

**Erosion and Sediment Control Plan**  
**For**  
**Anchorage Area Drainage Preservation:**  
**Dimond Blvd – Jewel Lake Rd to Seward Hwy**  
**Seward Hwy – Tudor Rd to 20th Ave**  
**0001822 / CFHWY01389**

**Anchorage, Alaska**



**Alaska Department of Transportation & Public Facilities**  
**Central Region**  
**P.O. Box 196900**  
**Anchorage, Alaska USA 99519-6900**

**Prepared By: Tanner Lees, EIT**

**Company Name: DOWL**

**ESCP Preparation Date: November 2025**

*The following Erosion and Sediment Control Plan (ESCP) has been prepared by the Alaska Department of Transportation and Public Facilities (DOT&PF) to assist bidders in successfully planning their construction means and methods to comply with the 2021 Alaska Construction General Permit (CGP), United States Army Corps of Engineers (USACE) 404/10 Permit, Alaska Department of Environmental Conservation (DEC) 401 Water Quality Certification, Alaska Department of Fish and Game (ADF&G) Title 16, and other permits associated with this project. This document is not intended to be all inclusive of the best management practices (BMP's) that will be required to reduce the potential for sediment discharge during construction and comply with permit conditions or construction specifications. This ESCP is intended to guide contractors during the bidding process and assist in the preparation of the contractor's Storm Water Pollution Prevention Plan (SWPPP) that must be approved prior to commencing construction after award. The contractor is responsible for the risk assessment analysis, planning, preparation and implementation of the SWPPP.*

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## APPENDICES

Appendices that are marked with **(ESCP)** are to be filled out by the Designer. All other appendices are to be filled out by the SWPPP preparer and will not be included in the ESCP.

- Appendix A Site Maps and Drawings **(ESCP)**
- Appendix B BMP Details **(ESCP)**
- Appendix C Project Schedule
- Appendix D Supporting Documentation: **(ESCP)**
  - TMDLs
  - Endangered Species
  - Historic Properties
  - DEC Non-Domestic Wastewater Plan Review Non-Objection Letter (if required)
  - DEC Dewatering Permit (if required)
  - Environmental Permits and Commitments
  - Other Permits or Requirements
- Appendix E Project Specific ESCP Discussion & Comments **(ESCP – not part of the SWPPP template)**

The above Appendix E is for ESCP writers only and should include any additional information that the Designer would like to share with the SWPPP preparer. Below is the list of Appendices to be included in the SWPPP.

- Appendix E Delegation of Authority (25D-107, 25D-108), Subcontractor Certifications (25D-105), Project Staff Tracking (25D-127) and Personnel Qualifications
- Appendix F Permit Conditions:
  - Copy of Signed Notice of Intent
  - Copy of Letters from DEC Authorizing Coverage, with DEC NOI Tracking Number
  - Copy of 2021 Alaska Construction General Permit
- Appendix G Grading and Stabilization Records (25D-110)
- Appendix H Monitoring Plan (if applicable) and Reports
- Appendix I Training Records (25D-125)
- Appendix J Corrective Action Log and Delayed Action Item Reports (25D-112, 25D-113)
- Appendix K Inspection Records (25D-100)
- Appendix L SWPPP Preconstruction Site Visit (25D-106)
- Appendix M SWPPP Amendment Log (25D-114)
- Appendix N Daily Record of Rainfall (25D-115)
- Appendix O Hazardous Materials Control Plan
- Appendix P Treatment Chemical/Active Treatment Systems (if applicable)
- Appendix Q Other
  - Anti-Degradation Analysis (if applicable)
  - Correspondence with Regulatory Agencies
  - Notices of Termination

## 1.0 PERMITTEE (5.3.1)

The Department of Transportation & Public Facilities (DOT&PF) will be a permittee for the project. Upon the approval of the contractor's Storm Water Pollution Prevention Plan (SWPPP) by DOT&PF, the contractor will be required to submit a Notice of Intent (NOI) and obtain permit coverage as an operator. The contractor's contact information as well as contact information for all subcontractors must be included in the contractor's SWPPP. All subcontractors will be required to sign a certification (DOT&PF Form 25D-105) that they have read the Alaska Construction General Permit (CGP) and the contractor's SWPPP and will adhere to their terms and conditions.

### 1.1 Operator(s)/Contractor(s)

Operator Information			
Organization: Enter Text	Name: Enter Text	Title: Enter Text	
Phone: Enter Text	Fax (optional): Enter Text	Email: Enter Text	
Mailing Address:	Street (PO Box): Enter Text		
	City: Enter Text	State: Enter Text	Zip: Enter Text
Area of Control	Day-to-day operational control of those activities at a site which are necessary to ensure compliance with a SWPPP or other permit conditions.		

The contractor has day-to-day operational control over activities in the field, including subcontractors, installing, maintaining, and inspecting all erosion and sediment controls and implementation of the SWPPP.

Owner/Operator Information			
Organization: State of Alaska Department of Transportation and Public Facilities (DOT&PF)	Name: Enter Text	Title: Enter Text	
Phone: Enter Text	Fax (optional): Enter Text	Email: Enter Text	
Mailing Address:	Street (PO Box): P.O. Box 196900		
	City: Anchorage	State: Alaska	Zip: 99519-6900
Area of Control	Operational control over construction plans and specifications, including the ability to make modifications to those plans and specifications.		

## 1.2 Subcontractors

Subcontractor Information			
Organization: Enter Text		Name: Enter Text	Title: Enter Text
Phone: Enter Text	Fax (optional): Enter Text	Email: Enter Text	
Mailing Address:	Street (PO Box): Enter Text		
	City: Enter Text	State: Enter Text	Zip: Enter Text
Area of Control	Insert Area of Control (if more than one operator at site)		

Repeat as necessary to include all subcontractors. Include any Utility company and the Utility companies' contractors' doing concurrent relocation as a subcontractor – see subsection 641-1.07.

## 2.0 STORM WATER CONTACTS (5.3.2)

Identify the qualified persons responsible for the following required positions (note: a small project may have all these responsibilities carried out by one person):

Superintendent; DOT&PF's Project Engineer; Storm Water Lead (5.3.2.1); SWPPP Preparer (5.3.2.2); Person(s) Conducting Inspections- Contractor's SWPPP Manager and DOT&PF's Storm Water Inspector (5.3.2.3); Person(s) Conducting Monitoring (if applicable, 5.3.2.4), and Person(s) Operating Active Treatment System (if applicable, 5.3.2.5).

Document that the named individuals are Qualified Persons as described in CGP Appendix C. Include documentation of qualifications in Appendix E of the SWPPP.

Qualified Personnel	Responsibility
<p><b>Contractor's Superintendent</b>                      Company                      Name                      Address                      City, State, Zip Code                      Telephone #                      Fax/Email</p>	<p>The Contractor's duly authorized representative in responsible charge of the work. Authority for the overall operation of the Project and for Contractor furnished sites and facilities directly related to the Project.</p>
<p><b>DOT&amp;PF's Project Engineer</b>                      Company                      Name                      Address                      City, State, Zip Code                      Telephone #                      Fax/Email</p>	<p>The DOT&amp;PF's duly authorized representative in responsible charge of the work. Authority to stop and/or modify construction activities as necessary to comply with the SWPPP and the terms and conditions of the permit. Must approve all amendments.</p>
<p><b>SWPPP Manager (Storm Water Lead and Inspector)</b>                      Company                      Name                      Address                      City, State, Zip Code                      Telephone #                      Fax/Email</p>	<p>Authority to stop and/or modify construction activities as necessary to comply with the SWPPP and the terms and conditions of the permit. Assess conditions at the construction site that could impact storm water quality. Assess the effectiveness of any erosion and sediment control measures selected to control the quality of storm water discharge, and familiar with Part 6 as a means to ensure compliance with the permit.</p>
<p><b>SWPPP Preparer</b>                      Company                      Name                      Address                      City, State, Zip Code                      Telephone #                      Fax/Email</p>	<p>Possess the skills to assess conditions at the construction site that could impact storm water quality. Familiar with Part 5 as a means to implement the permit.</p>

<b>DOT&amp;PF's Storm Water Inspector</b> Company Name Address City, State, Zip Code Telephone # Fax/Email	Assess conditions at the construction site that could impact storm water quality. Assess the effectiveness of any erosion and sediment control measures selected to control the quality of storm water discharge, and familiar with Part 6 as a means to ensure compliance with the permit.
<b>Monitoring Person (If Applicable)</b> Company Name Address City, State, Zip Code Telephone # Fax/Email	Knowledgeable in the principles and practices of water quality monitoring who is familiar with Part 7 and the monitoring plan for the site and how to conduct water quality sampling, testing, and reporting.
<b>Active Treatment System Operator (If Applicable)</b> Company Name Address City, State, Zip Code Telephone # Fax/Email	Knowledgeable in the principles and practices of treatment systems that employs chemical coagulation, chemical flocculation or electrocoagulation to aid in the treatment of storm water runoff. Familiar with Part 4.5 as a means to implement and comply with the permit.

A SWPPP Project Staff Tracking log (Form 25D-127) will be included in Appendix E to document any changes in personnel for the positions of Superintendent, Project Engineer, SWPPP Manager, and Inspectors.

Delete the information below prior to submittal of SWPPP. This information is provided for the SWPPP Preparer and is not part of the SWPPP template.

## 2.1 Contact Information for SWPPP Preparation

The following people may be contacted for questions when writing the SWPPP:

<u>Name</u>	<u>Phone</u>	<u>Email</u>
Athena Marinkovic	(907) 269-0436	<a href="mailto:athena.marinkovic@alaska.gov">athena.marinkovic@alaska.gov</a>

### 3.0 PROJECT INFORMATION (5.3.3)

#### 3.1 Project Information

Project Name: Anchorage Area Drainage Preservation			
Location Address:	Street/Location: Dimond Blvd – Jewel Lake Rd to Seward Hwy Seward Hwy – Tudor Rd to 20th Ave	Borough or similar government subdivision: Municipality of Anchorage (MOA)	
	City: Anchorage	State: Alaska	Zip: 99502 (Dimond) 99503 (Seward)
Latitude (decimal degree, 5 places): Dimond Blvd 61.141183° N Seward Hwy 61.190342° N		Longitude (decimal degree, 5 places): Dimond Blvd – 149.904694° W Seward Hwy – 149.867508° W	
Determined By: <input type="checkbox"/> GPS <input type="checkbox"/> Web Map:		<input type="checkbox"/> USGS Topo Map, Scale:	<input checked="" type="checkbox"/> Other: Google Earth

Dimond Blvd from Jewel Lake Rd to Seward Hwy is classified as a Principal Arterial roadway with an annual average daily traffic (AADT) ranging from 18,000 - 34,500 vehicles per day, as presented on the Alaska Traffic Data website <https://alaskatrafficdata.drakewell.com/publicmultinodemap.asp>. The project consists of 6 driving lanes and a median for most of the project, with intermittent turn lanes. The section of Dimond Blvd from Jewel Lake Rd to Arlene St has a narrower corridor with 4 driving lanes.

Seward Hwy from Tudor Rd to 20th Ave is classified as an Interstate (Urban) with an AADT ranging from 43,500 – 50,900 vehicles per day, as presented on the Alaska Traffic Data website <https://alaskatrafficdata.drakewell.com/publicmultinodemap.asp>. The project consists of 6 driving lanes and a median for most of the project, with intermittent turn lanes. From Fireweed Ln to 20th Ave Seward Hwy has 8 driving lanes.

As noted within the Geotechnical Recommendations Anchorage Area Pavement Preservation – Group A, the existing areas contain circular corrugated metal pipe (CMP), CMP arch pipe with concrete invert, circular corrugated polyethylene pipe (CPEP), and perforated CMP. Sizes vary from 12-inch to 81-inch-wide pipes. Depth of cover varies between 2-feet and 20-feet.

The purpose of the project is to rehabilitate existing piping that has deteriorated with CIPP repair methods.

#### 3.2 Project Site-Specific Conditions (5.3.3)

##### Mean annual precipitation based on nearest weather stations (inches):

According to the Western Regional Climate Center Internet website <https://wrcc.dri.edu/summary/Climsmak.html>, Anchorage INTL AP, Alaska (500280) has an average rainfall of over 16 inches per year.

# ANCHORAGE INTL AP, ALASKA (500280)

## 1981-2010 Monthly Climate Summary

	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Annual
Average Max. Temperature (F)	23.6	26.7	34.1	44.4	55.9	62.7	65.2	63.2	55.0	40.5	27.2	24.7	43.7
Average Min. Temperature (F)	11.6	13.7	19.5	29.1	39.5	47.6	51.9	49.8	41.9	29.0	16.3	13.0	30.4
Average Total Precipitation (in.)	0.79	0.69	0.59	0.49	0.69	1.00	1.81	3.10	3.02	2.14	1.14	1.13	16.58

Unofficial values based on averages/sums of smoothed daily data. Information is computed from available daily data during the 1981-2010 period. Smoothing, missing data and observation-time changes may cause these 1981-2010 values to differ from official NCDC values. This table is presented for use at locations that don't have official NCDC data. No adjustments are made for missing data or time of observation. Check [NCDC normals](#) table for official data.

Western Regional Climate Center, [wrcc@dri.edu](mailto:wrcc@dri.edu)

### Size of the 2-yr, 24-hr storm event (in inches):

According to the Natural Resources Conservation Service website, available at [https://hdsc.nws.noaa.gov/hdsc/pfds/pfds\\_map\\_cont.html?bkmrk=nc](https://hdsc.nws.noaa.gov/hdsc/pfds/pfds_map_cont.html?bkmrk=nc), the 2-yr, 24-hr storm event is 1.41 inches at the Anchorage INTL AP, Alaska (500280) station.

### Soil Type(s)

Based on the Draft Geotechnical Recommendations: Anchorage Area Pavement Preservation – Group A, the existing project soil types for Dimond Blvd consists of 2-3” Hot Mix Asphalt (HMA), Type V, PG 58-, STE-1 Tack Coat, 2-4” HMA, 6” Base Course, 42-72” Selected Material or Borrow, Types A or B, and existing subgrade.

Based on the Draft Geotechnical Recommendations: Anchorage Area Pavement Preservation – Group A, the existing project soil types for Seward Hwy consists of 2” HMA, Type VH, STE-1 Tack Coat, 2.5-7” HMA, 2-6” Aggregate Base Course, Grading D-1, 6” Subbase, Grading B, 12-18” Selected Material, and existing subgrade.

### Slopes:

For both the Dimond Blvd and Seward Hwy project corridors, the profile of the road is generally flat and the grades of the foreslopes vary from the beginning of project (BOP) to the end of project (EOP). As a result of work pertaining to this project, no changes to foreslopes and profile slopes are anticipated.

### Landscape Topography:

The project passes through varied terrain.

### Drainage Patterns:

Drainage is primarily contained and managed with storm water drainage systems and equalization culverts for the entire length of the project. Existing road drainage patterns within the project corridors will not be modified.

### Type of Existing Vegetation:

Vegetation within the project area primarily consists of grass, low-height shrubbery, and deciduous trees.

### Approximate Growing Season:

According to the Alaska Regional Supplement to the Corps of Engineers Wetland Delineation Manual (2007), at <https://usace.contentdm.oclc.org/utis/getfile/collection/p266001coll1/id/7608>, the city of Anchorage is located in the “Cook Inlet” Ecoregion of Alaska, which has a growing season from approximately May 8 to October 5.

**Seeding Dates:**

As referenced in Section 618 of the project’s specifications and special provisions, the application of hydraulically applied seed in disturbed areas is to take place after the permanent cessation of ground-disturbing activities. Seeding shall be done within the time period specified in Section 4.5.1 of the 2021 ACGP and section 641 of the project specifications. Seeding shall be performed between May 15 and August 15; written approval from the Engineer must be obtained for any seeding that takes place outside of the specified timeframe.

**Time Period to Avoid Vegetation Clearing:**

According to the U.S. Fish & Wildlife Service’s (USFWS) Construction Advisory for Protecting Migratory Birds/Land Clearing Guidance for Alaska document, the clearing of forest, woodland, shrub, and open habitats located in the Southeast Region should be avoided between April 15 to July 15.

**Fish Window:**

A restriction on construction activities due to a fish window is not applicable since an Alaska Department of Fish and Game (ADF&G) Fish Habitat Permit is not required. A Fish Habitat Permit is not required because there is no in-water work within the corridors of the project.

**Historic site contamination is evident from existing site features and known past usage of the site:**

A search of the State of Alaska Department of Environmental Conservation (DEC) Contaminated Site Database indicated four ‘Active’ and seven ‘Cleanup Complete – Institutional Controls’ contaminated sites located within a 1,500-foot radius of the Dimond Blvd corridor. Five ‘Active and four ‘Cleanup Complete – Institutional Controls’ contaminated sites located within a 1,500-foot radius of the Seward Hwy corridor were found.

**Dimond Boulevard**

Site Name	Hazard ID	Location	Status
Holiday Station Store #624/ Williams Express Store #5024	23844	8803 Jewel Lake Rd	Cleanup Complete - Institutional Controls
Jewel Lake Dry Cleaners - Lot 13B	4001	9001 Jewel Lake Rd	Cleanup Complete - Institutional Controls
Jewel Lake Dry Cleaners - Lot 12B	25895	Lot 12B; E of 9001 Jewel Lake Rd	Cleanup Complete - Institutional Controls
Texaco / Hanna Car Care Center	23025	2201 W. Dimond Blvd	Active
Chevron - #90932	24583	2200 W. Dimond Blvd	Cleanup Complete - Institutional Controls
Youngs Firehouse	23861	1090 and 1100 W. Dimond Blvd	Cleanup Complete - Institutional Controls
Texaco - #91 Oehrlis	24054	919 East Dimond Blvd	Active
Pink Elephant Car Wash Tanks 6 & 7	27973	8215 Old Seward Hwy	Active
Pink Elephant Car Wash	24788	8215 Old Seward Hwy	Cleanup Complete - Institutional Controls
Holiday Station Store #606, former Williams Express Store #5006	23847	1501 Abbott Rd	Cleanup Complete - Institutional Controls
Denali Fuel Company	24028	8223 Hartzell Rd	Active

### Seward Highway

Site Name	Hazard ID	Location	Status
Tesoro - Olson Gas Service #2 (former)	23371	854 East 36th Ave	Active
MOA Gambell Street Right of Way near Benson Blvd.	4273	Gambell Street Right of Way	Active
Chevron - #1518 (Former) - Wendy's	23595	2927 New Seward Hwy	Active
Tesoro Northstore #79, Formerly Garrett's #1	24727	2811 New Seward Hwy	Cleanup Complete - Institutional Controls
Former Midas Facility	3165	2700 Seward Hwy	Cleanup Complete - Institutional Controls
Former Holiday Station Store #608, formerly Williams Express Store #5008	23362	717 E Northern Lights Blvd	Cleanup Complete - Institutional Controls
Chevron - #5799	23820	2500 New Seward Hwy	Active
Cal Worthington Ford USTs 1-3	23707	1950 Gambell St	Cleanup Complete - Institutional Controls
Cal Worthington Ford	26632	1950 Gambell St	Active

Additional information about these sites is available on the DEC Division of Spill Prevention and Response website: <https://www.arcgis.com/home/item.html?id=315240bfbaf84aa0b8272ad1cef3cad3>. Include only those sites listed as 'Active' or 'Cleanup Complete – Institutional Controls'

### 3.3 Reference Documents Available

Listed below are the reference documents available for this project. Please contact the Project Engineer for assistance in obtaining these documents.

- Project Specific Permits – located in Appendix D and in the Special Provisions Package
- Geotechnical Report – Geotechnical Recommendations, Anchorage Area Pavement Preservation – Group A, Dimond BLVD MP 0-3.31, Project Number CFHWY00851/000173, November 2024, prepared by Central Region Materials, DOT&PF available from the Plans room during the bidding process or download from the Bid Express project site (<https://ui.bidx.com/login?referer=%2Fak%2Flettings> – login required)
- Environmental Commitment Memo – available at Preconstruction Meeting
- Environmental Document – Categorical Exclusion Documentation Form, Anchorage Pavement Preservation Group A, Dimond Blvd MP 0.00-3.31, Seward Highway MP 123.57-125.09, State Project number: CFHWY00851, February 2023, prepared by DOT&PF, available for review in the DOT&PF Preliminary Design & Environmental section

## 4.0 NATURE OF CONSTRUCTION ACTIVITY (5.3.4)

### 4.1 Scope of Work

The Proposed Action would:

- Repair and rehabilitate approximately 5,075 linear feet of storm drain and culvert pipes for Dimond Blvd. through Cured in Place Pipe (CIPP) repair methods and
- Repair and rehabilitate approximately 3,554 linear feet of storm drain and culvert pipes for Seward Hwy. through Cured in Place Pipe (CIPP) repair methods.

### 4.2 Project Function (5.3.4.1)

The purpose of this project is to improve drainage in support of an upcoming pavement preservation project.

### 4.3 Support Activities (As Applicable)

Modify support activities table, as necessary. "Dedicated" only applies to activities exclusively for the project, i.e. commercial concrete or asphalt plants would be marked "No" under the "Dedicated" column. Location must be provided for ALL support activities, even those which are commercial or off-site. Provide a physical address for the support activities. For private and/or commercial support activities locations, include the name of the individual and/or company and their physical address. Location may be an address or other descriptive location, i.e. NE corner of staging area.

Support activities for this project are:

Support Activity	Location	Dedicated	
		Yes	No
Concrete Batch Plant	N/A – Batch plant not dedicated to project	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Asphalt Batch Plant	N/A – Batch plant not dedicated to project	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Equipment Staging Yards	N/A – Not anticipated due to quick moving of locations	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Material Storage Areas	N/A – Not anticipated due to quick moving of locations	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Excavated Material Disposal Areas	N/A – Disposal area not dedicated to project	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Borrow Areas	N/A – Disposal area not dedicated to project	<input type="checkbox"/>	<input checked="" type="checkbox"/>

### 4.4 Sequence and Timing of Soil-disturbing Activities (5.3.4.2)

Limit ground disturbed by construction activities and not permanently stabilized between all roadways combined, at any specific time, to a maximum of 11,000 feet parallel to the roadway(s), unless additional length is approved. Stabilize disturbed ground according to Section 641 Erosion, Sediment, and Pollution Control."

The contractor will be required to finish, either temporary or final stabilized, individual areas prior to moving on to the next area. The contractor will be required to prepare a detailed schedule for review and approval prior to commencement of construction activities and is to be included in the SWPPP. The schedule will detail the sequence of activities and describe the stabilization schedule. The contractor must adapt this section with their specific plans in the project SWPPP.

#### 4.5 Size of Property and Total Area expected to be Disturbed (5.3.4.3)

The following are estimates of the construction site:

Description	Number	Remarks
Total project area: (Acres)	Dimond: 62 Seward: 23	ROW to ROW
Construction-site area to be disturbed (Acres):	Dimond: 0 Seward: 0	Due to the scope of work, no disturbed area is expected
Percentage impervious area BEFORE construction:	Dimond: <b>96%</b> Seward: <b>80%</b>	
Runoff Coefficient BEFORE construction:	Dimond: <b>.92</b> Seward: <b>.77</b>	
Percentage impervious area AFTER construction:	Dimond: <b>96%</b> Seward: <b>80%</b>	
Runoff coefficient AFTER construction:	Dimond: <b>.92</b> Seward: <b>.77</b>	

The values shown in the table above were calculated with the information available at the time of the final design. The contractor's values will be different due to staging areas, batch plants, material stockpiles, etc. **A weighted "C" value from the Rational Method** was used to calculate the Runoff Coefficient. If a discrepancy is found, contact the Project Engineer to request further information.

#### 4.6 Identification of All Potential Pollutant Sources (5.3.4.5)

Identify and list all potential sources of sediment from construction materials and activities which may affect the quality of storm water discharges from the construction site.

Identify and list all potential sources of pollution, other than sediment, from construction materials and activities which may affect the quality of storm water discharges from the construction site.

Potential sources of sediment to storm water runoff:

##### Vehicle Tracking and CIPP Lining materials

Potential pollutants and sources, other than sediment, to storm water runoff:

Trade Name Material	Storm Water Pollutants	Location
Diesel Fuel/Gasoline/Hydraulic Oil/Lubricants	Petroleum distillate, oil, grease, naphthalene, xylene	Within the project limits and material staging areas
Coolant	Ethylene Glycol, heavy metals (copper, lead, zinc)	Within the project limits and material staging areas
CIPP materials		Areas requiring CIPP repair

## **5.0 SITE MAPS (5.3.5)**

Site map(s) and drawings are located in Appendix A.

The SWPPP must include a legible site map (or set of maps for large projects) showing the entire site and identifying the following site-specific information:

1. North Arrow **(ESCP)**
2. Property boundaries **(ESCP)**
3. Locations where earth-disturbing activities will occur, noting any phasing dictated by design **(ESCP)**
4. Location of areas that will not be disturbed and natural features to be preserved **(ESCP)**
5. Locations of all storm water conveyances including ditches, pipes, and swales **(ESCP)**
6. Locations of storm water inlets and outfalls, with a unique identification code for each outfall **(ESCP)**
7. Location where storm water and/or authorized non-storm water discharges to waters of the U.S. (including wetlands) or a Municipal Separate Storm Sewer Systems (MS4), if present **(ESCP)**
8. Direction of storm water flow and approximate slopes anticipated after grading activities **(ESCP)**
9. Locations where control measures will be installed **(ESCP)**
10. Locations where exposed soils will be or have been stabilized
11. Locations where post-construction storm water controls will be installed (i.e. seeding areas, matting, riprap, sedimentation basins, etc.) **(ESCP)**
12. Locations of support activities, if known
13. Locations where authorized non-storm water will be used
14. Locations and sources of run-on to the site from adjacent property that may contain quantities of pollutants (e.g., sediment, fertilizers and/or pesticides, paints, solvents, fuels) which could be exposed to rainfall, or snowmelt, and could be discharged from your construction site, if applicable **(ESCP)**
15. Locations of all waters of the U.S. (including significant wetland areas 10,000 square feet or greater) on the site within 2,500 feet of the site boundary (~1/2 mile on each side of road) that may be affected by storm water discharges from the site (see Section 7.1) **(ESCP)**
  - a. This can be shown on a general location map (USGS quad map, a portion of a city or county map, or other map) with enough detail to identify the location of the construction site and waters of the U.S. within the one mile distance.
16. Location of existing public water system (PWS) drinking water protection areas (DWPA) for PWS sources (e.g. springs, wells, or surface water intakes) that intersect the boundary of the proposed project/permit area. The DWPAs can be found using the interactive web map application, "Alaska DEC Drinking Water Protection Areas", located at <http://dec.alaska.gov/das/GIS/apps.htm>. **(ESCP)**
  - a. A copy of the webpage from the above URL will work with the addition of the project boundary and labels for the DWPAs by their ID numbers (see Section 9).
17. Sampling point(s), if applicable
18. Areas where final stabilization has been accomplished
19. Location of staging and material storage areas (construction materials, hazardous materials, fuels, etc.) **(ESCP, if known)**
20. Dumpsters
21. Porta-potties
22. Concrete, paint, or stucco washout areas
23. Stabilized construction exits **(ESCP, if known)**

## 6.0 DISCHARGES

Due to the nature and scope of this project, discharge points were not identified.

Subject to compliance with the terms and conditions of the CGP, the permittee is authorized to discharge pollutants in storm water discharges from the site. If the permittee is eligible for coverage under CGP and does not comply with the requirements of the CGP, the permittee may be in violation of this general permit for otherwise eligible discharges.

Instructions:

Describe and identify the location of any storm water discharge associated with support activities, including discharges from dedicated asphalt and concrete plants covered by the CGP (5.3.8).

### 6.1 Locations of Other Industrial Storm Water Discharges (5.3.8)

The contractor is required to identify discharges from related support activities. Portable batch plants located on department-supplied property must be included in the contractor's SWPPP and related inspections. If the DOT&PF is not a CGP operator for the site or sites listed in this subsection, then describe the sites and BMPs for them in a separate SWPPP2. In this section, explain which areas are covered within this SWPPP and which are covered within a separate SWPPP2. Also provide information on where the SWPPP2 is available for review.

### 6.2 Allowable Non-Storm Water Discharges (1.4.3; 4.3.7; 5.3.9)

The contractor must list all allowable non-storm water discharges and describe how the discharges will be minimized and managed to reduce pollution to storm water in the contractor's SWPPP.

Allowable Non-Storm Water Discharges:

- Discharges from fire-fighting activities (1.4.3.1)
- Fire hydrant flushing (1.4.3.2)
- Waters used to wash vehicles where detergent are not used (1.4.3.3)
- Water used to control dust (1.4.3.4)
- Potable water including uncontaminated water line flushings (1.4.3.5)
- Routine external building wash down that does not use detergents (1.4.3.6)
- Pavement wash waters where spills or leaks of toxic or hazardous materials have not occurred (unless all spilled material has been removed) and where detergents are not used (1.4.3.7)
- Uncontaminated air conditioning or compressor condensate (1.4.3.8)
- Uncontaminated, non-turbid discharges of ground water or spring water (1.4.3.9)
- Foundation or footing drains where flows are not contaminated with process materials such as solvents or contaminated groundwater (1.4.3.10)
- Uncontaminated construction dewatering waters that are treated by an appropriate control measure in compliance with Part 4.4.2 or have been treated with treatment chemicals in compliance with Part 4.6 (1.4.3.11)
- Landscape irrigation (1.4.3.12)

## 7.0 DOCUMENTATION OF PERMIT ELIGIBILITY RELATED TO TOTAL MAXIMUM DAILY LOADS (3.2, 5.6)

A search of the “Alaska’s Final **2024** Integrated Water Quality Monitoring and Assessment Report” found listings or impairments. **Campbell Lake, Campbell Creek, and Fish Creek are listed as Category 4a for Fecal Coliform. Little Campbell Creek and Chester Creek are listed as Category 5 for Escherichia Coli (E. Coli).**

### 7.1 Identify Receiving Waters (5.3.3.3)

#### Description of receiving waters:

The following bodies of water were identified to be within or near the vicinity of the project based on the Fish Monitor website at:

<https://adfg.maps.arcgis.com/apps/webappviewer/index.html?id=f5aac9a8e4bb4bf49dc39db33f950bbd>.

In addition, AWC Codes for each listed body of water are provided.

Campbell Lake

AWC Code: 247-60-10340-0010

Campbell Creek

AWC Code: 247-60-10340

Creeks in the Anchorage Bowl join with Campbell Creek as it flows in a southwesterly direction before discharging into Turnagain Arm. Campbell Creek is partially impounded by Campbell Lake.

South Fork Little Campbell Creek

AWC Code: 247-60-10340-2018-3006

The headwaters of South Fork Little Campbell Creek originate from a freshwater forested/shrub wetland habitat near Echo Park. South Fork Little Campbell Creek travels northwest before merging into Campbell Creek.

Chester Creek

AWC Code: 247-50-10050

The headwaters of Chester Creek originate north of Campbell Creek Canyon to the east of Anchorage. The creek travels westward before discharging into the Knik Arm.

#### Outstanding Natural Resource Waters (2.1.6):

The DEC must be consulted, at least 30 days prior to construction activities, when determining requirements for water quality analysis on all projects that meet the following:

- Will or may discharge storm water to a Tier 3 water body, also known as Outstanding Natural Resource Waters (ONRW).

No ONRW are designated in Alaska as of the date of this document.

#### Description of storm sewer and/or drainage systems:

Stormwater within the proposed project area enters the storm drain system and drains into the closest receiving water body authorized by the joint MOA/DOT&PF Municipal Separate Storm Sewer System (MS4). Storm drainage systems are within the vicinity of the project area and are utilized as most of the discharge points.

## 7.2 Identify TMDLs (5.6.1)

All bodies listed in 7.1 are impaired.

Is an EPA-established or approved TMDL published for the receiving water(s) listed in Section 7.1?

Yes  No

### TMDL:

Campbell Lake, Campbell Creek, and Fish Creek have a TMDL for Fecal Coliform. Little Campbell Creek and Chester Creek have a TMDL for Escherichia Coli (E. Coli).

### Summary of consultation with state or federal TMDL authorities (5.6.2):

No consultation is required as the approved TMDL's for the water bodies of the project are not for turbidity or sediment.

### Measures taken to ensure compliance with TMDL (5.6.3):

No action necessary.

Are there impaired receiving waters listed in Section 7.1 without an approved TMDL?  Yes  No

## 8.0 DOCUMENTATION OF PERMIT ELIGIBILITY RELATED TO ENDANGERED SPECIES (3.3, 5.7)

### 8.1 Information on Endangered or Threatened Species or Critical Habitat (5.7.1)

Are endangered or threatened species and critical habitats on or near the project area?

Yes  No

Will species or habitat be adversely affected by storm water discharge?

Yes  No

#### Describe how this determination was made:

Utilizing the USFWS Information, Planning, and Conservation (IPAC) development tool located at the website <https://ecos.fws.gov/ipac/>, these ensuing determinations were made:

1. There are no Endangered Species Act-listed species within the project area.
2. There are no critical habitats within the project area.

Provide summary of necessary measures (5.7.5): **Insert Text**

## 9.0 APPLICABLE FEDERAL, STATE, TRIBAL, OR LOCAL REQUIREMENTS (4.10, 4.15)

The project will comply with all applicable Federal, State, Local, and Tribal requirements for soil erosion control and storm water management.

The contractor will be responsible for obtaining all necessary permits and clearances for material and disposal sites, and/or equipment storage areas in accordance with the CGP for Storm Water Discharges from Construction Activities.

## 9.1 Historic Properties

SHPO consultation: **Due to the nature of this project scope, SHPO consultation was not conducted for this project.**

All project activities are covered under Programmatic Allowances, thus a 106 PA Streamlined Project Review Screening Record was utilized to complete Section 106, with the entire project reviewed per Appendix D of the Programmatic Agreement.

Listed or Eligible Properties (or potentially so) and reasoning for coverage under Programmatic Allowances: (Following text is from the Streamlined Project Review Screening Records)

The Seward Highway is exempt from consideration under the A CH P's Exemption Regarding Historic Preservation Review Process for Effects to the Interstate Highway System published in the Federal Register, Vol. 70, No. 46, Thursday, March 10, 2005. The two bridges on the Seward Highway scheduled for repair, #0391 and #1433, are exempt from consideration per the 2012 Program Comment for Common Post-1945 Concrete and Steel Bridges.

Examination of Dimond Boulevard show that this is a well-developed urban street with existing curb cuts, raised medians, intersections with signals and striping, sidewalks and pedestrian facilities. Any changes imposed by a pavement preservation project would be minor, in kind, and in visual and historic context for these facilities.

If cultural or paleontological resources are discovered after the initial commencement of construction activities, work that would disturb such resources is to be stopped, and the Office of History and Archaeology, a Division of Parks and Outdoor Recreation of the Alaska Department of Natural Resources (<http://dnr.alaska.gov/parks/ohal/>), is to be notified immediately at (907) 269-8721.

It is the Contractor's responsibility, thru the Project Engineer, to get clearance for material and disposal sites that have not been assessed during the Design phase of the project.

## 9.2 Projects near Public Water System (PWS) (4.10)

The project boundary intersects **Insert Number** Public Water System (PWS) Drinking Water Protection Area(s) (DWPA) and **Insert Number** Provisional Protection Area(s), and will have to follow the requirements of the 2021 CGP Part 4.10. The PWS contact will need to be notified by whatever method is most expedient: email, phone, or post (4.10.1). This should be done by the DOT&PF Project Engineer on behalf of both parties.

The intersecting DWPAs and Provisional Protect Areas ID numbers (PWSID) with contact information are:

Water System Name	PWSID	Contact Name	Phone #	Address	Email

The water system name, number, name of contact, and all methods of contact can be found at:  
<https://dec.alaska.gov/eh/dw/dwp/protection-areas-map/>.

If the project is near a PWS, add language that addresses the following items:

1. Within the identified DWPA, restrict project activities that could significantly change the natural surface water drainage or groundwater gradient (4.10.2).
2. Immediately notify the nearby PWS of any identified potential contamination, such as spills or excess erosion (4.10.3).

Record the time, date, and method of contact and enter into the SWPPP in Appendix Q. Either a copy of the email, or a formal memo stating the date of phone call, or a receipt from certified mail will fulfill this obligation.

## General Principles for Erosion and Sediment Controls.

The contractor must design, install, and maintain effective erosion and sediment controls to minimize the discharge of pollutants. At a minimum, such controls must be designed, installed, and maintained to:

- Control storm water volume and velocity to minimize soil erosion and pollutant discharges;
- Control storm water discharges, including both peak flowrates and total storm water volume, to minimize channel and streambank erosion and scour in the immediate vicinity of discharge points;
- Minimize the amount of soil exposed during construction activity;
- Minimize the disturbance of steep slopes;
- Minimize sediment discharges from the site. The design, installation, and maintenance of erosion and sediment controls must address factors such as the amount, frequency, intensity, duration of precipitation; the nature of resulting storm water runoff; and soil characteristics, including the range of soil particle sizes expected to be present on the site;
- Provide and maintain natural buffers around waters of the U.S., direct storm water to vegetated areas and maximize storm water infiltration to reduce pollutant discharges, unless infeasible;
- Minimize soil compaction. Minimizing soil compaction is not required where the intended function of a specific area of the site dictates it to be compacted.
- Unless infeasible, preserve topsoil. Preserving topsoil is not required where the intended function of a specific area of the site dictates that the topsoil be disturbed or removed.

## Additional Erosion and Sediment Controls Selection and Design Considerations:

Preventing storm water from coming into contact with polluting materials is generally more effective, and less costly, than removing pollutants from storm water;

Using a combination of control measures is more effective than using control measures in isolation for minimizing pollutants in the storm water discharge;

Using technologically available, economically practicable, and achievable methods in light of best industry practices;

Assessing the type and quantity of pollutants, including their potential to impact receiving water quality, is critical to designing effective control measures that will achieve the limits in this permit;

Minimizing impervious areas at the permittees facility and infiltrating runoff onsite (including bioretention cells, green roofs, and pervious pavement, among other approaches) can reduce runoff and improve groundwater recharge and stream base flows in local streams, although care must be taken to avoid ground water contamination;

Dissipate storm water runoff into open vegetated swales and natural depressions to reduce in stream impacts of erosive flows;

Conserving and/or restoring of riparian buffers will help protect streams from storm water runoff and improve water quality; and

Using treatment interceptors (e.g., sand filters) may be appropriate in some instances to minimize the discharge of pollutants.

Describe the Best Management Practices (BMPs) to be implemented to control pollutants in storm water discharges. For each major activity identified:

- Clearly describe appropriate control measures.
- Describe the general sequence during the construction process in which the measures will be implemented.
- Describe maintenance and inspection procedures to be undertaken for that specific BMP.
- Include protocols, thresholds, and schedules for cleaning, repairing, and/or replacing damaged or failing BMPs.
- Identify staff responsible for maintaining BMPs. (If your SWPPP is shared by multiple operators, indicate the operator responsible for each BMP.)

Categorize each BMP under one of the following areas of BMP activity as described below:

1. *Minimize the Amount of Soil Exposed during Construction Activity (4.2.2) & Site Delineation (4.2.1)*
2. *Maintain Natural Buffer Areas (4.2.3) & Clearing Vegetation (4.2.4)*
3. *Control Storm Water Discharges and Flow Rates (4.2.5)*
4. *Protect Steep Slopes (4.2.6)*
5. *Storm Water Inlet Protection (4.3.1)*
6. *Water Body Protection (4.3.2)*
7. *Down-Slope Sediment Controls (4.3.3)*
8. *Stabilized Construction Vehicle Access and Exit Points (4.3.4)*
9. *Track-Out from vehicles (4.3.5)*
10. *Dust Generation (4.3.6)*
11. *Stockpile Management (4.3.7)*
12. *Sediment Basins (4.3.9)*
13. *Dewatering (4.4)*
14. *Soil Stabilization (4.5)*
15. *Treatment Chemicals / Active Treatment Systems (4.6)*
16. *Good Housekeeping Measures (4.8)*
17. *Spill Notification (4.9)*
18. *Construction and Waste Materials (5.3.7)*
19. *Permanent/Post-Construction BMPs (4.11)*
20. *Projects near a Public Water System (PWS) (4.10)*

- Note the location of each BMP on your site map(s).
- Any structural BMPs should have design specifications and details referred to in Section 11 or included in Appendix B.

For more information or ideas on BMPs, see the DEC *Alaska Storm Water Guide*:

<https://dec.alaska.gov/water/wastewater/stormwater/resources/guidance/> & for a list of Alaska specific BMPs look at the DOT&PF *Alaska SWPPP Guide*'s Appendix B - BMP Guide for Erosion & Sediment Control at [http://dot.alaska.gov/stwddes/desenviron/assets/pdf/bmp/bmp\\_all.pdf](http://dot.alaska.gov/stwddes/desenviron/assets/pdf/bmp/bmp_all.pdf)

## 10.0 CONTROL MEASURES/BEST MANAGEMENT PRACTICES (4.0; 5.3.6)

Much of the guidance in this section is for both the ESCP & SWPPP preparers. Carefully read through the requirements listed below when filling out Section 10. When developing this section, think about how they are going to construct the project. Look at means and measures but do not direct the contractor...merely suggest. Consider 'prior to/upon construction' methods (i.e. upon placing culvert install a fiber roll and outlet protection). The following sections describe BMPs that will or may be used as necessary to prevent erosion and control sediment.

The selection, design, installation, maintenance, and removal of control measures must be in accordance with good engineering practices, manufacturer specifications, and address site-specific conditions such as precipitation, site topography, soil characteristics, and growing season.

The plan preparer will use this section to describe the types and locations of control measures and BMPs to be installed and maintained in accordance with CGP Part 4.0.

Describe each control measure and BMP, including installation schedule and maintenance, inspection, and removal requirements. You may include a brief description of each BMP in this section and refer to detailed installation, maintenance, inspection, removal requirements, and manufacturer's specifications that **MUST** be included in the Appendix B.

If a control measure or BMP will be used to comply with more than one element of this section, you do not need to repeat the detailed installation, maintenance, inspection, removal requirements, and manufacturer's information. For each repeated element, identify the control measure or BMP to be used, and refer to the section or Appendix B where the detailed information is presented.

The person(s) identified in Section 2.0 of this SWPPP will be responsible for ensuring compliance with the installation, maintenance, inspection, and removal of these control measures.

The format to be used is:

**BMP Description:**

Describe purpose, applicability, limitations and design. If using a BMP manual or publication, this information may be found there.

**BMP Manual/Publication:**

Provide the citation information as described below. If referencing Appendix B, where the BMP details are provided, ensure the attached sheets clearly identify this information.

**Installation Schedule:**

Identify the activity or phase prior to which the BMP will be installed or the activity that requires this BMP to be installed before it can begin.

**Maintenance and Inspection:**

Describe the thresholds and/or indicators for maintenance and protocols for inspecting the BMP. Describe the maintenance procedures. If using a BMP manual or publication, this information may be found there.

**Responsible Staff:**

Name the position and company who is responsible for installation and maintenance.

How to Cite a BMP Publication:

DOT&PF requires citations for the BMP manual or publication used to select and design the BMP, along with a description of the BMP. If no BMP manual or publication was used to select or design a given BMP then state "No BMP manual or publication was used in the design or selection of this BMP". BMP designs submitted by the contractor and approved by the Project Engineer may be used but still must state that no manual or publication was used.

BMP Manuals/Publications: BMP manuals describe each BMP and outline details such as installation, design parameters, applicability/limitations, maintenance, and targeted pollutants. To cite a manual, include the title, author (individual or agency) and date of publication.

Be careful when citing outside of the state control measures or BMPs. Read through them to make sure they do not put any additional restrictions that go beyond the CGP. If citing outside of state BMPs, make sure to mark out any requirements that do not apply to this project or do not meet CGP requirements and cite as 'modified from (insert BMP manual title).

DOT&PF Specifications and Plan Sheets: The publication cited may be the DOT&PF contract specifications and plan sheets provided that the minimum information regarding the BMP is included (those listed above).

When the plans and specifications are used, the reference must include the sheet or page number and these must be appended to the SWPPP. If the specifications and plan sheets do not provide the minimum information, the plan preparer must provide the missing information in the plan. Any drawing or description developed by the plan preparer must include the statement "No BMP manual or publication was used for this design."

Manufacturer's Specification Sheet: Referencing a manufacturer's specification sheet is suitable only if it includes all the necessary information listed in the above subsection. When using the manufacturer's specification sheet(s), provide the product name, manufacturer, and date of copyright, and attach copies of the specification sheet(s) to the plan. It may also be helpful to provide the manufacturer's website if the information was obtained online. You may deviate from manufacturer's specifications where you provide justification for such deviation and include documentation of your rationale in the ESCP/SWPPP.

Permanent/Post-Construction Control Measures: Identify any permanent/post-construction control measures that will be installed during the construction process and not discussed elsewhere in the SWPPP (permanent Soil Stabilization measures should be covered in section 10.13).

## 10.1 Minimize Amount of Soil Exposed during Construction Activity (4.2.2)

Describe how the disturbed land areas (e.g., clearing and grading) and undisturbed land areas (e.g., trees, boundaries of sensitive areas, or buffers established by CGP Part 4.2.3) will be delineated.

Describe the areas that will be disturbed for each phase of construction, and the methods you will use (e.g., signs, fences, etc.) to protect the areas that are to be left undisturbed. Construction activities must be phased to minimize the extent and duration of exposed soil.

Identify natural features and describe how each will be protected during construction activity.

Describe how native topsoil will be preserved. Native topsoil should be preserved for later use with on-site stockpiles, unless deemed infeasible by space constraints or site design criteria creates impervious surfaces (CGP Part 4.2.2.1).

**BMP Description:** Site Delineation, BMP 54.00

**BMP Manual/Publication:** DOT&PF, Alaska SWPPP Guide, October 2016

Permanent

Temporary

<b>Installation Schedule:</b>	Prior to the initiation of construction activities such as clearing, full depth pavement removal, and pullout regrading and paving. Site delineation is intended to remain until construction activity is completed.
<b>Maintenance and Inspection:</b>	Inspection: Look for flagging or fencing that does not adequately delineate undisturbed areas, trees, boundaries of sensitive areas, or natural buffer areas. Maintenance: Make repairs if any conditions noted under inspection are found.
<b>Responsible Staff:</b>	SWPPP Manager & Superintendent, Contractor

### 10.1.1 Site Delineation (4.2.1)

Work limits in areas requiring clearing will be delineated by slope stakes, silt fences, fiber rolls, or some other means prior to initiating construction activities. Vegetation and the root mass shall be left in place wherever possible to the greatest extent practicable, unless otherwise directed by the engineer

## 10.2 Maintain Natural Buffer Areas (4.2.3)

Are stream crossings or waters of the U.S. located within or immediately adjacent to the property?

Yes       No

If YES, describe the control measures to be implemented to comply with the CGP Part 4.2.3 (e.g., buffer areas, perimeter controls, etc.).

You must maintain natural buffer areas at stream crossings and around the edge of any waters of the U.S. that are located within or immediately adjacent to the construction activity in accordance with the following:

- The buffer must be a minimum of 25 feet wide, or the width as required by local ordinance, unless infeasible based on site dimensions;
- Exceptions are allowed for water dependent activities, specific water access activities, or necessary water crossings;
- A permittee should, to the extent practicable, use perimeter controls adjacent to buffers and direct storm water sheet flow to buffer areas to increase sediment removal and maximize storm water infiltration.

As described in Section 7.1 Identify Receiving Water Bodies, stream crossings are present at North Fork and South Fork Little Campbell Creek, Little Campbell Creek, Campbell Creek, and the Turnagain Arm. Construction activities are anticipated to take place in and immediately adjacent to these water bodies. Streambanks disturbed during construction shall be reconstructed to their natural functional capacity to filter runoff and prevent erosion. The following measures shall be employed to minimize the impacts of construction activities on these water bodies and their natural buffer areas:

1. Fueling, maintenance, and cleaning of equipment will not take place within 100 feet of any water body.
2. Sanitation facilities and construction waste will not be placed within 50 feet of any water body.
3. Natural buffer areas shall be delineated by acceptable means in accordance with Section 10.1.1.
4. Construction waste materials will not be stored within a vicinity of the water bodies which could negatively impact their water quality.
5. Any aggregate material stockpiles stored upstream of water bodies will have appropriate BMPs and perimeter controls installed to prevent sediment laden discharge to the water bodies.

### 10.2.1 Clearing Vegetation (4.2.4)

Clearing of vegetation that disturbs the vegetative mat and exposes soil is **prohibited** prior to obtaining authorization under the CGP.

Cutting of trees and brush while the ground is frozen without disturbing the vegetative mat for the purpose of clearing in accordance with the U.S. Fish & Wildlife Service “Recommended Time Periods for Avoiding Vegetation Clearing” is allowed prior to the submittal of a project’s NOI. If vegetation clearing that disturbs the vegetative mat and occurs after the onset of spring thaw (as defined in Appendix C) or conditions that consist of above freezing temperatures that cause melting of snow, the permittee must develop a SWPPP and file an NOI. Operators must receive authorization under this permit and otherwise comply with the terms of this permit prior to such clearing.

### 10.3 Control Storm Water Discharges and Flow Rates (4.2.5)

Describe control measures to comply with the CGP (e.g., divert storm water around the site, slow down or contain storm water, use of velocity dissipation devices, installing permanent storm water management controls prior to construction of site improvements to the extent practicable, etc.). Storm water that may concentrate must be slowed down or contained.

**BMP Description:** Fiber Rolls for Erosion and Sediment Control, BMP 10.00

**BMP Manual/Publication:** Alaska SWPPP Guide, October 2016

**Permanent**

**Temporary**

<b>Installation Schedule:</b>	Install prior to soil disturbance in the contributing drainage area. Place fiber rolls perpendicular to flow and parallel to the slope contour.
<b>Maintenance and Inspection:</b>	Inspection: Look to see that fiber roll ends are tightly abutted and that fiber rolls are in contact with the soil and entrenched. Also look for scouring underneath the rolls. Maintenance: If rolls are crushed, torn, slumping, or split, the damaged sections must be replaced. Remove sediment that has accumulated upslope of the roll when it reaches one-half the distance between the top of the fiber roll and the ground surface.
<b>Responsible Staff:</b>	SWPPP Manager & Superintendent, Contractor

### 10.3.1 Protect Steep Slopes (4.2.6)

Will steep slopes be present at the site during construction?  Yes  No

If YES, describe control measures to be implemented to comply with CGP Part 4.2.6 (e.g., reduce continuous slope length, divert storm water around slopes, stabilized exposed areas, etc.).

#### Sediment Controls:

Sediment control measures (e.g. sediment ponds, traps, filters, etc.) must be constructed as one of the first steps in grading. These control measures must be functional before other land disturbing activities take place.

### 10.4 Storm Water Inlet Protection Measures (4.3.1)

Describe control measures (e.g., filter berms, perimeter controls, temporary diversion dikes, etc.) to be implemented to protect all inlets receiving storm water from the project during the duration of the project.

**BMP Description:** Storm Drain Inlet Sediment Protection – Curb and Area Inlets, BMP 25.00-29.00

**BMP Manual/Publication:** Alaska SWPPP Guide, October 2016

Permanent  Temporary

<b>Installation Schedule:</b>	Install prior to construction to prevent sediment from entering downgradient storm drainage systems.
<b>Maintenance and Inspection:</b>	<u>Inspection:</u> Check for sediment depth, undermining, or bypassing of run-off. <u>Maintenance:</u> Remove accumulated sediment before it reaches one-third the design depth or available storage. Remove and dispose of any rock debris that has accumulated behind the sediment barrier to prevent further clogging. Replace frayed or torn fabric or materials and repair any structural damage.
<b>Responsible Staff:</b>	SWPPP Manager & Superintendent, Contractor

### 10.5 Water Body Protection Measures (4.3.2)

Describe control measures selected to minimize discharge of sediment prior to entry into water bodies located on or immediately downstream of the site.



## 10.9 Soil Management and Soil Stockpile (4.3.7)

Will soil stockpiles be at the site during construction?  Yes  No

If YES, describe control measures intended to control sediment loss from the stockpiles (e.g., tarps or perimeter straw wattles). Show location(s) of stockpile(s) on site maps, if known. Stockpiles must be stabilized or covered, protected with sediment controls and located away from storm water inlets, conveyance channels, or water bodies, if possible.

## 10.10 Authorized Non-Storm Water Discharges (4.3.8)

A permittee must minimize any non-storm water authorized by this permit. List any authorized non-storm water discharges.

## 10.11 Sediment Basins (4.3.9)

Refer to CGP Part 4.3.8 to determine if a sediment basin is required for your site.

Will a sediment basin be required during construction?  Yes  No

If YES, provide a brief description of the sediment basin here. Append detailed design information in appendices (e.g., calculated volume of runoff from a two-year, 24-hour storm, or other assumptions used to calculate appropriate sediment-basin size). Show location of sediment basin(s) on site maps.

## 10.12 Dewatering (4.4)

Describe dewatering practices to be implemented if water must be removed from an area so construction activity can continue.

Will dewatering be conducted during construction?  Yes  No

Will excavation dewatering be conducted within 1,500 feet of a DEC mapped contaminated site found on the DEC website?  Yes  No

There will be no ground disturbance, just rerouting existing storm water

For DEC's contaminated sites:

<http://www.arcgis.com/home/item.html?id=315240bf84aa0b8272ad1cef3cad3>.

If yes to above question, review and comply with the DEC General Permit for Excavation Dewatering (AKG002000 - <https://dec.alaska.gov/water/wastewater/stormwater/permits-approvals/dewater/>), or most current version, for specific requirements

Describe control measures to be implemented to comply with dewatering discharges authorized either under the CGP or the DEC General Permit for Excavation Dewatering requirements.

### 10.13 Permanent/Post-Construction BMPs (4.11)

Describe any permanent/post-construction control measures that will be installed during the construction process AND have not been discussed elsewhere in this document.

Examples of these measures are:

- Biofilters
- Detention/Retention Devices
- Earth Dikes, Drainage Swales, and Lined Ditches
- Infiltration Basins
- Vegetated Strips and/or Swales

#### 10.13.1 Soil Stabilization (4.5, 5.3.6.3)

The project must stabilize all disturbed areas of the site to minimize on-site erosion and sedimentation and the resulting discharge of pollutants.

Soil stabilization requirements vary depending on the mean annual precipitation for the site. Refer to CGP Part 4.5 for specific requirements.

Refer to the Alaska Plant Materials Center's Alaska Coastal Revegetation & Erosion Control Guide and Interior Alaska Revegetation & Erosion Control Guide at <http://plants.alaska.gov> for help in selecting appropriate seed mixes and information on methods for revegetation.

Describe permanent & temporary stabilization control measures and sequence of installation.

Describe how the site will be stabilized prior to seasonal freeze-up.

### 10.14 Treatment Chemicals (4.6; 5.3.6.4)

Provide documentation for all treatment chemicals and/or an Active Treatment System (ATS) to comply with CGP Part 4.6. Submit cationic treatment chemical use or ATS to DEC at least 14 days for approval before installing.

Will treatment chemicals be used to control erosion and/or sediment during construction?

Yes       No

If YES, comply with CGP Part 4.6 and complete the following sections (10.15 & 10.16).

#### 10.15 Treatment Chemicals (4.6.1)

The use of treatment chemicals to reduce erosion from the land or sediment in a storm water discharge is allowed provided all the requirements of CGP Part 4.6 are met. Use conventional sediment controls before and after the application of treatment chemicals. Chemicals may only be applied where storm water is treated upstream and is directed to a sediment control (e.g., sediment trap, sediment basin) before discharge.

No treatment chemicals will be used on this project.

## 10.16 Active Treatment System Information or Cationic Treatment Chemicals (4.6.7)

A permittee who uses an Active Treatment System (ATS) or cationic treatment chemicals as a control measure must submit information required by the DEC for review at least 14 days prior to start of operation of the ATS at the project. Specific submittal requirements can be found at 4.6.7.

Will an ATS or cationic treatment chemicals be used as a control measure at the site?

Yes  No

If YES, simply include the packet submitted to DEC in Appendix P and refer to this documentation below.

## 10.17 Good Housekeeping Measures (4.8)

The project must design, install, implement, and maintain effective good housekeeping measures to prevent and/or minimize the discharge of pollutants. The project must include appropriate measures for any of the following activities at the site.

Consult the DEC Storm Water Guide or other resources for more information or ideas on BMPs. See also the EPA's National Menu of BMPs at <https://www.epa.gov/npdes/national-menu-best-management-practices-bmps-stormwater> for a list of Alaska specific BMPs look at the *Alaska SWPPP Guide's* Appendix B - BMP Guide for Erosion & Sediment Control at [http://www.dot.state.ak.us/stwddes/desenviron/assets/pdf/bmp/bmp\\_all.pdf](http://www.dot.state.ak.us/stwddes/desenviron/assets/pdf/bmp/bmp_all.pdf)

### 10.17.1 Washing of Equipment and Vehicles (4.8.1)

Will equipment and vehicle washing and/or wheel wash-down be conducted at the site?

Yes  No

### 10.17.2 Fueling and Maintenance Areas (4.8.2)

Describe equipment/vehicle fueling and maintenance practices to be implemented to control pollutants to storm water (e.g., secondary containment, drip pans, spill kits, etc.).

Describe spill prevention and control measures to be implemented, including ways to reduce the chance of spills, stop the source of spills, contain and clean up spills, dispose of materials contaminated by spills, and train personnel responsible for spill prevention and control.

Will equipment and vehicle fueling or maintenance be conducted at the site?

Yes  No

HMCP or SPCC: For the specific sections in the Good Housekeeping BMPs that deal with fueling and oiling, equipment care and maintenance, waste materials, etc., it should be mentioned, by referencing the specific page and section, this requirement for BMP reference and citation is met. Also, it will/can create less conflict within the SWPPP due to the HMCP being project specific and the BMP citations more generic.

### 10.17.3 Staging and Material Storage Areas (4.8.3)

Designate areas to be used for staging and material storage areas. Locate such activities, to the extent practicable, away from storm water conveyance channels, storm water inlets, and waters of the U.S.; and minimize the exposure to precipitation and storm water and vandalism for all chemicals, treatment chemicals, liquid products, petroleum products, and other materials that have the potential to pose a threat to human health or the environment.

### 10.17.4 Washout of Applicators/Containers Used for Paint, Concrete, and Other Materials (4.8.4)

Describe location(s) and controls to minimize the potential for storm water pollution from washout areas for concrete mixers, paint, stucco, etc.

Will washout areas for trucks, applicators, or containers of concrete, paint, or other materials be used at the site?  Yes  No

### 10.17.5 Fertilizer or Pesticide Use (4.8.5)

Describe fertilizers and/or pesticides expected to be used and/or stored on-site and procedures for storage of materials to minimize exposure of the materials to storm water.

Will fertilizers or pesticides be used at the site?  Yes  No

Contractors will obtain authorization to spray pesticides through DOT&PF M&O utilizing the DOT&PF Integrated Vegetation Management Plan (IVMP). A permit from DEC is only required (in addition to IVMP authorization obtained via working through the regional M&O environmental analysts and a TCP from ROW) if the contractor is applying pesticide to a water body/aquatic site. Also, if spraying within the MOA, a local permit must be obtained from the MOA as well. For more information and contacts, visit <http://dot.alaska.gov/stwdmno/ivmp/index.shtml>.

### 10.18 Spill Notification (4.9)

The contractor shall describe spill-notification procedures, including relevant federal, state, tribal, and local agency contact information, to be implemented in the event of a leak, spill, or release of hazardous substances or oil that occur at the construction site. Refer to CGP Part 4.9 for permit requirements.

Contractor shall use DOT&PF Hazardous Material Control Plan template at [http://www.dot.state.ak.us/stwddes/dcsconst/assets/docs/constforms/hmcp\\_template.doc](http://www.dot.state.ak.us/stwddes/dcsconst/assets/docs/constforms/hmcp_template.doc) to create project specific plan. Include final plan as approved by DOT&PF in Appendix O.

### 10.19 Construction and Waste Materials (4.8.6, 5.3.7)

Describe in general terms the type of construction and waste materials expected to be stored at the site, with updates as appropriate, and describe the measures for handling and disposal of all wastes generated at the site, including clearing and demolition debris or other waste soils removed from the site, construction and domestic waste, hazardous or toxic waste, and sanitary waste. Refer also to CGP Parts 4.8.3 Staging and Material Storage Areas, and 4.8.6 Storage, Handling, and Disposal of Construction Waste.

Building materials and other construction site wastes must be properly managed and disposed of to reduce the risk of pollution from materials such as surplus or refuse building materials or hazardous wastes. Practices such as trash disposal, recycling, proper material handling, and spill prevention and cleanup

measures can reduce the potential for storm water runoff to mobilize construction site wastes and contaminate surface or groundwater.

The contractor must establish proper building and material storage areas to avoid pollutants coming in contact with rainfall or flowing storm water. Any materials that have the potential to pollute storm water will be covered to prevent rainfall from coming into contact with them. Garbage containers will be covered to prevent debris from blowing away as well. Any contractor supplied staging area must be included in inspections and the SWPPP. No materials will be staged or stored, even temporarily in flowing water.

The contractor should designate a waste collection area on site that does not receive substantial amount of runoff from upland areas and does not drain directly to a water body.

<b><i>BMP Description: General Construction Site Waste Management</i></b>	
<b><i>BMP Manual/Publication: DEC Alaska Storm Water Guide, December 2011</i></b>	
<b><i>Installation Schedule:</i></b>	Continuously during construction activities
<b><i>Maintenance and Inspection:</i></b>	<u>Inspection:</u> Inspect storage and use areas and identify containers or equipment that could malfunction and cause leaks or spills. Check equipment and containers for leaks, corrosion, support or foundation failure, or other signs of deterioration, and test them for soundness. <u>Maintenance:</u> Immediately repair or replace any that are found to be defective.
<b><i>Responsible Staff:</i></b>	SWPPP Manager & Superintendent, Contractor

## 11.0 INSPECTIONS (5.4; 6.0)

Minimum requirements for the locations and scope of site inspections are described in the CGP Part 6.4.

Inspection requirements for linear projects are described in the CGP Part 6.5.

Describe the frequency inspections will occur at your site, including any correlations to storm frequency and intensity.

Note that inspection details for particular BMPs should be included in Section 11 or Appendix B.

### 11.1 Inspection Schedules (5.4.1.2; 6.1; 6.2; 6.6)

Refer to CGP Part 6.1 for inspection frequency requirements.

Required inspection frequency is based on mean annual precipitation for the site. Refer to Section 3.2 for annual precipitation data and can be found in the project specifications.

A permittee must allow an authorized representative of DEC, EPA or the MS4 operator to conduct a site inspection in accordance with the CGP Part 6.6.

Inspection Frequency:

The inspection frequency in Central Region will now be once every seven calendar days.

Inspection frequency: **Once every seven calendar days**

#### **Justification for reduction in inspection frequency, if applicable:**

Based on Section 6.2 of the ACGP, the project may reduce inspection frequency as follows:

- If the entire site is temporarily stabilized, the frequency of inspections may be reduced to at least once every thirty (30) calendar days and within two business days of the end of a storm event, at actively staffed sites, that resulted in a discharge from the site;
- If portions of the site have achieved final stabilization, but construction activity remains on other portions of the site, inspections may be suspended for those portions that have achieved final stabilization; however, subsequent inspections may need to be conducted within two business days of the end of a storm event, at actively staffed sites, that results in erosion and causes a discharge from that portion of the site previously considered finally stabilized;
- If the project is undergoing winter shutdown, inspections may stop fourteen (14) calendar days after the anticipated fall freeze-up and shall resume at least twenty-one (21) calendar days prior to the anticipated spring thaw; or
- If the entire site has been finally stabilized and a Notice of Termination (NOT) has been submitted, no further inspection requirements apply to the site.

As defined by the CGP, winter shutdown means the cessation of soil disturbing or soil stabilizing construction activity for winter. Typically this period is from October/November to April/May and is approximately from Fall Freeze-up to Spring Thaw.

CGP Definition of Fall Freeze-up: For the purposes of this permit, means for planning purposes in the development of the SWPPP and initial planning of control measure maintenance the date in the fall that air temperatures will be predominately below freezing. It is the date in the fall that has an 80% probability that a minimum temperature below a threshold of 32.5 degrees Fahrenheit will occur on or after the given date.

CGP Definition of Spring Thaw: For the purposes of this permit, means for planning purposes in the development of the SWPPP and initial planning of control measure maintenance the date in the spring

that air temperatures will be predominately above freezing. It is the date in the spring that has a 20% probability that a minimum temperature below a threshold of 32.5 degrees Fahrenheit will occur on or after the given date.

These dates can be found by looking up the “Fall ‘Freeze’ Probabilities” & “Spring ‘Thaw’ Probability” for the weather station closest to the site on the website: [www.wrcc.dri.edu/summary/Climsmak.html](http://www.wrcc.dri.edu/summary/Climsmak.html). NOTE: this estimation of “Fall Freeze-up” & “Spring ‘Thaw’” is for planning purposes only. During construction, the permittee will need to maintain control measures based on actual conditions.

**Estimated date of winter shutdown:**

This project may extend through multiple construction seasons. If more than one construction season is required to complete the project, a winter shutdown period will be necessary. The actual dates of winter shutdown shall be based upon temperatures and general weather conditions. For the purposes of the contractor’s SWPPP based upon this ESCP, the fall freeze-up is the date in the fall that air temperatures will be predominately below freezing and the spring thaw date is the date in the spring that air temperatures will be predominately above freezing.

Based on the Fall ‘Freeze’ Probabilities and Spring ‘Freeze’ Probabilities for the weather station closest to the project, Anchorage Intl. AP, Alaska (500280), the estimated date of fall freeze-up is September 30 and the estimate date of spring-thaw is May 16. Winter shutdown may start on or after October 14 (14 days after anticipated fall freeze-up). Inspections shall resume on or before March 25 (21 days prior to anticipated spring-thaw). Fall and Spring Freeze probabilities for Anchorage Intl. AP, Alaska (500280) are shown below.

Fall 'Freeze' Probabilities (Jul. 31 - Dec. 31)											
ANCHORAGE INTL AP, s (500280)											
Temp F	Earliest	10%	20%	30%	40%	50%	60%	70%	80%	90%	Latest
36.5	08/14	08/28	09/05	09/08	09/10	09/13	09/15	09/19	09/23	09/26	10/04
32.5	08/28	09/10	09/16	09/20	09/23	09/24	09/27	09/29	09/30	10/05	10/16
28.5	09/11	09/22	09/24	09/27	09/30	10/02	10/05	10/09	10/11	10/14	10/20
24.5	09/22	10/03	10/08	10/09	10/11	10/13	10/16	10/18	10/20	10/27	11/09
20.5	09/24	10/08	10/13	10/15	10/19	10/23	10/26	10/27	11/01	11/09	11/23

[Graphic Output](#)

Earliest - Earliest date when a minimum temperature below the threshold occurred.  
 \*\*/\*\* means there has never been a year when the minimum temperature went below the threshold temperature during the July 31 to Dec. 31 period or insufficient data to determine a date.  
 07/31 means the minimum temperature can go below the threshold temperature any day during the July 31 to Dec. 31 period.

xx% - Percent probability that a minimum temperature below the threshold will occur on or before the given date.  
 \*\*/\*\* means non-occurrence of the threshold or insufficient data to determine a threshold.

Latest - Latest date when a minimum temperature below the threshold occurred.  
 \*\*/\*\* means there has been a year when the minimum temperature didn't go below the threshold temperature during the July 31 to Dec. 31 period or insufficient data to determine a date.

Western Regional Climate Center; [wrcc@dri.edu](mailto:wrcc@dri.edu)

**Spring 'Freeze' Probabilities (Jan 1 - Jul 31)**

Temp F	ANCHORAGE INTL AP, s (500280)										
	Earliest	90%	80%	70%	60%	50%	40%	30%	20%	10%	Latest
36.5	05/07	05/15	05/17	05/18	05/21	05/21	05/23	05/26	05/29	06/03	07/01
32.5	04/19	04/29	05/01	05/03	05/05	05/08	05/09	05/13	05/16	05/18	05/22
28.5	04/02	04/14	04/19	04/21	04/23	04/26	04/28	04/30	05/03	05/06	05/16
24.5	03/18	03/31	04/05	04/09	04/10	04/13	04/16	04/19	04/21	04/24	05/10
20.5	03/07	03/19	03/25	03/27	04/01	04/03	04/08	04/12	04/15	04/20	05/10

[Graphic Output](#)

Earliest - Earliest date when a minimum temperature below the threshold occurred.  
 \*\*/\*\* means there has been a year when the minimum temperature didn't go below the threshold temperature during the Jan. 1 to July 31 period or that there was insufficient data to determine a date.

xx% - Percent probability that a minimum temperature below the threshold will occur on or after the given date.  
 \*\*/\*\* means non-occurrence of the threshold or insufficient data to determine a threshold.

Latest - Latest date when a minimum temperature below the threshold occurred.  
 \*\*/\*\* means there has never been a year when the minimum temperature went below the threshold temperature during the Jan. 1 to July 31 period or insufficient data to determine a date.  
 07/30 means the minimum temperature can go below the threshold temperature any day during the Jan. 1 to July 31 period.

Western Regional Climate Center, [wrcc@dri.edu](mailto:wrcc@dri.edu)

The inspections will be conducted jointly with department personnel as directed by the Project Engineer. The schedule for site inspections will be established and updated daily as necessary to meet the requirements of the CGP and provide the department with notice and opportunity to participate in the site inspection.

### 11.2 Inspection Form or Checklist (5.4.1.3; 6.7)

Contractor is required to attach Form 25D-100 in Appendix K. An Inspection Report will be completed after each inspection, identifying BMPs installed at the time of inspection, noting corrective actions required, and documenting complete-by-date for any actions discovered during the inspection. Each report will be certified by the Contractor's Superintendent and DOT&PF's Project Engineer.

### 11.3 Corrective Action Procedures (5.4.1.4; 8.0)

Identify how conditions found that require corrective action will be addressed:

The following guidelines apply for setting corrective action complete-by dates as required by the CGP:

For conditions that are easily remedied (i.e., removal of tracked sediment, maintenance of control measures, or spill clean-up), the permittee must initiate appropriate steps to correct the problem within twenty-four hours from the time of discovery and correct the problem as soon as possible; or

If installation of a new control measure is needed or an existing control measure requires significant redesign and reconstruction or replacement, the permittee must install the new or modified measure and make it operational within seven calendar days from the time of discovery of the need for the corrective action, unless infeasible.

If a discharge occurs during a local 2-year, 24-hour storm event, a corrective action must be initiated the day after the storm event ends as described in CGP Part 8.1.1.

For corrective actions that could affect a subcontractor, notify the subcontractor within three calendar days of taking the corrective action.

Additionally, deadlines for completion of corrective actions shall be selected to protect water quality and prior to the next storm event unless impracticable.

### **Corrective Action Log**

The corrective action log will document the following within 24 hours of discovery of any conditions listed in CGP Part 8.1 (use Form 25D-112 and include in Appendix J):

- Date the problem was identified
- Summary of corrective action taken or to be taken
- Notice of whether SWPPP modifications were required as a result of this discovery or corrective action
- Date corrective action completed and name of person completing the action

In the event there is a reason (outside of the project staff's control) that a corrective action cannot practicably be completed by the set complete-by date, DOT&PF will complete a Delayed Action Item Report (Form 25D-113). This form will set a new complete-by date and document the reason that the previous date could not be met.

## **11.4 Inspection Recordkeeping (5.4.2)**

Records (including inspection reports, corrective action logs, delayed action item reports, grading and stabilization logs, amendment logs, staff tracking logs, rainfall logs, and training logs) will be maintained for a minimum period of at least three (3) years after the permit is terminated. A hard copy and electronic copy of the final SWPPP, including all appendices, will be transmitted to DOT&PF when the project's NOTs are filed.

## **12.0 MONITORING PLAN (IF APPLICABLE) (5.5; 7.0)**

### **12.1 Determination of Need for Monitoring Plan**

Is there an EPA-established or approved TMDL for **Campbell Lake, Campbell Creek, Fish Creek, Little Campbell Creek and Chester Creek**?  Yes  No

Is the receiving water listed as impaired for turbidity and/or sediment?  Yes  No

## **13.0 POST-AUTHORIZATION RECORDS (5.8)**

### **Copy of Permit Requirements (5.8.1)**

The contractor's SWPPP must contain the following documents:

- copy of CGP (5.8.1.1)
- copy of the signed and certified NOI form submitted to DEC (5.8.1.2)
- upon receipt, a copy of letter from DEC authorizing permit coverage, providing tracking number (5.8.1.3)

These documents must be included in Appendix F.

### 13.1 Additional Documentation Requirements (5.8.2)

The Grading and Stabilization Log, Form 25D-110 in Appendix G, will be filled out to satisfy the following CGP requirements:

- Dates when grading activities occur (5.8.2.1.1)
- Description of grading activities and location (5.8.2.1.2)
- Dates when construction activities temporarily or permanently cease on a portion of the site (5.9.2)
- Dates when stabilization measures are initiated (5.8.2.1.4)
- Description of Stabilization Measure (5.8.2.1.5)
- Date of beginning and ending period for winter shutdown (5.8.2.2)

Other documents will be included as shown below:

- Copies of inspection reports (5.4.2; 5.8.2.3; insert in Appendix K).
- Copies of monitoring reports, if applicable (7.3.9.2; 5.8.2.4; 5.8.2.5; 5.5.2; 9.1; insert in Appendix H).
- Documentation in support of chemical-treatment processes (4.6; 5.8.2.7; insert in Appendix P).
- Documentation of maintenance and repairs of control measures (5.8.2.9; 8.1; 8.2; insert in Appendix J).
- Copy of DEC Letter of Non-Objection (insert in Appendix D).

#### 13.1.1 Records of Employee Training (4.14; 5.8.2.8)

Training staff and subcontractors is an effective BMP. Document all training conducted for your staff, those with specific storm water responsibilities (e.g. installing, inspecting, and maintaining BMPs), and subcontractors. Use the Training Log (Form 25D-125) in Appendix I.

Describe Training Conducted: **Insert Text**

General storm water and BMP awareness training for staff and subcontractors:

During safety meetings and schedule briefings, corrective actions from the previous period will be reviewed. The contractor is encouraged to discuss timing of activities and stabilization requirements. Records of the training topics, attendees, and length must be maintained in the contractor's SWPPP.

Detailed training for staff and subcontractors with specific storm water responsibilities:

**Insert Text**

Individual(s) Responsible for Training:

**Insert Names, Titles, and Contact Numbers here**

Documentation of training conducted shall be record on Form 25D-125 and included in Appendix I.

## 14.0 MAINTAINING AN UPDATED SWPPP (5.9)

This section does not need to be filled out but is a list of reminders for the applicant.

The permittee must modify the SWPPP, including site map(s), in response to any of the following:

- Whenever changes are made to construction plans, control measures, good housekeeping measures, monitoring plan (if applicable), or other activities at the site that are no longer accurately reflected in SWPPP (5.9.1.1);
- If inspections of site investigations by staff or by local, state, tribal, or federal officials determine SWPPP modifications are necessary for permit compliance (5.9.1.2); and
- To reflect any revisions to applicable federal, state, tribal, or local laws that affect control measures implemented at the construction site (5.9.1.3).

## **14.1 SWPPP Amendment Log (5.9.2)**

A permittee must keep a log showing dates, name of person authorizing the change, and a brief summary of changes for all significant SWPPP modifications (e.g., adding new control measures, changes in project design, or significant storm events that cause replacement of control measures). Use DOT&PF construction form 25D-114. **Amendments must be approved by an AK-CESCL or equivalently certified individual and be included in Appendix M. The Superintendent and the SWPPP Manager are the only persons authorized to amend the SWPPP and update the SWPPP Amendment Log. Amendments must be approved by the Project Engineer. This approval must be documented in the “PE’s Initials column” by the Project Engineer.**

## **14.2 Deadlines for SWPPP Modifications (5.9.3)**

Revisions to the SWPPP must be completed within seven days of the inspection that identified the need for a SWPPP modification or within seven days of substantial modifications to the construction plans or changes in site conditions.

# **15.0 ADDITIONAL SWPPP REQUIREMENTS (5.10)**

## **15.1 Retention of SWPPP (5.10.1)**

A copy of the SWPPP (including a copy of the permit), NOI, and acknowledgement letter from DEC must be retained at the construction site.

## **15.2 Main Entrance Signage (5.10.2)**

A sign or other notice must be posted conspicuously near the main entrance of the site. The sign or notice must include a copy of the completed NOI for both DOT&PF and the contractor.

## **15.3 Availability of SWPPP (5.10.3)**

The permittee must keep a current copy of the SWPPP at the site. The SWPPP must be made available to subcontractors, government and tribal agencies, and MS4 operators, upon request.

## **15.4 Signature and Certification (5.10.4)**

As co-permittees, the SWPPP is signed, dated, and certified by both the contractor and by DOT&PF. DOT&PF requires the use of its forms, instead of those provided as examples in the DEC template. The contractor must complete the SWPPP Contractor Certification (Form 25D-111) once DOT&PF approves the SWPPP and include it in Appendix E. Either the contractor’s corporate officer or their duly authorized representative can certify the SWPPP. If a duly authorized representative certifies, the Delegation of Signature Authority form must be included in Appendix E.

Upon approval, DOT&PF will provide the contractor with signed DOT&PF forms for the DOT&PF SWPPP Certification (Form 25D-109) and DOT&PF Delegation of Authority (Form 25D-107) for inclusion in Appendix E of the SWPPP.

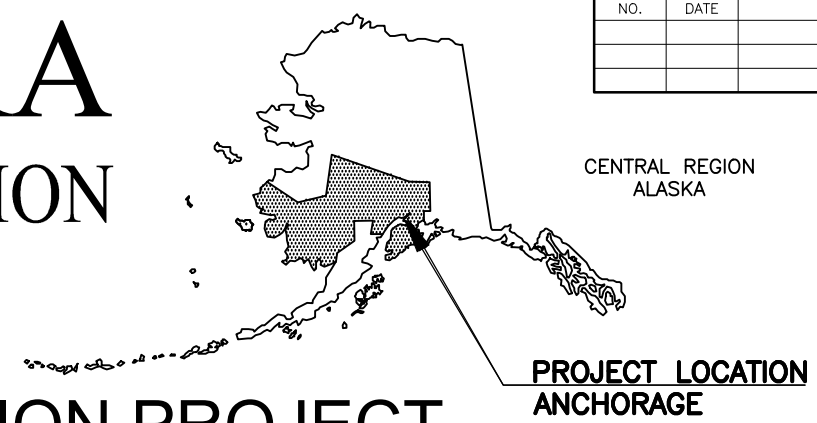
**APPENDIX A**  
**SITE MAPS AND DRAWINGS**



NO.	DATE	REVISION	STATE	PROJECT DESIGNATION	YEAR	SHEET NO.	TOTAL SHEETS
			ALASKA	0001822/CFHWY01389	2026	ESCP1	ESCP12
			ROUTE ID	133700	MILEPOINT	0 TO 3.71	
			LATITUDE	61.141183	LONGITUDE	-149.904694	
			ROUTE ID	1020000X000	MILEPOINT	123.569 - 125.081	
			LATITUDE	61.190997	LONGITUDE	-149.867806	

# STATE OF ALASKA

## DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES



### PROPOSED HIGHWAY PRESERVATION PROJECT

## ANCHORAGE AREA DRAINAGE PRESERVATION:

### DIMOND BLVD - JEWEL LAKE RD TO SEWARD HWY

### SEWARD HWY - TUDOR RD TO 20TH AVE

## PROJECT NO. 0001822/CFHWY01389

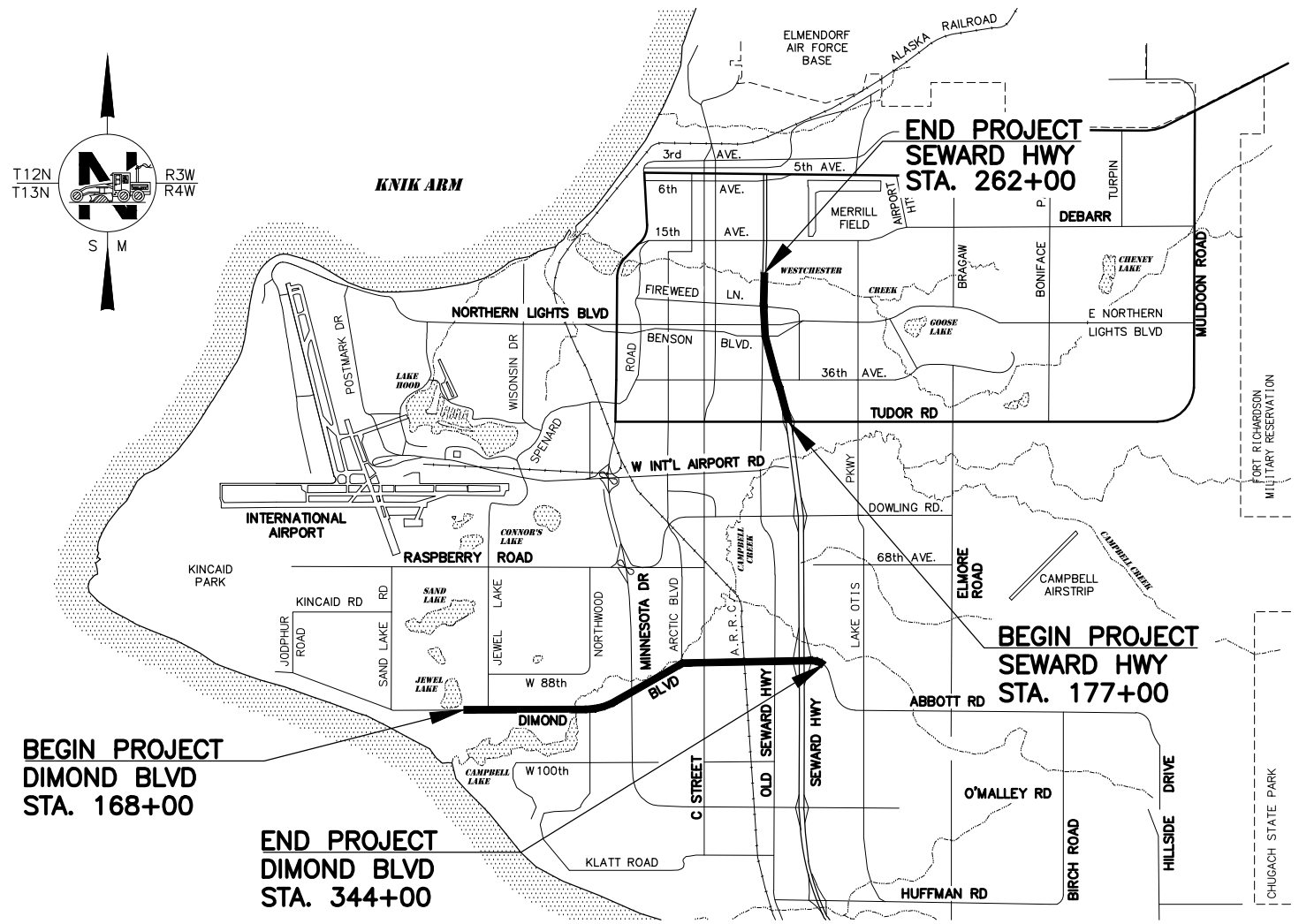
### CIPP EROSION SEDIMENT CONTROL PLAN

DESIGNED BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DRAFTED BY: \_\_\_\_\_

SCALE: N/A

DATE: 11/20/2025 3:58 PM

TIME: \_\_\_\_\_



#### ESCP LEGEND

— FR —	FIBER ROLLS		MANUFACTURED INLET PROTECTION SYSTEM
<b>ROW</b>	RIGHT-OF-WAY		DRAINAGE PATHWAY
- - - -	CLEAN DRAINAGE SYSTEM - PIPES		CURED-IN-PLACE PIPE

- #### GENERAL ESCP NOTES:
- REFER TO ESCP REPORT FOR EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs).
  - CONTRACTOR SHALL UTILIZE BMPs MOST APPROPRIATE FOR CONDITIONS ON-SITE. IF INSPECTION REVEALS EROSION CONTROL MEASURES ARE INSUFFICIENT, THE CONTRACTOR SHALL IMMEDIATELY IMPLEMENT CORRECTIVE ACTION, AS NECESSARY, TO CORRECT THE DEFICIENCY.
  - ALL WORK SHALL TAKE PLACE WITHIN THE RIGHT-OF-WAY (ROW). ALL STAGING AREAS SHALL BE LOCATED WITHIN THE ROW.
  - THE CONTRACTOR SHALL PROVIDE INLET PROTECTION ON ALL STORM DRAIN INLETS OR CULVERTS WITHIN 25 FEET OF DISTURBED GROUND. INLET PROTECTION SHALL BE SEQUENCED TO PROTECT EXISTING INLETS AS PROJECT PHASING OCCURS.

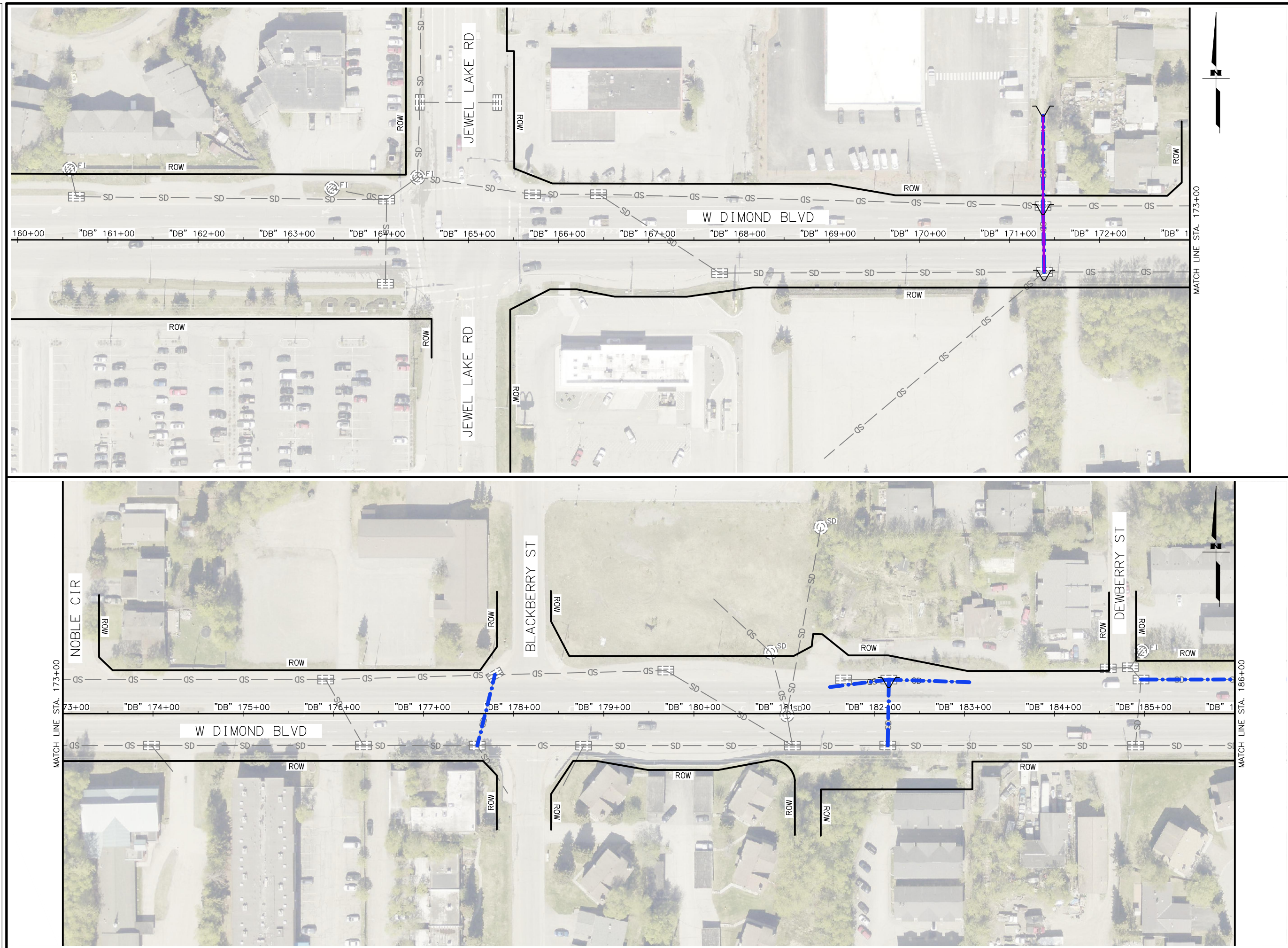
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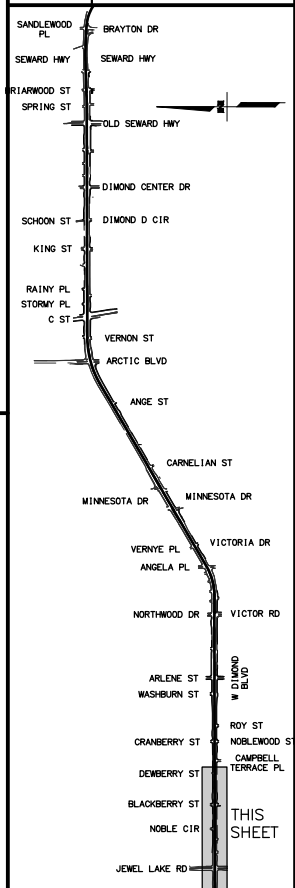
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SHEET NO.	TOTAL SHEETS
ESCP2	ESCP12
STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
CFHWY01389**

NO.	REVISION



**PIH/PS&E  
REVIEW**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES

ANCHORAGE AREA  
DRAINAGE PRESERVATION

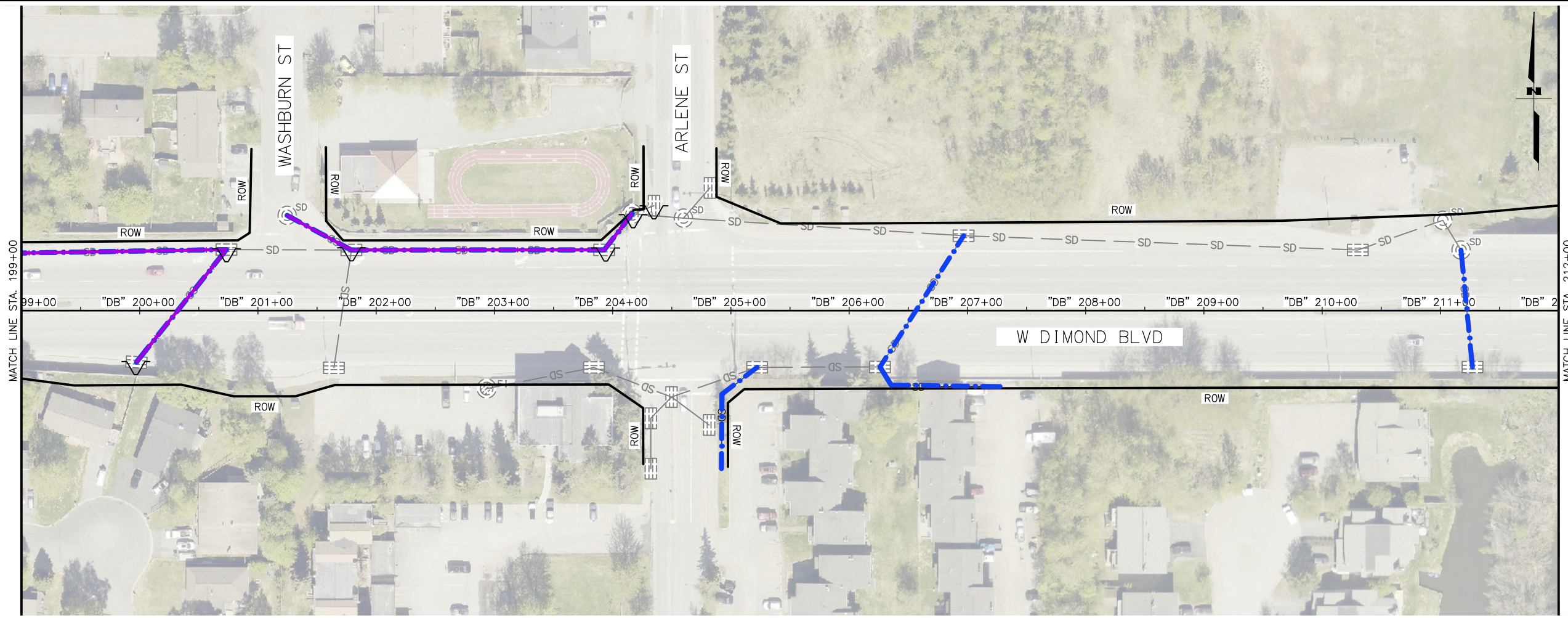
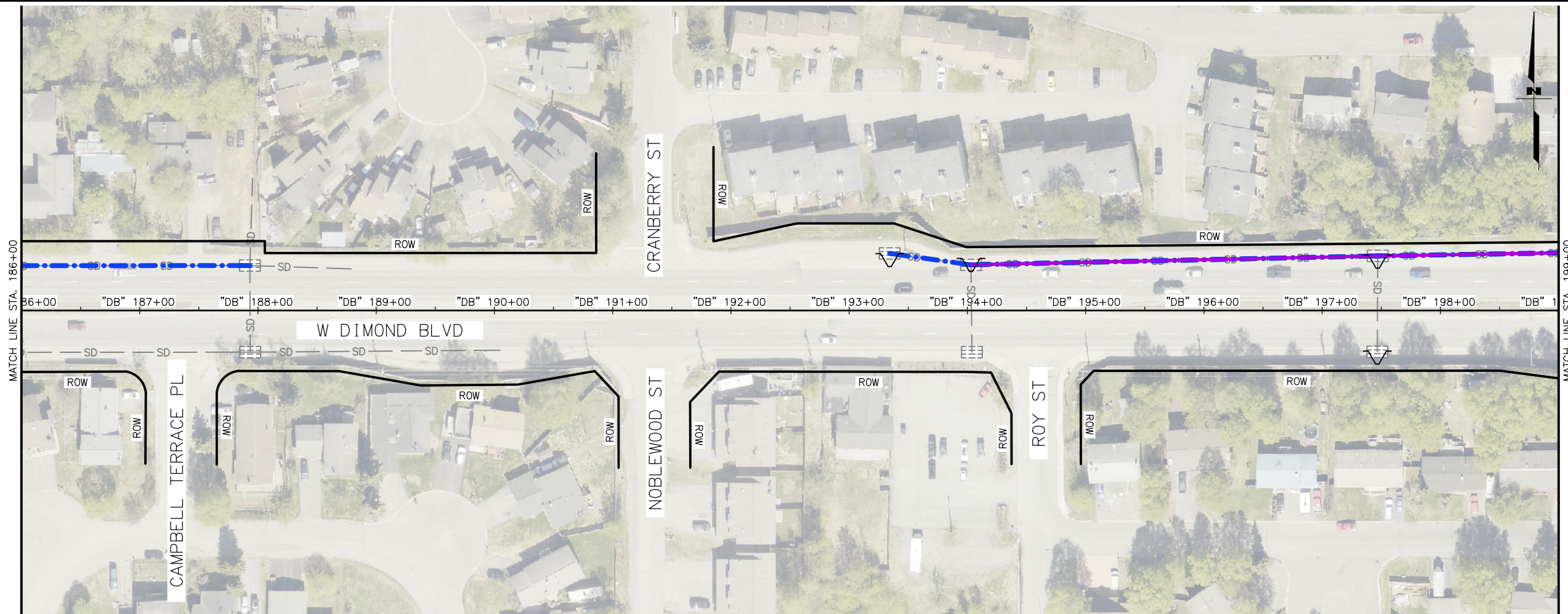
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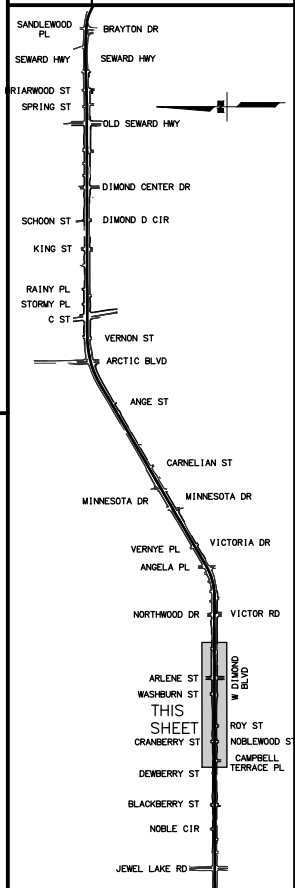
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ESCP3	ESCP12
STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
CFHWY01389**

NO.	REVISION



**PIH/PS&E  
REVIEW**

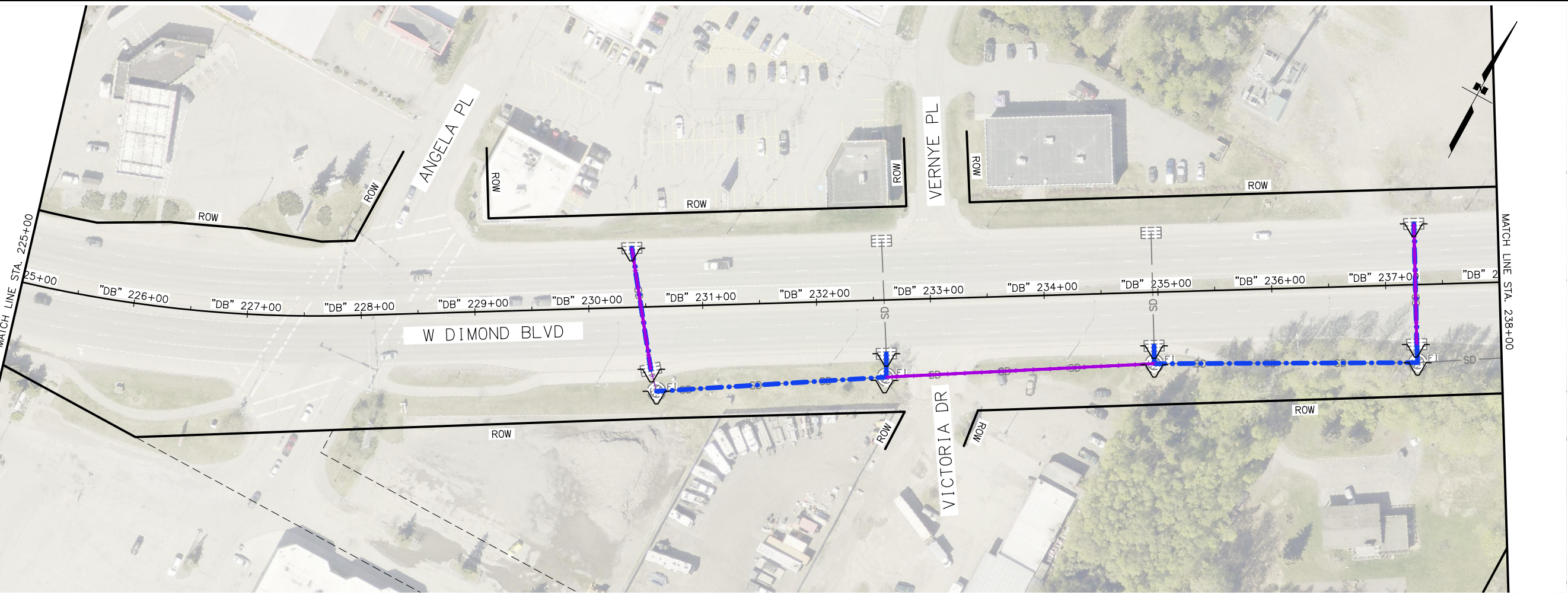
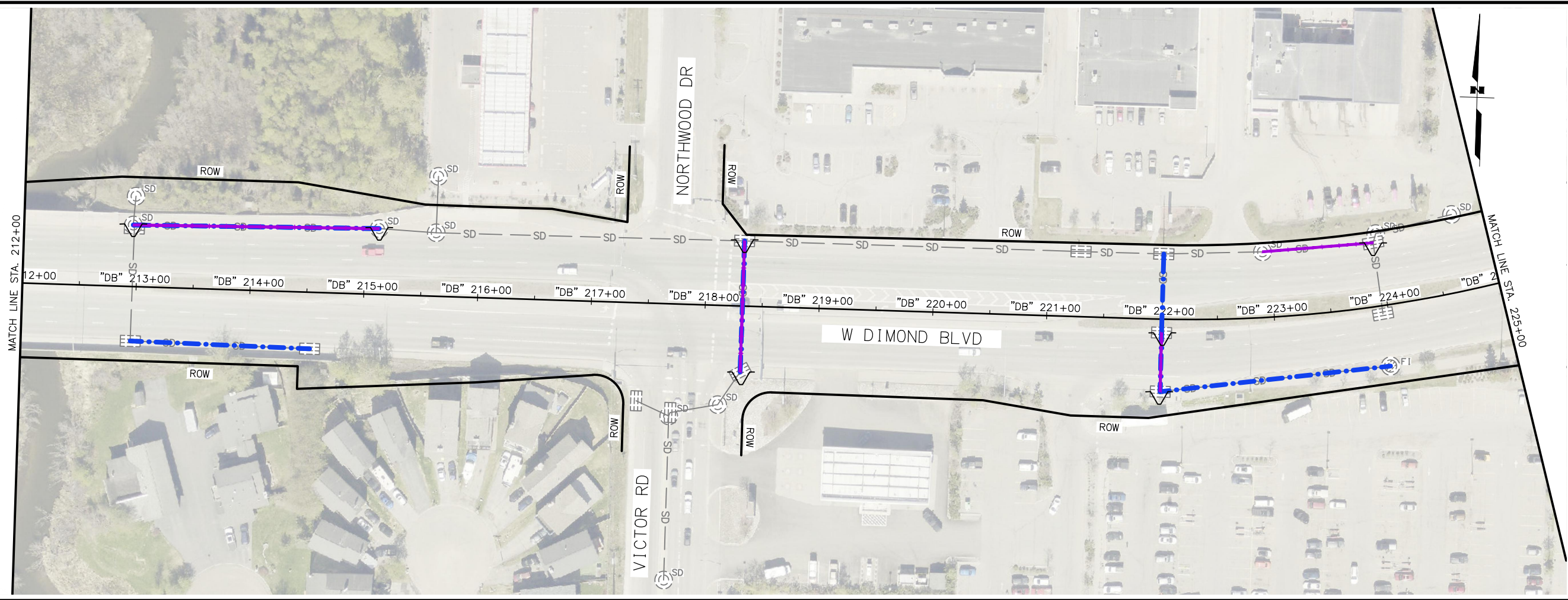
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DRAINAGE PRESERVATION  
  
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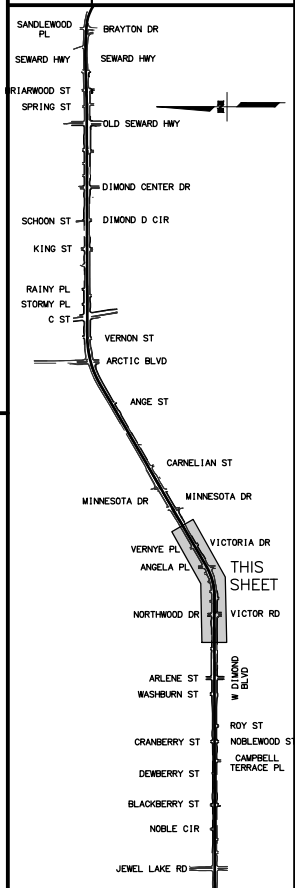
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STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
CFHWY01389**

NO.	REVISION



**PIH/PS&E  
REVIEW**

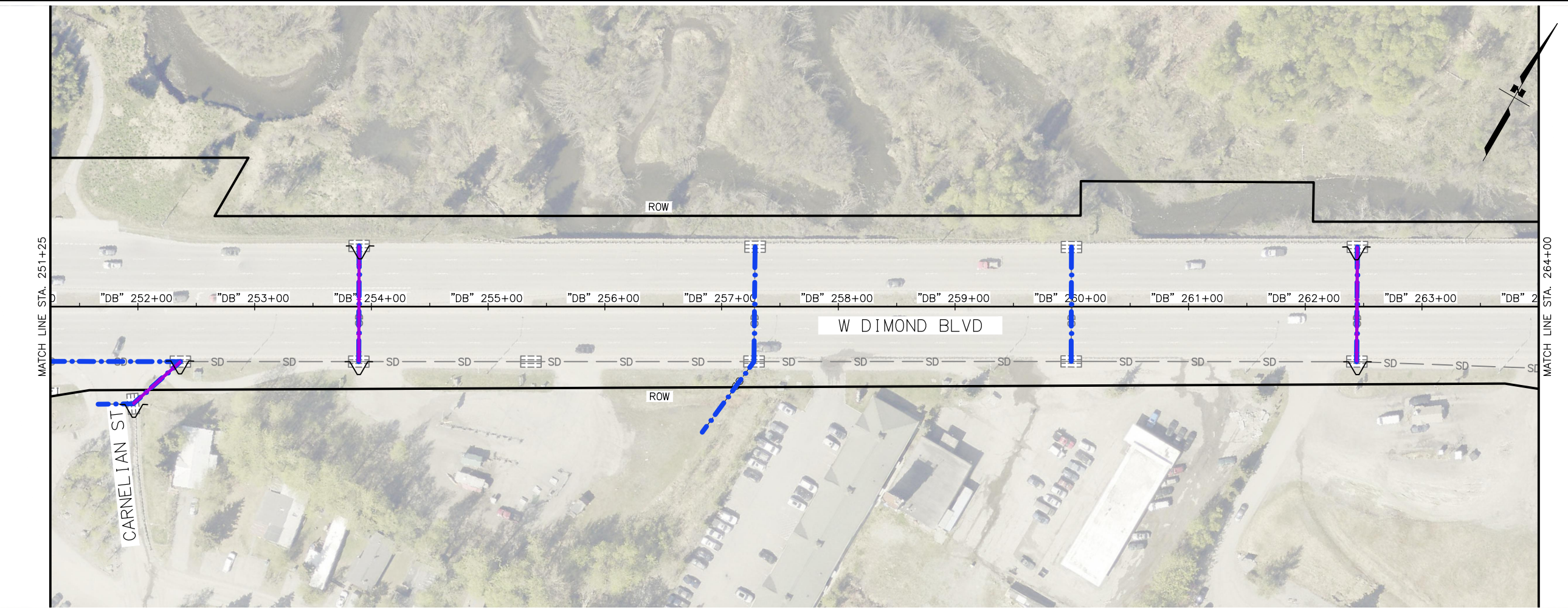
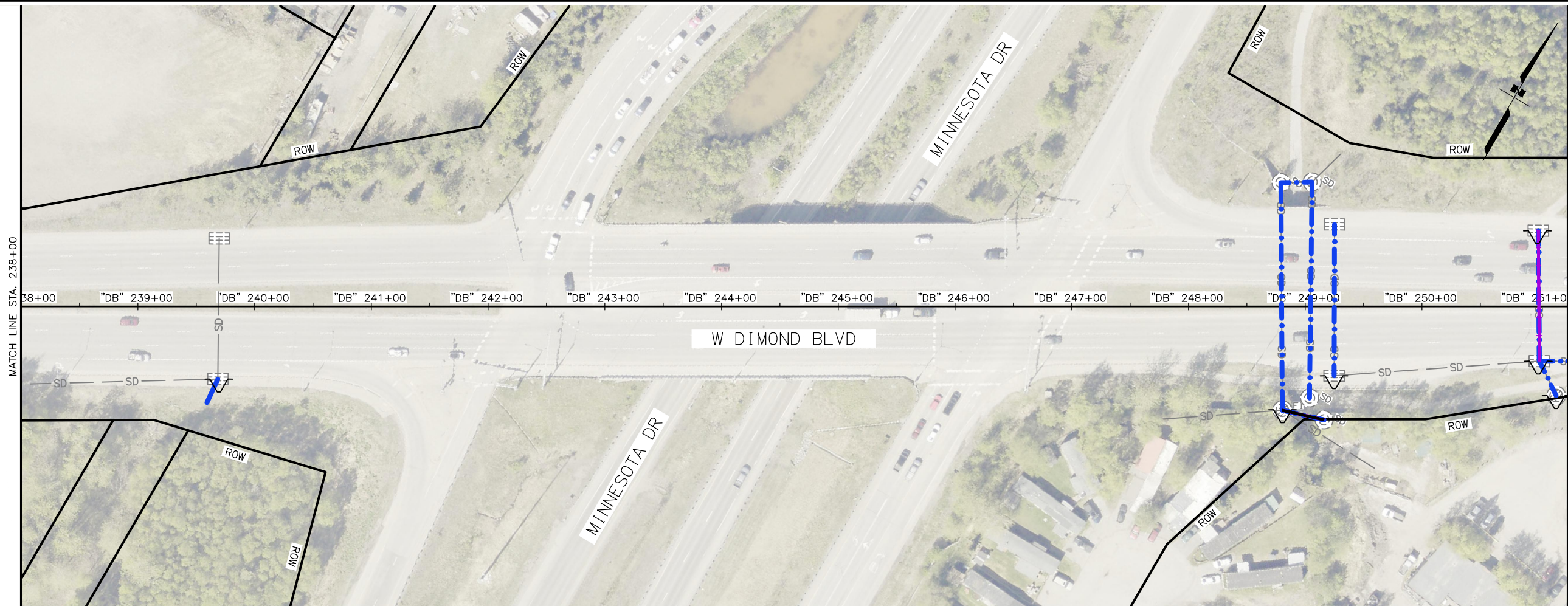
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AND PUBLIC FACILITIES  
  
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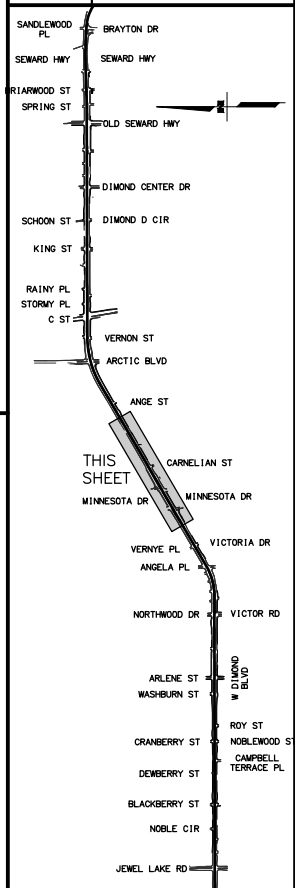
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STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
CFHWY01389**

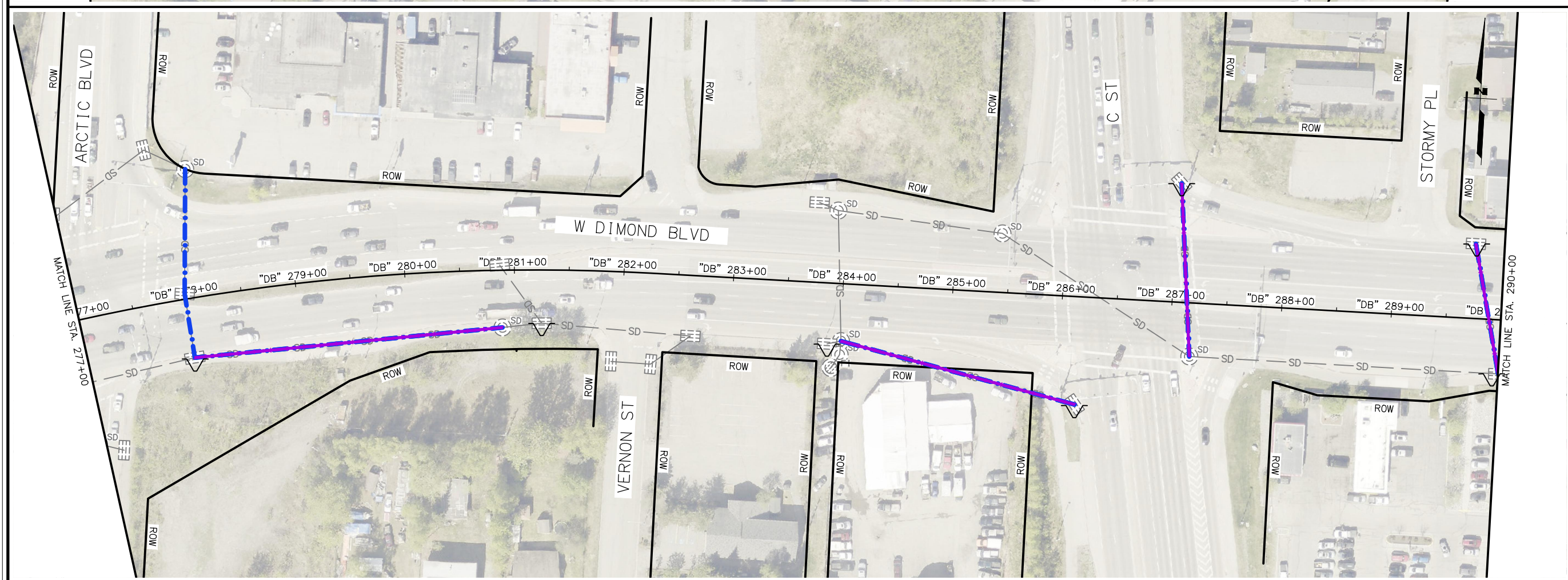
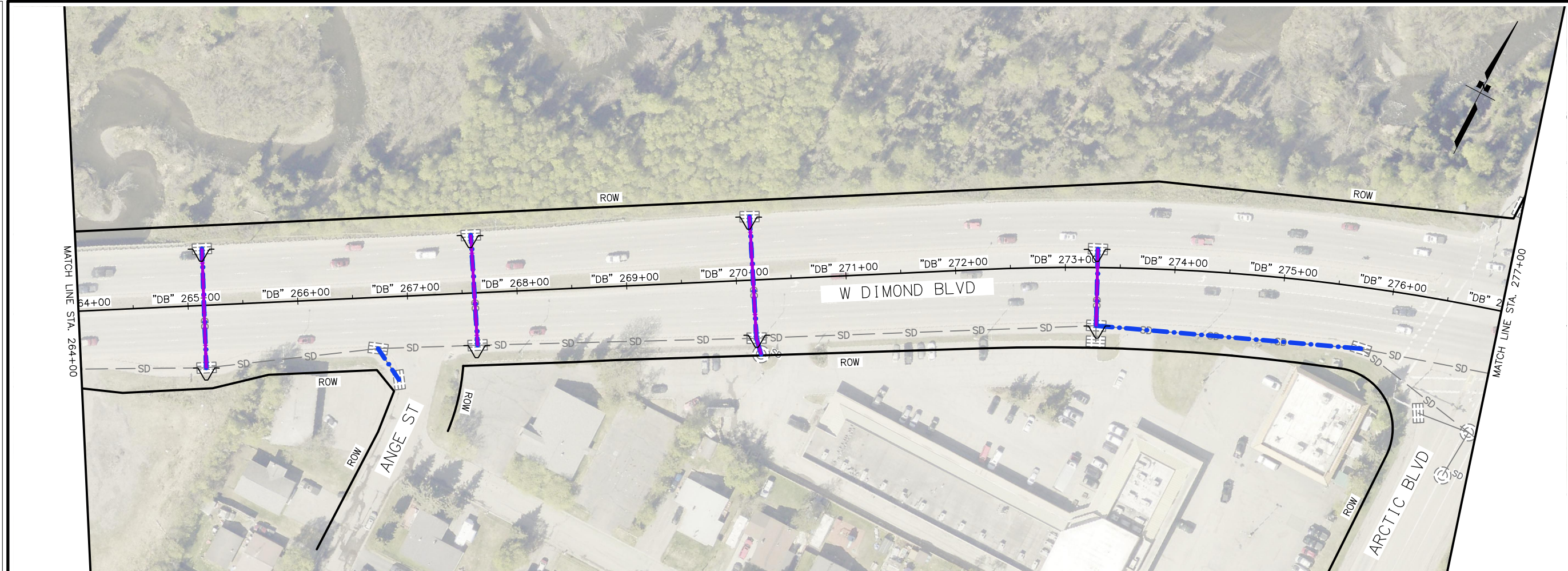
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**PIH/PS&E  
REVIEW**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
  
ANCHORAGE AREA  
DRAINAGE PRESERVATION  
  
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CIPP ESCP**

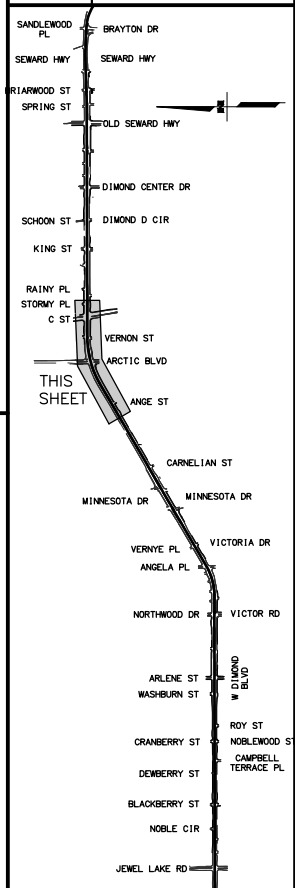
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SHEET NO.	TOTAL SHEETS
ESCP6	ESCP12
STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
 CFHWY01389**

NO.	REVISION



**PIH/PS&E  
 REVIEW**

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 ANCHORAGE AREA  
 DRAINAGE PRESERVATION  
**DIMOND BLVD  
 CIPP ESCP**

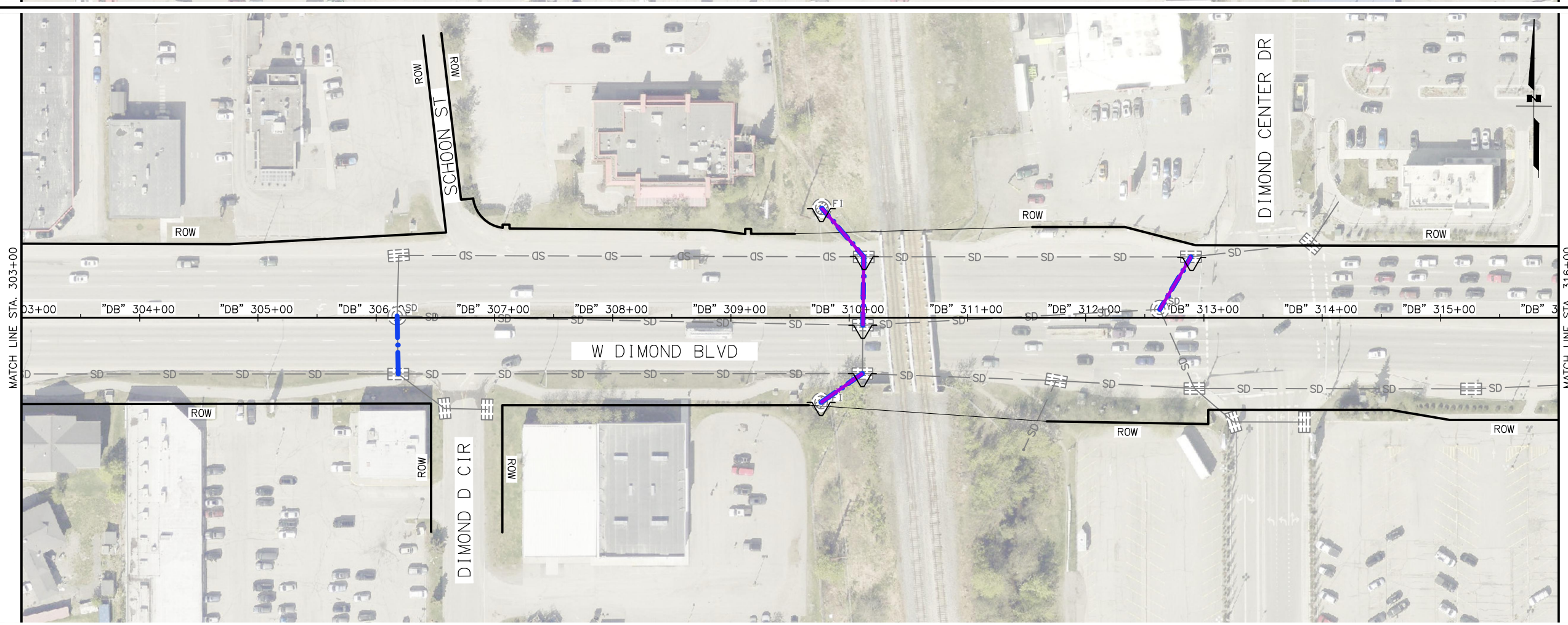
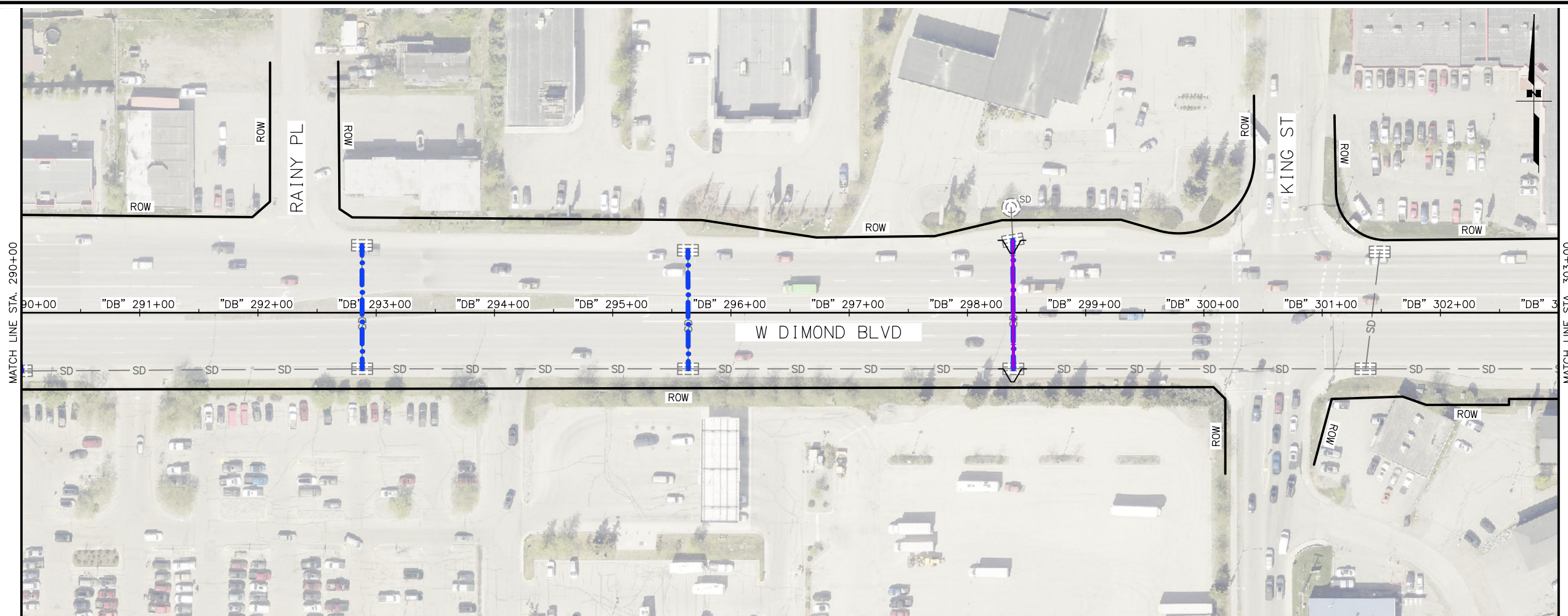
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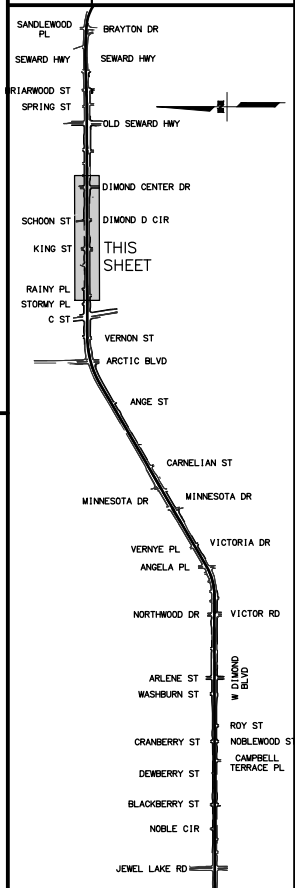
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SHEET NO.	TOTAL SHEETS
ESCP7	ESCP12
STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
CFHWY01389**

NO.	REVISION



**PIH/PS&E  
REVIEW**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES

ANCHORAGE AREA  
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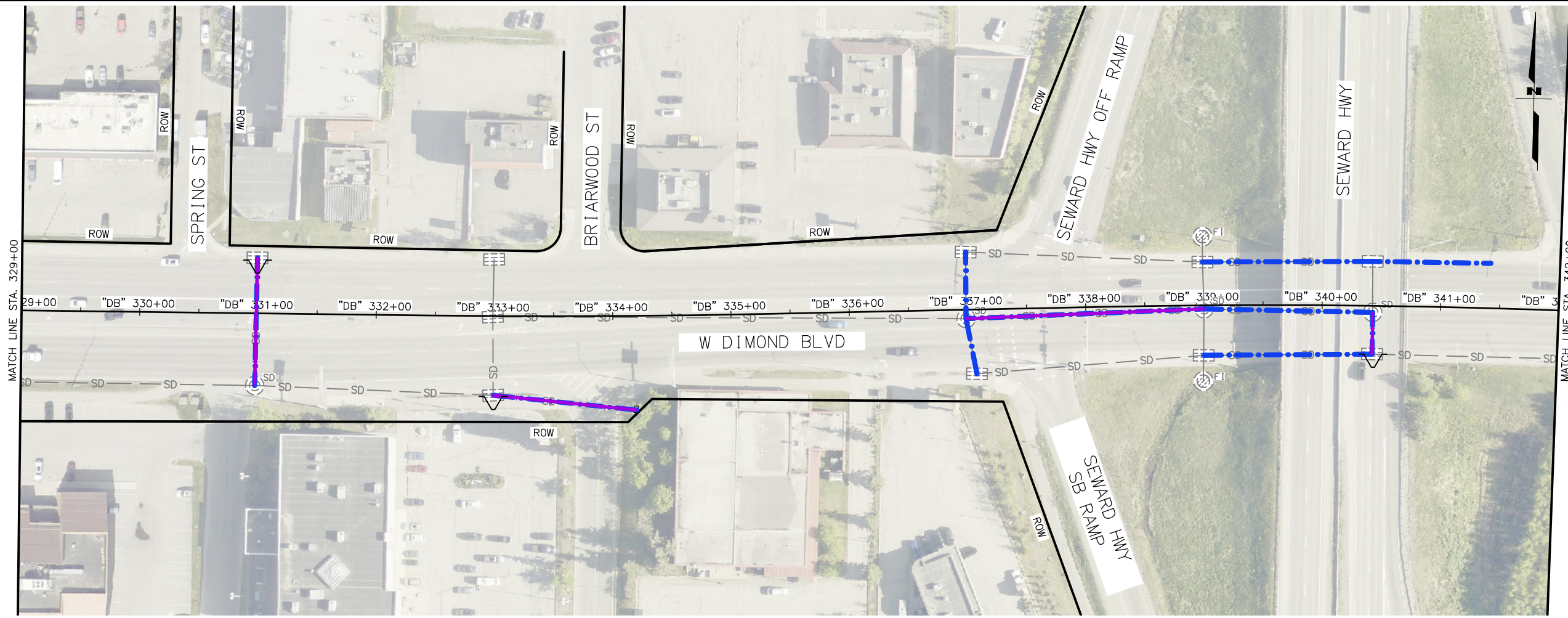
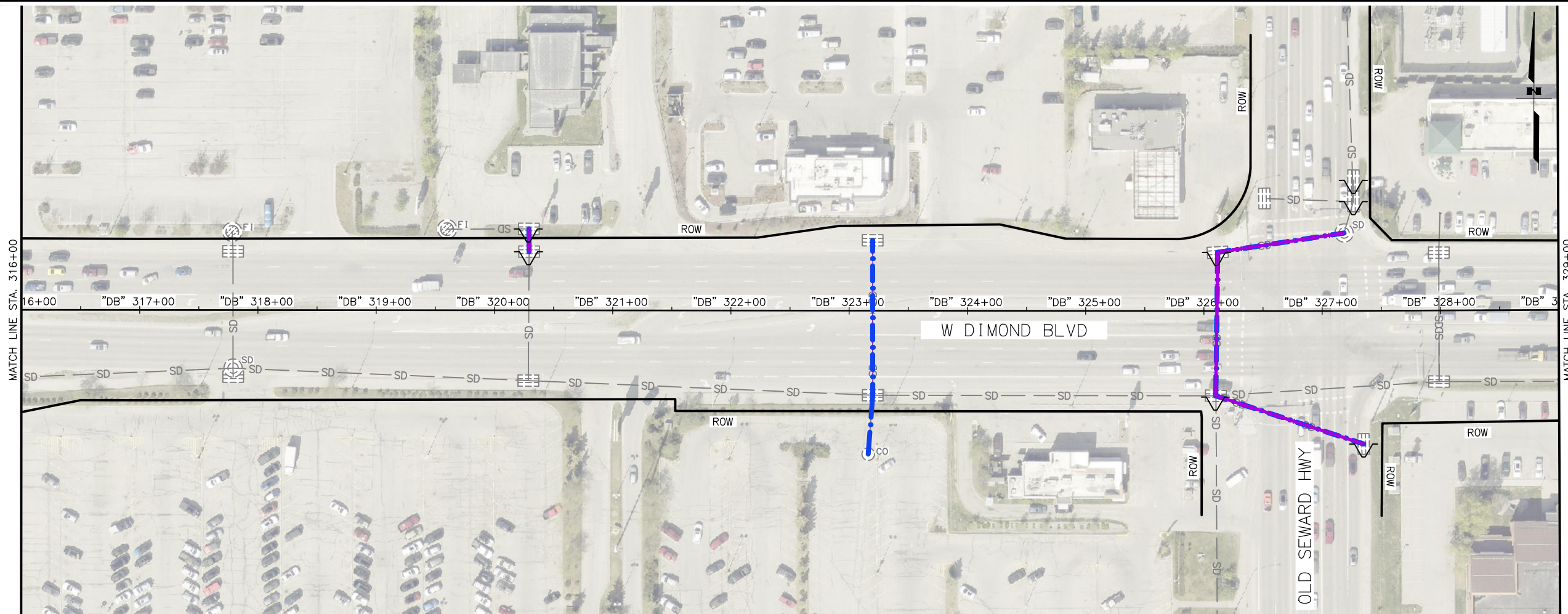
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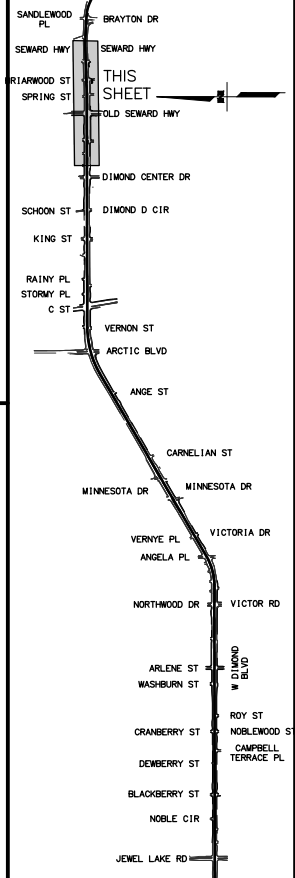
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ESCP8	ESCP12
STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
CFHWY01389**

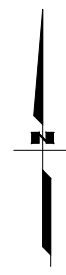
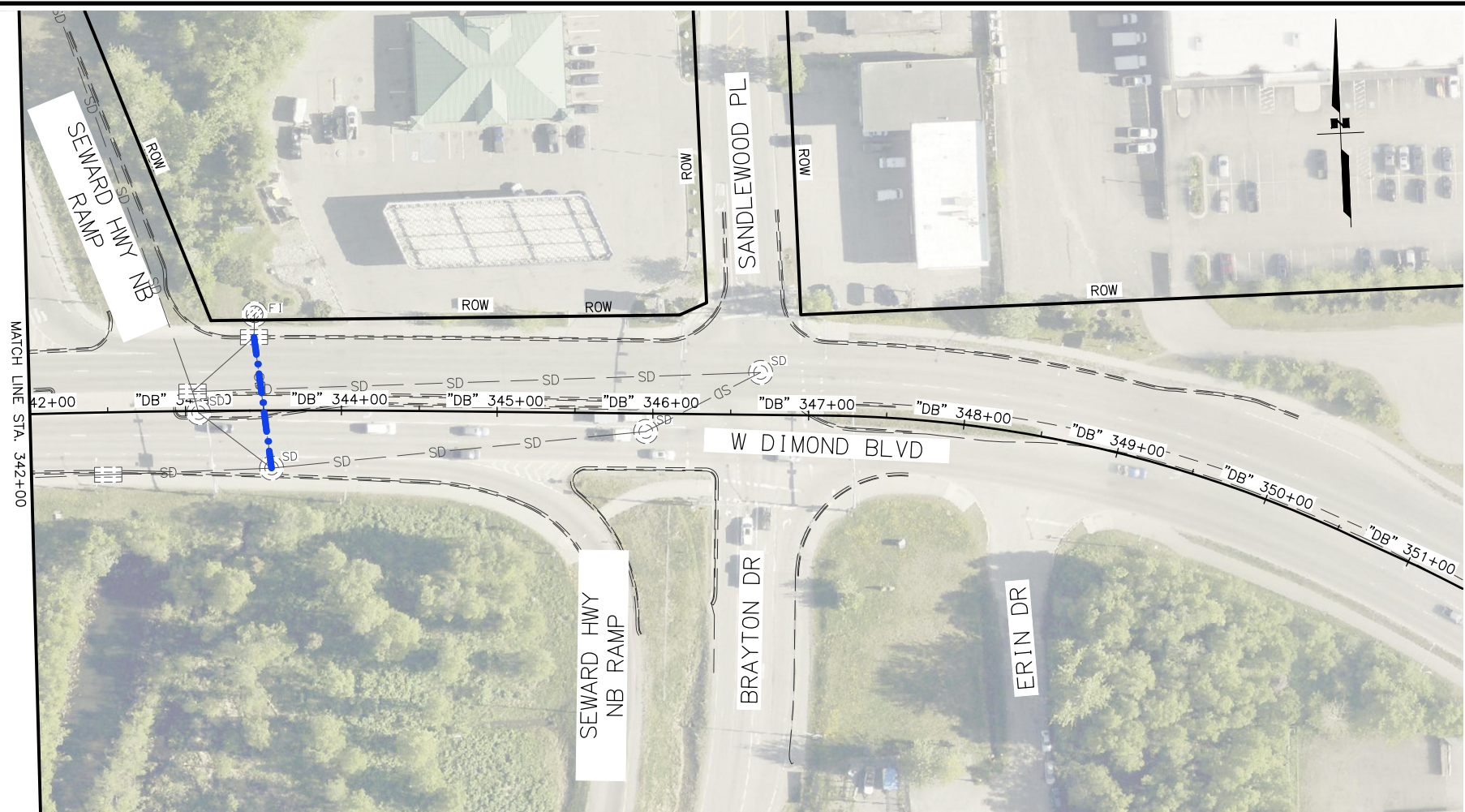
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**PIH/PS&E  
REVIEW**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
  
ANCHORAGE AREA  
DRAINAGE PRESERVATION  
  
**DIMOND BLVD  
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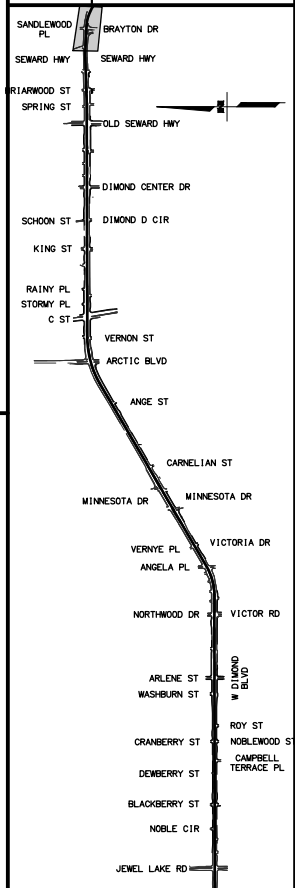
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SHEET NO.	TOTAL SHEETS
ESCP9	ESCP12
STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
 0001822/  
 CFHWY01389

NO.	REVISION



**PIH/PS&E  
 REVIEW**

STATE OF ALASKA  
 DEPARTMENT OF TRANSPORTATION  
 AND PUBLIC FACILITIES  
 ANCHORAGE AREA  
 DRAINAGE PRESERVATION  
 DIMOND BLVD  
 CIPP ESCP

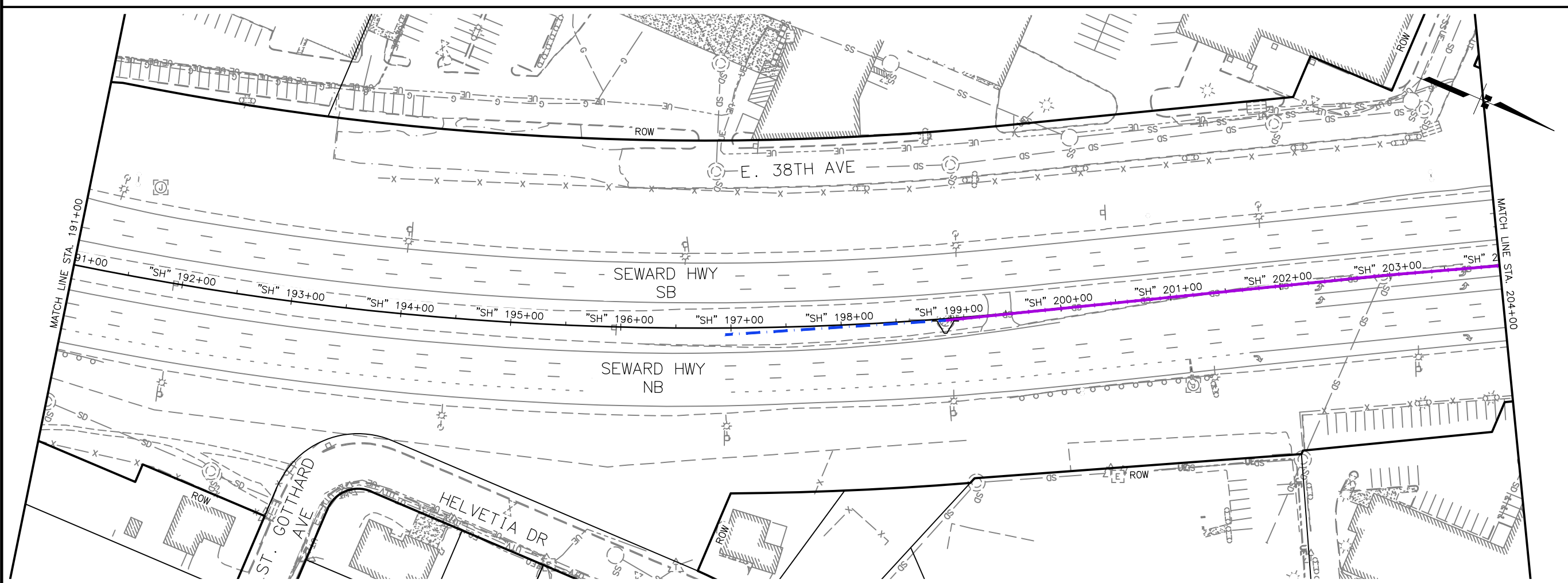
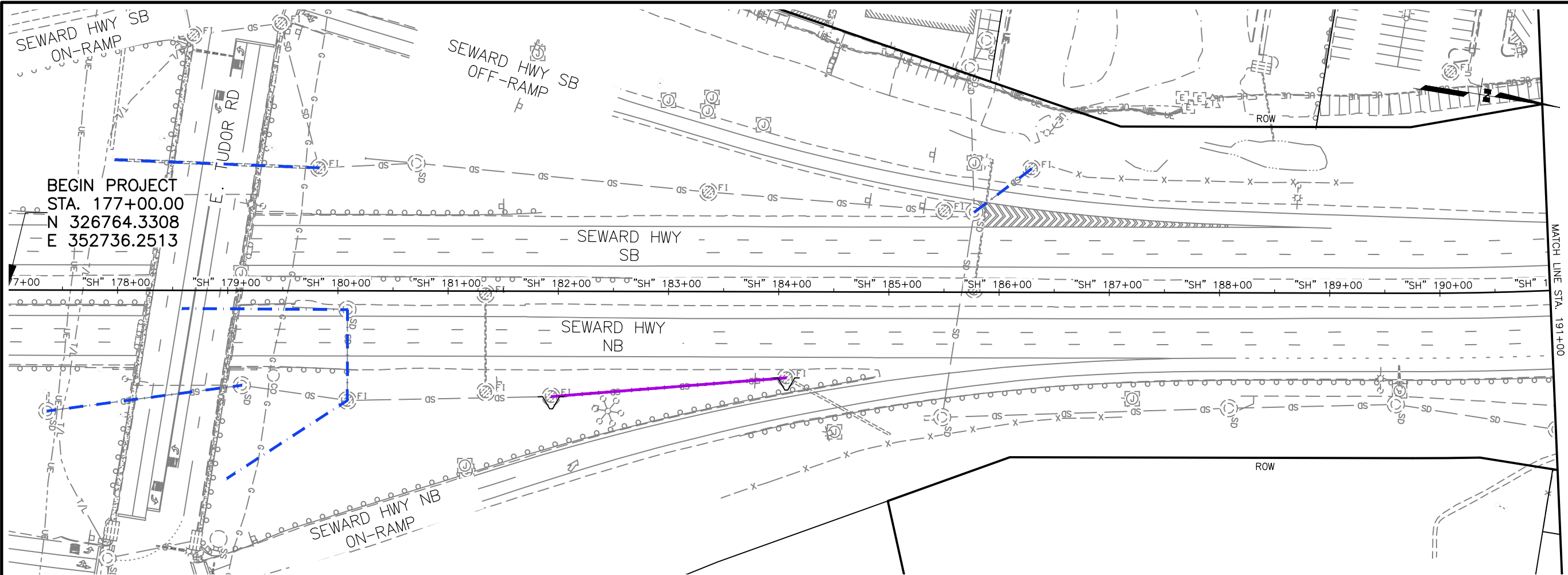
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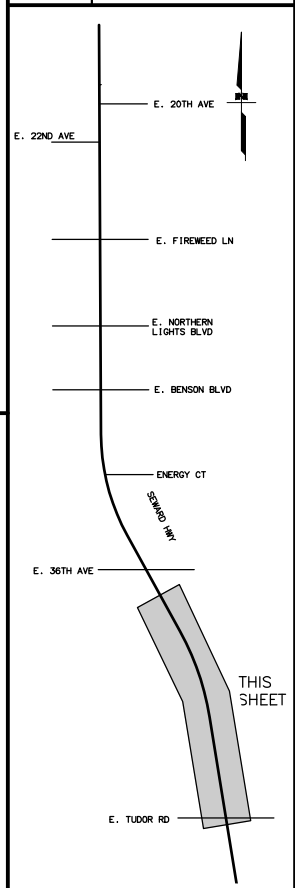
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STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
CFHWY01389**

NO.	REVISION



**PIH/PS&E  
REVIEW**

STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES

ANCHORAGE AREA  
DRAINAGE PRESERVATION

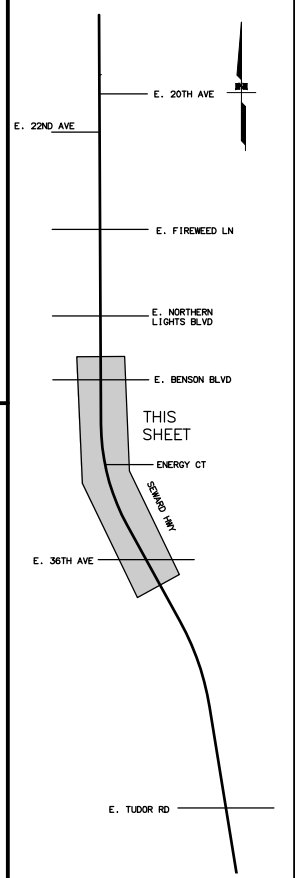
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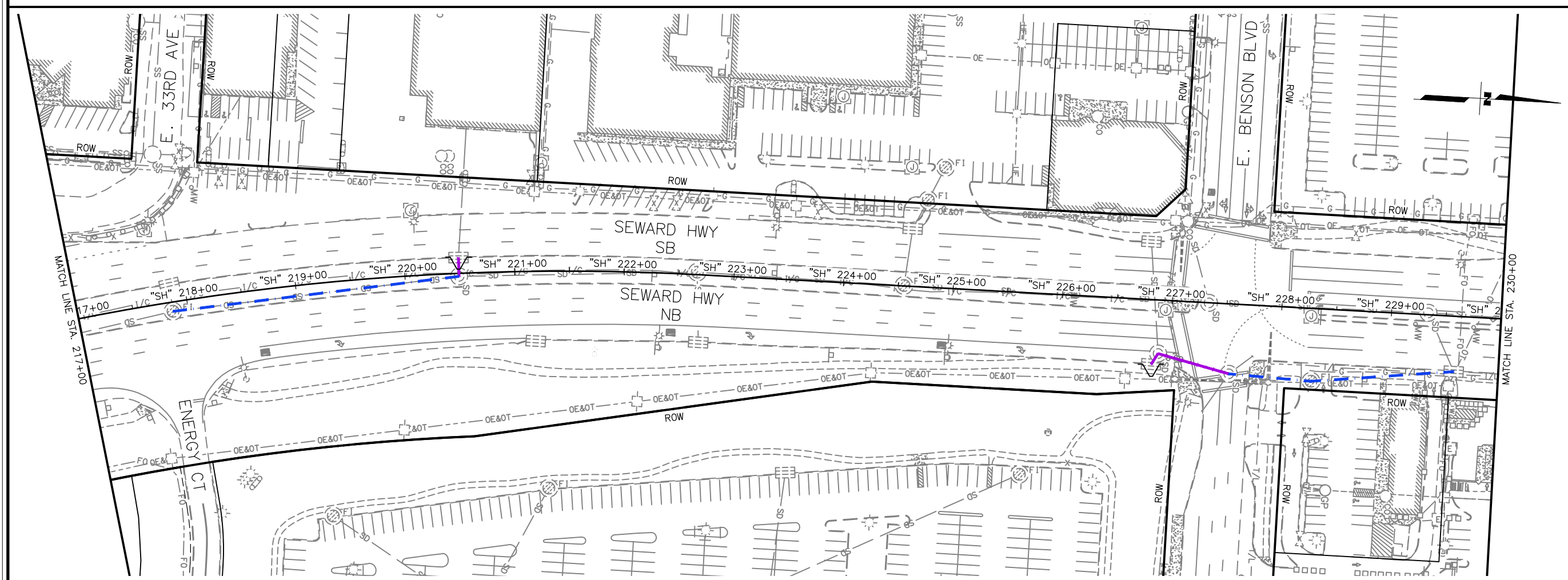
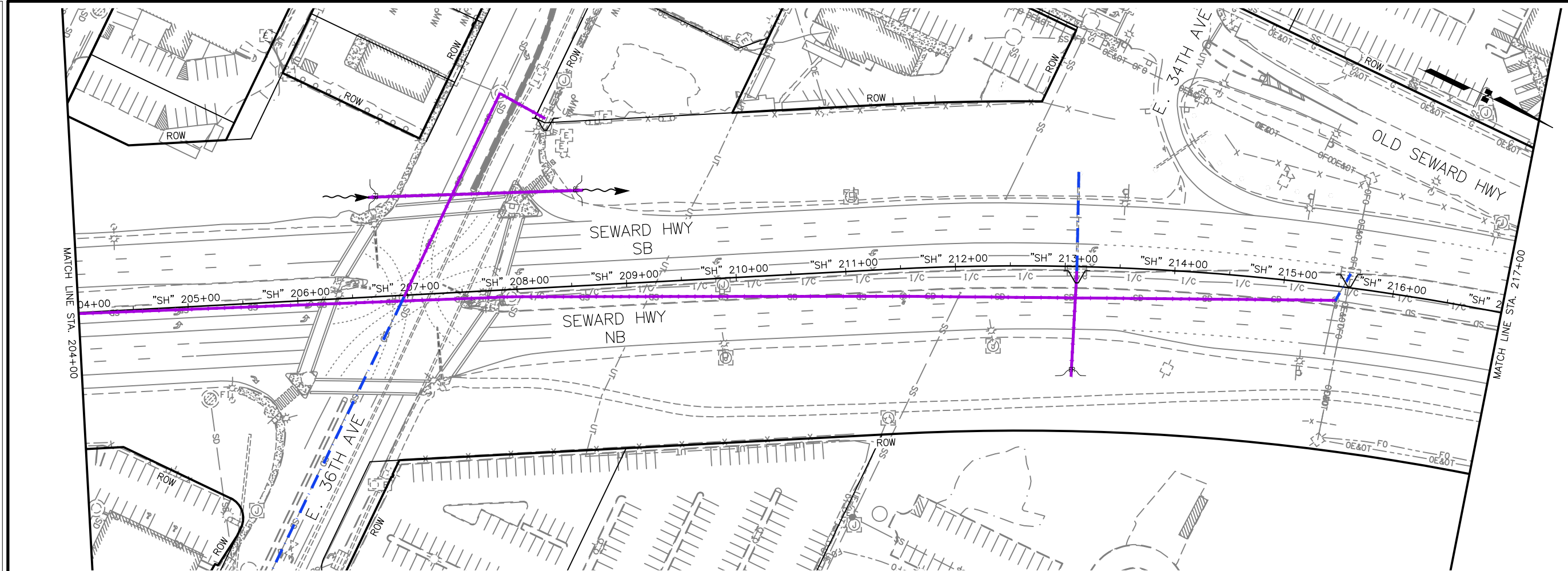
PROJECT DESIGNATION  
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 CFHWY01389**

NO.	REVISION



**PIH/PS&E  
 REVIEW**

STATE OF ALASKA  
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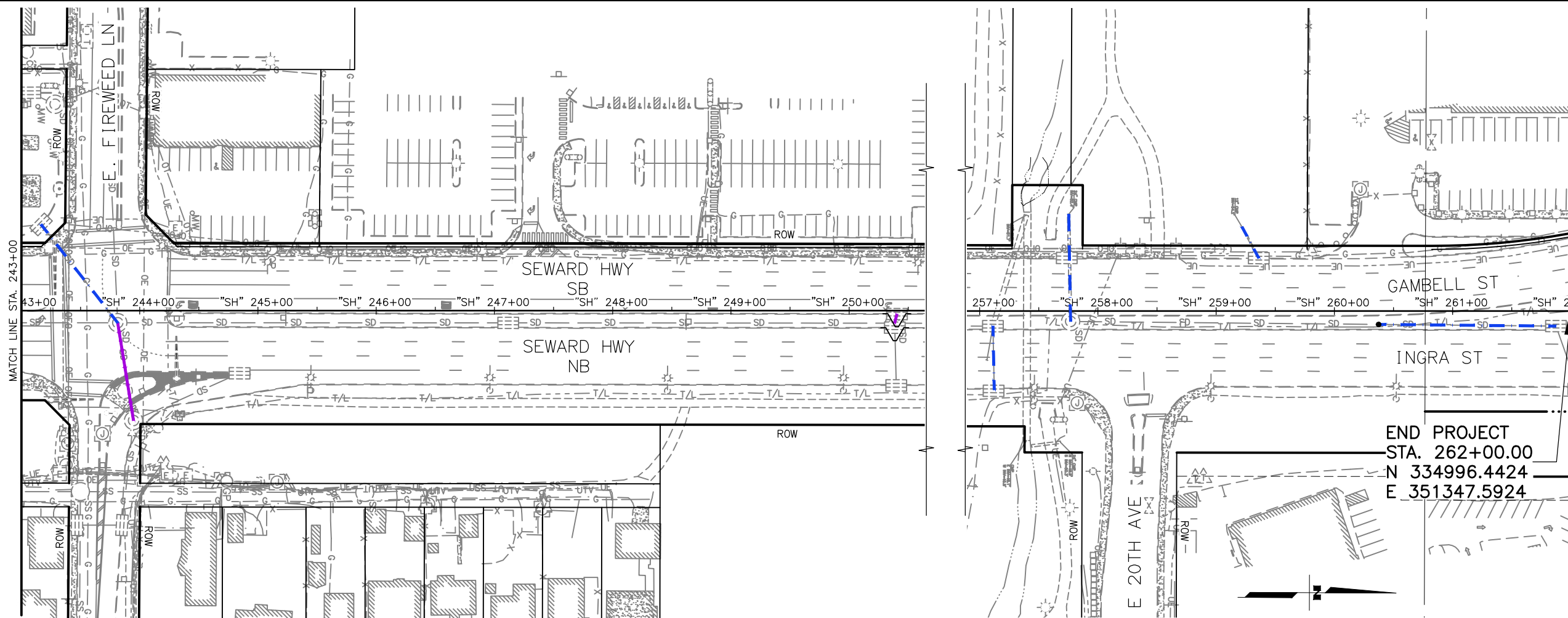
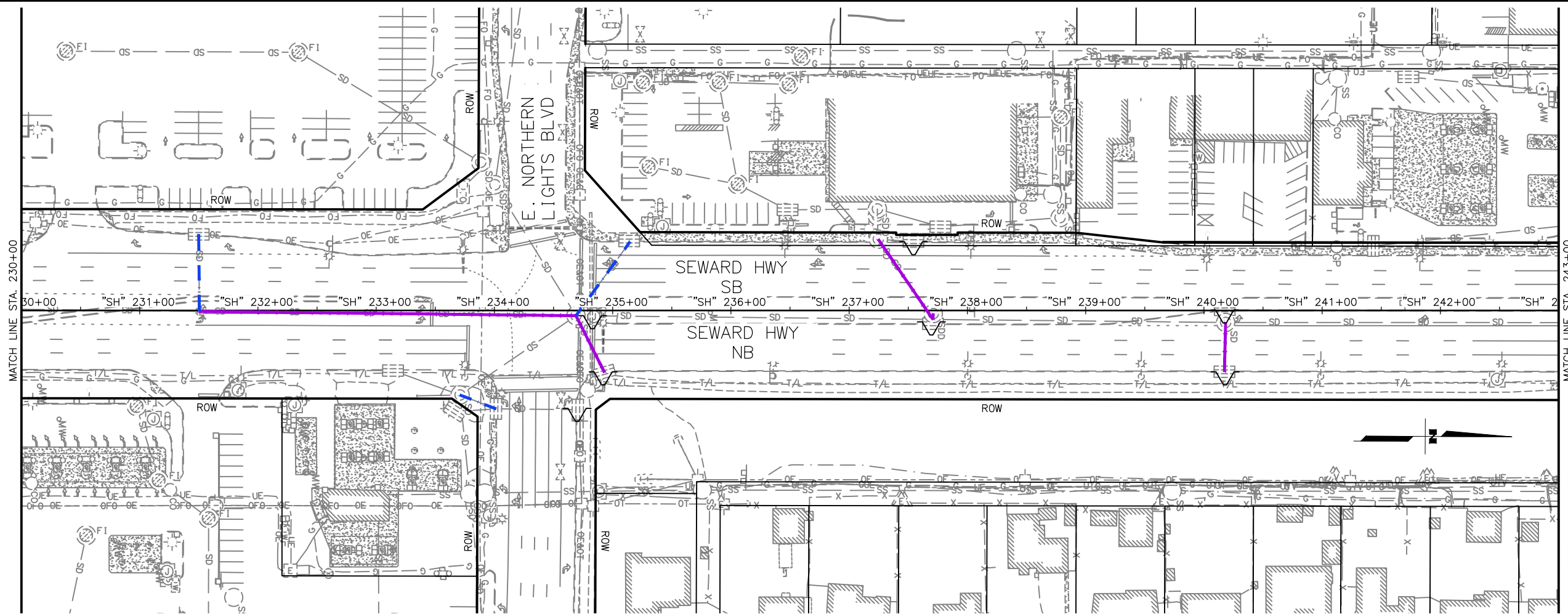


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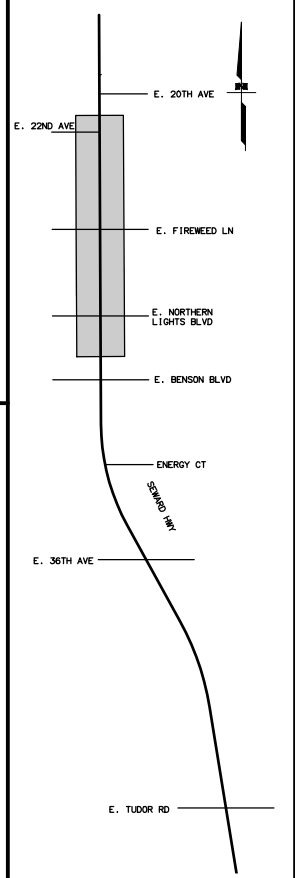
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STATE	YEAR
ALASKA	2026

PROJECT DESIGNATION  
**0001822/  
CFHWY01389**

NO.	REVISION



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REVIEW**

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STATE OF ALASKA  
DEPARTMENT OF TRANSPORTATION  
AND PUBLIC FACILITIES  
  
ANCHORAGE AREA  
DRAINAGE PRESERVATION  
  
SEWARD HWY  
CIPP ESCP

**APPENDIX B**  
**BMP DETAILS**



**APPENDIX D**  
**SUPPORTING DOCUMENTATION**





State of Alaska  
Department of Transportation & Public Facilities

**CATEGORICAL EXCLUSION DOCUMENTATION FORM**  
(NEPA Assignment Program Projects)

*The environmental review, consultation, and other actions required by the applicable Federal environmental laws for this project are being, or have been carried out by the DOT&PF pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated November 3, 2017, and executed by FHWA and DOT&PF.*

**I. Project Information**

**A. Project Name:** Anchorage Area Pavement Preservation Group A

**B. State Project Number:** CFHWY00851

**C. Federal Project Number:** 0001739

**D. Primary/Ancillary Project Connections:** N/A

**E. COA Determination:** 23 CFR 771.117(d)(13)

**F. Project Scope:**

**TIP or STIP:** STIP

**Need ID:** 18924

**Project Scope:**

Crack sealing, surface treatment drainage, signage, guardrail, illumination, and other refurbishments to prolong the life of road pavement and bridges and their safety related structures. Project includes NHS Lane Delineators, Destination & Distance Signing, Pavement Markings and Signalization, Abandoned Vehicle Program, Road Surfacing and Transfer, Road Surface Treatments, and improve curb ramps to meet ADA standards (in coordination with Need ID 30397). The scope does not include landscaping or other elements inconsistent with a pavement preservation focus.

**G. Project Purpose And Need:**

Extend the service life and reduce future maintenance costs for Dimond Boulevard (from MP 0.00-3.31), the Seward Highway (from MP123.57-125.09), Northern Lights Boulevard (from MP 3.69-6.68), L Street (from MP 0.07-0.75), and Minnesota Drive (from MP 3.06 - 3.17).

The project is needed because the facilities subject to this project are beginning to show signs of wear, and repairing or replacement of facility elements before significant deterioration occurs can prevent extensive and costly repairs in the future.

## H. Project Description:

The Alaska Department of Transportation and Public Facilities (DOT&PF) has assumed the responsibilities of the Federal Highway Administration under 23 U.S.C. 327 and is preparing to resurface multiple routes in Anchorage. Work may include:

- Paving
- Resurfacing
- Automated traffic recorders
- Dig-outs
- Drainage improvements (culverts, ditching, storm drain)
- Improvements to roadside hardware
- Intersection improvements
- Pedestrian and ADA improvements/facilities
- Bridge work
- Guardrail improvements
- Lighting, signal, and signage improvements
- Raised medians
- Striping
- Utility relocations
- Vegetation grubbing and clearing

## Attachments

### Environmental Consequences

#### Project Plans & Location Information

- Project Location and Vicinity Map.pdf CFHWY00851.pdf

#### Historic Properties and Cultural Impacts

- 00851 Fig 1 Revised.pdf CFHWY00851.pdf
- CFHWY00851 Anchorage Area Pavement Preservation Group A SL106\_9.22.2022\_update\_EDH\_Sig.pdf CFHWY00851.pdf
- CFHWY00851 Compiled Anchorage Area Pavement Preservation Group A SL106 forms.pdf CFHWY00851.pdf
- Fig 2 Minnesota.pdf CFHWY00851.pdf
- Plan View Compilation.pdf CFHWY00851.pdf

#### Section 4(f)/6(f) Impacts

- SEO Section 4(f) Determination RE\_ 4(f) No Use ....pdf CFHWY00851.pdf

#### Contaminated Sites and Hazardous Materials Impacts

- 17-013-01 BGES Groundwater Monitoring Report\_2210 West Dimond Blvd.pdf CFHWY00851.pdf

- 3rd response\_ ADEC Consultation re\_ CFHWY00851Anchorage Area Pavement Preservation Group A.pdf CFHWY00851.pdf
- Additional Response to\_ ADEC Consultation re\_ CFHWY00851Anchorage Area Pavement Preservation Group A.pdf CFHWY00851.pdf
- Drinking Water Program Response\_ ADEC Consultation re\_ CFHWY00851Anchorage Area Pavement Preservation Group A.pdf CFHWY00851.pdf
- FW\_ ADEC Consultation re\_ CFHWY00851Anchorage Area Pavement Preservation Group A.pdf CFHWY00851.pdf
- Response to\_ ADEC Consultation request CFHWY00851Anchorage A....pdf CFHWY00851.pdf

**Floodplain Impacts (23 CFR 650, Subpart A)**

- LHS CFHWY00851.pdf
- Public Involvement Documentation CFHWY00851.pdf

**Fish and Wildlife Impacts**

- EFH Effect Determination - Chester Creek at Seward Hwy 7.22.22.pdf CFHWY00851.pdf

**Water Quality Impacts**

- DEC Drinking Water - Coment Response for CFHWY00851.pdf CFHWY00851.pdf

**Comments and Coordination**

**Public Involvement**

- ADN anchorage area pavement preservation.pdf CFHWY00851.pdf
- OPN.pdf CFHWY00851.pdf

**Agency Involvement**

- Agency Scoping Documents.pdf CFHWY00851.pdf
- Scoping Contacts CFHWY00851.pdf CFHWY00851.pdf
- Agency Scoping Responses.pdf CFHWY00851.pdf

## II. Environmental Consequences

### A. Land Use and Transportation Plans

Yes No

1. Were land use plans for this area reviewed? If yes, include source, link, and date accessed.

Anchorage 2040 Land Use Plan

<https://www.muni.org/Departments/OCPD/Planning/Publications/Pages/Anchorage2040LandUsePlan.aspx>

3/9/2022

Anchorage 2020, Anchorage Bowl Comprehensive Plan

<https://www.muni.org/Departments/OCPD/Planning/Publications/Pages/Anchorage2020.aspx>

3/10/2022

a. Is the project consistent with land use plan(s)?

2. Were transportation plans for this area reviewed?

Anchorage 2020, Anchorage Bowl Comprehensive Plan

<https://www.muni.org/Departments/OCPD/Planning/Publications/Pages/Anchorage2020.aspx>

3/9/2022

Transit On the Move 2020 Transit Plan

[https://www.muni.org/departments/transit/peoplemover/documents/transit%20on%20the%20move/20200311\\_transit\\_on\\_the\\_move\\_final\\_plan.pdf](https://www.muni.org/departments/transit/peoplemover/documents/transit%20on%20the%20move/20200311_transit_on_the_move_final_plan.pdf)

3/9/2022

Alaska Highway Safety Plan, Federal Fiscal Year 2021

[https://www.nhtsa.gov/sites/nhtsa.gov/files/documents/ak\\_fy21\\_hsp.pdf](https://www.nhtsa.gov/sites/nhtsa.gov/files/documents/ak_fy21_hsp.pdf)

3/9/2022

a. Is the project consistent with transportation plan(s)?

3. Would the project induce adverse indirect and cumulative effects on land use or transportation?

### Summary

Summarize how the project is consistent or inconsistent with land use and transportation plan(s).

Upgrade and maintenance of existing public roads and their associated elements protects assets of a safe and efficient transportation system utilized for public and private transportation as well as emergency response. These concepts are consistent with the land and transportation plans reviewed. The Anchorage 2040 Land Use Plan (2040 LUP) supplements the Municipality's Comprehensive Plan for the Anchorage Bowl. The Anchorage Bowl Comprehensive Plan, adopted by the Municipal Assembly on February 20, 2001 and amended on September 10, 2002, is a blueprint for development in the Anchorage Bowl. The plan expresses, among many other ideas, the need to design and maintain roads, bus stops, sidewalks, bike lanes, and trails for year-round use as well as promote community connectivity with safe, convenient, year-round auto and non-auto travel routes within and between neighborhoods, and to neighborhood commercial centers and public facilities (found in Chapter 4, pages 64 and 65, Anchorage 2020 Planning Principles.)

Maintenance of existing appropriate road infrastructure is supported in the concepts articulated by the plans mentioned and is a basic requirement for the success of the Transit On the Move 2020 Transit Plan and the Alaska Highway Safety Plan, Federal Fiscal Year 2021.

**B. Right-of-Way Impacts**

**Yes No**

1. Are there any temporary right-of-way (ROW) impacts (e.g., Temporary Construction Easements (TCEs), Temporary Construction Permits (TCPs), utility relocates, construction staging area)?
2. Is additional permanent ROW required?

**Summary**

Summarize ROW impacts, if any. Include any project-specific commitments or mitigative measures in Section V.

TCEs and TCPs may be needed for curb ramp upgrades.

**C. Environmental Justice Impacts (E.O. 12898)**

**Yes No**

1. Is there potential to affect environmental justice (EJ) populations?

**Summary**

Summarize EJ population impacts and mitigation, if any. Include any project-specific commitments or mitigative measures in Section V.

Review of the Environmental Protection Agency (EPA) Environmental Justice Screening and Mapping Tool was made of an area encompassing all five project corridors. This review indicated populations closely matching those found nationally, with the mapping tool's People of Color indicator (indicators defined below) at five percent above the state and national averages. The Demographic Index fell one percentage point above the State average and nineteen percentage points below the National average. The Low Income demographic was a lower percentage than both the State and National percentages. The "Linguistically Isolated Population" demographic at four percent equaled the National percentage and came in double the State percentage. It is unlikely the proposed project would negatively impact any population mentioned here, as pavement preservation projects return function to existing roads and upgrade safety features. The proposed project would have temporary minor impacts such as detours or delays which would affect the local population during construction.

Definitions of indicators taken from the Environmental Protection Agency (EPA) Environmental Justice Screening and Mapping Tool :

<https://www.epa.gov/ejscreen/overview-socioeconomic-indicators-ejscreen>

People of color - The percent of individuals in a block group who list their racial status as a race other than white alone and/or list their ethnicity as Hispanic or Latino. That is, all people other than non-Hispanic white-alone individuals. The word "alone" in this case indicates that the person is of a single race, not multiracial.

Low-income - The percent of a block group's population in households where the household income is less than or equal to twice the federal "poverty level."

The Demographic Index is based on the average of two socioeconomic indicators; low-income and people of color.

**D. Historic Properties and Cultural Impacts**

**Yes No**

1. Is a National Register of Historic Places listed or eligible property in the proposed Area of Potential Effect (APE)?

**D. Historic Properties and Cultural Impacts**

**Yes No**

2. Was a programmatic allowance processed for the project under the Section 106 Programmatic Agreement?

**Attachments**

- 00851 Fig 1 Revised.pdf CFHWY00851.pdf
- CFHWY00851 Anchorage Area Pavement Preservation Group A SL106\_9.22.2022\_update\_EDH\_Sig.pdf CFHWY00851.pdf
- CFHWY00851 Compiled Anchorage Area Pavement Preservation Group A SL106 forms.pdf CFHWY00851.pdf
- Fig 2 Minnesota.pdf CFHWY00851.pdf
- Plan View Compilation.pdf CFHWY00851.pdf

3. Was Section 106 consultation initiated or a Direct to Findings worksheet completed?

4. Are there any unresolved issues with consulting parties, including project issues or concerns of a federally-recognized Indian Tribe [36 CFR 800.16(m)]?

**Summary**

Summarize impacts to historic properties and mitigation, if any. List affected sites (by AHRS number only) and any commitments or mitigative measures. Also include any project-specific commitments or mitigative measures in Section V.

All project activities are covered under Programmatic Allowances, thus a 106 PA Streamlined Project Review Screening Record was utilized to complete Section 106, with the entire project reviewed per Appendix D of the Programmatic Agreement. An update was uploaded on 9/28/22 with the addition of the Minnesota Drive portion of the project. With this addition, the project remained covered under Programmatic Allowances.

Listed or Eligible Properties (or potentially so) and reasoning for coverage under Programmatic Allowances : (Following text is from attached Streamlined Project Review Screening Records)

The APE includes the roadways depicted in the attached figures. Activities are for pavement preservation and are limited to repair, replacement, or refurbishment of existing facilities within the existing prism and adjacent ditches, e.g. "ditch to ditch."

The Seward Highway is exempt from consideration under the A CH P's Exemption Regarding Historic Preservation Review Process for Effects to the Interstate Highway System published in the Federal Register, Vol. 70, No. 46, Thursday, March 10, 2005. The two bridges on the Seward Highway scheduled for repair, #0391 and #1433, are exempt from consideration per the 2012 Program Comment for Common Post-1945 Concrete and Steel Bridges.

Examination of L Street, Northern Lights, and Diamond Boulevard show that these are well developed urban streets with existing curb cuts, raised medians, intersections with signals and striping, sidewalks and pedestrian facilities. Any changes imposed by this pavement preservation project would be minor, in kind, and in visual and historic context for these facilities.

**E. Section 4(f)/6(f) Impacts**

**Yes No**

1. Section 4(f) (23 CFR 774)

a. Was detailed Section 4(f) resource identification conducted for this project, other than that required for Section 106 compliance?

b. Does a Section 4(f) resource exist within or adjacent to the project area?

c. Does an exception listed in 23 CFR 774.13 apply to this project?

**E. Section 4(f)/6(f) Impacts**

**Yes No**

d. Does the project result in the “use” of a Section 4(f) property?

**2. Section 6(f) (36 CFR 59)**

a. Does a Section 6(f) Land and Water Conservation Fund Act (LWCFA) resource exist within or adjacent to the project area?

b. Will the action result in a conversion of a Section 6(f) property?

**Summary**

Summarize Section 4(f)/6(f) involvement, if any.

A review of the Anchorage Parks and Recreation Master Plans website and the Alaska Department of Fish and Game (ADF&G) website on November 2, 2021 indicated several recreational facilities adjacent to the project area.

- The Dimond Boulevard project corridor runs adjacent to Rovenna Park and Emerald Hills Park in the Section 6 (f) Campbell Creek Greenbelt Park System.
- The L Street project corridor crosses the DeLaney Park [Section 6(f)].
- The Seward Highway project corridor is adjacent to the Chester Creek Greenbelt and Eastchester/Woodside Park [Section 6(f)] as well as the Seward Highway Buffer Park North. To the South, the highway corridor is adjacent to the Helen Louise McDowell Sanctuary.
- The Northern Lights Boulevard project corridor is adjacent to Huntington Park and LaHonda Park on the West end of the corridor, as well as Seward highway Buffer Park North toward the East end of the corridor.
- No recreational facilities were identified within or adjacent to the Minnesota traffic recorder update project area.

The project is anticipated to require TCE and TCP, however, these would not occur on recreational properties. In addition, the project is not anticipated to result in a use (permanent incorporation, adverse temporary occupancy, or constructive use) of any Section 4(f) resource. No conversion of any Section 6(f) property or facility is anticipated. It is anticipated that project activities will avoid any use or incorporation of these resources. A determination of No Section 4(f) Use was reached in consultation with the State Environmental Office. The State NEPA Manager provided his concurrence with the Section 4(f) determination on March 22, 2022. The addition of the Minnesota Drive project location did not result in additional Section 4(f) consultation with the State NEPA Manager since there was no Section 4 (f) property within or adjacent to it.

**Attachments**

- SEO Section 4(f) Determination RE\_ 4(f) No Use ....pdf CFHWY00851.pdf

**F. Contaminated Sites and Hazardous Materials Impacts**

**Yes No**

1. Include source, link, and date accessed of databases used.

Alaska Department of Environmental Conservation (ADEC) Contaminated Sites

<https://www.arcgis.com/apps/mapviewer/index.html?webmap=315240bfbaf84aa0b8272ad1cef3cad3>

November 1, 2021

2. Are there known or potentially contaminated sites within or adjacent to the existing ROW?

3. Would a documented hazardous material site be acquired?

**F. Contaminated Sites and Hazardous Materials Impacts**

Yes No

4. Are there contaminated sites within 1,500 feet of where excavation dewatering is anticipated?

**Summary**

Summarize the contaminated site impacts and mitigation, if any.

A review of the ADEC Contaminated Sites Database indicated multiple contaminated sites within, adjacent to, or within 1,500 feet of all but one project corridor.

The L Street project corridor has two active sites within 1,500 feet. These are Hazard Identification (HID) 23583 and HID 23921 (liquid underground storage tank (LUST) sites), one Cleanup Complete - Institutional Controls (CCIC) site (HID 3316) and 17 Cleanup Complete (CC) sites are also within 1,500 feet of the project corridor.

The Northern Lights Boulevard project corridor has eight active sites within 1,500 feet. Two Active sites are within or adjacent to the project corridor (HID 23313 and 23370 - both LUST sites). There are five CCIC sites within or adjacent to the project corridor (HID 23580, 22873, 23362, 24727, and 3165, all but the last being LUST sites). There are many CC sites within or adjacent to the project corridor as well as within 1,500 feet of the corridor.

The Seward Highway project corridor has five Active sites within 1,500 feet. Three are adjacent to the corridor (HID 23820, 23595, and 23371 are all LUST sites) and two are somewhat removed from the corridor (HID 26632 and 4273, neither of which is a LUST site). Four CCIC sites are found within or adjacent to the project corridor (HID 23707, 3165, 23362, 24727, of which all but 3165 are LUST sites). Additionally, there are 18 CC sites within 1,500 feet of the project corridor.

The Dimond Boulevard project corridor has six Active sites within 1,500 feet (HID 24583, 23025, 27228, 26815, 24028 and 24054, all but the last being LUST sites). Six CCIC sites are found within or adjacent to the project corridor (HID 23844, 23861, 24788, and 23847 are LUST sites, and 25895 and 4001 are not). Additionally, there are 20 CC sites within, adjacent to, or within 1,500 feet of the project corridor.

The Minnesota Drive project corridor has no identified hazardous waste sites within 1,500 feet.

Dewatering is likely in both the Seward Highway and Dimond Boulevard project corridors. As such, consultation was made with ADEC to identify specific concerns related to this likelihood. ADEC response indicated the application of the standard BMPS required for an excavation dewatering permit would suffice except in the case of Texaco/ Hanna Car Care Center Hazard ID 23025 where a Soil and Groundwater Management Work Plan must be submitted to the Contaminated Sites Program for review and approval in advance of the work. If previously unknown contamination in soil or groundwater is discovered anywhere in the project area during construction it will be reported to ADEC in accordance with 18 AAC 75.300.

**Attachments**

- 17-013-01 BGES Groundwater Monitoring Report\_2210 West Dimond Blvd.pdf CFHWY00851.pdf
- 3rd response\_ ADEC Consultation re\_ CFHWY00851Anchorage Area Pavement Preservation Group A.pdf CFHWY00851.pdf
- Additional Response to\_ ADEC Consultation re\_ CFHWY00851Anchorage Area Pavement Preservation Group A.pdf CFHWY00851.pdf
- Drinking Water Program Response\_ ADEC Consultation re\_ CFHWY00851Anchorage Area Pavement Preservation Group A.pdf CFHWY00851.pdf
- FW\_ ADEC Consultation re\_ CFHWY00851Anchorage Area Pavement Preservation Group A.pdf CFHWY00851.pdf
- Response to\_ ADEC Consultation request CFHWY00851Anchorage A....pdf CFHWY00851.pdf

**G. Floodplain Impacts (23 CFR 650, Subpart A)**

**Yes No**

1. Does the project encroach into a mapped base floodplain or a potential unmapped base floodplain?

**Attachments**

- LHS CFHWY00851.pdf
- Public Involvement Documentation CFHWY00851.pdf

a. Does the project encroach into a regulatory floodway?

b. Would the proposed action increase the base flood elevation (BFE) one-foot or greater, or any rise in a regulatory floodway?

c. Is there a longitudinal encroachment into the 100-year floodplain?

d. Is there significant encroachment as defined by 23 CFR 650.105(q)?

2. Does the project conform to local flood hazard requirements?

3. Is the project consistent with E.O. 11988 (Floodplain Protection)?

**Summary**

Summarize floodplain impacts and describe any temporary encroachment(s) and functionally dependent use(s).

A review of the Federal Emergency Management Agency Flood Insurance Rate Map panels 0200050733D, 0200050734D, 0200050739D, 0200050743D, 0200050744D, 0200050753D, 0200050754D, 0200050761D, and 0200050763D (effective date for each is 9/25/2009) indicated the following:

- The L Street proposed project corridor lies within Zone X, outside of a base floodplain (100-year floodplain).
- The Northern Lights Boulevard proposed project corridor has a small portion within Zone AE, an area within a floodplain with base flood elevations determined.
- The Seward Highway proposed project corridor includes a Zone AE floodway at the northern end of the corridor. Near the southern end the proposed corridor encroaches on Zone AE within a floodplain. It is anticipated that a portion of the culvert in the floodway at Chester Creek would require repair or replacement. Any work performed within a floodway would require a Municipality of Anchorage (MOA) Flood Hazard Permit. Work would be performed in compliance with permitting.
- The Dimond Boulevard proposed project corridor encroaches on Zone AE longitudinally. No work is anticipated to be performed in the Campbell Creek floodway.
- The Minnesota Drive project corridor is completely within Zone X, outside the 0.2 percent annual chance floodplain.

Project actions that may encroach on a base floodplain could include vegetation clearing and drainage improvements including culvert and bridge work. Base floodplain encroachments are anticipated to be minor, with no net changes to base flood elevations, therefore they will not result in significant encroachments as defined in 23 CFR 650.105(q) and USDOT Order 5650.2(4)(p). Similarly, work within the Chester Creek Floodway is anticipated to return the floodway to its as-built conditions, thus no rise in the base flood elevation is anticipated. It is anticipated that a Municipality of Anchorage Flood Hazard Permit will be required.

**H. Wetland and Waterbody Impacts**

**Yes No**

1. Would the project affect wetlands or other Waters of the U.S. (WOTUS), as defined by the U.S. Army Corps of Engineers (USACE) (Section 404).

**H. Wetland and Waterbody Impacts**

**Yes**      **No**

**2. Wetlands?**

**a.** Are the wetlands delineated in accordance with the “Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Alaska Region (Version 2.0) Sept. 2007”?

**b.** Estimated area of wetland involvement (acres): 0.95

**c.** Estimated fill quantity: 0 cubic yards

**d.** Estimated dredge quantities: 150 cubic yards

**e. Wetlands Finding**

**i.** Are there practicable alternatives to the proposed construction in wetlands?

**ii.** Does the project include all practicable measures to minimize harm to wetlands?

**iii.** Only practicable alternative: Based on the evaluation of avoidance and minimization alternatives, there are no practicable alternatives that would avoid the project’s impacts on wetlands. The project includes all practicable measures to minimize harm to the affected wetlands as a result of construction.

**3. Waters?**

**a.** Estimated fill quantities below:

OHW: 200 cubic yards

MHW: 0 cubic yards

HTL: 0 cubic yards

**b.** Estimated dredge quantities: 200 cubic yards

**4.** Does the project involve work within or over navigable waters as defined by the USACE (Section 10)?

**5.** Proposed waterbody involvement:

- Culvert

**6.** Is a USACE authorization anticipated?

- Nationwide Permit

**7.** Will the project involve navigable waters as defined by the U.S. Coast Guard (USCG) (Section 9)?

**8.** Will the project affect a designated Wild and Scenic River or land adjacent to a Wild and Scenic River, including those on the Nationwide Rivers Inventory?

**Summary**

Summarize wetland and waterbody impacts and mitigation, if any.

A review of the U.S. Fish and Wildlife Service (USFWS) National Wetland Inventory identified USACE jurisdictional wetlands within and/or adjacent to the five project corridors. Drainage improvements may cause minor impacts to these waterbodies and/or wetlands, although the project will be designed to minimize impacts to the extent practicable. A culvert in Chester Creek at Seward Highway will be partially replaced. Other activities anticipated to occur include ditch maintenance and brush clearing as needed. Any excavated material not utilized in the construction process would be disposed of in a permitted upland area. Such work would be anticipated to meet the terms and conditions of a

USACE Section 404 Nationwide Permit (NWP) and an ADEC Section 401 Clean Water Act Certification. It is anticipated the project will fall under a NWP 3 for maintenance and potential addition of riprap to stabilize the inlet of the aforementioned Chester Creek culvert.

**I. Fish and Wildlife Impacts**

**Yes No**

**1. Anadromous and resident fish habitat.**

**a.** Include source, link, and date accessed of databases used.

ADF&G Anadromous Waters Catalog Interactive Mapper

<https://www.adfg.alaska.gov/sf/SARR/AWC/index.cfm?ADFG=main.interactive>

Accessed November 1, 2022

- b.** Is anadromous or resident fish habitat present in project area (Title 16.05.841 and 16.05.871)?
- c.** Are there adverse effects on spawning habitat?
- d.** Are there adverse effects on rearing habitat?
- e.** Are there adverse effects on migration corridors?
- f.** Are there adverse effects on subsistence species?
- g.** Are there temporary impacts to fish habitat?

**2. Essential Fish Habitat (EFH).**

**a.** Include source, link, and date accessed of databases used.

ADF&G Anadromous Waters Catalog Interactive Mapper

<https://www.adfg.alaska.gov/sf/SARR/AWC/index.cfm?ADFG=main.interactive>

Accessed November 1, 2022

- b.** Is EFH present in project area?
- c.** Does the project propose construction in EFH?
- d.** May the project adversely affect EFH?
- e.** Does the project include conservation recommendations proposed by NMFS.

**3. Threatened and Endangered (T&E) Species**

**a.** Include source, link, and date accessed of databases used.

On November 1, 2012, the USFWS issued a letter stating that there are no federally listed or proposed species, or designated or proposed critical habitat under USFWS jurisdiction in the Matanuska-Susitna or Anchorage areas. No impact to threatened or endangered species or critical habitat areas is expected to occur as a result of the proposed project. No adverse effect to EFH is anticipated per the attached Essential Fish Habitat Determination.

**I. Fish and Wildlife Impacts**

**Yes**      **No**

**b.** Are listed threatened or endangered species present in the project area?

**4. Marine Mammals.**

**a.** Is the project located in the marine environment?

**5. Wildlife Resources:**

**a.** Is the project in an area of high wildlife/vehicle accidents?

**b.** Would the project bisect migration corridors?

**c.** Would the project segment habitat?

**6. Bald and Golden Eagle Protection Act.**

**a.** Include source, link, and date accessed of databases used.

USFWS Information for Planning and Consultation (IPAC) system

<https://ipac.ecosphere.fws.gov/>

Accessed November 1, 2022

Southeast Alaska GIS Library interactive map

<https://data-seakgis.opendata.arcgis.com/datasets/seakgis::documented-eagle-nest-sites/>

Accessed May 26, 2022

**b.** Is the project visible from an eagle nesting tree?

**c.** Is the project within 330 feet of an eagle nesting tree?

**d.** Is the project within 660 feet of an eagle nesting tree?

**e.** Will the project require blasting or other activities that produce extreme loud noises within 1/2 a mile from an active nest?

**f.** Is an eagle permit required?

**7.** Is the project consistent with the Migratory Bird Treaty Act?

## Summary

Summarize fish and wildlife impacts and mitigation, if any.

### Fish

A review of the ADF&G Anadromous Waters Catalog identified several anadromous streams within the project corridor. The West end of the proposed Northern Lights Boulevard project corridor crosses over the anadromous Fish Creek (AWC Code: 247-50-10046) which contains coho salmon. The North end of the proposed Seward Highway project corridor crosses over anadromous Chester Creek (AWC Code: 247-50-10050) which contains coho, pink, and sockeye salmon as well as dolly varden. The proposed Dimond Boulevard project corridor crosses and parallels anadromous Campbell Creek (AWC Code: 247-60-10340) which contains chinook, coho, pink, and sockeye salmon as well as dolly varden. These water bodies are identified as Essential Fish Habitat (EFH), habitat necessary to fish for spawning, breeding, feeding, or growth to maturity as defined by the National Oceanic and Atmospheric Administration (NOAA). The scope of work for a pavement preservation project may include bridge work and drainage improvements including culvert repair or replacement (either in-stream or drainage) if existing structures are damaged or failing. All work in an anadromous water body or a water body containing resident fish will include impact minimization through application of ADF&G guidelines and compliance with all requirements of a State of Alaska (SOA) Title 16 Fish Habitat Permit.

### Wildlife

Review of the ADF&G Moose-Vehicle Collisions (MVC) in Alaska Anchorage webpage indicated that from 2013-2016 the several project corridors had incidences of MVC. The vicinity of the W. Dimond Boulevard, Minnesota Drive, and E. Northern Lights project corridors had the highest incidence, each rating a moderate level of MVC in the project corridor or a portion thereof. The remainder of the proposed project corridors demonstrated low to moderately low potential for MVC. No other wildlife species found within the project area is likely to cause a similar level of common driving hazard, though other wild or domestic animals may be found in the area. The proposed project is not anticipated to segment or disrupt wildlife habitat or migration corridors as no new road segments are to be built. Some wildlife may avoid the project area during construction activities, but the proposed project is not likely to cause permanent or adverse impacts to wildlife.

### Migratory Birds and Eagle Nests

Review of the USFWS IPAC system indicated that several bird species of conservation concern migrate through and may nest within the proposed project area. These species could be disturbed by clearing operations. Vegetation clearing would be avoided from May 1 through July 15, as recommended by the USFWS guidelines. Bald eagles are identified by IPAC as found within the project area. Additionally, the Southeast Alaska GIS Library identifies an eagle nest almost 600 feet northeast of the east end of the Northern Lights project segment. Prior to construction, DOT&PF may conduct a survey of the project area to determine if active eagle nests occur within the primary (330 foot) or secondary (660 foot) zones. If active eagle nests are identified within 660 feet of the project area prior to or during construction, DOT&PF will seek guidance from the USFWS on how to proceed.

### Attachments

- EFH Effect Determination - Chester Creek at Seward Hwy 7.22.22.pdf CFHWY00851.pdf

### J. Invasive Species Impacts

Yes No

1. Include source, link, and date accessed of databases used.

University of Alaska, Anchorage Exotic Plants Information Clearinghouse database and mapping application

<https://aknhp.uaa.alaska.edu/apps/akepic/#map?lg=f37ef462-d080-11e3-a36b-00219bfe5678&z=4&ll=60.00000%2C-140.00000>

November 1, 2021

**J. Invasive Species Impacts**

Yes No

- 2. Are invasive species present in project area?
- 3. Does the project include all practicable measures to minimize the introduction or spread of invasive species, making the project consistent with E.O. 13112 (Invasive Species)?

**Summary**

Summarize invasive species impacts and mitigation, if any.

Review of the University of Alaska, Anchorage Exotic Plants Information Clearinghouse Invasive Plants Mapper identified non-native plants within and adjacent to the project area. Ground cover disturbing activities would be minimized, and disturbed areas re-vegetated with native soil and seed to minimize potential introduction of invasive species, in accordance with Executive Order 13112.

**K. Water Quality Impacts**

Yes No

- 1. Will there be temporary degradation of water quality?
- 2. Is a public or private drinking water source or protection area within or adjacent to the project?

**Attachments**

- DEC Drinking Water - Coment Response for CFHWY00851.pdf CFHWY00851.pdf

- 3. Would the project result in a discharge of storm water to a WOTUS? [40 CFR 230.3(o)]
- 4. Would the project discharge storm water into or affect an ADEC-designated Impaired Waterbody?

a. List name(s), location(s), and pollutant(s) causing impairment:

Campbell Creek, Campbell Lake, Chester Creek and Fish Creek are all identified as impaired waterbodies based upon levels of fecal coliform bacteria. No established Total Maximum Daily Loads are listed for any of these water bodies. Fecal coliform bacteria is not a pollutant discharged by pavement preservation projects. Although the proposed project may discharge into the above-listed waterbodies, the impacts would be limited to temporary increases of turbidity. Construction activities associated with the proposed project would not result in an increase in fecal coliform bacteria.

- 5. Will the project involve more than one (1) acre of ground-disturbing activities?
- 6. Is there a Municipal Separate Storm Sewer System (MS4) APDES permit, or will runoff be mixed with discharges from an APDES permitted industrial facility?

a. List APDES permit number and type:

APDES Permit No. AKS-052558

MS4 Permit

**Summary**

Summarize the water quality impacts and mitigation, if any.

A review of the 2018 ADEC Impaired Waters mapper identified Campbell Creek, Chester Creek, and Fish Creek as impaired waterbodies within or adjacent to the proposed project area. During construction, ground disturbing activities and storm water runoff may result in temporarily increased turbidity of nearby streams, wetlands, and other water bodies. Adverse impacts to water quality would be minimized by implementing the CGP including a SWPPP and

utilizing best management practices during construction. Additionally, DEC's *Recommendations for General Project Activities near a PWS* (public water source) will be followed as it applies to three of the five project areas, the exceptions being the L Street project area and the Minnesota Drive project area south of Raspberry Road.

<b>L. <u>Air Quality Impacts</u></b>	<b>Yes</b>	<b>No</b>
1. Will there be temporary degradation of air quality?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Is the project located in an air quality maintenance area or nonattainment area (CO or PM-10 or PM-2.5)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
• CO		
3. Is the project exempt from an air quality analysis per 40 CFR 93.126 (Table 2 and Exempt Projects)?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
4. Is the project included in a conforming Metropolitan Transportation Plan (MTP) and Transportation Improvement Program (TIP)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. Was a project-level analysis (identify below) completed meeting the requirements of Section 93.123 of the conformity rule? The results satisfy the requirements of Section 93.116.	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Summary**

Summarize air quality impacts and mitigation, if any.

This project is exempt from air quality analysis per 40 CFR 93.126. Review of the EPA AirData Air Quality Monitors web page utilizing non-attainment layers for criteria pollutants indicated that the project is located within an air quality maintenance area for carbon monoxide (1971 Standard). In accordance with 40 CFR 93.126 (Table 2), as a "pavement resurfacing and/or rehabilitation" (pavement preservation) project it is exempt from the requirement to determine air quality conformity. Air quality impacts from project construction are anticipated to be minimal and temporary. No long-term impacts to air quality are anticipated as a result of the project.

<b>M. <u>Noise Impacts (23 CFR 772)</u></b>	<b>Yes</b>	<b>No</b>
1. Will there be temporary noise impacts?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Does the project involve any of the following Type I project actions listed below (23 CFR 772.5)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Summary**

Summarize noise impacts and mitigation, if any.

The project does not meet the definition of a Type 1 project as defined in 23 CFR 772.5, thus no noise analysis is needed. No noise impact is anticipated beyond the timeframe required for construction.

The noise generated by construction equipment will vary greatly depending upon the equipment type and model, mode and duration of operation, and specific type of work effort; however, typical noise levels may occur in the 75 decibel (dBA) to 95 dBA range at 50 feet. Variations in building setbacks and land use activity zones, local intensity of specific construction activities, and special and temporal distribution of activities will result in varying degrees of exposure to construction noise and hence varying levels of resulting impacts. Adverse effects related to construction noise are anticipated to be localized, temporary, and transient in nature.

<b>N. <u>Social and Economic Impacts</u></b>	<b>Yes</b>	<b>No</b>
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**N. Social and Economic Impacts**

**Yes      No**

- |  |                                     |                                     |
|--|-------------------------------------|-------------------------------------|
| 1. Would the project affect neighborhoods or community cohesion?   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 2. Would the project affect school boundaries, recreation areas, churches, businesses, police and fire protection, etc.?   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 3. Would the project affect the elderly, handicapped, non-drivers, transit-dependent, minority and ethnic groups, or the economically disadvantaged?   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 4. Would the project affect travel patterns and accessibility (e.g., vehicular, commuter, bicycle, or pedestrian)?   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| a. Would the project include temporary delays and detours of traffic?  | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| 5. The project will have adverse economic impacts on the regional and/or local economy, such as effects on development, tax revenues and public expenditures, employment opportunities, accessibility, and retail sales. | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| 6. The project will adversely affect established businesses or business districts.   | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| a. Would the project have temporary impacts on businesses?   | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |

**Summary**

Summarize social and economic impacts and mitigation, if any.

No adverse social or economic effects are anticipated from this project. No long term impact to neighborhoods or communities is anticipated. Disadvantaged groups such as economically disadvantaged, handicapped, minority, etc. are not anticipated to be negatively impacted based upon the minimal nature and of a pavement preservation project and the restoration of aging but necessary facilities. There will be short term impacts to the traveling public, which will be mitigated through application of standard best management practices such as timing of work and appropriate flagging and delay avoidance and minimization. The project is anticipated to positively impact all facility users by providing safer, smoother, updated travel surface.

**III. Comments and Coordination**

**A. Public Involvement** **Yes** **No**

- 1. Was public involvement for project completed?
- 2. Was the project public noticed?  
  - a. Newspaper name and date of notice:

Anchorage Daily News

January 30, 2022

**Attachments**

- ADN anchorage area pavement preservation.pdf CFHWY00851.pdf

b. Alaska Online Public Notice date:

January 28, 2022 (Published) - March 4, 2022 (Archive Date)

**Attachments**

- OPN.pdf CFHWY00851.pdf

- c. Were public notices completed for specific resource impacts (e.g., floodplain, Section 4(f))?
- 3. Was a public meeting held?
- 4. Is there any unresolved controversy on human, natural, or economic grounds?

**Summary**

Summarize public comments and coordination efforts for this project. Discuss pertinent issues raised.

Public floodplain notice is included in the attached NOI. No use of Section 4(f) resources is anticipated, thus no public notice was made for this category of resource. No public comments were received. No controversy is evident for this project.

**B. Agency Involvement** **Yes** **No**

- 1. Was an agency scoping conducted?

Agency scoping sent January 26, 2022 and is attached.

**Attachments**

- Agency Scoping Documents.pdf CFHWY00851.pdf
- Scoping Contacts CFHWY00851.pdf CFHWY00851.pdf

- 2. Was an agency scoping meeting held?
- 3. Was a field review completed with agencies?

## Summary

Summarize agency coordination efforts for this project.

Scoping responses were received from ADEC (various agencies), ADF&G, ADNR, and National Marine Fisheries Service (NMFS). See below for scoping documents sent and responses.

Air Quality - ADEC determined the project exempt per 40 CFR 93.126 and requested precautions related to open burning and to other construction activities to prevent particulate matter emission into ambient air.

Water Quality- ADEC provided locations and guidance to protect public water systems.

Hazardous Waste- ADEC provided guidance related to when consultation is required related to hazardous waste. See Section III G. attachments for consultation correspondence.

ADF&G provided information regarding when a fish habitat permit is required and requested (if within budget and scope) replacement of the Chester Creek culvert under Seward Highway with a culvert designed to fish passage standards.

DNR SHPO relayed the project appears to qualify to be streamlined and remains available for further consultation.

NMFS relayed requirements for consultation and permitting related to essential fish habitat.

## Attachments

- Agency Scoping Responses.pdf CFHWY00851.pdf

## **IV. Permits and Authorizations**

**A. Permits and Authorizations**

**Yes No**

- 1. USACE, Section 404/10 Includes Abbreviated Permit Process, Nationwide Permit, and General Permit
- 2. Coast Guard, Section 9
- 3. ADF&G Fish Habitat Permit (Title 16.05.871 and Title 16.05.841)
- 4. Flood Hazard
- 5. ADEC Non-domestic Wastewater Plan Approval
- 6. Requires 401 Cert
- 7. ADEC APDES
- 8. Eagle Permit
- 9. Incidental Take Authorization
- 10. Local (Borough or City) permit (e.g., noise)

Municipality of Anchorage (MOA) Flood Hazard Permit (FHP)

- 10. Other Permits

**Summary**

The permits anticipated to be needed are noted in V. 1-10 above.

**V. Environmental Commitments**

**A. Environmental Commitments and Mitigation Measures [23 CFR 771.109(b)]**

**Yes No**

- 1. Are there project-specific environmental commitments for this project?

**Summary**

DOT&PF and their Contractor(s) shall:

This project will follow DEC's *Recommendations for General Project Activities near a PWS* where applicable to public water supplies identified within or near the project area. This memorandum is applicable to three of the five project areas, the exception being the L Street project area and the project area for the Minnesota Drive traffic recorder update.

Texaco/ Hanna Car Care Center Hazard ID 23025 is active and a Soil and Groundwater Management Work Plan must be submitted to the Contaminated Sites Program for review and approval in advance of the work. If previously unknown contamination in soil or groundwater is discovered anywhere in the project area during construction it will be reported to ADEC in accordance with 18 AAC 75.300.

**VI. Environmental Documentation Approval**

**A. Environmental Documentation Approval**

Yes No

1. Do any unusual circumstances exist, as described in 23 CFR 771.117(b)?

2. Does the project meet the criteria of one of the following DOT&PF Programmatic Approvals authorized in the Nov. 13, 2017 "Chief Engineer Directive - Programmatic Categorical Exclusions"?

- Programmatic Approval 2

**Summary**

This project meets the following criteria for programmatic approval based upon the exclusion found within General Programmatic Approval Condition m.

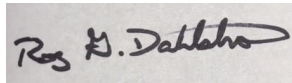
**General Programmatic Approval Condition m.**

No actions encroaching on a regulatory floodway, or work affecting the base floodplain (100-year flood) elevation of a watercourse or lake, pursuant to Executive Order 11988 and 23 CFR 650 subpart A;

Note: "Work affecting the base floodplain" is defined as one foot or greater increase to the base flood elevation. Functionally-dependent uses that encroach on a regulatory floodway and that do not result in a change to the conveyance capacity of the floodway are excluded from this condition.

**Environmental Documentation Approval Signatures**

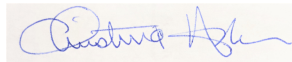
Prepared by:



Date: 1/4/2023

Roy Dahlstrom  
Environmental Impact Analyst II

Reviewed by:



Date: 1/4/2023

Christina Huber  
Engineer/Architect III

Approved by:



Date: 2/17/2023

Brian Elliott  
Central Region Environmental Manager

**APPENDIX E**  
**PROJECT SPECIFIC ESCP DISCUSSIONS & COMMENTS**

