



## AK DOT&PF CENTRAL REGION MAILBOX PRACTICES 2021

### Supporting Information - References

#### 13 AAC 02.340. Stopping, standing, or parking on highway and in other locations

(a) No person may stop, park, or leave standing a vehicle, whether attended or unattended, upon or within eight feet of a roadway, except where the roadway is of sufficient width and design to allow parking without interfering with the normal flow of traffic or with snow removal or other highway maintenance, and where the parking, stopping, or standing is not prohibited by an official traffic-control device, unless directed to do so by a flag person or police officer. If an emergency requires a vehicle to be parked or stopped on a controlled or divided highway, that vehicle must have activated four-way emergency flashers, three operating highway flares, or three reflective triangles in place.

(b) This section and sec. 365 of this chapter do not apply to the driver of a vehicle performing an official duty which requires stopping, standing or parking upon or within eight feet of a roadway or to the driver of a vehicle which is disabled in a manner and to an extent that it is impossible to avoid stopping and temporarily leaving the vehicle upon or within eight feet of a roadway. The driver of a disabled vehicle shall comply with the requirements of sec. 345 of this chapter.

(c) A vehicle that is stopped, parked, or standing in violation of a statute, regulation, or ordinance, is considered to have been stopped, parked or left standing by the registered owner of the vehicle unless the registered owner is able to prove that at the time of the violation the vehicle was driven or parked without his consent.

(d) Except when necessary to avoid conflict with other traffic, or to comply with statutes, regulations or ordinances, the directions of a police officer, fireman, authorized flagman, or official traffic-control device, no person may

(1) stop, stand or park a vehicle

(A) on the roadway side of a vehicle stopped or parked at the edge or curb of a street;

(B) on a sidewalk;

(C) within an intersection;

(D) within a crosswalk;



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(E) between a safety zone and the adjacent curb or within 30 feet of points on the curb immediately opposite the ends of a safety zone, unless a different length is indicated by signs or markings;

(F) alongside or opposite a street excavation or obstruction when stopping, standing, or parking would obstruct traffic;

(G) upon a bridge or other elevated structure upon a highway or within a highway tunnel;

(H) on a railroad track;

(I) on a controlled-access highway;

(J) in the area between roadways of a divided highway, including crossovers;

(K) at a place where official traffic-control devices prohibit stopping, standing, or parking;  
or

(L) in a designated zone for persons with disabilities unless the vehicle is displaying current license plates or permit in accordance with [AS 28.10.181](#) (d) or 28.10.495(a) and the following conditions are immediately present:

(i) a person with a disability of 70 percent or more is on board the vehicle; and



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- (i) a person with a disability of 70 percent or more is on board the vehicle; and
- (ii) that person with a disability exits the vehicle upon stopping or parking in the designated handicapped zone;
- (2) stand or park a vehicle, whether occupied or not, except momentarily to pick up or discharge a passenger or passengers
- (A) in front of a public or private driveway;
- (B) on private property without the consent of the owner or person in control of the property;
- (C) in an alley in a business district where no parking spaces are provided, except for the expeditious loading or unloading of freight, materials or passengers, or in a manner or under conditions which leaves less than 10 feet of the width of the alley for the movement of vehicular traffic, or in a position which blocks the driveway entrance to abutting property;
- (D) within 15 feet of a fire hydrant;
- (E) within 20 feet of a crosswalk at an intersection;



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(F) within 30 feet on the approach side of a flashing signal, stop sign, yield sign, or traffic-control signal located at the side of a roadway;

(G) within 20 feet of the driveway entrance to a fire station or within the area directly across the street from the entrance to a fire station, if the distance from the driveway entrance of the station to the opposite side of the street or roadway or opposite curb is less than 75 feet; or

(H) at a place where an official traffic-control device prohibits standing;

(3) park a vehicle

(A) within 50 feet of the nearest rail of a railroad crossing; or

(B) at a place where official traffic-control devices prohibit parking.

**History:** In effect before 7/28/59; am 12/15/61, Register 3; am 8/10/66, Register 22; am 12/31/69, Register 31; am 6/28/79, Register 70; am 10/2/92, Register 123

**Authority:** [AS 28.05.011](#)

**Editor's note:** With Register 179, October 2006 and under the authority of [AS 44.62.125](#), the regulations attorney changed obsolete terminology concerning persons with disabilities in conformity with ch. 25, SLA 2006.

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#### 17 AAC 10.011. Types of encroachments authorized

(g) The provisions of this section do not apply to driveway permits issued under 17 AAC 10.020, utility permits issued under 17 AAC 15, or the installation of mail boxes, including newspaper boxes, within a highway right-of-way.

#### 17 AAC 10.020. Driveways and approach roads

(a) A driveway or approach road constructed under permit within a highway right-of-way is the property of the state, but all cost and liability arising from the construction, operation, or maintenance of a driveway or approach road is at the sole expense of those lands served. The department is not obligated to change its maintenance practices to accommodate a driveway or approach road constructed under a permit, or to incur any additional expense removing snow berms or other obstructions from a driveway or approach road within a right-of-way resulting from the department's activities, or activities under a permit issued under 17 AAC 15.

(b) A permittee under this section shall construct and maintain a driveway or approach road in such a manner that the highway, and all of the highway's appurtenances or facilities, including drainage facilities, pipes, culverts, ditches, traffic control devices, street lights, pathways, and sidewalks are not impaired or endangered in any way by the construction or maintenance. The department will include in a permit express conditions that provide for the enforcement of this subsection and (a) of this section.



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## Supporting Information - References

**M-20.15**

SHEET  
1 of 1

### GENERAL NOTES:

1. Install mailboxes conforming to U.S. Postal Service requirements.
2. Mailbox supports shall not present a rigid, unyielding impact resistant hazard to road traffic, but shall be flexible and yielding to vehicular impact. Install crashworthy supports in accordance with Standard Plan M-23.
3. Installation shall be on the right side of roadway in the direction of mail carrier travel with the exception of one-way streets where they may be placed on either side.
4. Locate mailboxes to minimize dangers to road traffic, carriers and postal recipients.
5. Provide a minimum shoulder width of 8' unless otherwise approved by Engineer. Install single and double mailbox supports separated by at least 12' desirable/8' minimum. A minimum of two boxes on a single support is allowable only as shown on Standard Plan M-23.
6. Newspaper receptacles shall conform to the same standards as mailbox supports. Mailboxes and newspaper receptacles shall be mounted together, the newspaper receptacle may be mounted beneath the mailbox or on the side of the mailbox support opposite the reflecting marker.

$V_c$  = Average Daily Traffic on Cross Road (vehicles per day)  
 $V_m$  = Average Daily Traffic on Main Road (vehicles per day)  
 $n$  = Number of Mailboxes of Mail Stop

Posted Main Road Speed Limit	10" Distance (ft)	12" Distance (ft)
≤ 40	65	295
41-50	100	400
51-60	150	600
61-70	200	800
71-80	250	1000
81-90	300	1200
91-100	350	1400

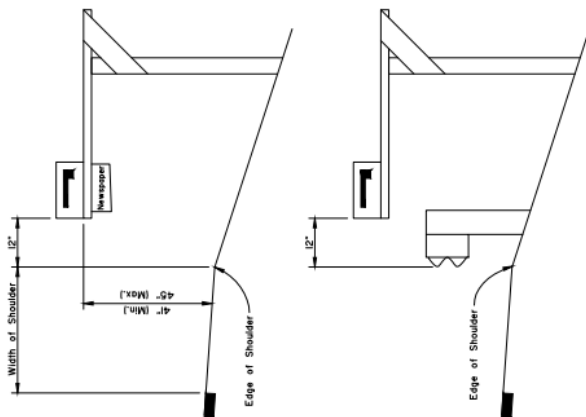
Posted Main Road Speed Limit	10" Distance (ft)	12" Distance (ft)
≤ 40	65	295
41-50	100	400
51-60	150	600
61-70	200	800
71-80	250	1000
81-90	300	1200
91-100	350	1400

10" Distance (ft)	12" Distance (ft)
Preferred Minimum	150
Maximum	200

10" Distance (ft)	12" Distance (ft)
Preferred Minimum	100
Maximum	150

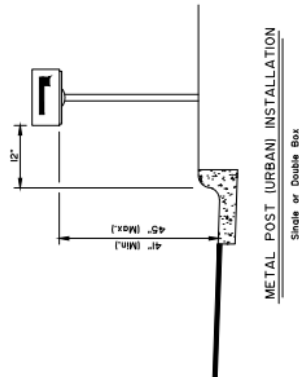
Desirable	Minimum
MPH	C <sub>1</sub>
≤ 40	15'
41-50	20'
51-60	25'
61-70	30'
71-80	35'
81-90	40'
91-100	45'

\* Mailboxes should be placed on the far side of driveway driveway centerline. Minimum values D1 cannot be met.



### WOOD POST (RURAL) INSTALLATION

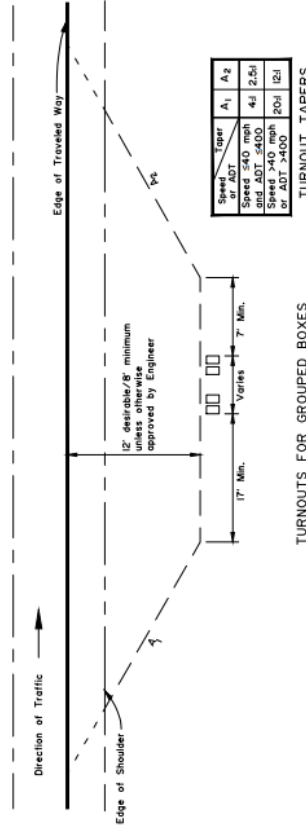
Single or Double Box



### METAL POST (URBAN) INSTALLATION

Single or Double Box

### MAILBOX LOCATION AT INTERSECTIONS AND DRIVEWAYS



### TURNOUTS FOR GROUPED BOXES

### TURNOUT TAPERS

Speed Limit	Turnout Taper	Length (ft)
≤ 40	A <sub>1</sub>	40
41-50	A <sub>2</sub>	60
51-60	A <sub>3</sub>	80
61-70	A <sub>4</sub>	100
71-80	A <sub>5</sub>	120
81-90	A <sub>6</sub>	140
91-100	A <sub>7</sub>	160

### State of Alaska DOT&PF ALASKA STANDARD PLAN MAILBOX LOCATION

Adopted as an Alaska Standard Plan by:  
*Cecilya Mordue*  
 Chief Engineer

Adoption Date: 7/17/2020

Last Code and Title Review  
 By: KLM Date: 7/6/2020

Next Code and Standards Review date: 7/6/2030

M-20.15



# AK DOT&PF CENTRAL REGION MAILBOX PRACTICES 2021

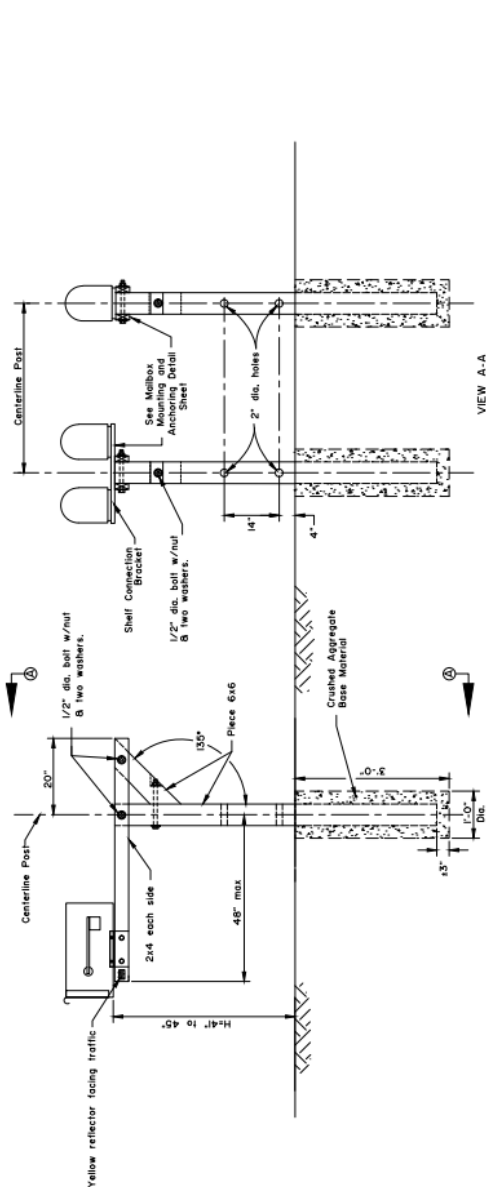
## Supporting Information - References

**M-23.13**

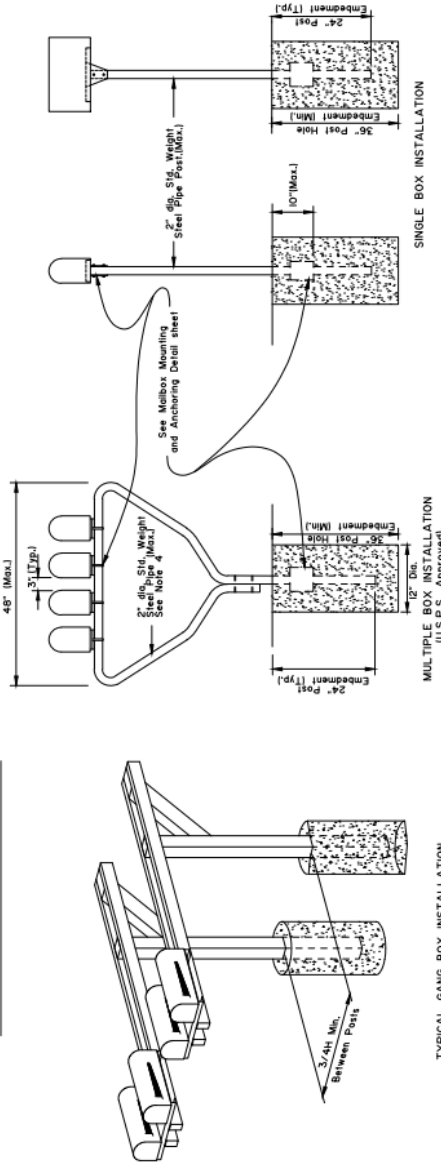
SHEET  
1 of 2

### GENERAL NOTES:

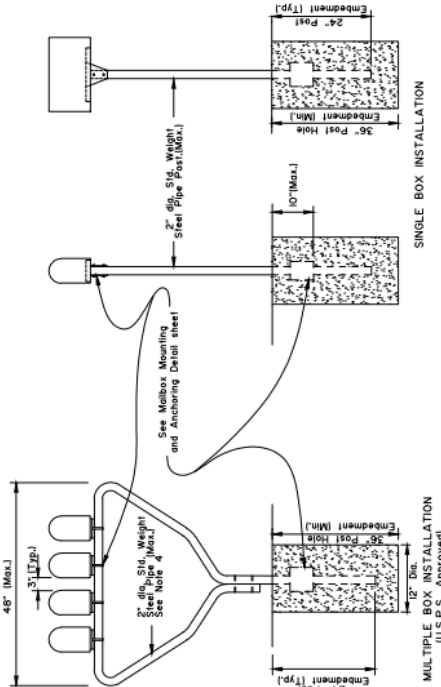
1. See Standard Plan M-20 for locating posts and boxes along roadway.
2. Posts shall be 6"x6" Treated Wood Post S4S or 2" (Max.) Standard Weight Steel Pipe.
3. Each support structure shall not accommodate more than two mailboxes unless the design is approved by the Engineer and conforms to the requirements of the U.S. Postal Service and is approved by the Engineer.
4. Other steel or aluminum structural sections may be used except, the stiffness properties equivalent to the 2" dia. standard weight steel pipe shall not be exceeded.
5. Reflectors shall have a minimum area of 4.5 sq. in.



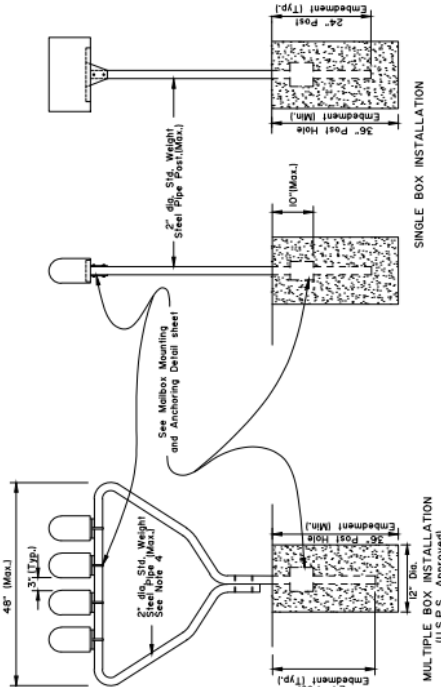
TYPICAL WOOD CANTILEVER INSTALLATION



TYPICAL GANG BOX INSTALLATION



MULTIPLE BOX INSTALLATION  
(U.S.P.S. Approved)



SINGLE BOX INSTALLATION

METAL POST SUPPORTS (URBAN ONLY)

State of Alaska DOT&PF  
ALASKA STANDARD PLAN

### MAILBOX INSTALLATION

Adapted as an Alaska Standard Plan by:  
*Carey Mordhouse*  
Carol Mordhouse, P.E.  
Civil Engineer

Adoption Date: 7/17/2020

Last Code and Size, Review  
By: KLM Date: 7/8/2020

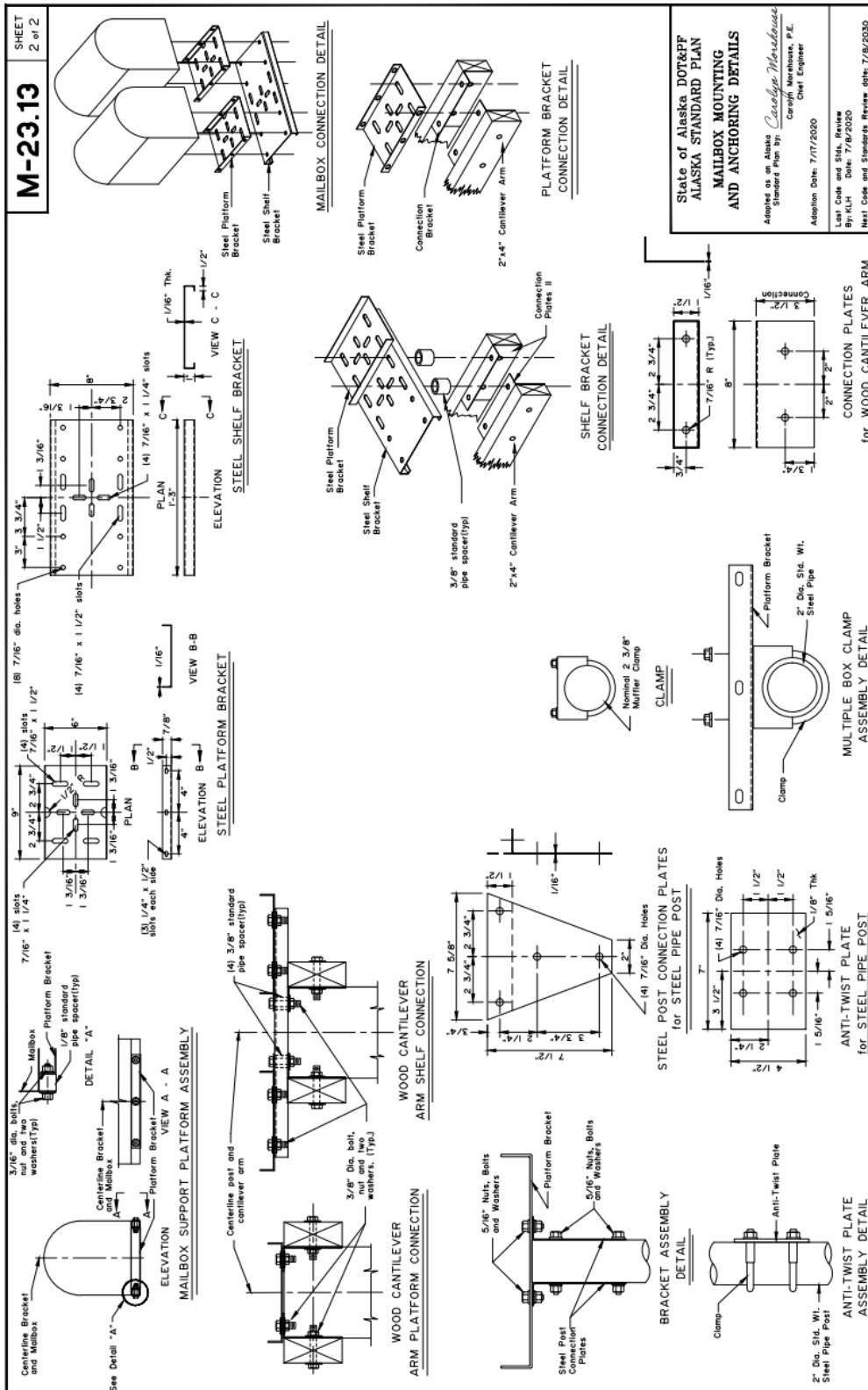
Next Code and Standards Review date: 7/8/2030

M-23.13



# AK DOT&PF CENTRAL REGION MAILBOX PRACTICES 2021

## Supporting Information - References







## AK DOT&PF CENTRAL REGION MAILBOX PRACTICES 2021

### Supporting Information - References

# Update Notice

## Postal Operations Manual, POM Issue 9 July 2002

*Postal Operations Manual, POM Issue 9*, was last printed in July 2002. To inform you of changes since that time, we periodically update this online edition of the POM. We use vertical bars (i.e., revision bars) in the margin to indicate text changed since July 2002.

### 631.31 **General**

The available options for residential areas, aside from apartment houses, transient mobile or trailer homes, colleges and universities, and other sites are covered under 615; delivery mode options are constrained by USPS policies and procedures, in light of the characteristics of the area to be served and the methods needed to provide adequate service. Delivery options, under the regulations given below, are curbside, sidewalk, or central delivery.

### 631.32 **Curbside Delivery**

Delivery may only be provided to boxes at the curb with prior approval from the Postal Service, and so long as they can be efficiently, safely, and conveniently served by the carrier from the carrier's vehicle, and so that customers have reasonable and safe access. Mail receptacles may be grouped, two to a property line where possible.



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#### 632.523 Posts and Supports

The Postal Service does not regulate mailbox supports in any way except for purposes of carrier safety and delivery efficiency. Posts and other supports for curbside mailboxes are owned and controlled by customers, who are responsible for ensuring that posts are neat and adequate in strength and size. Heavy metal posts, concrete posts, and miscellaneous items of farm equipment, such as milk cans filled with concrete, are examples of potentially dangerous supports. The ideal support is an assembly that bends or falls away when struck by a vehicle. Post or support designs may not represent effigies or caricatures that disparage or ridicule any person. Customers may attach the box to a fixed or movable arm.

The Federal Highway Administration (FHWA) has determined that mailbox supports no larger than 4 inches by 4 inches, or a 2-inch diameter standard steel or aluminum pipe, buried no more than 24 inches, should safely break away if struck by a vehicle. According to FHWA, the mailbox must also be securely attached to its post to prevent separation if struck.

#### 632.524 Location

Curbside mailboxes must be placed so that they may be safely and conveniently served by carriers without leaving their conveyances. They must be reasonably and safely accessed by customers. Boxes must also be on the right-hand side of the road and in the carrier's direction of travel in all cases where driving on the left-hand side of the road to reach the boxes would pose a traffic hazard or violate traffic laws and regulations. On new rural or highway contract routes, all boxes must be on the right side of the road in the carrier's direction of travel. Boxes must be placed to conform to state laws and highway regulations. Carriers are subject to the same traffic laws and regulations as are other motorists. Customers must remove obstructions, including vehicles, trash cans, and snow, that make delivery difficult. Generally, mailboxes are installed at a height of 41 to 45 inches from the road surface to the bottom of the mailbox or point of mail entry. Mailboxes are set back 6 to 8 inches from the front face of the curb or road edge to the mailbox door. Because of varying road and curb conditions and other factors, the Postal Service recommends that customers contact the postmaster or carrier before erecting or replacing their mailboxes and supports.

#### 632.525 Grouping

Boxes should be grouped wherever possible, especially at or near crossroads, service turnouts, or other places where a considerable number of boxes are presently located.



## AK DOT&PF CENTRAL REGION MAILBOX PRACTICES 2021

### Supporting Information - References

#### 632.53 Nonconforming Mailboxes

Carriers must report to the postmaster any existing mailboxes that no longer conform to postal regulations. The postmaster sends PS Form 4056, *Your Mailbox Needs Attention*, to the owners of these boxes to request that they remedy the irregularities or defects. All newly installed or replacement mailboxes must be approved models in accordance with USPS-STD-7. A current listing of approved manufacturers and models can be obtained from the office listed in section 632.511.

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Rural Carrier Duties and Responsibilities

Exhibit 131.1b

Form 4056, Your Mailbox Needs Attention (Front & Back)

**Your Mailbox Needs Attention**

(Date) \_\_\_\_\_

WHICH ONE LOOKS LIKE YOUR MAIL BOX?

Postal regulations require customers to provide and erect at their own expense rural mail boxes which meet official standards. A recent inspection disclosed the following faults.

1. Your box is not an approved box	10. The Signal Flag needs attention
2. The door needs attention	11. Your box is too near the road
3. Box must be located so carrier can serve it without leaving vehicle	12. Your box is too far from the road
4. Your box is not waterproof	13. Your box should face the road
5. Your box should be raised _____ inches	14. Your box should be securely fastened to its support
6. Your box should be lowered _____ inches	15. Your box should be made level, and the post firmly planted
7. Your box must be located on the right-hand side of the road in the carrier's direction of travel	16. A new post for your box should be provided
8. The approach to your box should be filled and properly graded & kept unobstructed at all times	17. The rural box number must be printed in numerals not less than one inch high on the side of the box visible to the carrier as they approach it or on the box door if the boxes are grouped
9. The approach to your box should be kept clear of snow, vehicles, and other obstacles.	18. Your box should be painted to prevent rusting
	19. Your house number must be clearly displayed _____ on your house or _____ on your box

20. Other Faults \_\_\_\_\_

Please help us provide you with better service by correcting these faults by \_\_\_\_\_.

After this date, delivery service may be suspended until the faults are corrected. Your cooperation is greatly appreciated.

If you have any questions please see your carrier or contact me.

(Postmaster) \_\_\_\_\_

PS Form 4056, February 1991

**INSTRUCTIONS**

Whenever possible, boxes must be located so that carrier's vehicle is off pavement when serving them.

Rural box number (or house number) must be painted on box in numerals not less than one inch.

Post must be neat and of adequate strength and size.

Approach to box should be a hard level surface (gravel, cinders, stone).

Boxes must be placed to conform with state laws and highway regulations.

Suggested installation of a single box.

Suggested installation where snow removal is a problem.

Extend box beyond end of post. Attach board holding box to post with screws or doublehead nails, permitting easy removal if necessary to adjust box height.

PS Form 4056, February 1991 (Reverse)

Example USPS Exhibit 131.1b, Notification Form 4056 Mailbox needs attention (8-1/2 x 5-1/2)



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### Supporting Information - References

DOTPF Hwy Preconstruction Manual

#### **1130.4. Mailboxes**

Mailboxes are generally found in the clear zone and, to maintain mail service, they usually cannot be relocated outside of the clear zone. Although a mailbox and the supporting structure are obstacles, you can reduce the hazard to an acceptable level.

The vertical support in the single mailbox installation is the critical member. The support should yield on impact. The vertical support member size and its ground embedment length establish stiffness. Chapter 11 in the *AASHTO 2002 Roadside Design Guide* addresses mailboxes, location, and mailbox turnout design. Alaska Standard Drawings M-20 and M-23 comply with the AASHTO guide. Cantilever supports are preferable because the vertical member is offset farther from the traveled way and there is less conflict with snow removal.

With multiple mailbox installations, the vertical support system is stiffer because of the horizontal member that transfers load. The horizontal member itself is a problem because its level allows it to penetrate a windshield. Avoid this situation; Alaska Standard Drawings M-20 and M-23 show acceptable mountings.

Existing mailbox installations that resemble the Alaska standards from the standpoint of structural stiffness may remain in the clear zone based on the designer's judgment. Remove other installations from the clear zone unless this is not cost-effective in accordance with Section 1130.6.

#### **1140.3.1 General Design**

PM projects do not require a formal 3R analysis per Section 1160, but should consider maintaining or enhancing the current level of safety and accessibility. Consider addressing isolated or obvious deficiencies.

- e. Consider treatment of roadside obstacles



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#### **1160.3.12 Safety Mitigation**

Even though these 3R standards may not require a geometric improvement, the designer should anticipate circumstances where mitigating improvements could be made at minimum cost. For example, geometric changes at an intersection or horizontal curve to increase sight distance may not be cost-effective, but cutting brush or trees can partially alleviate the problem.