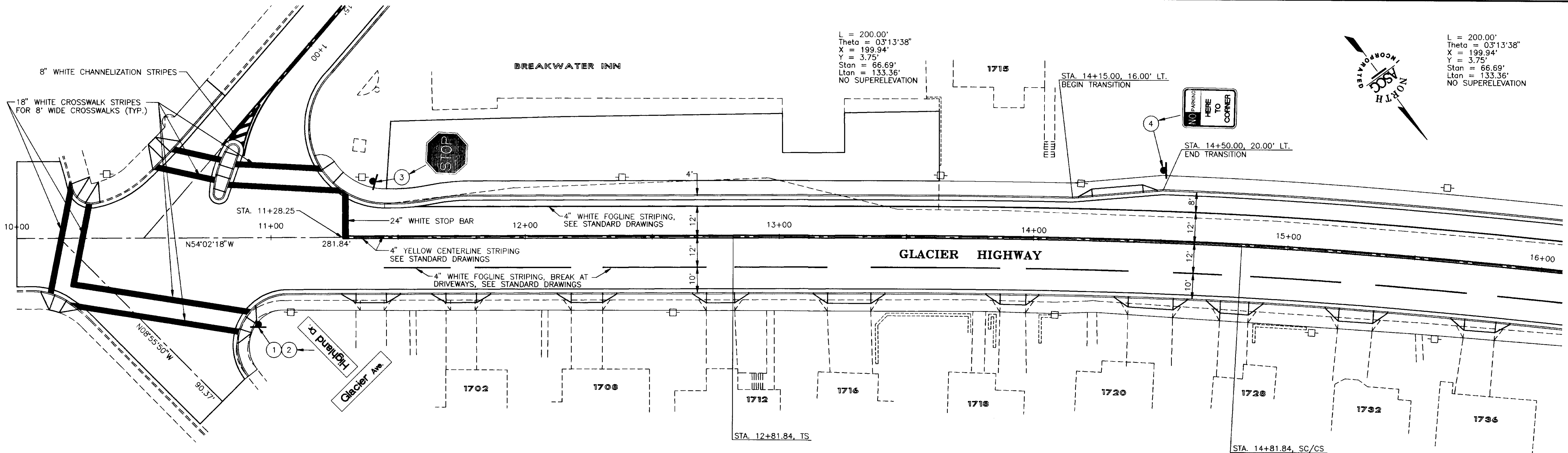
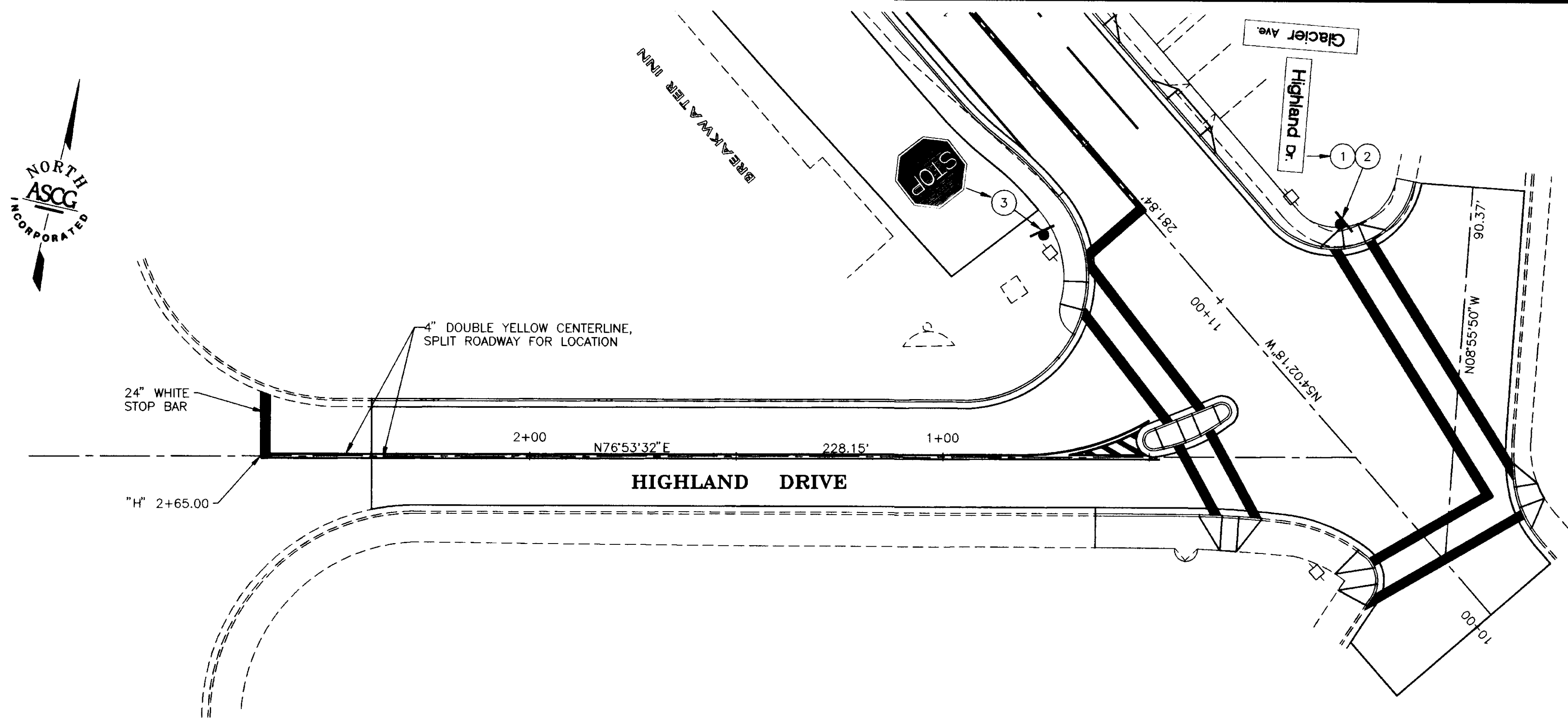


**RAJ BHARGAVA ASSOCIATES**  
ENGINEERING IN ALASKA

301 E. FIREWEED LANE, ANCHORAGE, ALASKA 99503 (907) 276-3768

STATE OF ALASKA DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
**JUNEAU-GLACIER HIGHWAY ILLUMINATION DETAILS**

SHEET 47A OF 5  
FILE NO. AT03-



L = 200.00'  
 Theta = 03°13'38"  
 X = 199.94'  
 Y = 3.75'  
 Stan = 66.69'  
 Ltan = 133.36'  
 NO SUPERELEVATION

L = 200.00'  
 Theta = 03°13'38"  
 X = 199.94'  
 Y = 3.75'  
 Stan = 66.69'  
 Ltan = 133.36'  
 NO SUPERELEVATION

NO.	DATE	DESCRIPTION OF CHANGE

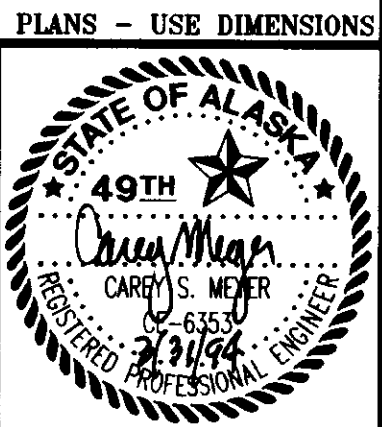
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 SIGNING STRIPING & UTILITIES  
 STA. 10+00.00 TO STA. 16+00.00

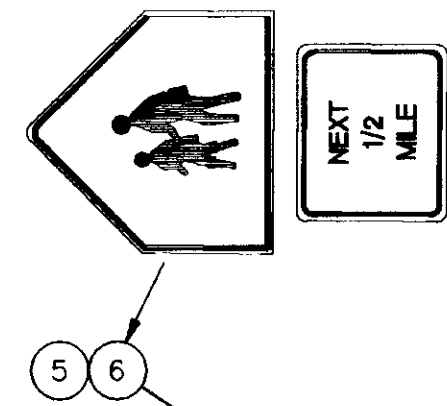


DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

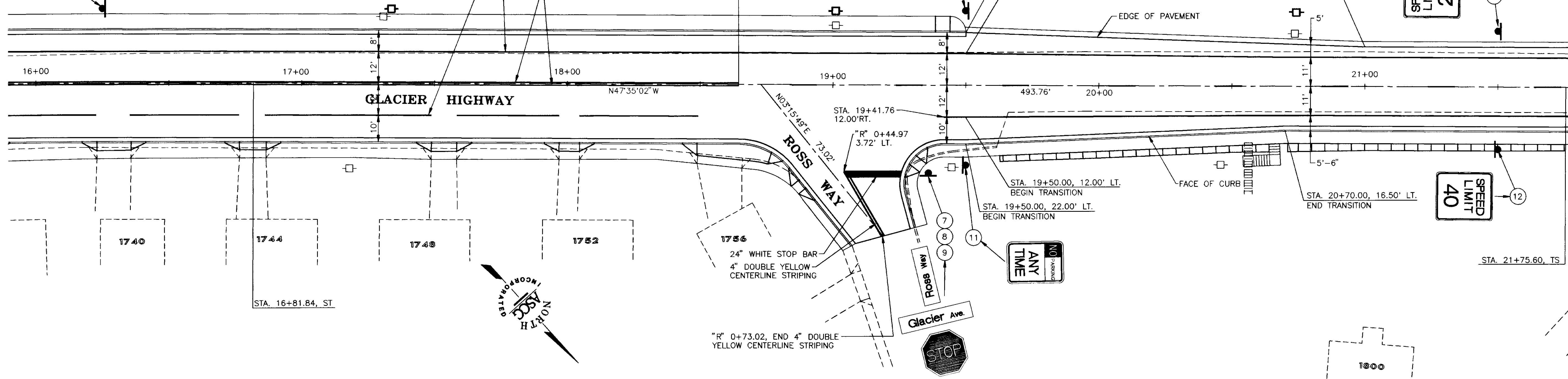
PROJECT No. 69062  
 DATE: MARCH 1994  
 SHEET 48 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



L = 200.00'  
 Theta = 03°13'38"  
 X = 199.94'  
 Y = 3.75'  
 Stan = 66.69'  
 Ltan = 133.36'  
 NO SUPERELEVATION



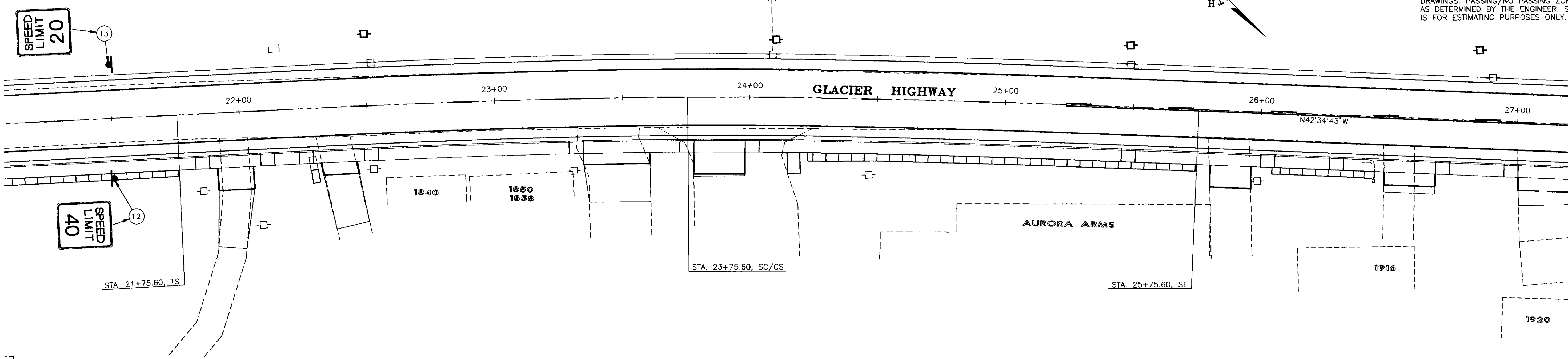
STA. 18+64.50, END 4" DOUBLE YELLOW CENTERLINE STRIPING, STA. 18+64.50 TO STA. 81+00.00 (E.O.P.) CENTERLINE STRIPING TO BE AS PER STANDARD DWGS.

NOTE:  
 CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY TO STA. 78+00.00 TO BE AS PER STANDARD DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE AS DETERMINED BY THE ENGINEER. STRIPING SHOWN IS FOR ESTIMATING PURPOSES ONLY.

L = 200.00'  
 Theta = 02°30'09"  
 X = 199.96'  
 Y = 2.91'  
 Stan = 66.68'  
 Ltan = 133.35'  
 S = 3%  
 BST = 21+75.60  
 BFS = 23+75.60

L = 200.00'  
 Theta = 02°30'09"  
 X = 199.96'  
 Y = 2.91'  
 Stan = 66.68'  
 Ltan = 133.35'  
 S = 3%  
 EFS = 23+75.60  
 EST = 25+75.60

NOTE:  
 CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY TO STA. 78+00.00 TO BE AS PER STANDARD DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE AS DETERMINED BY THE ENGINEER. STRIPING SHOWN IS FOR ESTIMATING PURPOSES ONLY.



BY:	DATE:	DESCRIPTION OF CHANGE:
<b>RECORD OF REVISIONS</b>		

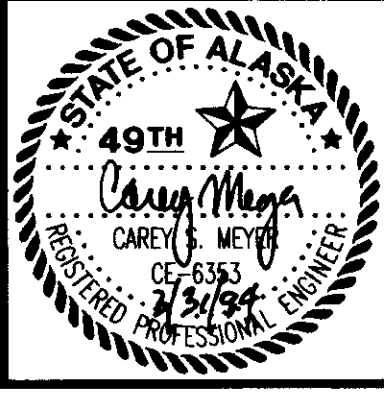
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 SIGNING STRIPING & UTILITIES  
 STA. 16+00.00 TO STA. 27+00.00



DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

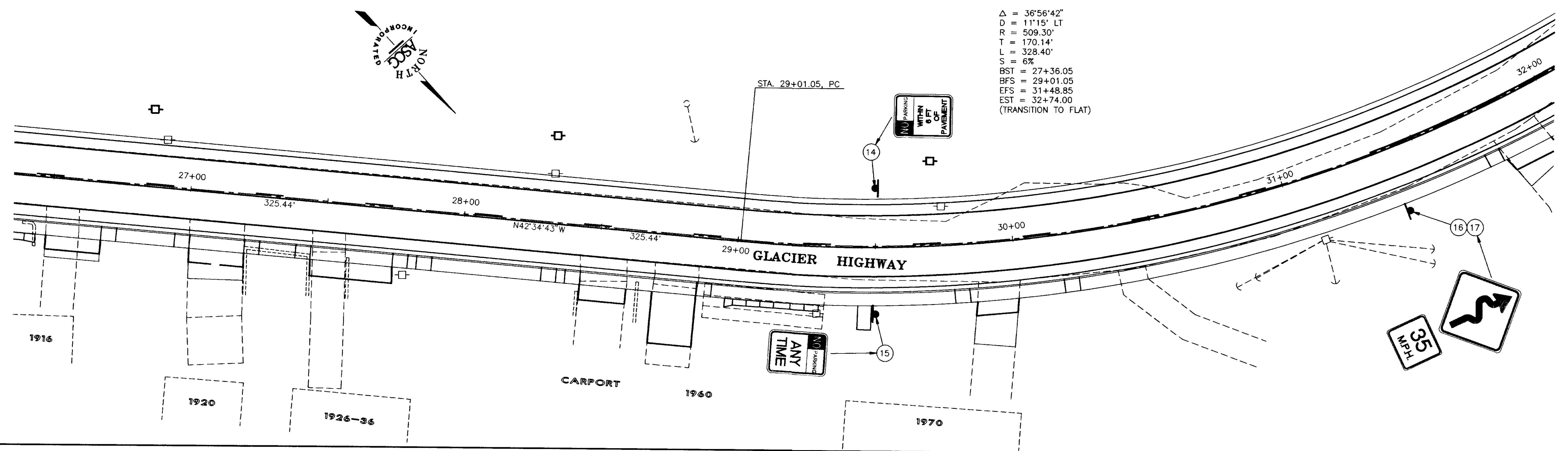
PROJECT No. 69062  
 DATE: MARCH 1994  
 SHEET 49 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

**NOTE:**

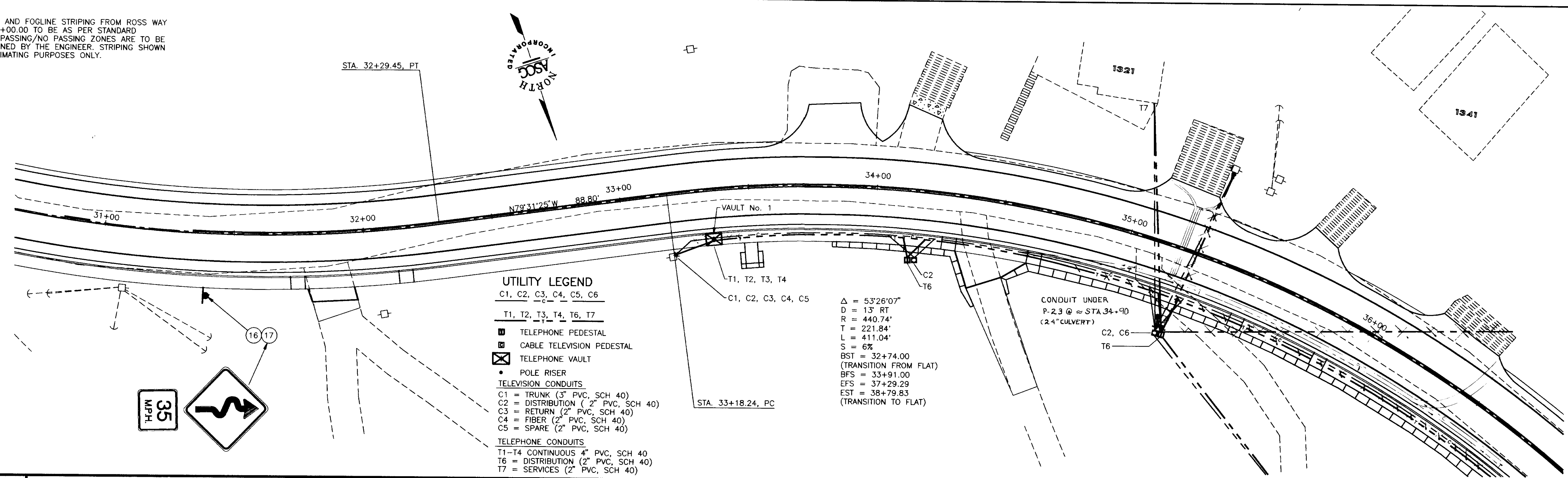
CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY TO STA. 78+00.00 TO BE AS PER STANDARD DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE AS DETERMINED BY THE ENGINEER. STRIPING SHOWN IS FOR ESTIMATING PURPOSES ONLY.



$\Delta = 36'56''42''$   
 $D = 11'15''$  LT  
 $R = 509.30'$   
 $T = 170.14'$   
 $L = 328.40'$   
 $S = 6\%$   
 $BST = 27+36.05$   
 $BFS = 29+01.05$   
 $EFS = 31+48.85$   
 $EST = 32+74.00$   
 (TRANSITION TO FLAT)

**NOTE:**

CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY TO STA. 78+00.00 TO BE AS PER STANDARD DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE AS DETERMINED BY THE ENGINEER. STRIPING SHOWN IS FOR ESTIMATING PURPOSES ONLY.



**UTILITY LEGEND**

C1, C2, C3, C4, C5, C6

T1, T2, T3, T4, T6, T7

TELEPHONE PEDESTAL  
 CABLE TELEVISION PEDESTAL  
 TELEPHONE VAULT  
 POLE RISER

**TELEVISION CONDUITS**

C1 = TRUNK (3" PVC, SCH 40)  
 C2 = DISTRIBUTION (2" PVC, SCH 40)  
 C3 = RETURN (2" PVC, SCH 40)  
 C4 = FIBER (2" PVC, SCH 40)  
 C5 = SPARE (2" PVC, SCH 40)

**TELEPHONE CONDUITS**

T1-T4 CONTINUOUS 4" PVC, SCH 40  
 T6 = DISTRIBUTION (2" PVC, SCH 40)  
 T7 = SERVICES (2" PVC, SCH 40)

$\Delta = 53'26''07''$   
 $D = 13'$  RT  
 $R = 440.74'$   
 $T = 221.84'$   
 $L = 411.04'$   
 $S = 6\%$   
 $BST = 32+74.00$   
 (TRANSITION FROM FLAT)  
 $BFS = 33+91.00$   
 $EFS = 37+29.29$   
 $EST = 38+79.83$   
 (TRANSITION TO FLAT)

CONDUIT UNDER  
 P-2.3 @ STA. 34+90  
 (2.4" CULVERT)

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

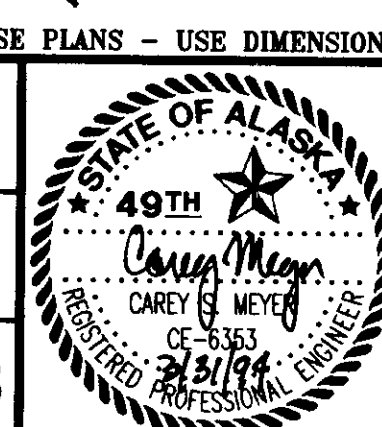
NO.	DATE	DESCRIPTION OF CHANGE

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 SIGNING STRIPING & UTILITIES  
 STA. 27+00.00 STA. 36+00.00



DESIGNED BY:	D.L.M.	PROJECT No.	69062
DRAWN BY:	J.E.M.	DATE:	MARCH 1994
CHECKED BY:	D.L.M.	SHEET	50 OF 55



**UTILITY LEGEND**

C1, C2, C3, C4, C5, C6

T1, T2, T3, T4, T6, T7

- ☐ TELEPHONE PEDESTAL
- ☐ CABLE TELEVISION PEDESTAL
- ☒ TELEPHONE VAULT
- POLE RISER

**TELEVISION CONDUITS**

- C1 = TRUNK (3" PVC, SCH 40)
- C2 = DISTRIBUTION (2" PVC, SCH 40)
- C3 = RETURN (2" PVC, SCH 40)
- C4 = FIBER (2" PVC, SCH 40)
- C5 = SPARE (2" PVC, SCH 40)

**TELEPHONE CONDUITS**

- T1-T4 CONTINUOUS 4" PVC, SCH 40
- T6 = DISTRIBUTION (2" PVC, SCH 40)
- T7 = SERVICES (2" PVC, SCH 40)

L = 150.00'  
 Theta = 09°21'15"  
 X = 149.60'  
 Y = 8.15'  
 Stan = 50.13'  
 Ltan = 100.14'  
 S = 6%  
 BST = 38+79.83  
 (TRANSITION FROM FLAT)  
 BFS = 40+29.83

L = 150.00'  
 Theta = 09°21'15"  
 X = 149.60'  
 Y = 8.15'  
 Stan = 50.13'  
 Ltan = 100.14'  
 S = 6%  
 EFS = 40+29.83  
 EST = 41+88.00  
 (TRANSITION TO FLAT)

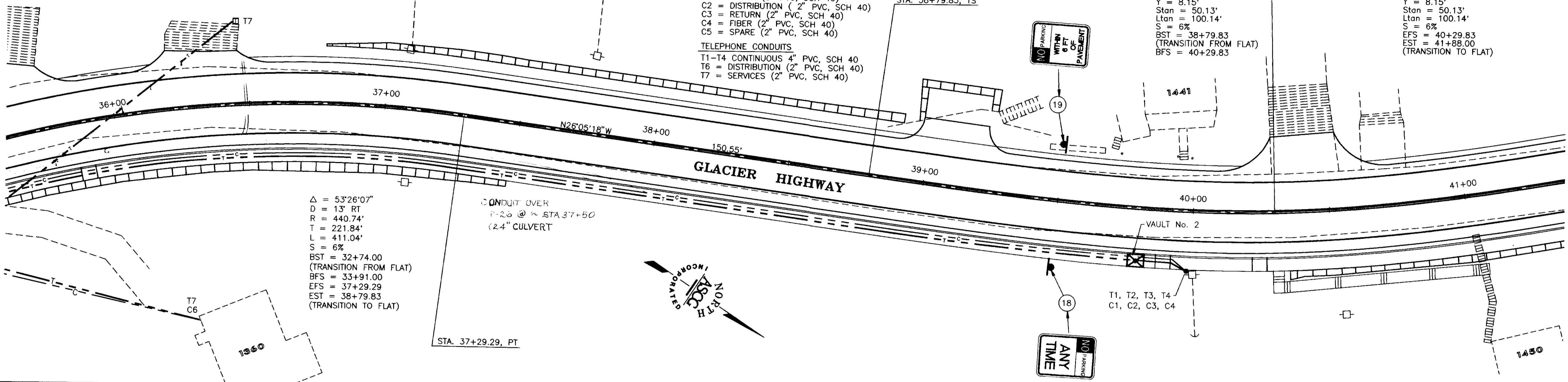
Δ = 53°26'07"  
 D = 13' RT  
 R = 440.74'  
 T = 221.84'  
 L = 411.04'  
 S = 6%  
 BST = 32+74.00  
 (TRANSITION FROM FLAT)  
 BFS = 33+91.00  
 EFS = 37+29.29  
 EST = 38+79.83  
 (TRANSITION TO FLAT)

CONDUIT OVER  
 P-26 @ STA 37+50  
 (2.4" CULVERT)



**NOTE:**

CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY TO STA. 78+00.00 TO BE AS PER STANDARD DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE AS DETERMINED BY THE ENGINEER. STRIPING SHOWN IS FOR ESTIMATING PURPOSES ONLY



**NOTE:**

CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY TO STA. 78+00.00 TO BE AS PER STANDARD DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE AS DETERMINED BY THE ENGINEER.

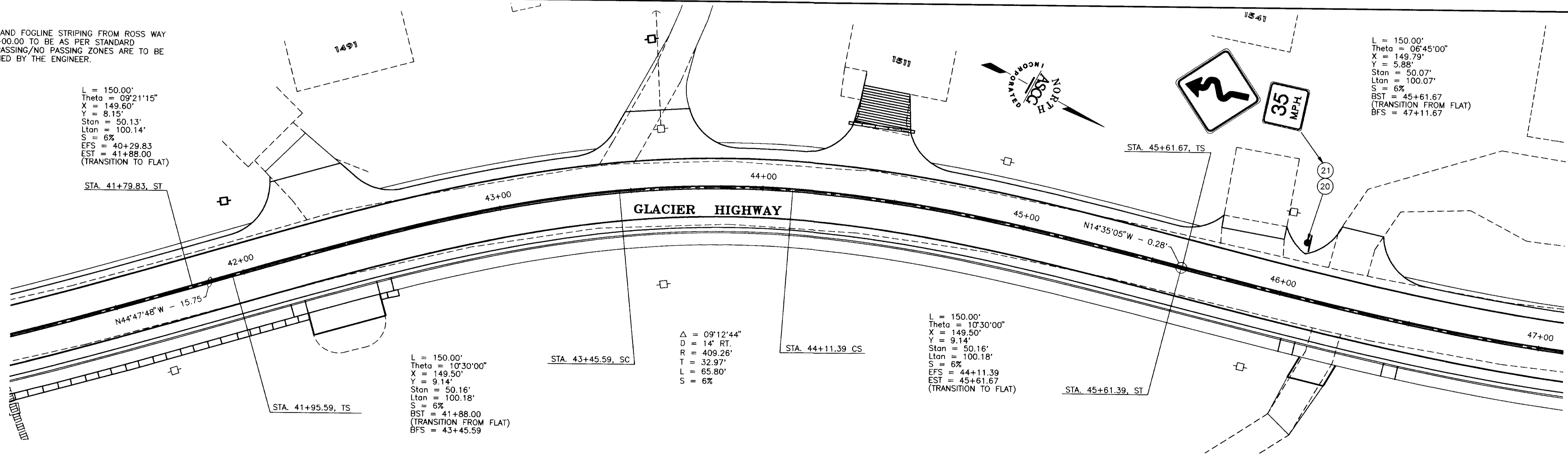
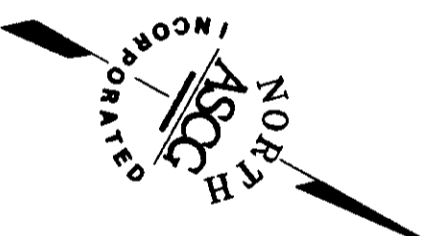
L = 150.00'  
 Theta = 09°21'15"  
 X = 149.60'  
 Y = 8.15'  
 Stan = 50.13'  
 Ltan = 100.14'  
 S = 6%  
 EFS = 40+29.83  
 EST = 41+88.00  
 (TRANSITION TO FLAT)

L = 150.00'  
 Theta = 06°45'00"  
 X = 149.79'  
 Y = 5.88'  
 Stan = 50.07'  
 Ltan = 100.07'  
 S = 6%  
 BST = 45+61.67  
 (TRANSITION FROM FLAT)  
 BFS = 47+11.67

L = 150.00'  
 Theta = 10°30'00"  
 X = 149.50'  
 Y = 9.14'  
 Stan = 50.16'  
 Ltan = 100.18'  
 S = 6%  
 BST = 41+88.00  
 (TRANSITION FROM FLAT)  
 BFS = 43+45.59

Δ = 09°12'44"  
 D = 14' RT  
 R = 409.26'  
 T = 32.97'  
 L = 65.80'  
 S = 6%

L = 150.00'  
 Theta = 10°30'00"  
 X = 149.50'  
 Y = 9.14'  
 Stan = 50.16'  
 Ltan = 100.18'  
 S = 6%  
 EFS = 44+11.39  
 EST = 45+61.67  
 (TRANSITION TO FLAT)



NO.	DATE	DESCRIPTION OF CHANGE

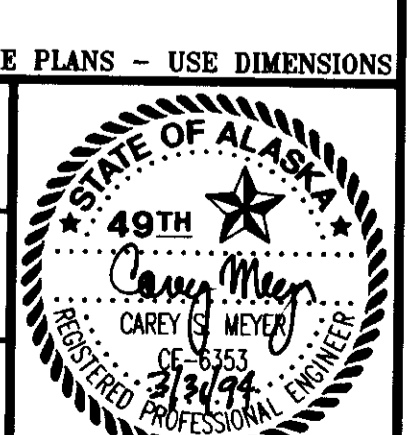
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 SIGNING STRIPING & UTILITIES  
 STA. 36+00.00 TO STA. 47+00.00

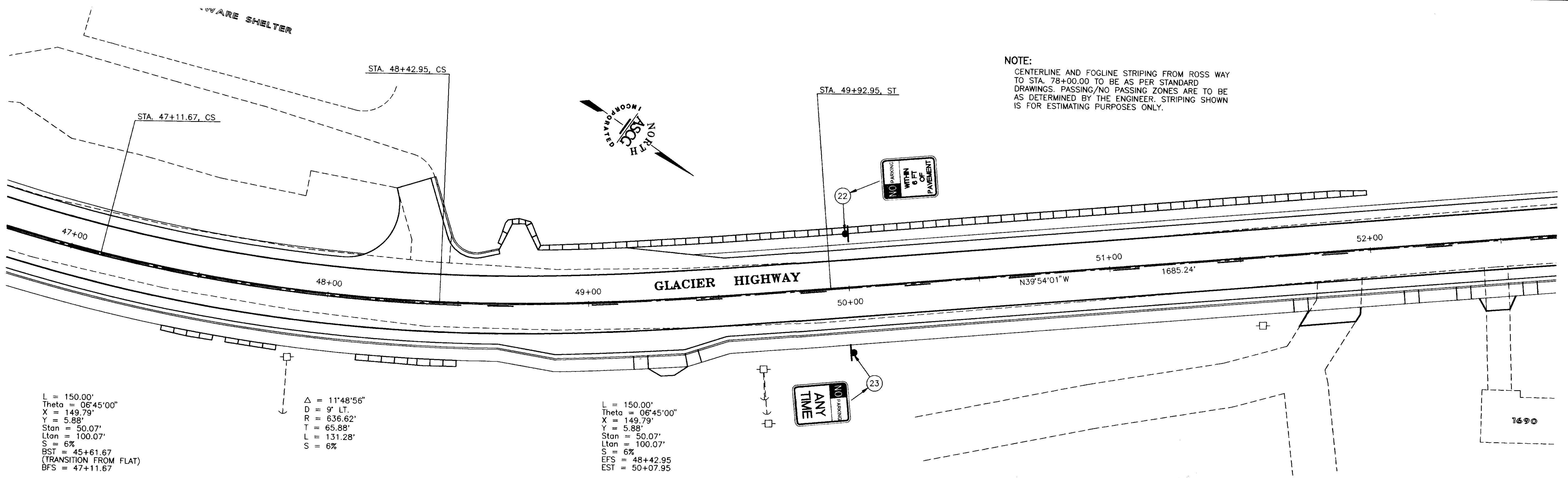


DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No. 69062  
 DATE: MARCH 1994  
 SHEET 51 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS



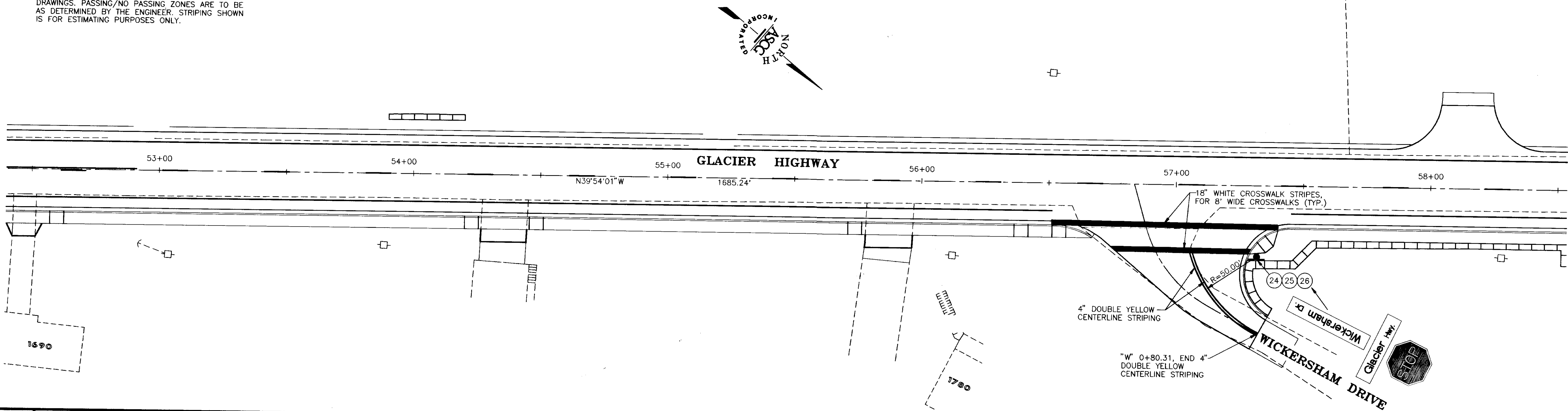
NOTE:  
 CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY  
 TO STA. 78+00.00 TO BE AS PER STANDARD  
 DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE  
 AS DETERMINED BY THE ENGINEER. STRIPING SHOWN  
 IS FOR ESTIMATING PURPOSES ONLY.

L = 150.00'  
 Theta = 06°45'00"  
 X = 149.79'  
 Y = 5.88'  
 Stan = 50.07'  
 Lton = 100.07'  
 S = 6%  
 BST = 45+61.67  
 (TRANSITION FROM FLAT)  
 BFS = 47+11.67

Δ = 11°48'56"  
 D = 9' LT.  
 R = 636.62'  
 T = 65.88'  
 L = 131.28'  
 S = 6%

L = 150.00'  
 Theta = 06°45'00"  
 X = 149.79'  
 Y = 5.88'  
 Stan = 50.07'  
 Lton = 100.07'  
 S = 6%  
 EFS = 48+42.95  
 EST = 50+07.95

NOTE:  
 CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY  
 TO STA. 78+00.00 TO BE AS PER STANDARD  
 DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE  
 AS DETERMINED BY THE ENGINEER. STRIPING SHOWN  
 IS FOR ESTIMATING PURPOSES ONLY.



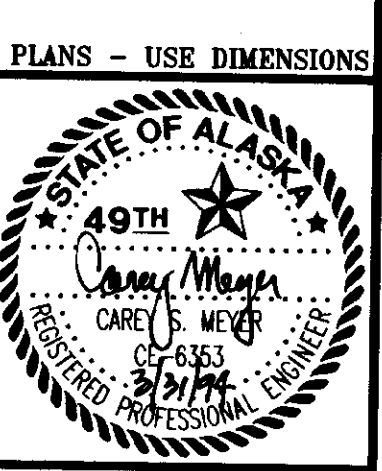
DATE:	DESCRIPTION OF CHANGE:

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
 FM-0976(1) 89062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 SIGNING STRIPING & UTILITIES  
 STA. 47+00.00 TO STA. 58+50.00

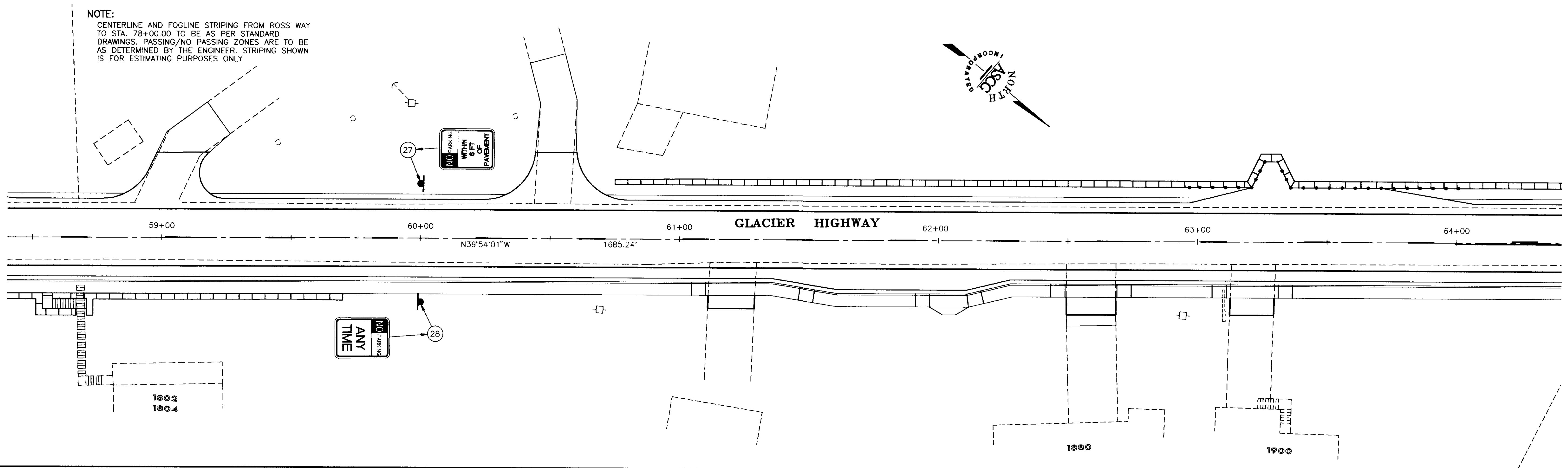
**ASCG**  
 INCORPORATED  
 ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY:	D.L.M.	PROJECT No.	69062
DRAWN BY:	J.E.M.	DATE:	MARCH 1994
CHECKED BY:	D.L.M.	SHEET	52 OF 55



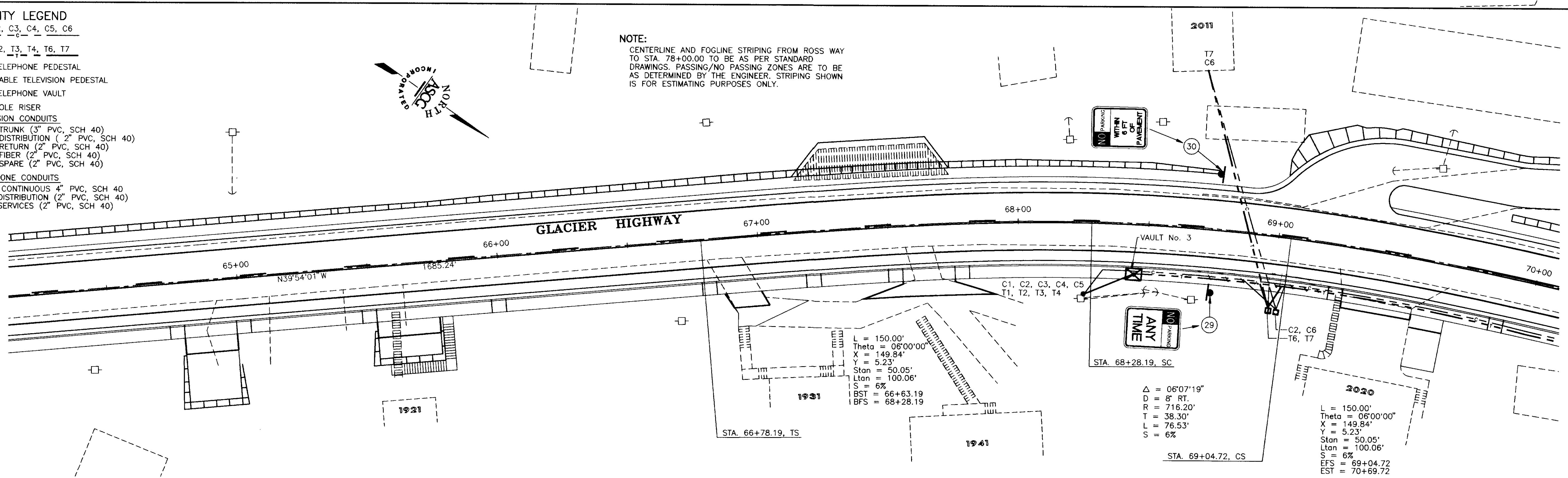
DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

NOTE:  
 CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY  
 TO STA. 78+00.00 TO BE AS PER STANDARD  
 DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE  
 AS DETERMINED BY THE ENGINEER. STRIPING SHOWN  
 IS FOR ESTIMATING PURPOSES ONLY



- UTILITY LEGEND**  
 C1, C2, C3, C4, C5, C6  
 T1, T2, T3, T4, T6, T7
- ☐ TELEPHONE PEDESTAL
  - ☐ CABLE TELEVISION PEDESTAL
  - ☒ TELEPHONE VAULT
  - POLE RISER
- TELEVISION CONDUITS**  
 C1 = TRUNK (3" PVC, SCH 40)  
 C2 = DISTRIBUTION (2" PVC, SCH 40)  
 C3 = RETURN (2" PVC, SCH 40)  
 C4 = FIBER (2" PVC, SCH 40)  
 C5 = SPARE (2" PVC, SCH 40)
- TELEPHONE CONDUITS**  
 T1-T4 CONTINUOUS 4" PVC, SCH 40  
 T6 = DISTRIBUTION (2" PVC, SCH 40)  
 T7 = SERVICES (2" PVC, SCH 40)

NOTE:  
 CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY  
 TO STA. 78+00.00 TO BE AS PER STANDARD  
 DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE  
 AS DETERMINED BY THE ENGINEER. STRIPING SHOWN  
 IS FOR ESTIMATING PURPOSES ONLY.



$L = 150.00'$   
 $\text{Theta} = 06'00'00''$   
 $X = 149.84'$   
 $Y = 5.23'$   
 $\text{Stan} = 50.05'$   
 $\text{Ltan} = 100.06'$   
 $S = 6\%$   
 $\text{BST} = 66+63.19$   
 $\text{BFS} = 68+28.19$

$\Delta = 06'07'19''$   
 $D = 8' \text{ RT.}$   
 $R = 716.20'$   
 $T = 38.30'$   
 $L = 76.53'$   
 $S = 6\%$

$L = 150.00'$   
 $\text{Theta} = 06'00'00''$   
 $X = 149.84'$   
 $Y = 5.23'$   
 $\text{Stan} = 50.05'$   
 $\text{Ltan} = 100.06'$   
 $S = 6\%$   
 $\text{EFS} = 69+04.72$   
 $\text{EST} = 70+69.72$

DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS

NO.	DATE	DESCRIPTION OF CHANGE

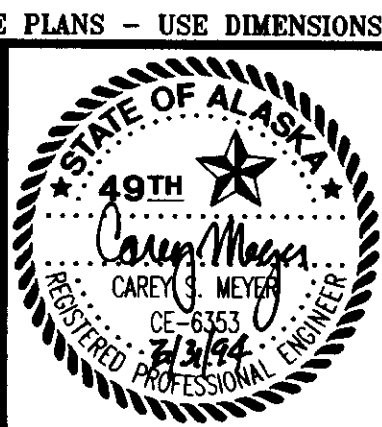
STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND  
 PUBLIC FACILITIES  
 SOUTHEAST REGION DESIGN & CONSTRUCTION

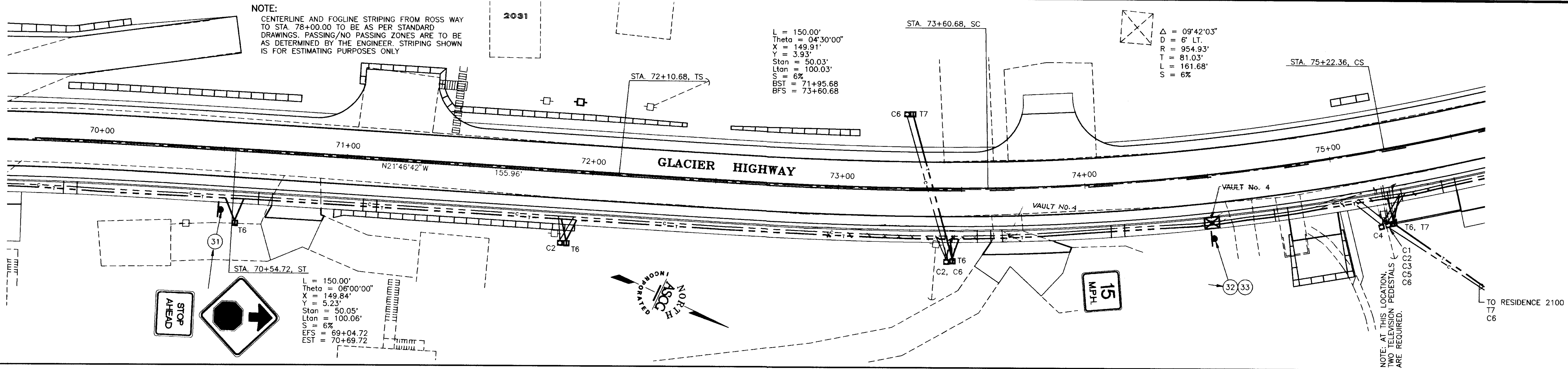
JUNEAU  
 FM-0978(1) 69062  
 GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
 SIGNING STRIPING & UTILITIES  
 STA. 58+50.000 STA. 70+00.00



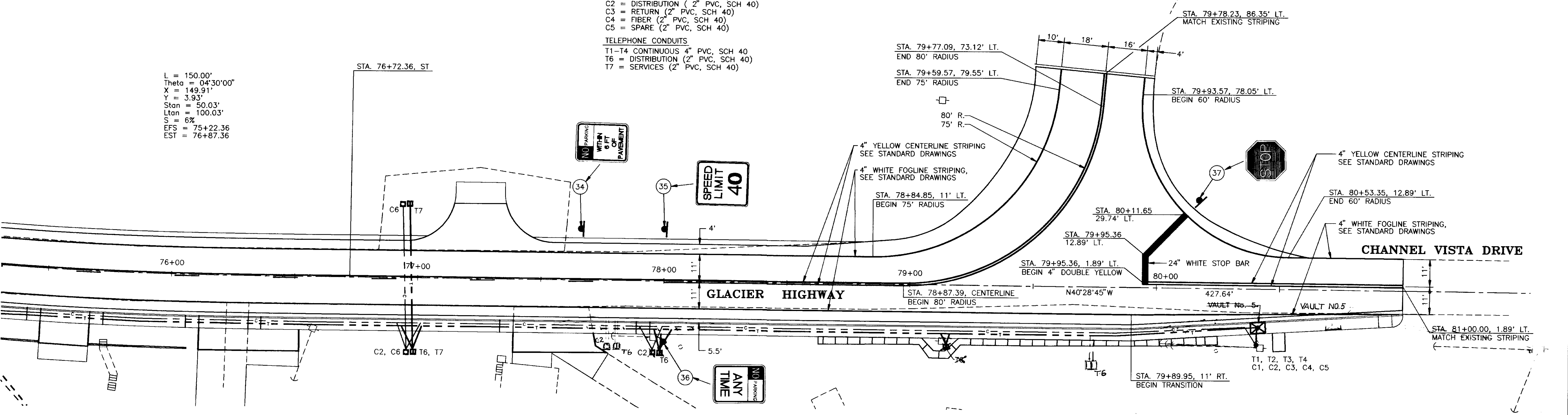
DESIGNED BY: D.L.M.  
 DRAWN BY: J.E.M.  
 CHECKED BY: D.L.M.

PROJECT No.  
 69062  
 DATE:  
 MARCH 1994  
 SHEET 53 OF 55





**NOTE:**  
CENTERLINE AND FOGLINE STRIPING FROM ROSS WAY TO STA. 78+00.00 TO BE AS PER STANDARD DRAWINGS. PASSING/NO PASSING ZONES ARE TO BE AS DETERMINED BY THE ENGINEER. STRIPING SHOWN IS FOR ESTIMATING PURPOSES ONLY.



NO.	DATE	DESCRIPTION OF CHANGE

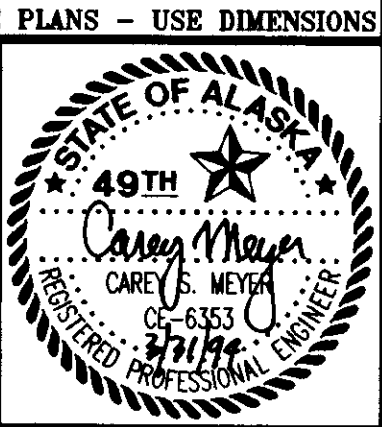
STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES  
SOUTHEAST REGION DESIGN & CONSTRUCTION

JUNEAU  
FM-0976(1) 69062  
GLACIER HIGHWAY, HIGHLAND DR. TO EGAN DR.  
SIGNING STRIPING & UTILITIES  
STA. 70+00.00 STA. 81+00.00

**ASCG**  
INCORPORATED  
ENGINEERS • ARCHITECTS • SCIENTISTS • SURVEYORS

DESIGNED BY: D.L.M.  
DRAWN BY: J.E.M.  
CHECKED BY: D.L.M.

PROJECT No. 69062  
DATE: MARCH 1994  
SHEET 54 OF 55



DO NOT SCALE FROM THESE PLANS - USE DIMENSIONS