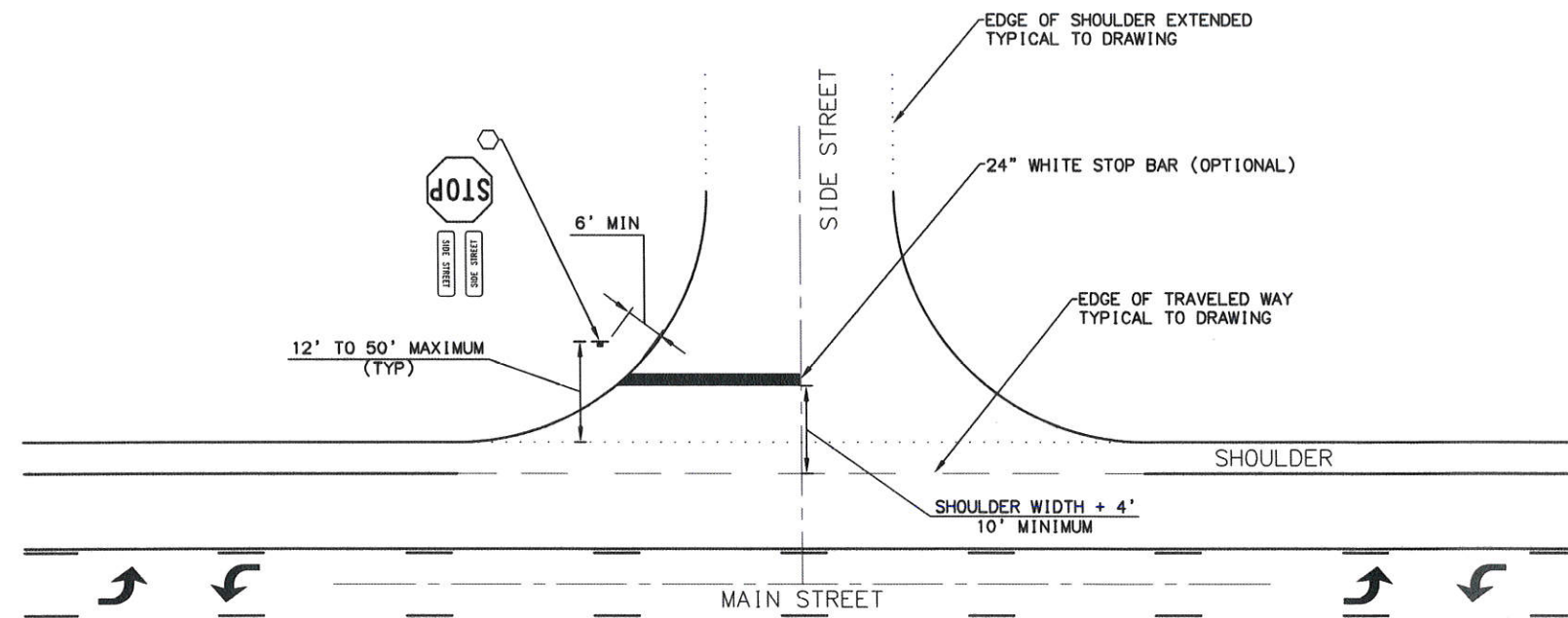


TYPICAL UNCURBED RETURN WITH PATHWAY



TYPICAL UNCURBED RETURN WITHOUT SIDEWALK

UNCURBED INTERSECTION NOTES: (IN PRIORITY ORDER)

SIGNING:

1. Locate STOP sign so it is visible to approaching traffic and near the stop bar.
2. Provide 2' of clearance between edge of STOP sign panel and edge of pathway or sidewalk.
3. Provide 6' of clearance between edge of STOP sign panel and edge of side street.
4. Place pathway regulatory signs at collector or arterial roadway junctions with side streets. Side streets are typically greater than 1000 vehicles a day, or connect through traffic to other collectors or arterials.
5. PATHWAY NO MOTOR VEHICLES signs are not required within the Municipality of Anchorage.
6. See plans for pathway signing required at side streets.

STRIPING:

1. Stop bars are not required when no pathway or sidewalk is present. See plans.
2. Locate stop bar 4' minimum behind the width of pathway or sidewalk.
3. Break centerline striping within intersections which have dedicated turn lanes.
4. Continue centerline striping through intersections with center two-way-left-turn-only lanes or when there are no mainline left turn lanes.
5. Continue lane "skip" striping through intersections.
6. Delete outermost edge of traveled way striping at intersections or wrap striping to side street.
7. Match side street striping if striping is present.

State of Alaska DOT&PF


CENTRAL REGION

STANDARD DETAIL

Un-Signalized Intersection:

Non-Curbed Stop and Crossing

Traffic Safety Details

Adopted as a Central  
Region Standard Detail by: 

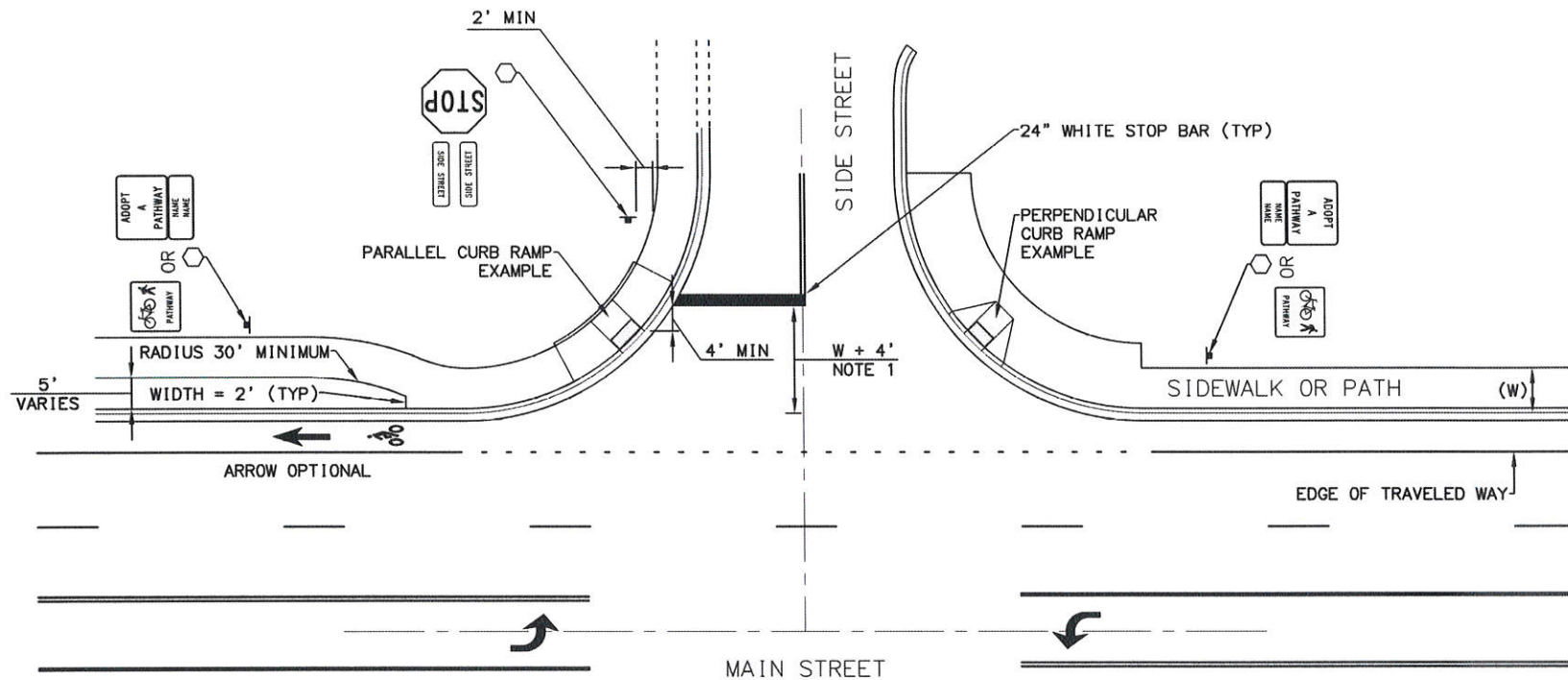
John R. Linnell, P.E.  
CR Preconstruction Engineer

Adoption Date: 06/30/2020

Last Code and Stds. Review  
By:                      Date:

Next Code and Standards Review date: 06/30/2030

CR-T-01.20



TYPICAL CURBED RETURN WITH SIDEWALK

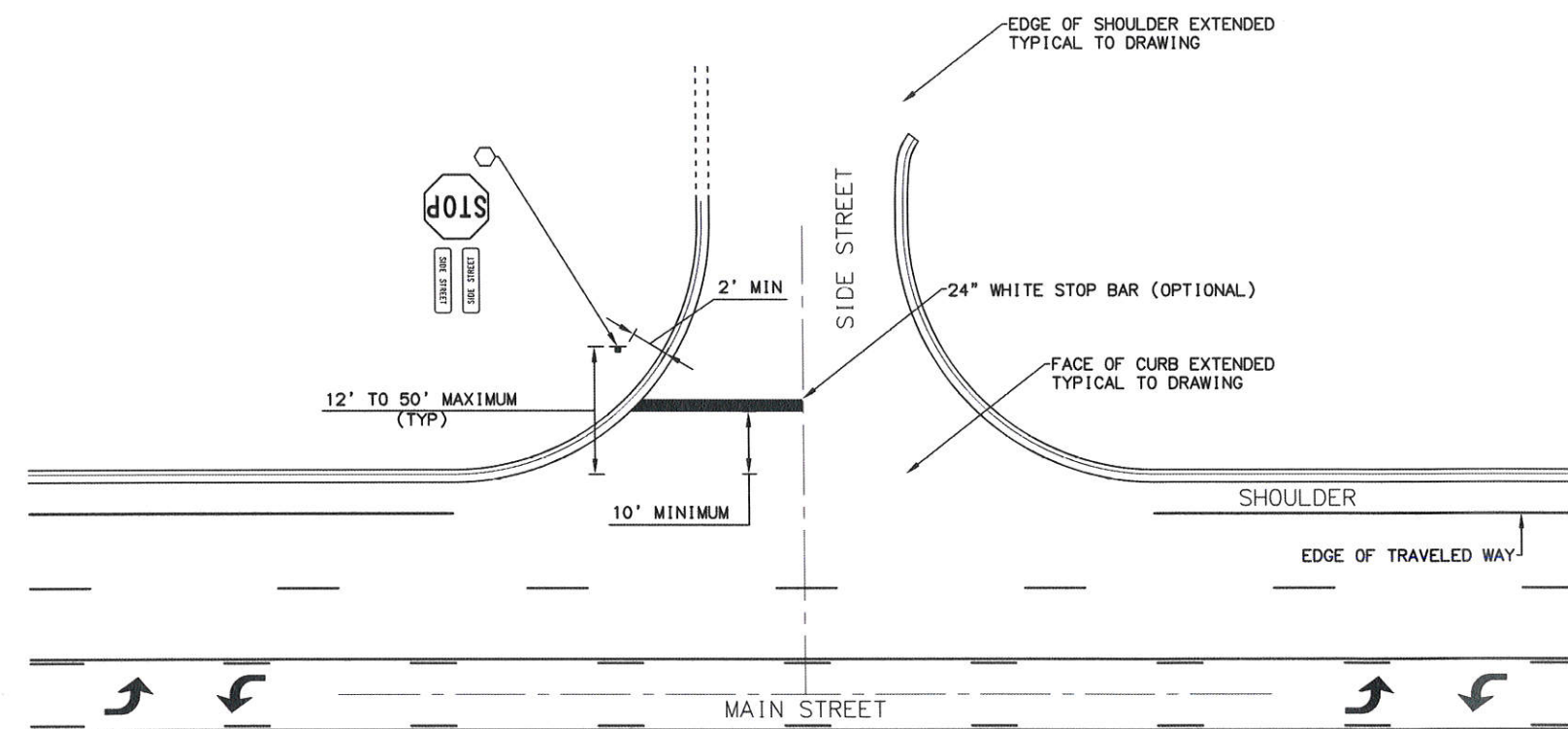
**CURBED INTERSECTION NOTES:**

**SIGNING:**

1. Locate STOP sign so it is visible to approaching traffic and near the stop bar.
2. Provide 2' of clearance between edge of STOP sign panel and edge of pathway or sidewalk.
3. Provide 6' of clearance between edge of STOP sign panel and side street face of curb.
4. Place pathway regulatory signs at collector or arterial roadway junctions with side streets. Side streets are typically greater than 1000 vehicles a day, or connect through traffic to other collectors or arterials.
5. PATHWAY NO MOTOR VEHICLES signs are not required within the Municipality of Anchorage.
6. See plans for pathway signing required at side streets.


**STRIPING:**

1. Stop bars are not required when no pathway or sidewalk is present. See plans.
2. Locate stop bar 4' minimum between the toe of curb ramp and edge of stop bar or a distance of the width of the sidewalk or pathway plus 4'.
3. Break centerline striping within intersections which have dedicated turn lanes.
4. Continue centerline striping through intersections with center two-way-left-turn-only lanes or when there are no mainline left turn lanes.
5. Continue lane "skip" striping through intersections.
6. Delete outermost edge of traveled way striping at intersections or wrap striping to side street.
7. Match side street striping if striping is present.



TYPICAL CURBED RETURN WITHOUT SIDEWALK

State of Alaska DOT&PF  
CENTRAL REGION  
STANDARD DETAIL  
Un-Signalized Intersection:  
Curbed Stop and Crossing  
Traffic Safety Details

Adopted as a Central  
Region Standard Detail by:   
John R. Linnell, P.E.  
CR Preconstruction Engineer

Adoption Date: 06/30/2020

Last Code and Stds. Review  
By: Date:

Next Code and Standards Review date: 06/30/2030